MONTGOMERY COUNTY HISTORIC PRESERVATION COMMISSION STAFF REPORT

Address: 15100 Barnesville Road, Boyds Meeting Date: 8/142024

Resource: Primary (1850-1935) Resource **Report Date:** 8/7/2024

Boyds Historic District

Applicant: Montgomery County Department of **Public Notice:** 7/31/2024

Transportation (Rebecca Park, Transportation

Unit Manager)

Review: HAWP Tax Credit: No

Proposal: For structural stabilization, fenestration and other alterations.

STAFF RECOMMENDATION

Staff recommends that the HPC <u>approve with three (3) condition</u> the HAWP application with final approval authority delegated to staff:

- 1. The applicant must consult with the Staff Archaeologist prior to ground disturbance along the foundation to identify areas that warrant archaeological testing. Complete archaeological testing as warranted through excavation units along the foundation intended to document historical construction and repair episodes. All final reports must be submitted to the Historic Preservation Office.
- 2. The applicant must continue to work with staff to refine the language on the historic marker, including, but not limited to, the description and history of the African American community in Boyds.
- 3. The applicant must submit revised drawings showing the new stairs and checkwalls showing elevation, section drawings, and labeling all materials, and showing final heights. Final approval authority to verify this work is compatible with the character of the site can be delegated to Staff.

ARCHITECTURAL DESCRIPTION

SIGNIFICANCE: Primary Resource within the Boyds Historic District

STYLE: Grist Mill DATE: 1915



Figure 1: The subject property at 15100 Barnesville Road is located mid-block on Barnesville Road, directly to the north of the Metropolitan Branch of the Baltimore & Ohio Railroad. The red outline is the boundary of the Boyds Master Plan Historic District. The adjacent buildings to the west, north, and northeast are outside of the environmental setting.

BACKGROUND

Hoyle's Mill has an extensive administrative history and its degrading condition has been well documented. The HPC last held a Preliminary Consultation for site improvements on August 16, 2023.¹

On May 22, 2024, the applicant presented a Preliminary Consultation to stabilize the foundation of the mill as part of dedicated capital improvement project funding.² The proposal presented involved pouring fillable flow concrete into the basement to a height of approximately 4' (four feet). The HPC expressed concerns about the proposal including how the concrete would interact with the existing wood posts and how it would limit future potential uses for the building. Many other questions asked by the HPC were technical in nature to allow the commissioners a fuller understanding of the proposal. Commissioners also requested the site be opened for a site visit. A site visit was on June 6, 2024 and was attended by Staff and four Commissioners.

The applicant has revised the proposal and seeks a HAWP.

PROPOSAL

The applicant proposes to stabilize the foundation and remove and replace the exterior stairs and cheekwall.

APPLICABLE GUIDELINES

The Historic Preservation Office and Historic Preservation Commission (HPC) consult several documents

¹ The August 16, 2023 Preliminary Consultation Staff Report is available here: https://montgomeryplanning.org/wp-content/uploads/2023/08/II.B-15100-Barnesville-Road-Boyds-1038325.pdf. The recording of the hearing is available here: https://mncppc.granicus.com/MediaPlayer.php?publish_id=b388082e-3d03-11ee-9e9a-0050569183fa.

² The Staff Report and application materials for the May 22 2024, Preliminary Consultation is available here: https://montgomeryplanning.org/wp-content/uploads/2024/05/II.A-15100-Barnesville-Road-Boyds-Prelim.pdf. The recording of the hearing is available here: https://mncppc.granicus.com/MediaPlayer.php?publish_id=6bab5eb2-2432-11ef-81ef-005056a89546.

when reviewing alterations and new construction within the Boyds Historic District. These documents include the *Montgomery County Code Chapter 24A* (*Chapter 24A*), *Secretary of the Interior's Standards for Rehabilitation* (*Standards*), and *MARC Rail Communities Sector Plan* (2019). The pertinent information in these documents is outlined below.

Montgomery County Code, Chapter 24A-8

The following guidance which pertains to this project are as follows:

- (b) The commission shall instruct the director to issue a permit, or issue a permit subject to such conditions as are found to be necessary to ensure conformity with the purposes and requirements of this chapter, if it finds that:
 - (1) The proposal will not substantially alter the exterior features of an historic site or historic resource within an historic district; or
 - (2) The proposal is compatible in character and nature with the historical, archeological, architectural or cultural features of the historic site or the historic district in which an historic resource is located and would not be detrimental thereto or to the achievement of the purposes of this chapter;
 - (3) The proposal would enhance or aid in the protection, preservation and public or private utilization of the historic site or historic resource located within an historic district in a manner compatible with the historical, archeological, architectural or cultural value of the historic site or historic district in which an historic resource is located; or
 - (4) The proposal is necessary in order that unsafe conditions or health hazards be remedied; or
 - (5) The proposal is necessary in order that the owner of the subject property not be deprived of reasonable use of the property or suffer undue hardship; or
 - (6) In balancing the interests of the public in preserving the historic site or historic resource located within an historic district, with the interests of the public from the use and benefit of the alternative proposal, the general public welfare is better served by granting the permit.
- (c) It is not the intent of this chapter to limit new construction, alteration or repairs to any 1 period or architectural style.
- (d) In the case of an application for work on an historic resource located within an historic district, the commission shall be lenient in its judgment of plans for structures of little historical or design significance or for plans involving new construction, unless such plans would seriously impair the

Secretary of the Interior's Standards for Rehabilitation

The Secretary of the Interior defines rehabilitation as "the act or process of making possible a compatible use for a property through repair, alterations, and additions while preserving those portions or features, which convey its historical, cultural, or architectural values." The applicable *Standards* are as follows:

2. The historic character of a property will be retained and preserved. The removal of distinctive materials or alteration of features, spaces and spatial relationships that characterize a property will be avoided.

- 7. Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.
- 8. Archeological resources will be protected and preserved in place. If such resources must be disturbed, mitigation measures will be undertaken.
- 9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.
- 10. New additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

MARC Rail Communities Sector Plan

The MARC Rail Communities Sector Plan was approved on April 30, 2019 by the Montgomery County Council. The Plan looks to the future by offering recommendations that support and better utilize existing transit assets, improve transportation connections and traffic safety in both station areas. In addition, the Plan proposes ways to revitalize the built environment around the Boyds and Germantown stations in a manner that appropriately complements surrounding residential neighborhoods and historic resources. The plan lists the following as one of its priorities, "Create a commuter and community-serving facility at the historic Hoyle's Mill site, integrating the mill as part of the facility."

The MARC Rail Communities Sector Plan includes the following specific references to the Hoyle's Mill site:

- Establish regular Ride On bus service to the Boyds MARC Station, including appropriate busrelated facilities at the station, to accommodate additional MARC riders (Page 38-39).
 - o Provide a shelter for commuters and allow public access to the historic structures on the property by adapting the Hoyle's Mill structure and any other retained buildings.
- Promote compatible infill development around the Boyds MARC Station, while protecting existing residential uses, historically significant structures and natural areas (Page 69-70).
 - Design the enhanced MARC station area, including the Anderson properties, to be sensitive to the historic Hoyle's Mill and other structures.
 - Prior to any changes, additions and/or removals, assess all the structures on the site for National Register of Historic Places significance. This site falls within the National Register Historic District.
 - Review all potential changes to the mill with the County's Historic Preservation Commission.
- Encourage the compatible reuse of structures that lend historic character to the Boyds and Germantown communities, and maintain the rail communities' historic integrity while revitalizing and enhancing MARC station areas (Page 93).

4

³ Montgomery County, MARC Rail Communities Sector Plan (2019), 16.

- Adaptively reuse the historic Hoyle's Mill within the Boyds Historic District and explore reuse of the nearby barn building outside of the historic district to establish a focal point for the Boyds commercial area.
- o Commemorate historical uses on the parcel, including, among others, the location of the former station house, which is no longer standing.
- Redesign the site to support the reuse of the mill building. Coordinate all changes to the mill with the Montgomery County Historic Preservation Commission
- Design alterations to the repurposed mill and any other buildings—including landscaping, lighting and walkways—in a manner that is sensitive to the surrounding historic district. Design landscaping, lighting, signage, paths and structures that support and are sensitive to the character of the historic district.

STAFF DISCUSSION

Hoyle's Mill is a Primary Resource in the Boyds Master Plan Historic District. A recent Historic American Engineering Record (HAER) survey of the building states the following:

Located alongside the Baltimore and Ohio Railroad's Metropolitan Branch, Hoyle's Mill is the oldest industrial building in the rural railroad community of Boyds. It is representative of the mature milling industry that flourished in Montgomery County when the region was a major agricultural center. With its original structure, sheathing, and fenestration intact, Hoyle's Mill retains a high level of integrity as an example of an early twentieth century family-owned mill that was later adapted for use as a storage building for farming and automotive supply businesses.

Hoyle's Mill has two stories and a basement and is 26' wide x 48' long. It measures 20'-0" vertically from the top of the basement to the roof eave; and 10'-4" vertically between the first and second floor windowsills. It is sheathed with galvanized, pressed-metal siding in a pitch-faced stone pattern, dating to the original construction of the mill. The siding panels measure 5' wide x 2'-4" tall, representing five courses of 7" x 12" blocks. The texture matches a pattern produced by W. F. Norman Corporation, which has produced galvanized steel and copper ceilings and siding since 1898.

Both the front north and rear south elevations have a door and three double-hung windows across the first floor, and four windows across the second floor. The windows are six over six and have a frame opening of 2'-4" x 4'-0". Both the front and rear doors are wood, sized 3'-6" x 6'-8". The door on the rear, trackside wall and existence of extra nail holes in the siding on the course below the door sill both suggest that there may have once been a loading dock serving railroad deliveries. The basement has three window openings on the rear wall measuring 2'-4" x 2'-8", where the concrete foundation wall flares out an additional 13" on a 3/12 pitch. Asphalt shingles cover the roof...⁴

5

⁴ For more information, https://tile.loc.gov/storage-services/master/pnp/habshaer/md/md2200/md2207/data/md2207data.pdf.



Figure 3: View of the Metropolitan Branch of the Baltimore & Ohio railroad at the newly constructed Boyds station, 1928 (left), and the rear (track facing) elevation, 1984. The red arrow points to Hoyle's Mill. Source: John R. King (left) and John S. Collier (right).

Foundation Stabilization

The primary focus of the work proposed at this stage of site redevelopment is the stabilization of the foundation. The existing foundation has deteriorated with most of the damage occurring to the north and east foundation walls. Stabilizing the foundation of the mill must happen before any other work on the site can be carried out due to its delicate condition. The existing exterior concrete stairs have also settled and cracked and have deteriorated to the point they are no longer structurally stable.

The proposal discussed at the May 2024 Preliminary Consultation involved filling the basement space with a flowable fill concrete. The applicant has significantly revised the proposal and now proposes to support the existing north and east foundation walls with helical piles before installing new reinforced concrete foundation walls and footings. New stairs and cheek walls will be installed on the east side of the mill to replace the existing, significantly deteriorated stairs, in the same location. Section drawings of the stairs and cheekwall were not included with the submitted application materials. The window openings will remain boarded up until they can be repaired as part of a future adaptive reuse project. Aside from the new stairs, all of the proposed work is below grade and will not result in a visual change to the building.

The applicant provided a proposed sequence of work, which is as follows:

- 1. De-energize and remove power lines.
- 2. Remove the metal shed located at the east wall of the mill building.
- 3. Remove and discard the steel stair platform from the west side of the building.
- 4. Perform exploratory archaeology (shovel test pits) of the building's exterior perimeter prior to excavation.
- 5. Install helical piles at north and east walls to stabilize existing foundations.
- 6. Install temporary shoring of first floor framing within crawl space.
- 7. Install continuous steel repair plates with adhesive anchors across cracks in existing concrete walls.
- 8. Remove loose trash/debris from inside building. Salvage historic artifacts such as mill equipment on upper floors. Retain existing mill equipment in lower level as is.
- 9. Cap and abandon existing well.
- 10. Demolish existing stone masonry footings at north and east foundation walls.
- 11. Demolish existing concrete mass at north side of crawl space.
- 12. Demolish and replace existing stair and cheek walls leading down into crawl space.
- 13. Excavate, install formwork, and place concrete for new reinforced concrete foundation walls and footings at north and east walls.
- 14. Demolish existing stone masonry foundation at south foundation wall.
- 15. Excavate, install formwork, and place concrete for new reinforced concrete foundation wall and footing at south wall.
- 16. Backfill excavations around north and east walls.

- 17. Repair holes in floors with plywood.
- 18. Remove and replace deteriorated basement doors and frame with new steel door and frame.
- 19. Remove any insect and bird nests.

Staff finds the revised proposal to be preferable to the proposal presented at the Preliminary Consultation. First, the majority of the work will be conducted on the exterior of the mill, below grade, leaving the crawlspace/basement accessible for its future use. At the Preliminary Consultation, the HPC indicated disappointment that filling the basement with concrete would eliminate the possibility of utilizing that space in the future. Second, this work could conceivably be reversed at some time in the future if necessary; whereas the concrete fill proposed in the Prelim was an irreversible treatment. Third, the proposed work will not interact with the wood posts, so there are no concerns about how the concrete will impact the long-term preservation of the posts. Because section drawings for the stairs and checkwall were not submitted with the application material, Staff recommends the HPC include a condition requiring the submission of those drawings before Staff releases the final approval documents. Final approval authority to verify this work is compatible with the character of the site can be delegated to Staff. Staff find this solution is appropriate and recommends the HPC approve the foundation stabilization and stair replacement under 24A-8(b)(1), (2), (3), (4), and (6); Standards 2 and 7; and the objectives of the *MARC Rail Communities Sector Plan*.

The Staff Archaeologists reviewed the proposal to perform exploratory archaeology and found the description of the proposed work identified in the proposal required additional specificity and recommended the HPC include a condition to the HAWP that:

- The applicant continues to consult with the Staff Archaeologist prior to ground disturbance along the foundation to identify areas that warrant archaeological testing and
- Complete archaeological testing as warranted through excavation units along the foundation intended to document historic construction and repair episodes.

With the recommended condition, Staff finds the proposed archaeology is consistent with Standard 8.

Staff notes the applicant also proposes to install a historic plaque on site after the foundation has been stabilized. The proposed language states:

BOYDS, MARYLAND, AND HOYLE'S MILL

THE TOWN OF BOYDS ORIGINATED AS A CAMP FOR WORKERS BUILDING THE B&O RAILROAD METROPOLITAN BRANCH IN 1866-1873 UNDER RAILROAD CONTRACTOR JAMES A. BOYD. THE NEW RAILROAD STATION WAS NAMED FOR BOYD, WHO HAD SETTLED NEARBY ON A LARGE FARM. JUST DOWN THE ROAD WAS A COMMUNITY FOUNDED BY BLACK FAMILIES WHO HAD BEEN ENSLAVED ON FARMS IN THE AREA UNTIL 1864. THE HOYLE FAMILY HAD A WATER-POWERED GRAIN AND SAW MILL ON LITTLE SENECA CREEK FROM THE EARLY 1800S TO 1903. IN 1915, SMITH HOYLE OPENED A NEW GASOLINE-POWERED FLOUR MILL ACROSS FROM BOYD'S STATION, WHICH HE OPERATED UNTIL 1940.

MARYLAND HISTORICAL TRUST MARYLAND TRANSIT ADMINISTRATION

One of the Staff historians reviewed the text of the proposed marker and recommended additional research related to the nearby postbellum community established by formerly enslaved families. Staff recommends the HPC add a condition to the approval of this HAWP that requires the applicant to continue to work with HPC Staff to refine the language of the marker including, but not limited to, the description and history of the African American community in Boyds.



Figure 4: View of the facade of Hoyle's Mill, 2023.

STAFF RECOMMENDATION

Staff recommends that the Commission <u>approve</u> <u>with three (3) condition</u> the HAWP application with final approval authority delegated to staff:

- The applicant must consult with the Staff Archaeologist prior to ground disturbance along the foundation to identify areas that warrant archaeological testing. Complete archaeological testing as warranted through excavation units along the foundation intended to document historical construction and repair episodes. All final reports must be submitted to the Historic Preservation Office.
- 2. The applicant must continue to work with staff to refine the language on the historic marker, including, but not limited to, the description and history of the African American community in Boyds.
- 3. The applicant must submit revised drawings showing the new stairs and checkwalls showing elevation, section drawings, and labeling all materials, and showing final heights. Final approval authority to verify this work is compatible with the character of the site can be delegated to Staff.

under the Criteria for Issuance in Chapter 24A-8(b)(1), (2), (3), (4), and (6), having found that the proposal will not substantially alter the exterior features of the historic resource and is compatible in character with the district and the purposes of Chapter 24A;

and with the Secretary of the Interior's Standards for Rehabilitation #2, #7, and #8;

and with the objectives of the MARC Rail Communities Sector Plan;

and with the general condition that the applicant shall present an electronic set of drawings, if applicable, to Historic Preservation Commission (HPC) staff for review and stamping prior to submission for the Montgomery County Department of Permitting Services (DPS) building permits;

and with the general condition that final project design details, not specifically delineated by the Commission, shall be approved by HPC staff or brought back to the Commission as a revised HAWP application at staff's discretion;

and with the general condition that the applicant shall notify the Historic Preservation Staff if they propose to make **any alterations** to the approved plans. Once the work is completed the applicant will contact the staff person assigned to this application at 301-563-3400 or dan.bruechert@montgomeryplanning.org to schedule a follow-up site visit.



APPLICATION FOR HISTORIC AREA WORK PERMIT HISTORIC PRESERVATION COMMISSION 301.563.3400

FOR STAFF ONLY: HAWP#_ DATE ASSIGNED____

APPLICANT:

Name:	E-mail:
Address:	City: Zip:
Daytime Phone:	Tax Account No.:
AGENT/CONTACT (if applicable):	
Name:	E-mail:
Address:	City: Zip:
Daytime Phone:	Contractor Registration No.:
LOCATION OF BUILDING/PREMISE: MIHP # of	Historic Property
map of the easement, and documentation from Are other Planning and/or Hearing Examiner App (Conditional Use, Variance, Record Plat, etc.?) If Supplemental information.	
	;
Lot: Block: Subdiv TYPE OF WORK PROPOSED: See the checklis for proposed work are submitted with this a	et on Page 4 to verify that all supporting items application. Incomplete Applications will not
☐ Grading/Excavation ☐ Roof I hereby certify that I have the authority to make and accurate and that the construction will com	Shed/Garage/Accessory Structure Solar Tree removal/planting /Landscape Window/Door Other: e the foregoing application, that the application is correct apply with plans reviewed and approved by all necessary this to be a condition for the issuance of this permit.

HAWP APPLICATION: MAILING ADDRESSES FOR NOTIFING [Owner, Owner's Agent, Adjacent and Confronting Property Owners] Owner's mailing address Owner's Agent's mailing address Adjacent and confronting Property Owners mailing addresses

Description of Property: Please describe the building and surrounding environment. Include information on significant str	ructures
landscape features, or other significant features of the property:	
Description of Work Proposed: Please give an overview of the work to be undertaken:	

Work Item 1:				
Description of Current Condition:	Proposed Work:			
Work Item 2:				
Description of Current Condition:	Proposed Work:			
Work Item 3:				
Description of Current Condition:	Proposed Work:			

HISTORIC AREA WORK PERMIT CHECKLIST OF APPLICATION REQUIREMENTS

	Required Attachments						
Proposed Work	I. Written Description	2. Site Plan	3. Plans/ Elevations	4. Material Specifications	5. Photographs	6. Tree Survey	7. Property Owner Addresses
New Construction	*	*	*	*	*	*	*
Additions/ Alterations	*	*	*	*	*	*	*
Demolition	*	*	*		*		*
Deck/Porch	*	*	*	*	*	*	*
Fence/Wall	*	*	*	*	*	*	*
Driveway/ Parking Area	*	*		*	*	*	*
Grading/Exc avation/Land scaing	*	*		*	*	*	*
Tree Removal	*	*		*	*	*	*
Siding/ Roof Changes	*	*	*	*	*		*
Window/ Door Changes	*	*	*	*	*		*
Masonry Repair/ Repoint	*	*	*	*	*		*
Signs	*	*	*	*	*		*

BEGINNING THE PROJECT.

DATE

DATED JUNE, 2014.

DATE

DEVELOPER'S/BUILDER'S CERTIFICATION

DESIGN CERTIFICATION

TIMOTHY H. CUPPLES, P.E., CHIEF

JASON D. COSLER, P.E. MD REGISTRATION NO. 28467

FEET OR 1.28 ACRES.

DATE

PAMELA H. DESTINO, P.E.

MD REGISTRATION NO. 42708

DIVISION OF TRANSPORTATION ENGINEERING

CERTIFICATION OF QUANTITIES

FILL AS SHOWN ON THESE PLANS HAVE BEEN COMPUTED TO BE 4,500 CUBIC YARDS OF EXCAVATION AND 1,700 CUBIC YARDS OF FILL AND THAT THE TOTAL AREA TO BE DISTURBED AS SHOWN ON THESE PLANS HAS BEEN DETERMINED TO BE A MAXIMUM OF 55,965 SQUARE

I FURTHER CERTIFY THAT THE TOTAL AMOUNTS OF EXCAVATION AND

I HEREBY CERTIFY THAT ALL CLEARING, GRADING, CONSTRUCTION AND/OR DEVELOPMENT WILL BE

I HEREBY CERTIFY THAT THIS PLAN HAS BEEN PREPARED IN ACCORDANCE WITH THE "2011 MARYLAND"

MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS AND TRANSPORTATION "STORM DRAIN DESIGN CRITERIA"

STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL", MONTGOMERY COUNTY

DEPARTMENT OF PERMITTING SERVICES EXECUTIVE REGULATIONS 5-90, 7-02AM AND 36-90, AND

CONSTRUCTION PROJECT WILL HAVE A CERTIFICATE OF ATTENDANCE OF A DEPARTMENT OF NATURAL RESOURCES APPROVED TRAINING PROGRAM FOR THE CONTROL OF SEDIMENT AND EROSION BEFORE

DONE PURSUANT TO THIS PLAN AND THAT ANY RESPONSIBLE PERSONNÉL INVOLVED IN THE

MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION

HOYLE'S MILL STRUCTURAL STABILIZATION -

30% DESIGN RESUBMISSION

C. I. P. PROJECT 501915

SHA TRACKING NO. 22-AP-MO-022-XX

Seneca Lake LIMIT OF WORK STA. 11+36 **PROJECT** CSX TRANSPORTATION (MARC) LOCATION BOYDS CEM. PROJECT LENGTH = 0.08 MILES MONTGOMERY COUNTY

VICINITY MAP SCALE : 1"= 500'

MD 117 (BARNESVILLE ROAD) DESIGN DATA DESIGN SPEED: 40 M.P.H. | 2022 AADT: 5,193

> OWNER/ADDRESS: MONTGÓMERY COUNTY DEPARTMENT OF TRANSPORTATION 100 EDISON PARK DRIVE, 4TH FLOOR GAITHERSBURG, MD 20878

REBECCA PARK, P.E. 240-777-7263

PROFESSIONAL CERTIFICATION.

OF MARYLAND, LICENSE NO.__

EXPIRATION DATE:_

HEREBY CERTIFY THAT THESE DOCUMENTS

THAT I AM A DULY LICENSED PROFESSIONAL

ENGINEER UNDER THE LAWS OF THE STATE

WERE PREPARED OR APPROVED BY ME. AND Whitman, Requardt & Associates, LLP 801 South Caroline Street, Baltimore, Maryland 21231

MONTGOMERY				
DEPARTMENT OF T				
GAITHERSBURG,				
GAITIEROBORG,				
RECOMMENDED FOR APPROVAL				
NEGOTIMENDED FOR ALTROVAL				
Chief, Transportation Planning and Design Section				
APPROVED				
Chief, Division of Transportation Engineering				
Designed by: <u>LJH</u> Drawn by:	BY	DATE	REVISION	NO.

TYPE OF PERMIT REQD NOT REQD RESTRICTION DATES M.C.D.E.P. Floodplain District WATERWAY/WETLANDS a. Corps of Engineers c. M.D.E. Water **Quality Certification** M.D.E. Dam Safety DPS Roadside Tree Protection Plan TBD TBD N.P.D.E.S. NOTICE OF INTENT TBD TBD M.C.D.P.S. STORMWATER 285472 N/AMANAGEMENT 288386 TBD FEMA LOMR
(REQUIRED POST CONSTRUCTION) D.P.S. BUILDING PERMIT TBD TBD M.C.P.D.S SEPTIC SYSTEM PERMIT TBD TBD OTHERS: (PLEASE LIST SHA ACCESS PERMIT | 22-AP-MO-022-XX|

GENERAL NOTES

1. TRANSIT IMPROVEMENTS PROJECT, SITE CLEANUP AND GROUNDWATER REMEDIATION SHALL BE DONE UNDER A SEPARATE CONTRACT.

- 2. THE SPECIFICATIONS FOR THIS CONTRACT WILL BE THOSE OF THE LATEST EDITION OF THE MARYLAND DEPARTMENT OF TRANSPORTATION STATE HIGHWAY ADMINISTRATION, THE MARYLAND DEPARTMEMNT OF TRANSPORTATION STATE HIGHWAY ADMINISTRATION BOOK OF STANDARDS FOR HIGHWAY AND INCIDENTAL STRUCTURES, THE MARYLAND DEPARTMENT OF TRANSPORTATION STATE HIGHWAY ADMINISTRATION 2023 STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS, THE MARYLAND WASHINGTON SUBURBAN SANITARY COMMISSION (W.S.S.C.) STANDARDS, MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION STANDARDS, AND SOIL CONSERVATION SERVICE POND CONSTRUCTION SPECIFICATIONS FOR MARYLAND.
- 3. HORIZONTAL DATUM: NAD 83(1991) VERTICAL DATUM: NAVD 88..

MONTGOMERY COUNTY

DEPARTMENT OF TRANSPORTATION

GAITHERSBURG. MARYLAND

Designed by: <u>LJH</u> Drawn by: <u>FIE</u>

Checked by: FAH

- 4. INFORMATION CONCERNING UNDERGROUND UTILITIES WAS OBTAINED FROM AVAILABLE RECORDS, BUT THE CONTRACTOR MUST DETERMINE THE EXACT LOCATIONS AND ELEVATIONS OF THE LINES BY DIGGING TEST PITS BY HAND AT ALL UTILITY CROSSINGS, WELL IN ADVANCE OF TRENCHING. IF CLEARANCES ARE LESS THAN SHOWN OR SIX (6) INCHES, WHICHEVER IS LESS, CONTACT MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION'S PROJECT INSPECTOR AND THE APPROPRIATE UTILITY OWNER BEFORE PROCEEDING WITH CONSTRUCTION.
- 5. REPAIRS TO UTILITIES OR PROPERTY DAMAGE AS A RESULT OF THE CONTRACTOR'S NEGLIGENCE OR METHOD OF OPERATION MUST BE MADE AT THE CONTRACTOR'S EXPENSE AT NO ADDITIONAL COST TO THE COUNTY BEFORE PROCEEDING WITH CONSTRUCTION.
- 6. DISTURBED AREAS ADJACENT TO ESTABLISHED LAWNS SHALL BE SODDED. OTHER DISTURBED AREAS SHALL BE SEEDED AND MULCHED.
- 7. THE CONTRACTOR SHALL OBTAIN A ROADSIDE TREE PERMIT FOR ANY MAINTENANCE, TREATMENT, PLANTING, REMOVAL, OR ROOT CUTTING ON TREES WITHIN THE PUBLIC RIGHT OF WAY. PERMIT REQUIREMENTS MAY BE OBTAINED FROM THE DEPARTMENT OF NATURAL RESOURCES, MARYLAND FOREST, PARK AND WILDLIFE SERVICE, TELEPHONE 301-854-6060.
- 8. CONTACT THE WASHINGTON SUBURBAN SANITARY COMMISSION SYSTEM MAINTENANCE ENGINEER BEFORE EXCAVATING BENEATH OR IN THE VICINITY OF EXISTING WATER OR SEWER LINES. BACKFILL TO BE DONE UNDER SUPERVISION OF WSSC MAINTENANCE ENGINEER, CALL 301-206-9772.
- 9. CALL "MISS UTILITY" AT 1-800-257-7777 48 HOURS PRIOR TO THE START OF WORK. THE EXCAVATOR MUST NOTIFY ALL PUBLIC UTILITY COMPANIES WITH UNDERGROUND FACILITIES IN THE AREA OF PROPOSED EXCAVATION AND HAVE THOSE FACILITIES LOCATED BY UTILITY COMPANIES PRIOR TO COMMENCING EXCAVATION. THE EXCAVATOR IS RESPONSIBLE FOR COMPLIANCE WITH REQUIREMENTS OF CHAPTER 36A OF THE MONTGOMERY COUNTY CODE.

MCDPS-SC/SWM SHEET NO. 1 OF 8

288386

285472

ESD TO THE MEP = 3,397 CF, QN & QL WAIVER N/A

TI-01 TITLE SHEET HOYLE'S MILL STRUCTURAL STABILIZATION BOYDS, MARYLAND

SCALE : NTS 24 JULY 2024 Project No. : <u>32207.003</u> SHEET <u>1</u> of 8

LEGEND

DOOR NUMBER SYMBOL

WINDOW NUMBER SYMBOL

ROOM NUMBER SYMBOL

BUILDING SECTION SYMBOL

WALL SECTION/ELEVATION SYMBOL

TITLE AND DETAIL REFERENCE SYMBOL

WALL/BUILDING SECTION SYMBOL

NUMBER-CONSTRUCTION NOTE

NUMBER-DEMOLITION NOTE

NORTH ARROW (CONSTRUCTION NORTH)

PARTITION TYPES

REVISION

TITLE AND DETAIL REFERENCE SYMBOL

CONCRETE MASONRY UNITS GYPSUM BOARD PARTITIONS WOOD-FINISH GRADE WOOD BLOCKING RIGID WALL/PERIMETER INSULATION

CONCRETE

RIGID ROOF INSULATION

BATT INSULATION

POROUS FILL

ABBREVIATIONS

ABOVE

ABV

ACCESS DOOR (OR PANEL) AMERICAN WITH DISABILITIES ACT ADD ADJ ADDENDUM ADJACENT FIRE T ABOVE EXISTING SLAB ACCESS FLOOR AFF ABOVE FINISHED FLOOR AIR HANDLING UNIT FLR ALTERNATE ALUM ALUMINUM **APPROXIMATE** ARCH ARCHITECTURAL FTG ACOUSTICAL TILE CEILING (CONCEALED SUSPENSION) ATC ACOUSTICAL WALL PANEL GA BOARD GALV BEN BENCH GEN BETW BETWEEN GRD BUILDING **BLOCKING** GVP BEAM BMGYPB BOTTOM BR BRICK CENTER TO CENTER CABINET HDW CEM CEMENT CER CERAMIC CAST IRON CORNER GUARD CONTROL JOINT CENTERLINE HTR CLOS CLOSET CEILING HWCLR CLEAR CONCRETE MASONRY UNIT CO CLEAR OPENING COL COLUMN INSUL COMP COMPACTED CONC CONCRETE CONSTR CONSTRUCTION CONT CONTINUOUS CONV CONVECTOR CR COLD ROLLED CONNECT TO EXISTING DOUBLE DEGREE DEG DEMO DEMOLITION DETAIL DIAMETER DIR DIRECTORY DOWN DOOR OPENING DR DOOR DS DOWNSPOUT DWG DRAWING EAST EACH EXTERIOR FINISH SYSTEM

EXTERIOR INSULATION AND FINISH SYSTEM

ELECTRIC WATER COOLER - ACCESSIBLE

EXPANSION JOINT

ELECTRIC OR ELECTRICAL

EXPANDED POLYSTYRENE

ELECTRIC UNIT HEATER

ELECTRIC WATER COOLER

EXPANSION OR EXPOSED

ELEVATION

EPOXY

EQUAL

EQUIPMENT

EACH WAY

ESTIMATE

EXISTING

EXTERIOR

ELEC

EPS

EPX

EQ

EST

EUH

EW

EWCA

EXIST

EXP

EXT

LONG LEG VERTICAL LOCATION LOW POINT LIGHT LTG LIGHTING LOUVER LV MACH MACHINE MASONRY MAS MATL MAX

MATERIAL MAXIMUM METAL MEDIUM DENSITY FIBERBOARD MFB MINERAL FIBER BLANKET MECHANICAL METAL MFR MANUFACTURER MANHOLE MIN MINIMUM

FILLER

FAN COIL UNIT

FLAT HEAD

FIRE TREATED

FIXTURE

FLASHING

FIRE RATED

FOOTING

GAUGE

GALVANIZED

GENERAL

GROUND

HEAD

HARDWARE

HORIZONTAL

HIGH POINT

HOUR

HEIGHT

HEATER

INCH

JOINT

LINTEL

LONG

LINOLEUM FLOOR COVERING

INSULATION

INTERIOR

HOT WATER

HOLLOW METAL

FOOT OR FEET

FLOOR

FINISH OR FINISHED

FLOOR DRAIN OR FIRE DAMPER

FIRE EXTINGUISHER CABINET

FIBER-REINFORCED COATING

GYPSUM VENEER PLASTER

HORIZONTAL BLIND

GYPSUM BOARD (WALL OR CEILING)

GYPSUM BOARD SHAFT-WALL ASSEMBLY

HEATING, VENTILATING AND AIR CONDITIONING

FIRE EXTINGUISHER ON BRACKET

MARK METAL PANEL MOUNTED METAL

MISCELLANEOUS MASONRY OPENING SSM STAT STL STEEL STRUCT SYS

SOLID SURFACING MATERIAL STATIONARY STRUCTURAL OR STRUCTURE SYSTEM

NORTH

NUMBER

NOMINAL

OVERALL

ON CENTER

OPENING

OPPOSITE

PAVER TILE

PLASTIC FABRICATION

PLASTIC LAMINATE

PRESSURE TREATED

POLYVINYL CHLORIDE

RISER OR RADIUS

REINFORCING BAR

RESINOUS FLOORING

REINFORCED CONCRETE PIPE

REINFORCED OR REINFORCING

RECESSED WASTE RECEPTACLE

SILL, SOUTH OR SINGLE SCHEDULE OR SCHEDULED

STRUCTURAL FACING TILE

SANITARY NAPKIN DISPOSAL

ALUMINUM: PLASTIC PANEL)

SECTIONAL OVERHEAD DOOR (STEEL;

SOAP DISPENSER OR STORM DRAIN

ROOF DRAIN OR ROUND

PREFABRICATED

OUNCE

PIECE

PLATE

PLASTER

PARTITION

QUANTITY

REQUIRED

REQUIRED

RETURN

REVISION

ROOM

ROBE HOOK

ROOF VENT

SECTION

SHEET

SIMILAR

SQUARE FOOT

STEEL JOIST

SPECIFICATION

STAND PIPE

ROUGH OPENING

REMOVE EXISTING

OVERHEAD COILING DOOR

OVERHEAD COILING GRILLE

NTS

OA

OC

OHG

OPNG

OPP

ΟZ

PAV

PC

PLAM

PLAS

PREFAB

PRES 7

PTN

QTY

RCP

RD

REQ'D

REBAR

REINF

RESF

REQ

RET

REV

RX

SCH

SF

SFT

SIM

SJ

SP

T&B

Checked by: <u>FAH</u>

NOT APPLICABLE

NOT IN CONTRACT

NOT TO SCALE

TILE TOP & BOTTOM TONGUE & GROOVE

DRAWING INDEX

SHEET NAME	SHEET NUMBER	DRAWING TITLE
T1-01	1	TITLE SHEET
G1-01	2	INDEX, LEGEND AND ABBREVIATIONS
EC-01	3	EXISTING CONDITIONS
EC-02	4	DEMO PLAN
A1-01	5	BASEMENT PLAN AND WINDOW DETAILS
S0-01	6	STRUCTURAL GENERAL NOTES AND ABBREVIATIONS
S1-01	7	BASEMENT PLAN AND FIRST FLOOR PLAN
S3-01	8	BUILDING SECTIONS

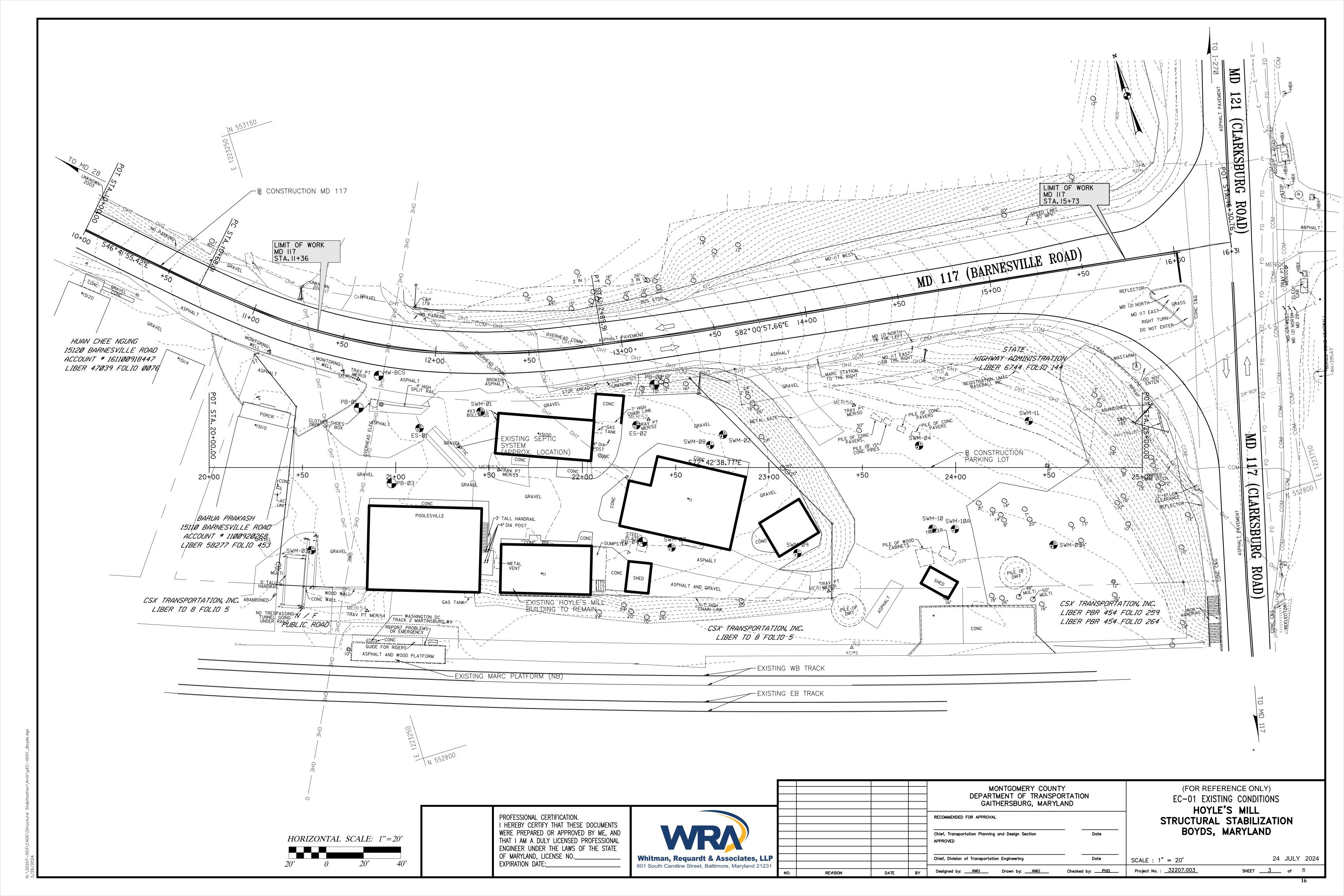
PROFESSIONAL CERTIFICATION. I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND, LICENSE NO.___ EXPIRATION DATE:___

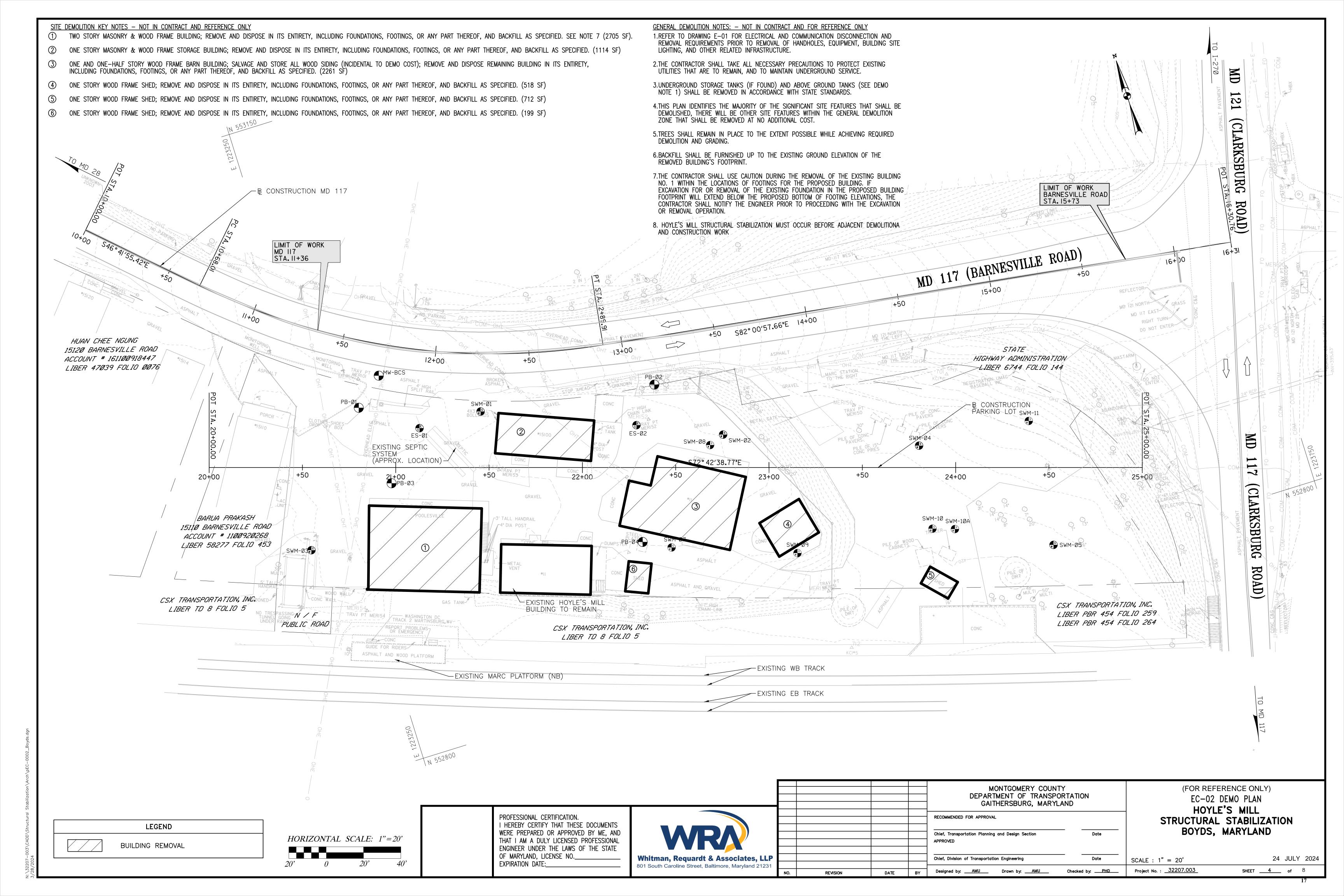


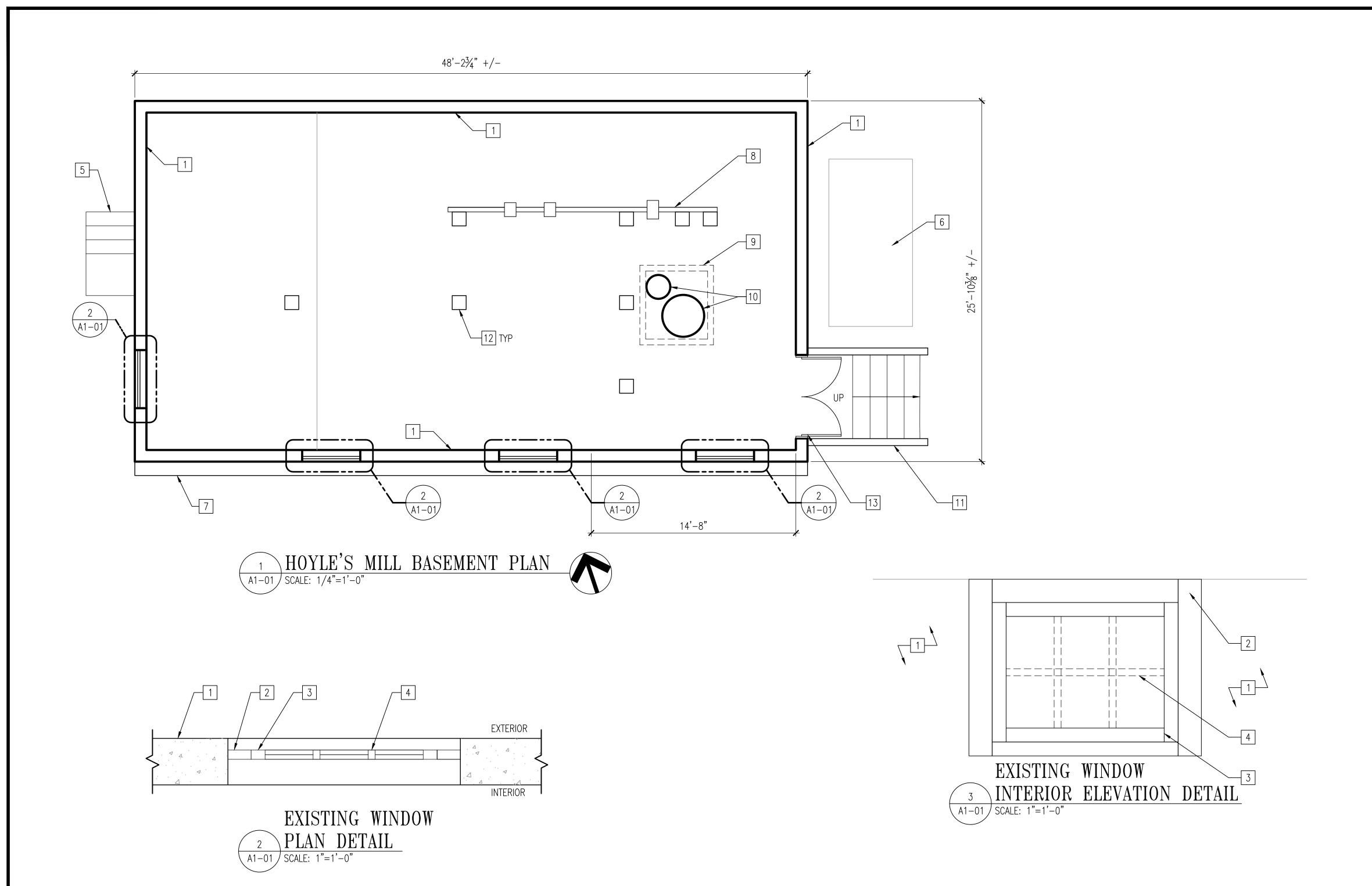
				MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION GAITHERSBURG, MARYLAND
				RECOMMENDED FOR APPROVAL
				Chief, Transportation Planning and Design Section APPROVED
				Chief, Division of Transportation Engineering
NO.	REVISION	DATE	BY	Designed by: <u>LJH</u> Drawn by: <u>FIE</u> Checked by

G1-01 INDEX, LEGEND AND ABBREVIATIONS HOYLE'S MILL STRUCTURAL STABILIZATION BOYDS, MARYLAND

SCALE : NTS 24 JULY 2024 Project No. : <u>32207.003</u> SHEET <u>2</u> of 8





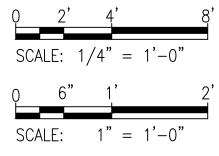


GENERAL NOTES

- 1. THE EXISTING STRUCTURE IS IN VERY POOR CONDITION, AND ACCESS TO THE BUILDING IS DANGEROUS. CONTRACTOR MUST PROVIDE TEMPORARY SHORING WITHIN THE BASEMENT PRIOR TO THE COMMENCEMENT OF THE WORK.
- 2. REMOVE AND DISPOSE OF ALL TRASH AND DEBRIS LOCATED WITHIN THE BASEMENT AREA, INCLUDING BUT NOT LIMITED TO TIRES AND ALL NON-STRUCTURAL COMPONENTS OTHER THAN COMPONENTS INDICATED TO REMAIN OR TO BE SALVAGED.
- 3. REMOVE AND DISPOSE OF LOOSE TRASH AND DEBRIS IN UPPER FLOORS, INCLUDING BUT NOT LIMITED TO UNUSED SHELVING, FLUORESCENT LIGHTING, FLAMMABLE/HAZARDOUS CHEMICALS, ETC. SALVAGE HISTORIC ARTIFACTS SUCH AS MILL EQUIPMENT. (NOT SHOWN ON PLAN).
- 4. DE-ENERGIZE AND REMOVE POWER LINES CONNECTED TO THE BUILDING.
- 5. REMOVE AND DISPOSE OF ALL INSECT AND BIRDS' NESTS.
- 6. CONDUCT EXPLORATORY ARCHAEOLOGY VIA SHOVEL TEST PITS IN THE BASEMENT AND AROUND THE FOUNDATION TO DETERMINE AGE OF EACH END OF BUILDING FOUNDATIONS BASED ON RESULTS FROM BUILDER TRENCHES.

KEYNOTES

- 1 EXISTING CONCRETE WALL AT BUILDING PERIMETER.
- 2 EXISTING WOOD WINDOW FRAME.
- 3 EXISTING WOOD WINDOW AWNING SASH INWARD
- 4 EXISTING MUNTINS MISSING FROM ALL SASH; PATTERN BASED ON SASH EVIDENCE.
- 5 REMOVE AND DISPOSE OF EXISTING STEEL STAIR PLATFORM.
- 6 REMOVE AND DISPOSE OF EXISTING METAL SHED.
- 7 EXISTING CONCRETE RETAINING WALL SET AGAINST EXISTING BUILDING TO REMAIN.
- 8 HISTORIC MILL EQUIPMENT INCLUDING PULLEY WHEELS, CRANKSHAFT, AND ALL HARDWARE TO REMAIN IN PLACE. WOOD POSTS SUPPORTING HISTORIC MILL EQUIPMENT TO REMAIN IN PLACE.
- 9 REMOVE AND DISPOSE OF EXISTING WOOD AND CMU ENCLOSURE SURROUNDING WELL ENCLOSURE.
- REMOVE AND DISPOSE OF EXISTING WELL PUMP EQUIPMENT. CAP AND ABANDON EXISTING WELL.
- DEMOLISH EXISTING EXTERIOR CONCRETE STAIR AND CHEEK WALLS, REPLACE IN—KIND.
- 12 EXISTING WOOD COLUMN TO REMAIN.
- DEMOLISH EXISTING WOOD DOOR AND FRAME, PROVIDE NEW STEEL DOOR AND FRAME.



PROFESSIONAL CERTIFICATION. I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND, LICENSE NO.___ EXPIRATION DATE:___



				MONTGOMERY COUN DEPARTMENT OF TRANSF GAITHERSBURG, MARY
				RECOMMENDED FOR APPROVAL
				Chief, Transportation Planning and Design Section APPROVED
				Chief, Division of Transportation Engineering
0.	REVISION	DATE	BY	Designed by: <u>LJH</u> Drawn by: <u>FIE</u>

MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION GAITHERSBURG, MARYLAND

RECOMMENDED FOR APPROVAL Chief, Transportation Planning and Design Section APPROVED Chief, Division of Transportation Engineering

Checked by: **FAH**

A1-01 BASEMENT PLAN AND WINDOW DETAILS HOYLE'S MILL STRUCTURAL STABILIZATION BOYDS, MARYLAND

SCALE : AS NOTED 24 JULY 2024 SHEET <u>5</u> of 8 Project No. : <u>32207.003</u>

GENERAL STRUCTURAL NOTES

GENERAL

- FIELD VERIFY DIMENSIONS, LOCATIONS AND ELEVATIONS SHOWN ON DRAWINGS FOR EXISTING STRUCTURES. BRING DISCREPANCIES TO THE ATTENTION OF THE ENGINEER. BEFORE PROCEEDING WITH THE WORK.
- 2. DO NOT DAMAGE EXISTING CONSTRUCTION TO REMAIN
- COORDINATE ACTIVITIES WITH THE OWNER.
- 4. THE DRAWINGS SHOW THE FINAL CONDITION OF THE STRUCTURES. PROVIDE MEANS TO STABILIZE THE STRUCTURES DURING TEMPORARY CONDITIONS.
- 5. SCALES NOTED ON THE DRAWINGS ARE FOR GENERAL INFORMATION ONLY. DO NOT OBTAIN DIMENSIONAL INFORMATION FROM DIRECT SCALING OF THE DRAWINGS.

SHALLOW FOUNDATIONS

- DESIGN OF SHALLOW FOUNDATIONS IS IN ACCORDANCE WITH THE RECOMMENDATIONS OF THE GEOTECHNICAL MEMORANDUM BY WRA DATED FEBRUARY 1, 2024
- 2. SHALLOW FOUNDATIONS MUST BEAR UPON UNDISTURBED SOIL OR COMPACTED ENGINEERED FILL WITH A MINIMUM ALLOWABLE BEARING CAPACITY OF 3000 PSF. OBTAIN THE SERVICES OF A GEOTECHNICAL ENGINEER LICENSED IN THE STATE OF MARYLAND WHO IS RESPONSIBLE FOR VERIFICATION OF THE SPECIFIED MINIMUM ALLOWABLE BEARING CAPACITY AT EACH FOOTING.
- 3. SHALLOW FOUNDATION ELEVATIONS SHOWN ON THE DRAWINGS ARE MINIMUM EXCAVATION DEPTHS. EXCAVATE FURTHER AS REQUIRED TO REMOVE UNSATISFACTORY SOILS TO A LAYER WITH THE MINIMUM SPECIFIED ALLOWABLE BEARING CAPACITY. WHERE REQUIRED, PROVIDE COMPACTED ENGINEERED FILL TO ACHIEVE THE REQUIRED SUBGRADE FLEVATIONS, NOTIFY THE ENGINEER OF ANY CONDITIONS THAT REQUIRE CHANGES IN FOUNDATION ELEVATIONS.
- 4. PLACE SHALLOW FOUNDATIONS ON THE SAME DAY THAT THE BEARING SURFACE IS INSPECTED BY THE CONTRACTOR'S GEOTECHNICAL ENGINEER. ANY BEARING SURFACE NOT PLACED ON THE SAME DAY OF INITIAL INSPECTION MUST BE RE-INSPECTED BY THE CONTRACTOR'S GEOTECHNICAL ENGINEER ON THE DAY CONCRETE IS PLACED.
- KEEP EXCAVATIONS DRY.
- MINIMUM DEPTH BELOW GRADE FOR BOTTOM OF FOUNDATIONS FOR FROST PROTECTION IS 30 INCHES.
- DO NOT PLACE BACKFILL AGAINST SUBSTRUCTURE WALLS UNTIL WALL CONCRETE ACHIEVES ITS SPECIFIED 28-DAY COMPRESSIVE STRENGTH USING FIELD-CURED CYLINDERS.
- 8. PROVIDE SUPPORT OF EXCAVATIONS REQUIRED TO COMPLETE THE WORK SHOWN ON THE DRAWINGS. SUPPORT OF EXCAVATION SYSTEMS MUST BE DESIGNED BY THE CONTRACTOR'S PROFESSIONAL ENGINEER

CONCRETE

- PROVIDE NORMAL-WEIGHT CONCRETE WITH A MINIMUM COMPRESSIVE STRENGTH OF 4500 PSI AT 28 DAYS.
- 2. CONCRETE MUST BE AIR ENTRAINED.
- 3. DETAIL AND CONSTRUCT REINFORCED CONCRETE IN ACCORDANCE WITH AMERICAN CONCRETE INSTITUTE ACI 301, "SPECIFICATION FOR STRUCTURAL CONCRETE", AND AS SPECIFIED HEREIN.
- 4. DETAIL REINFORCING STEEL IN ACCORDANCE WITH AMERICAN CONCRETE INSTITUTE ACI 315. "MANUAL OF STANDARD PRACTICE FOR DETAILING REINFORCED CONCRETE STRUCTURES" AND ACI SP-66, "ACI DETAILING MANUAL."
- 5. PROVIDE REINFORCING STEEL CONFORMING TO ASTM A615, GRADE 60, DEFORMED BARS.
- 6. UNLESS NOTED OTHERWISE ON THE DRAWINGS, PROVIDE CONCRETE COVER FOR REINFORCING STEEL AS FOLLOWS: A. CONCRETE CAST AGAINST AND PERMANENTLY EXPOSED TO EARTH, FILL: 3" B. CONCRETE EXPOSED TO WEATHER OR IN CONTACT WITH: EARTH OR FILL: 2"
- SUBMIT REINFORCING STEEL DETAILS AND JOINT LAYOUT (SHOP DRAWINGS) AND RECEIVE APPROVAL FROM THE ENGINEER BEFORE PROCEEDING WITH FABRICATION.
- 8. CHAMFER ALL EXPOSED CONCRETE EDGES 3/4" UNLESS NOTED OTHERWISE.
- 9. PLACE CONTROLLED LOW STRENGTH MATERIAL IN MAXIMUM LIFT HEIGHTS OF 3'-0" AND ALLOW TO SET BEFORE POURING NEXT LIFT.
- 10. COLD WEATHER PLACEMENT OF CONCRETE MUST BE IN ACCORDANCE WITH ACI 306R, ACI 306.1, AND THE SPECIFICATIONS.
- 11. HOT WEATHER PLACEMENT OF CONCRETE MUST BE IN ACCORDANCE WITH ACI 305R, ACI 305.1, AND THE SPECIFICATIONS.

ADHESIVE ANCHORS

- 1. THE ADHESIVE ANCHOR SYSTEM USED FOR POST INSTALLED ANCHORAGE TO CONCRETE MUST CONFORM TO THE REQUIREMENTS OF THE MOST RECENTLY PUBLISHED ACI 355.4. "ACCEPTANCE CRITERIA FOR QUALIFICATION OF POST-INSTALLED ADHESIVE ANCHORS IN CONCRETE AND COMMENTARY." EACH ADHESIVE ANCHOR SYSTEM MUST SATISFY THE STRENGTH REQUIREMENTS FOR ITS USE, BULK-MIXED ADHESIVES ARE NOT PERMITTED ADHESIVE ANCHORAGE DESIGN IS IN ACCORDANCE WITH ACI 318-14. ADHESIVE ANCHORS IN CONCRETE MUST BE QUALIFIED FOR USE IN CRACKED CONCRETE IN ACCORDANCE WITH ACI 355.4. PROVIDE THE FOLLOWING ANCHOR SYSTEMS, OR APPROVED EQUALS:
- A. ANCHORAGE TO CONCRETE i. HILTI HIT-HY 200 WITH HILTI HIT-Z-R ROD OR HAS-R THREADED ROD.

CONTINUE:

- 2. PROVIDE TYPE 316 STAINLESS STEEL ANCHORS.
- 3. CONCRETE AT THE TIME OF ADHESIVE ANCHOR INSTALLATION MUST HAVE A MINIMUM AGE OF 21 DAYS.
- INSTALL ADHESIVE ANCHORS WITH A MINIMUM EDGE DISTANCE OF 3 INCHES TO ANY FREE EDGE OF CONCRETE, OR EDGE DISTANCE INDICATED ON DRAWINGS, WHICHEVER IS GREATER.
- 5. INSTALL ADHESIVE ANCHORS WITH TRAINED QUALIFIED PERSONNEL, IN ACCORDANCE WITH THE MANUFACTURER'S PRINTED INSTALLATION INSTRUCTIONS.
- PROVIDE THOROUGHLY CLEANED ANCHOR HOLES PRIOR TO ADHESIVE INJECTION, AS REQUIRED BY THE MANUFACTURER'S PRINTED INSTALLATION INSTRUCTIONS. PROTECT DRILLED AND CLEANED ANCHOR HOLES FROM CONTAMINATION UNTIL THE ADHESIVE IS INSTALLED.
- PROVIDE ANCHORS CLEAN, OIL-FREE, AND FREE OF LOOSE RUST, PAINT, OR OTHER COATINGS.
- PROVIDE INSTALLED ADHESIVE ANCHORS SECURELY FIXED IN-PLACE TO PREVENT DISPLACEMENT WHILE THE ADHESIVE CURES.
- THE ENGINEER MUST RECEIVE DOCUMENTED CONFIRMATION THAT ALL PERSONNEL WHO INSTALL ANCHORS ARE TRAINED PRIOR TO THE COMMENCEMENT OF INSTALLING ANCHORS.
- 10. PERSONNEL INSTALLING HORIZONTAL OR OVERHEAD ADHESIVE ANCHORS MUST BE QUALIFIED PER THE SPECIFICATIONS.
- 11. DO NOT DAMAGE EXISTING REINFORCING STEEL IN THE CONCRETE DURING ANCHOR INSTALLATION, UNLESS OTHERWISE NOTED ON THE DRAWINGS. PRIOR TO ANCHOR INSTALLATION, DETERMINE LOCATION OF EXISTING REINFORCING STEEL BY NON-DESTRUCTIVE MEANS AND NOTIFY THE ENGINEER OF ANY CONFLICTS BETWEEN REINFORCING STEEL AND ANCHOR LOCATION PRIOR TO FABRICATION OF MATERIALS.
- 12. DESIGN BASIS FOR ADHESIVE ANCHORS IS INDICATED ABOVE, SUBSTITUTIONS WILL BE CONSIDERED, BUT PRODUCT MUST MEET OR EXCEED ALL CRITERIA OF THE SPECIFIED ANCHOR. SUBSTITUTION REQUESTS MUST BE APPROVED IN WRITING BY THE ENGINEER PRIOR TO USE. PROVIDE PRODUCT DATA AND CALCULATIONS DEMONSTRATING THE SUBSTITUTED PRODUCT IS CAPABLE OF ACHIEVING THE PERFORMANCE OF THE SPECIFIED PRODUCT. SUBSTITUTIONS WILL BE EVALUATED BY THEIR HAVING AN ICC ESR SHOWING COMPLIANCE WITH THE RELEVANT BUILDING CODE FOR SEISMIC USES, LOAD RESISTANCE, INSTALLATION CATEGORY, AND AVAILABILITY OF COMPREHENSIVE INSTALLATION INSTRUCTIONS. ADHESIVE ANCHOR EVALUATION WILL ALSO CONSIDER CREEP. IN-SERVICE TEMPERATURES AND INSTALLATION TEMPERATURE.

DELEGATED DESIGN

- DESIGN AND DETAILING RESPONSIBILITY FOR THE FOLLOWING ENGINEERED SYSTEMS AND COMPONENTS IS DELEGATED TO A QUALIFIED PROFESSIONAL ENGINEER. SELECTED AND HIRED BY THE CONTRACTOR. THESE SYSTEMS AND COMPONENTS INCLUDE, BUT ARE NOT LIMITED TO:
- GUARDRAILS AND HANDRAILS
- TEMPORARY SUPPORT OF EXCAVATION AND STRUCTURES
- CONCRETE FORMWORK AND SHORING
- D. HELICAL PILES
- 2. DELEGATED DESIGN ITEMS MUST COMPLY WITH THE APPLICABLE DESIGN CODES. STANDARDS. CRITERIA. AND LOADS INDICATED IN THE CONSTRUCTION DOCUMENTS.
- PROVIDE CALCULATIONS AND SHOP DRAWINGS FOR DELEGATED DESIGN ITEMS, STAMPED AND SIGNED BY A REGISTERED PROFESSIONAL ENGINEER LICENSED IN THE STATE OF MARYLAND, UNLESS OTHERWISE NOTED. SUBMIT CALCULATIONS AND SHOP DRAWINGS FOR REVIEW AND APPROVAL.

HELICAL PILES

- 1. HELICAL PILE FOUNDATIONS MUST BE DESIGNED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF MARYLAND, WITH AT LEAST 10 YEARS EXPERIENCE IN THE DESIGN OF HELICAL PILES. SUBMIT QUALIFICATIONS FOR REVIEW AND APPROVAL.
- 2. HELICAL PILE FOUNDATIONS MUST BE 2-1/4 INCH DIAMETER SS225 PILE SERIES HELICAL
- 3. PROVIDE HELICAL PILES AT THE LOCATIONS AND ELEVATIONS SHOWN WITH A MINIMUM ULTIMATE COMPRESSIVE CAPACITY OF 35 KIPS PER PILE, VERIFIED BY LOAD TESTING. HELICAL PILES MUST HAVE AN ALLOWABLE COMPRESSIVE CAPACITY OF 17.5 TONS PER PILE. ALLOWABLE PILE COMPRESSIVE CAPACITIES MUST BE ESTABLISHED AND VERIFIED BASED ON FINAL INSTALLATION TORQUE WITH A MINIMUM FACTOR OF SAFETY OF 2.0. OR AS APPROVED BY THE ENGINEER.
- HELICAL PILE FOUNDATIONS MUST HAVE A MINIMUM TIP ELEVATION OF -20.0 FEET RELATIVE TO THE BOTTOM OF THE EXISTING CONCRETE WALL TO BE SUPPORTED.
- INSTALLATION OF HELICAL PILE FOUNDATIONS MUST BE PERFORMED BY A QUALIFIED EXPERIENCED INSTALLER WHO SPECIALIZES IN HELICAL PILE WORK. INSTALLER MUST HAVE A MINIMUM OF FIVE YEARS OF EXPERIENCE INSTALLING HELICAL PILES ON PROJECTS OF 6 SIMILAR SIZE AND SCOPE.

CONTINUE:

- 6. INSTALLATION EQUIPMENT MUST BE A ROTARY TYPE, HYDRAULIC POWER-DRIVEN TORQUE MOTOR WITH CLOCKWISE AND COUNTER-CLOCKWISE ROTATION CAPABILITY. MOTOR MUST BE A HIGH TORQUE, LOW REVOLUTIONS PER MINUTE (RPM) MOTOR WITH THE ABILITY TO ADJUST RPM DURING INSTALLATION. TORQUE MOTOR MUST HAVE A TORQUE CAPACITY AT LEAST 15 PERCENT GREATER THAN THE FINAL INSTALLATION TORQUE REQUIRED FOR THE PROJECT.
- 7. CONTRACTOR MUST PREPARE AND SUBMIT TO THE ENGINEER FOR APPROVAL, HELICAL PILE MANUFACTURER AND PRODUCT INFORMATION, INSTALLER QUALIFICATIONS AND EQUIPMENT. AND HELICAL PILE FOUNDATION DESIGN ENGINEER QUALIFICATIONS AT LEAST 30 CALENDAR DAYS PRIOR TO THE PLANNED START OF INSTALLATION.
- 8. CONTRACTOR'S APPROVED HELICAL PILE FOUNDATION DESIGN ENGINEER MUST PREPARE AND SUBMIT TO THE ENGINEER FOR APPROVAL, A HELICAL PILE FOUNDATION DESIGN REPORT WITH CALCULATIONS, SHOP DRAWINGS, DETAILS AND SPECIFICATIONS FOR THE HELICAL PILES PROPOSED FOR USE ON THIS PROJECT; PLANNED PILE INSTALLATION DEPTH AND CONFIGURATION, MINIMUM FINAL PILE INSTALLATION TORQUE, AND REQUIREMENTS FOR CORROSION PROTECTION AT LEAST 14 CALENDAR DAYS PRIOR TO THE PLANNED START OF INSTALLATION.
- 9. CONTRACTOR MUST SUBMIT TO THE ENGINEER FOR APPROVAL, THE PROPOSED LOAD TESTING PROCEDURE WITH DETAILS OF THE STATIC COMPRESSION LOAD TEST SETUP INCLUDING A DRAWING OF THE APPARATUS INDICATING THE LOCATION OF DIAL GAUGES, REFERENCE BEAMS, AND REACTION FRAME. THE SUBMITTAL MUST BE PREPARED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF MARYLAND AND MUST INCLUDE CALCULATIONS VERIFYING THAT THE CAPACITY OF THE REACTION FRAME AND MEANS OF RESISTING THE APPLIED FORCE ARE SUFFICIENT TO SAFELY WITHSTAND THE MAXIMUM COMPRESSION TEST LOAD. CALIBRATION CERTIFICATES FROM WITHIN SIX MONTHS OF THE DATE OF SUBMITTAL MUST BE PROVIDED FOR HYDRAULIC JACKS, LOAD CELLS, AND MEASURING DEVICES.
- 10. PRIOR TO CONSTRUCTION OF HELICAL PILE FOUNDATIONS, THE CONTRACTOR MUST INSTALL ONE TEST PILE USING THE INSTALLATION METHODS, PROCEDURES, EQUIPMENT, PRODUCTS, AND FINAL INSTALLATION TORQUE PROPOSED FOR THE PRODUCTION HELICAL PILES. THE TEST PILE MUST NOT BE USED AS A PRODUCTION PILE FOR THE SUPPORT OF THE BUILDING.
- THE CONTRACTOR MUST PERFORM AT LEAST ONE ACCEPTABLE HELICAL PILE COMPRESSION TEST FOLLOWING THE "QUICK TEST" PROCEDURE IN ACCORDANCE WITH ASTM D1143. THE MAXIMUM TEST LOAD MUST BE 35 KIPS AND MUST BE CONSIDERED ACCEPTABLE PROVIDED THE MAXIMUM TEST LOAD IS APPLIED WITHOUT HELICAL PILE FAILURE AND THE DEFLECTION OF THE PILE HEAD AT THE ALLOWABLE DESIGN LOAD IS LESS THAN 1-INCH.
- 12. IF THE HELICAL PILE LOAD TEST FAILS THE ACCEPTANCE CRITERIA THE CONTRACTOR MUST MODIFY THE HELICAL PILE DESIGN AND/OR INSTALLATION METHODS AND RETEST THE MODIFIED PILE. AS DIRECTED BY THE OWNER. ANY MODIFICATIONS OF DESIGN OR CONSTRUCTION PROCEDURES, AND ANY PILE LOAD RETESTING REQUIRED MUST BE AT THE CONTRACTOR'S EXPENSE.
- 13. THE AVERAGE TORQUE FOR THE LAST THREE FEET OF PENETRATION MUST BE USED AS THE BASIS OF COMPARISON WITH THE MINIMUM INSTALLATION TORQUE AS REQUIRED. THE AVERAGE TORQUE MUST BE DEFINED AS THE AVERAGE OF THE LAST THREE READINGS RECORDED AT ONE FOOT INTERVALS.
- 14. UPON COMPLETION OF THE HELICAL PILE INSTALLATION, PROVIDE THE OWNER WITH COPIES OF THE PILE INSTALLATION LOGS, MAINTAINED DURING CONSTRUCTION, CONFIRMING THE HELICAL PILE CONFIGURATION, DEPTH OF INSTALLATION, AND RECORDED FINAL INSTALLATION TORQUE VALUES.
- 15. PROVIDE THE OWNER WITH A CERTIFICATION PREPARED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF MARYLAND, STATING THAT THE HELICAL PILES AS INSTALLED HAVE THE CAPACITY TO SUPPORT THE STRUCTURE AND THAT THE ALLOWABLE HELICAL PILE CAPACITIES HAVE BEEN VERIFIED.

RECOMMENDED SEQUENCES OF DEMOLITION AND CONSTRUCTION

- 1. INSTALL HELICAL PILES OR MICROPILES TO SUPPORT EXISTING NORTH AND EAST
- PILE SHAFTS OR APPROVED EQUAL. PROVIDE MAXIMUM HELIX PLATE DIAMETER OF 14". 2. INSTALL TEMPORARY SHORING OF FIRST FLOOR FRAMING WITHIN CRAWLSPACE.
 - 3. INSTALL CONTINUOUS STEEL REPAIR PLATES WITH ADHESIVE ANCHORS ACROSS EXISTING CRACKS IN EXISTING CONCRETE WALLS.
 - REMOVE ALL TRASH AND DEBRIS. AND WELL ENCLOSURE AND EQUIPMENT FROM CRAWLSPACE AREA. CAP AND ABANDON EXISTING WELL.
 - 5. ONCE EPOXY FOR ADHESIVE ANCHORS HAS SET, DEMOLISH EXISTING STONE MASONRY FOOTINGS FOR NORTH AND EAST WALLS, DEMOLISH EXISTING LARGE CONCRETE MASS, DEMOLISH CONCRETE PAD UNDER METAL SHED. DEMOLISH EXISTING SITE STAIR AND CHEEK WALLS, AND EXCAVATE BELOW WALLS TO THE BOTTOM ELEVATION OF THE NEW FOOTINGS.
 - INSTALL REINFORCEMENT, FORMWORK, AND PLACE CONCRETE FOR CONTINUOUS WALL FOOTINGS FOR NORTH AND EAST WALLS.

CONTINUE:

- INSTALL REINFORCEMENT. FORMWORK. AND PLACE CONCRETE FOR PERIMETER WALLS FOR NORTH AND FAST WALLS.
- FOR SECTION OF THE SOUTH WALL FOUNDED ON A STONE MASONRY FOOTING, DEMOLISH STONE MASONRY FOOTING, EXCAVATE TO THE BOTTOM ELEVATION OF THE NEW FOOTING, AND PLACE NEW CONCRETE FOOTINGS. PERFORM THIS WORK IN STAGES. TO LIMIT THE LENGTH OF EXISTING FOOTING REMOVED AT ANY ONE TIME, AND PROVIDE TEMPORARY SHORING AS NEEDED.
- 9. ERECT NEW CONCRETE SITE STAIR AND CHEEK WALLS.
- 10. PERFORM REMAINDER OF WORK INDICATED WITHIN CONTRACT DOCUMENTS.

RECOMMENDED SEQUENCES OF DEMOLITION AND CONSTRUCTION

- INSTALL HELICAL PILES OR MICROPILES TO SUPPORT EXISTING NORTH AND EAST WALLS.
- 2. INSTALL TEMPORARY SHORING OF FIRST FLOOR FRAMING WITHIN CRAWLSPACE.
- 3. INSTALL CONTINUOUS STEEL REPAIR PLATES WITH ADHESIVE ANCHORS ACROSS EXISTING CRACKS IN EXISTING CONCRETE WALLS.
- REMOVE ALL TRASH AND DEBRIS, AND WELL ENCLOSURE AND EQUIPMENT FROM CRAWLSPACE AREA. CAP AND ABANDON EXISTING WELL
- ONCE EPOXY FOR ADHESIVE ANCHORS HAS SET. DEMOLISH EXISTING STONE MASONRY FOOTINGS FOR NORTH AND EAST WALLS, DEMOLISH EXISTING LARGE CONCRETE MASS, DEMOLISH CONCRETE PAD UNDER METAL SHED, DEMOLISH EXISTING SITE STAIR AND CHEEK WALLS, AND EXCAVATE BELOW WALLS TO THE BOTTOM ELEVATION OF THE NEW FOOTINGS.
- 6. INSTALL REINFORCEMENT, FORMWORK, AND PLACE CONCRETE FOR CONTINUOUS WALL FOOTINGS FOR NORTH AND EAST WALLS.
- INSTALL REINFORCEMENT, FORMWORK, AND PLACE CONCRETE FOR PERIMETER WALLS FOR NORTH AND EAST WALLS.
- 8. FOR SECTION OF THE SOUTH WALL FOUNDED ON A STONE MASONRY FOOTING, DEMOLISH STONE MASONRY FOOTING, EXCAVATE TO THE BOTTOM ELEVATION OF THE NEW FOOTING, AND PLACE NEW CONCRETE FOOTINGS. PERFORM THIS WORK IN STAGES. TO LIMIT THE LENGTH OF EXISTING FOOTING REMOVED AT ANY ONE TIME. AND PROVIDE TEMPORARY SHORING AS NEEDED.
- 9. ERECT NEW CONCRETE SITE STAIR AND CHEEK WALLS.
- 10. PERFORM REMAINDER OF WORK INDICATED WITHIN CONTRACT DOCUMENTS.

DESIGN LOADS AND CRITERIA

ALL LOADS INDICATED BELOW ARE UNFACTORED. LOADS INDICATED ARE FOR THE DESIGN OF THE FOUNDATION FOR THE STRUCTURAL STABILIZATION DETAILED IN THESE DRAWINGS. EXISTING SUPERSTRUCTURE HAS NOT BEEN ANALYZED FOR ITS CAPACITY TO SUPPORT THESE LOADS.

1. RISK CATEGORY: II

2. DEAD LOADS:

- STRUCTURES: ACTUAL WEIGHT
- WEIGHT OF SOIL 100 PCF FOR RESISTING UPLIFT
- WEIGHT OF SOIL 120 PCF FOR DEAD LOAD
- D. EARTH PRESSURE COEFFICIENTS: i. AT-REST: KO = 0.5
- ii. ACTIVE: KA = 0.33
- iii. PASSIVE: KP = 3.0
- E. SUPERIMPOSED DEAD LOAD:
- i. ROOF: 20 PSF
- ii. 2ND FLOOR: 20 PSF
- iii. 1ST FLOOR: 20 PSF iv. SUPERIMPOSED DEAD LOAD INCLUDES COMBINED WEIGHT OF ALL
- ASSUMED PERMANENT NON-STRUCTURAL COMPONENTS SUPPORTED BY THE FRAMING. INCLUDING MEP COMPONENTS. ROOFING. FLOOR AND CEILING FINISHES. AND SPRINKLERS FOR A FUTURE ADAPTIVE REUSE OF THE BUILDING.
- LIVE LOADS:
- FIRST FLOOR: 100 PSF SECOND FLOOR: 100 PSF
- ATTIC SPACE: 100 PSF STAIRS AND LANDINGS: 100 PSF
- E. GUARDRAIL 200 LBS AT EACH POST OR 50 PLF ALONG THE TOP RAIL. WHICHEVER IS GREATER.

Chief, Division of Transportation Engineering

CONTINUE:

- 5. ROOF SNOW LOAD:
 - A. GROUND SNOW LOAD (PG): 30 PSF
 - B. EXPOSURE FACTOR (CE): 1.0
 - C. THERMAL FACTOR (CT): 1.2
 - D. SNOW LOAD IMPORTANCE FACTOR (IS): 1.0 E. SLOPED ROOF SNOW LOAD (PS): 25.2 PSF

6. WIND LOAD:

- A. ULTIMATE WIND SPEED (VULT): 115 MPH
- B. NOMINAL WIND SPEED (VASD): 89 MPH
- C. EXPOSURE CATEGORY: C
- INTERNAL PRESSURE COEFFICIENT: +/- 0.18 E. COMPONENTS AND CLADDING: PER ASCE 7-16

7. SEISMIC LOAD:

- A. SEISMIC IMPORTANCE FACTOR (IE): 1.0 B. MAXIMUM EARTHQUAKE SPECTRAL RESPONSE ACCELERATION AT
- SHORT PERIODS: SS=0.135G C. MAXIMUM EARTHQUAKE SPECTRAL RESPONSE ACCELERATION AT
- ONE-SECOND: S1=0.043G
- D. SITE CLASSIFICATION: D
- E. SITE SEISMIC COEFFICIENT: FA=1.6; FV=2.4
- SPECTRAL RESPONSE COEFFICIENTS: SDS = 0.144; SD1 = 0.069
- G. SEISMIC DESIGN CATEGORY: B H. ANALYSIS PROCEDURE: EQUIVALENT LATERAL FORCE
- I. BASIC SEISMIC FORCE RESISTING SYSTEM: LIGHT-FRAME (WOOD)
- BEARING WALLS WITH SHEAR PANELS. J. RESPONSE MODIFICATION FACTOR: R=2

CODES AND STANDARDS

- MARYLAND BUILDING PERFORMANCE STANDARDS (MBPS).
- 2. INTERNATIONAL BUILDING CODE IBC (2018), INCLUDING THE MODIFICATIONS MADE BY LOCAL JURISDICTION
- 3. AMERICAN CONCRETE INSTITUTE ACI 318 (2014), "BUILDING CODE REQUIREMENTS FOR STRUCTURAL CONCRETE"
- 4. AMERICAN SOCIETY OF CIVIL ENGINEERS ASCE 7 (2016), "MINIMUM DESIGN LOADS AND ASSOCIATED CRITERIA FOR BUILDINGS AND OTHER STRUCTURES"

STRUCTURAL ABBREVIATIONS

- AMERICAN CONCRETE INSTITUTE AMERICAN NATIONAL STANDARDS INSTITUTE AMERICAN SOCIETY FOR TESTING MATERIALS AMERICAN SOCIETY OF CIVIL ENGINEERS ASCE
- ARCH ARCHITECT
- **BOTTOM** CLR CLEAR
- COL COLUMN CONC CONCRETE
- CONCRETE REINFORCING STEEL INSTITUTE CRSI
- DEMO DEMOLITION/DEMOLISH DET
- DRAWING
- EQUAL **ELEVATION**
- **EXIST** EXISTING FOUNDATION

ENGINEER

- FOOT/FEET
- INTERNATIONAL BUILDING CODE
- MINIMUM
- POUNDS PER SQUARE INCH
- STRUCT STRUCTURAL
- VERIFY IN FIELD

TYPICAL

4. ROOF LIVE LOAD: 30 PSF

HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME. AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE

PROFESSIONAL CERTIFICATION.

OF MARYLAND, LICENSE NO.__

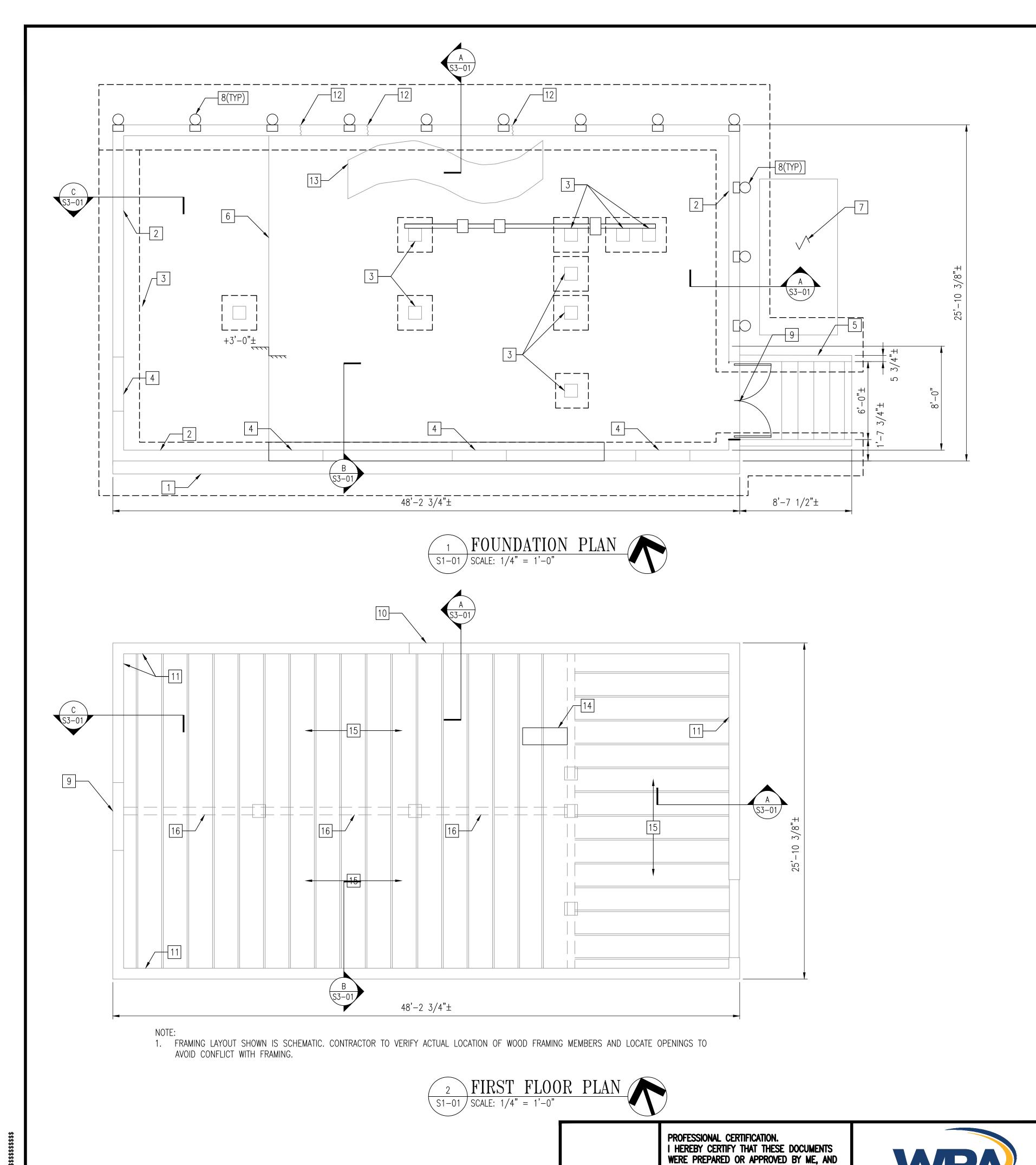
EXPIRATION DATE:_

Whitman, Requardt & Associates, LLP 801 South Caroline Street, Baltimore, Maryland 21231

MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION GAITHERSBURG. MARYLAND RECOMMENDED FOR APPROVAL Chief, Transportation Planning and Design Section

SO-01 STRUCTURAL GENERAL NOTES AND **ABBREVIATIONS** HOYLE'S MILL STRUCTURAL STABILIZATION BOYDS, MARYLAND

SCALE : NO SCALE 24 JULY 2024 Project No. : <u>32207.003</u> SHEET <u>6</u> of 8



SCALE: 1/4" = 1'-0"

THAT I AM A DULY LICENSED PROFESSIONAL

ENGINEER UNDER THE LAWS OF THE STATE

Whitman, Requardt & Associates, LLP

801 South Caroline Street, Baltimore, Maryland 21231

OF MARYLAND, LICENSE NO.___

EXPIRATION DATE:__

GENERAL SHEET NOTES

- 1. REFER TO SHEET SO-01 FOR STRUCTURAL GENERAL NOTES, BUILDING CODES AND STANDARDS, AND DESIGN LOADS.
- 2. FIELD VERIFY DIMENSIONS, LOCATIONS AND ELEVATIONS SHOWN ON DRAWINGS FOR EXISTING STRUCTURES. BRING DISCREPANCIES TO THE ATTENTION OF THE ENGINEER BEFORE PROCEEDING WITH WORK.
- 3. COORDINATE WORK WITH ARCHITECTURAL DRAWINGS.
- 4. THE EXISTING STRUCTURE IS IN VERY POOR CONDITION, AND ACCESS TO THE BUILDING IS DANGEROUS. CONTRACTOR MUST PROVIDE TEMPORARY SHORING WITHIN THE BASEMENT AREA PRIOR TO COMMENCING WITH ANY OTHER WORK WITHIN THE BASEMENT.
- SHEET KEYNOTES
- EXISTING SITE RETAINING WALL ADJACENT TO EXTERIOR WALL TO
- 2. EXISTING EXTERIOR CONCRETE WALL ON MASONRY FOOTINGS TO
- 3. EXISTING WOOD BUILDING COLUMN TO REMAIN.
- 4. EXISTING WINDOW OPENING. SEE ARCHITECTURAL DRAWINGS FOR MODIFICATIONS.
- 5. DEMOLISH EXISTING STAIR TO BASEMENT AND REPLACE WITH SIMILAR NEW CONCRETE STAIR DESIGNED TO MEET CURRENT CODE WITH GALVANIZED STEEL HANDRAILS ON EACH SIDE.
- 6. ELEVATION CHANGE IN EXISTING UNFINISHED BASEMENT FLOOR.
- 7. DEMOLISH EXISTING CONCRETE PAD UNDER EXISTING METAL SHED.
- 8. HELICAL PILES SPACED AT 6'-0" +/- TO SUPPORT EXISTING CONCRETE FOUNDATION WALL. HELICAL PILE DESIGN DELEGATED TO CONTRACTOR'S ENGINEER.
- 9. EXISTING DOUBLE DOOR. SEE ARCHITECTURAL DRAWINGS.
- 10. EXISTING DOOR.
- 11. EXISTING EXTERIOR WOOD-FRAMED WALLS.
- 12. EXISTING FULL-DEPTH, FULL-WIDTH CRACK IN EXISTING CONCRETE WALL. REPAIR WITH (2) 4" WIDE CONTINUOUS STEEL PLATES WITH ADHESIVE ANCHORS AT 12" EXTENDING TO ADJACENT PILES.
- 13. DEMOLISH, REMOVE, AND DISPOSE OF EXISTING LARGE CONCRETE MASS (APPROXIMATELY 40" WIDE X 36" DEEP).
- 14. PATCH HOLE IN FLOOR LOCATED IN FRONT OF EXISTING STAIR WITH PLYWOOD.
- 15. EXISTING 1/2" THICK WOOD BOARDS ON 2"X8" WOOD JOISTS SPACED AT 22" +/- ON CENTER.
- 16. EXISTING 7" WIDE X 9" DEEP WOOD GIRDERS.

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION GAITHERSBURG, MARYLAND

RECOMMENDED FOR APPROVAL Chief, Transportation Planning and Design Section APPROVED

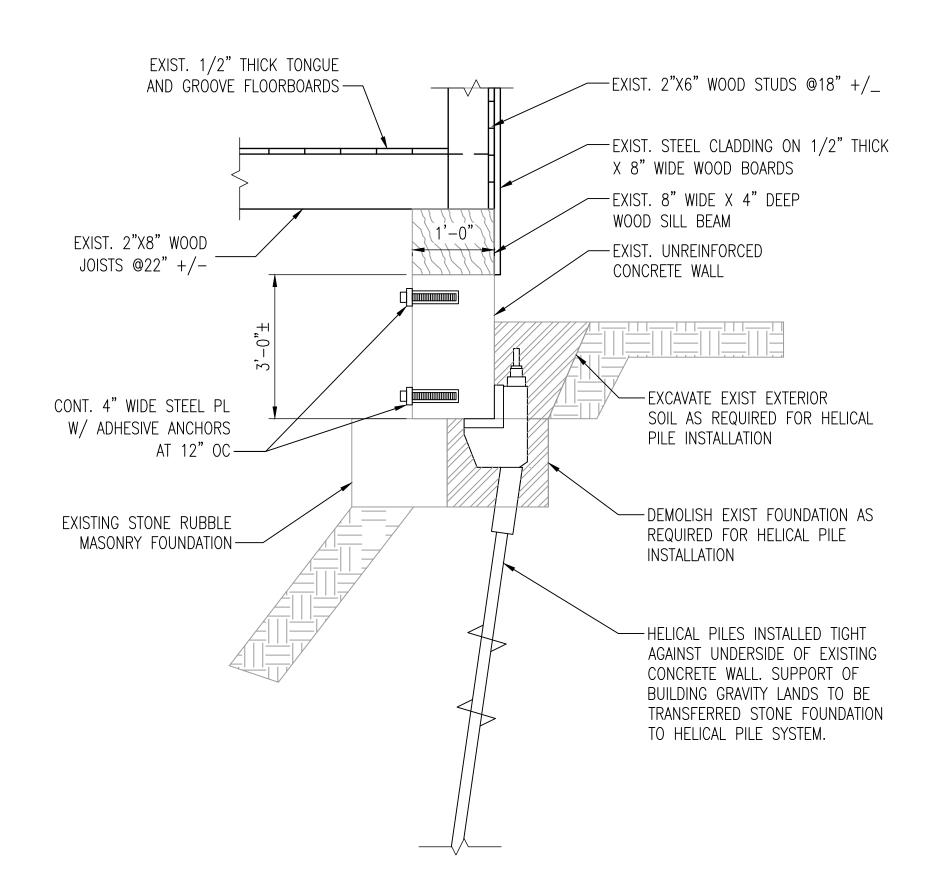
Designed by: SA Drawn by: SA

Chief, Division of Transportation Engineering

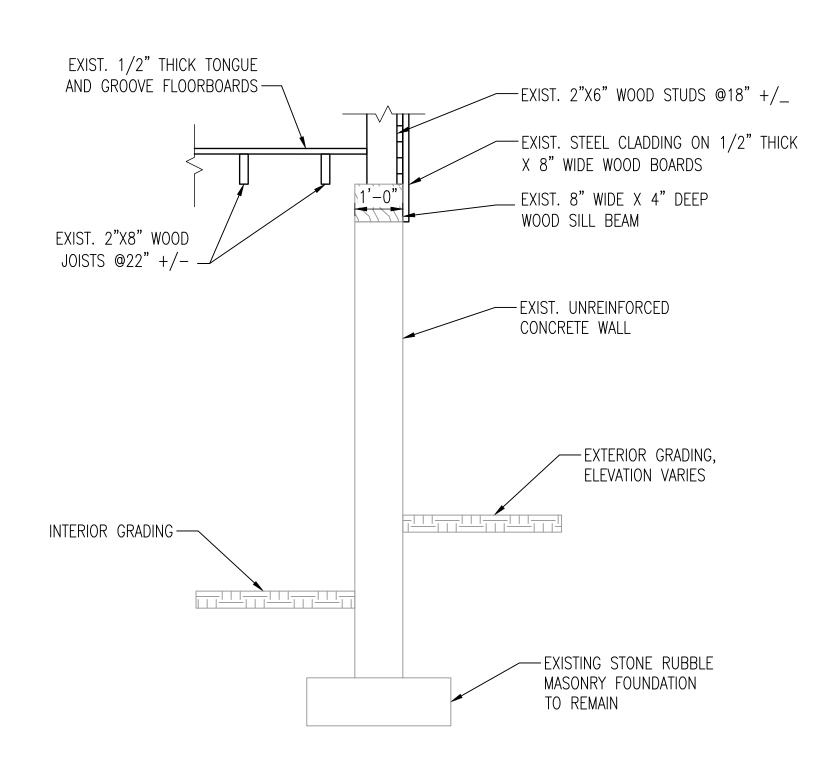
Checked by: BMB

S1-01 BASEMENT PLAN AND FIRST FLOOR PLAN HOYLE'S MILL STRUCTURAL STABILIZATION BOYDS, MARYLAND

SCALE : 1/4" = 1'-0" 24 JULY 2024 Project No. : 32207.003 SHEET _____ of 8



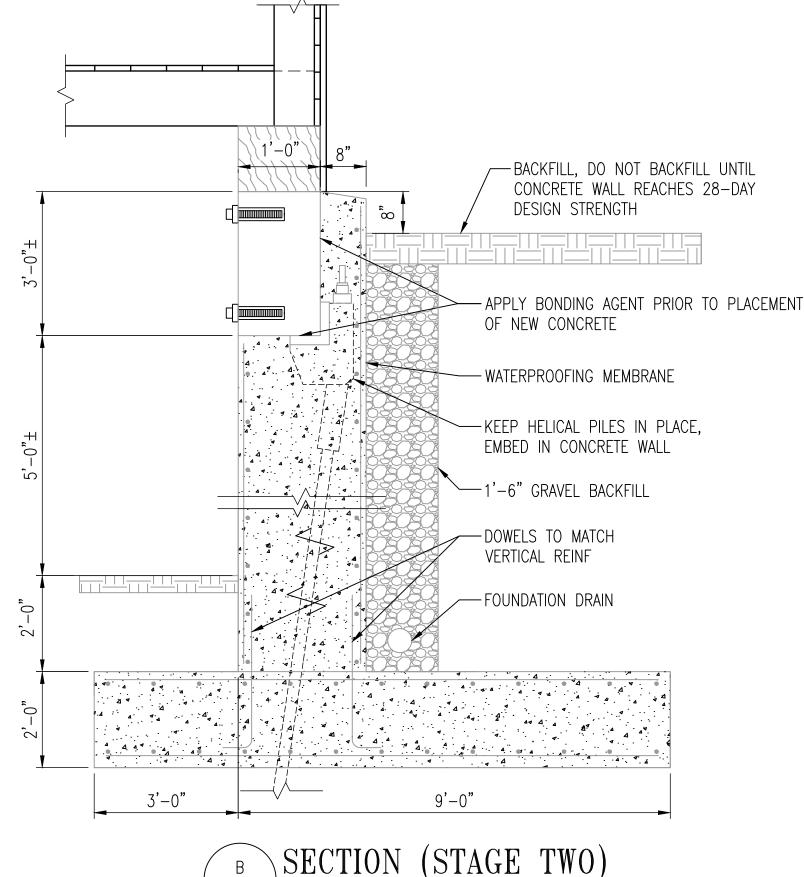
A SECTION (STAGE ONE) (S3-01) SCALE: 1/2" = 1'-0"



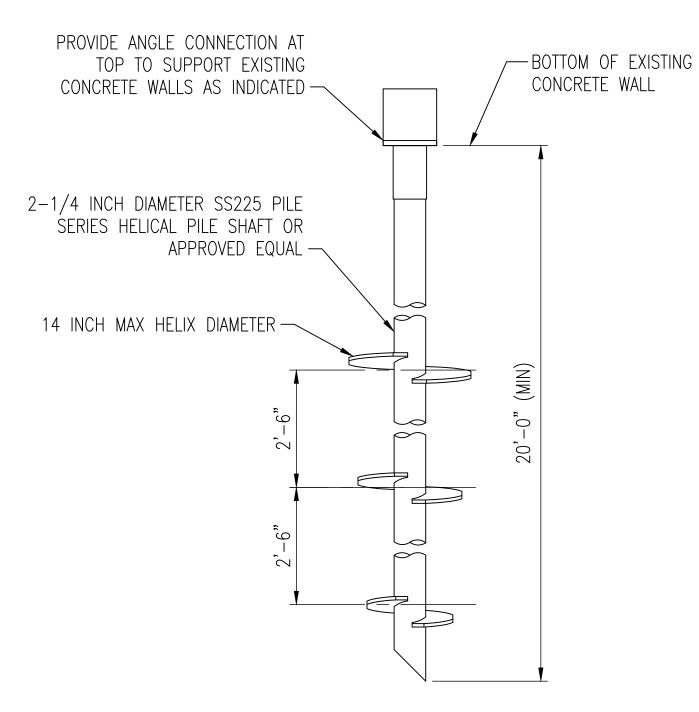
NOTE: THIS SECTION SHOWS ASSUMED EXISTING CONDITIONS. NO MODIFICATIONS TO THIS WALL ARE REQUIRED FOR STRUCTURAL STABILIZATION.

SCALE: 1/2" = 1'-0"



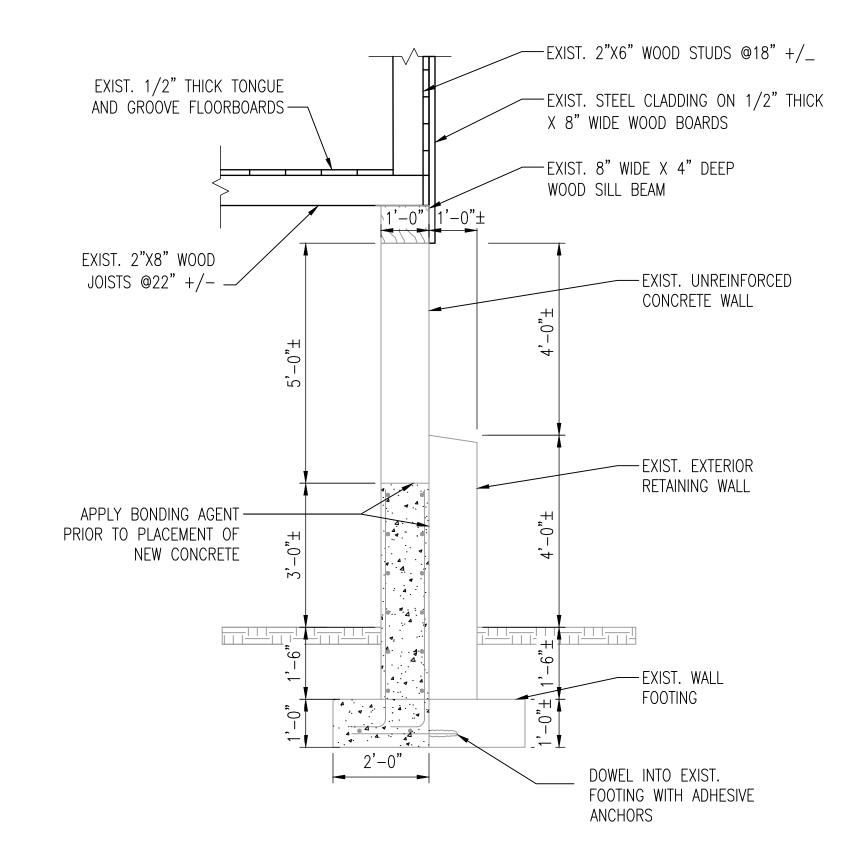


B SECTION (STAGE TWO) $\sqrt{S3-01}$ SCALE: 1/2" = 1'-0"



NOTE: HELICAL PILE PRODUCTS, INSTALLATION EQUIPMENT, TOOLING AND PROCEDURES, AND TERMINATION CRITERIA MUST BE IN ACCORDANCE WITH HELICAL PILE NOTES ON SHEET SO-01.

TYPICAL HELICAL PILE NOT TO SCALE



NOTE: PROVIDE TEMPORARY SHORING PRIOR TO COMMENCING DEMOLITION WORK NEEDED TO INSTALL NEW CONCRETE WALL AND FOUNDATION. TEMPORARY SHORING DESIGN BY CONTRACTOR.

> SECTION S3-01 SCALE: 1/2" = 1'-0"

PROFESSIONAL CERTIFICATION. I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND, LICENSE NO.__

EXPIRATION DATE:_



	NO.	REVISION	DATE	BY
_	· ·			· ·

MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION GAITHERSBURG, MARYLAND RECOMMENDED FOR APPROVAL Chief, Transportation Planning and Design Section APPROVED

Checked by: BMB

Chief, Division of Transportation Engineering

Designed by: <u>SA</u> Drawn by: <u>SA</u>

S3-01 BUILDING SECTIONS HOYLE'S MILL STRUCTURAL STABILIZATION BOYDS, MARYLAND

SCALE : 1/2" = 1'-0"24 JULY 2024 Project No. : <u>32207.003</u> SHEET <u>8</u> of 8