

MONTGOMERY COUNTY HISTORIC PRESERVATION COMMISSION
STAFF REPORT

| | | | |
|-----------------------|--|-----------------------|---------------|
| Address: | 15100 Barnesville Road, Boyds | Meeting Date: | 8/14/2024 |
| Resource: | Primary (1850-1935) Resource Boys Historic District | Report Date: | 8/7/2024 |
| Applicant: | Montgomery County Department of Transportation (Rebecca Park, Transportation Unit Manager) | Public Notice: | 7/31/2024 |
| Review: | HAWP | Tax Credit: | No |
| Permit Number: | 1038325 | Staff: | Dan Bruechert |
| Proposal: | For structural stabilization, fenestration and other alterations. | | |

STAFF RECOMMENDATION

Staff recommends that the HPC **approve with three (3) condition** the HAWP application with final approval authority delegated to staff:

1. The applicant must consult with the Staff Archaeologist prior to ground disturbance along the foundation to identify areas that warrant archaeological testing. Complete archaeological testing as warranted through excavation units along the foundation intended to document historical construction and repair episodes. All final reports must be submitted to the Historic Preservation Office.
2. The applicant must continue to work with staff to refine the language on the historic marker, including, but not limited to, the description and history of the African American community in Boyds.
3. The applicant must submit revised drawings showing the new stairs and checkwalls showing elevation, section drawings, and labeling all materials, and showing final heights. Final approval authority to verify this work is compatible with the character of the site can be delegated to Staff.

ARCHITECTURAL DESCRIPTION

SIGNIFICANCE: Primary Resource within the Boyds Historic District
STYLE: Grist Mill
DATE: 1915



Figure 1: The subject property at 15100 Barnesville Road is located mid-block on Barnesville Road, directly to the north of the Metropolitan Branch of the Baltimore & Ohio Railroad. The red outline is the boundary of the Boyds Master Plan Historic District. The adjacent buildings to the west, north, and northeast are outside of the environmental setting.

BACKGROUND

Hoyle's Mill has an extensive administrative history and its degrading condition has been well documented. The HPC last held a Preliminary Consultation for site improvements on August 16, 2023.¹

On May 22, 2024, the applicant presented a Preliminary Consultation to stabilize the foundation of the mill as part of dedicated capital improvement project funding.² The proposal presented involved pouring fillable flow concrete into the basement to a height of approximately 4' (four feet). The HPC expressed concerns about the proposal including how the concrete would interact with the existing wood posts and how it would limit future potential uses for the building. Many other questions asked by the HPC were technical in nature to allow the commissioners a fuller understanding of the proposal. Commissioners also requested the site be opened for a site visit. A site visit was on June 6, 2024 and was attended by Staff and four Commissioners.

The applicant has revised the proposal and seeks a HAWP.

PROPOSAL

The applicant proposes to stabilize the foundation and remove and replace the exterior stairs and cheekwall.

APPLICABLE GUIDELINES

The Historic Preservation Office and Historic Preservation Commission (HPC) consult several documents

¹ The August 16, 2023 Preliminary Consultation Staff Report is available here: <https://montgomeryplanning.org/wp-content/uploads/2023/08/II.B-15100-Barnesville-Road-Boyd-1038325.pdf>. The recording of the hearing is available here: https://mncppc.granicus.com/MediaPlayer.php?publish_id=b388082e-3d03-11ee-9e9a-0050569183fa.

² The Staff Report and application materials for the May 22 2024, Preliminary Consultation is available here: <https://montgomeryplanning.org/wp-content/uploads/2024/05/II.A-15100-Barnesville-Road-Boyd-Prelim.pdf>. The recording of the hearing is available here: https://mncppc.granicus.com/MediaPlayer.php?publish_id=6bab5eb2-2432-11ef-81ef-005056a89546.

when reviewing alterations and new construction within the Boyds Historic District. These documents include the *Montgomery County Code Chapter 24A (Chapter 24A)*, *Secretary of the Interior's Standards for Rehabilitation (Standards)*, and *MARC Rail Communities Sector Plan (2019)*. The pertinent information in these documents is outlined below.

Montgomery County Code, Chapter 24A-8

The following guidance which pertains to this project are as follows:

- (b) The commission shall instruct the director to issue a permit, or issue a permit subject to such conditions as are found to be necessary to ensure conformity with the purposes and requirements of this chapter, if it finds that:
 - (1) The proposal will not substantially alter the exterior features of an historic site or historic resource within an historic district; or
 - (2) The proposal is compatible in character and nature with the historical, archeological, architectural or cultural features of the historic site or the historic district in which an historic resource is located and would not be detrimental thereto or to the achievement of the purposes of this chapter;
 - (3) The proposal would enhance or aid in the protection, preservation and public or private utilization of the historic site or historic resource located within an historic district in a manner compatible with the historical, archeological, architectural or cultural value of the historic site or historic district in which an historic resource is located; or
 - (4) The proposal is necessary in order that unsafe conditions or health hazards be remedied; or
 - (5) The proposal is necessary in order that the owner of the subject property not be deprived of reasonable use of the property or suffer undue hardship; or
 - (6) In balancing the interests of the public in preserving the historic site or historic resource located within an historic district, with the interests of the public from the use and benefit of the alternative proposal, the general public welfare is better served by granting the permit.
- (c) It is not the intent of this chapter to limit new construction, alteration or repairs to any 1 period or architectural style.
- (d) In the case of an application for work on an historic resource located within an historic district, the commission shall be lenient in its judgment of plans for structures of little historical or design significance or for plans involving new construction, unless such plans would seriously impair the

Secretary of the Interior's Standards for Rehabilitation

The Secretary of the Interior defines rehabilitation as “the act or process of making possible a compatible use for a property through repair, alterations, and additions while preserving those portions or features, which convey its historical, cultural, or architectural values.” The applicable *Standards* are as follows:

- 2. The historic character of a property will be retained and preserved. The removal of distinctive materials or alteration of features, spaces and spatial relationships that characterize a property will be avoided.

7. Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.
8. Archeological resources will be protected and preserved in place. If such resources must be disturbed, mitigation measures will be undertaken.
9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.
10. New additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

MARC Rail Communities Sector Plan

The *MARC Rail Communities Sector Plan* was approved on April 30, 2019 by the Montgomery County Council. The Plan looks to the future by offering recommendations that support and better utilize existing transit assets, improve transportation connections and traffic safety in both station areas. In addition, the Plan proposes ways to revitalize the built environment around the Boyds and Germantown stations in a manner that appropriately complements surrounding residential neighborhoods and historic resources. The plan lists the following as one of its priorities, “Create a commuter and community-serving facility at the historic Hoyle’s Mill site, integrating the mill as part of the facility.”³

The *MARC Rail Communities Sector Plan* includes the following specific references to the Hoyle’s Mill site:

- Establish regular Ride On bus service to the Boyds MARC Station, including appropriate bus-related facilities at the station, to accommodate additional MARC riders (Page 38-39).
 - Provide a shelter for commuters and allow public access to the historic structures on the property by adapting the Hoyle’s Mill structure and any other retained buildings.
- Promote compatible infill development around the Boyds MARC Station, while protecting existing residential uses, historically significant structures and natural areas (Page 69-70).
 - Design the enhanced MARC station area, including the Anderson properties, to be sensitive to the historic Hoyle’s Mill and other structures.
 - Prior to any changes, additions and/or removals, assess all the structures on the site for National Register of Historic Places significance. This site falls within the National Register Historic District.
 - Review all potential changes to the mill with the County’s Historic Preservation Commission.
- Encourage the compatible reuse of structures that lend historic character to the Boyds and Germantown communities, and maintain the rail communities’ historic integrity while revitalizing and enhancing MARC station areas (Page 93).

³ Montgomery County, *MARC Rail Communities Sector Plan* (2019), 16.

- Adaptively reuse the historic Hoyle's Mill within the Boyds Historic District and explore reuse of the nearby barn building outside of the historic district to establish a focal point for the Boyds commercial area.
- Commemorate historical uses on the parcel, including, among others, the location of the former station house, which is no longer standing.
- Redesign the site to support the reuse of the mill building. Coordinate all changes to the mill with the Montgomery County Historic Preservation Commission
- Design alterations to the repurposed mill and any other buildings—including landscaping, lighting and walkways—in a manner that is sensitive to the surrounding historic district. Design landscaping, lighting, signage, paths and structures that support and are sensitive to the character of the historic district.

STAFF DISCUSSION

Hoyle's Mill is a Primary Resource in the Boyds Master Plan Historic District. A recent Historic American Engineering Record (HAER) survey of the building states the following:

Located alongside the Baltimore and Ohio Railroad's Metropolitan Branch, Hoyle's Mill is the oldest industrial building in the rural railroad community of Boyds. It is representative of the mature milling industry that flourished in Montgomery County when the region was a major agricultural center. With its original structure, sheathing, and fenestration intact, Hoyle's Mill retains a high level of integrity as an example of an early twentieth century family-owned mill that was later adapted for use as a storage building for farming and automotive supply businesses.

Hoyle's Mill has two stories and a basement and is 26' wide x 48' long. It measures 20'-0" vertically from the top of the basement to the roof eave; and 10'-4" vertically between the first and second floor windowsills. It is sheathed with galvanized, pressed-metal siding in a pitch-faced stone pattern, dating to the original construction of the mill. The siding panels measure 5' wide x 2'-4" tall, representing five courses of 7" x 12" blocks. The texture matches a pattern produced by W. F. Norman Corporation, which has produced galvanized steel and copper ceilings and siding since 1898.

Both the front north and rear south elevations have a door and three double-hung windows across the first floor, and four windows across the second floor. The windows are six over six and have a frame opening of 2'-4" x 4'-0". Both the front and rear doors are wood, sized 3'-6" x 6'-8". The door on the rear, trackside wall and existence of extra nail holes in the siding on the course below the door sill both suggest that there may have once been a loading dock serving railroad deliveries. The basement has three window openings on the rear wall measuring 2'-4" x 2'-8", where the concrete foundation wall flares out an additional 13" on a 3/12 pitch. Asphalt shingles cover the roof..⁴

⁴ For more information, <https://tile.loc.gov/storage-services/master/png/habshaer/md/md2200/md2207/data/md2207data.pdf>.

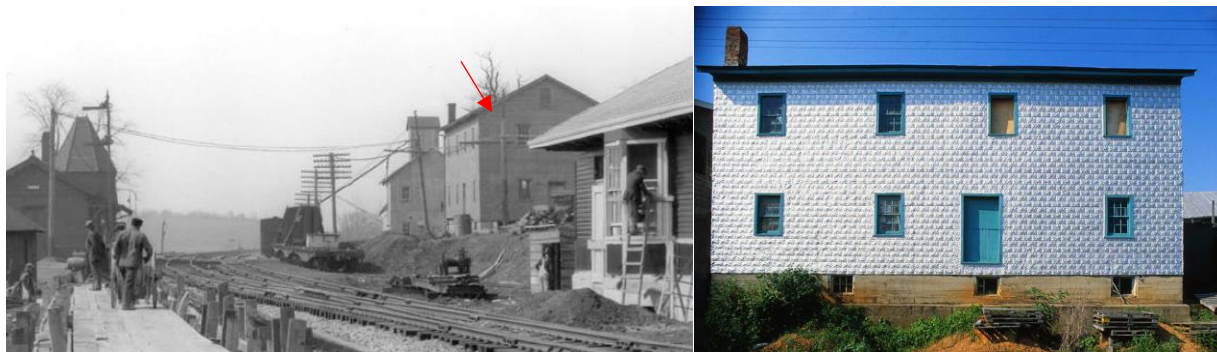


Figure 3: View of the Metropolitan Branch of the Baltimore & Ohio railroad at the newly constructed Boyds station, 1928 (left), and the rear (track facing) elevation, 1984. The red arrow points to Hoyle's Mill. Source: John R. King (left) and John S. Collier (right).

Foundation Stabilization

The primary focus of the work proposed at this stage of site redevelopment is the stabilization of the foundation. The existing foundation has deteriorated with most of the damage occurring to the north and east foundation walls. Stabilizing the foundation of the mill must happen before any other work on the site can be carried out due to its delicate condition. The existing exterior concrete stairs have also settled and cracked and have deteriorated to the point they are no longer structurally stable.

The proposal discussed at the May 2024 Preliminary Consultation involved filling the basement space with a flowable fill concrete. The applicant has significantly revised the proposal and now proposes to support the existing north and east foundation walls with helical piles before installing new reinforced concrete foundation walls and footings. New stairs and cheek walls will be installed on the east side of the mill to replace the existing, significantly deteriorated stairs, in the same location. Section drawings of the stairs and cheekwall were not included with the submitted application materials. The window openings will remain boarded up until they can be repaired as part of a future adaptive reuse project. Aside from the new stairs, all of the proposed work is below grade and will not result in a visual change to the building.

The applicant provided a proposed sequence of work, which is as follows:

1. De-energize and remove power lines.
2. Remove the metal shed located at the east wall of the mill building.
3. Remove and discard the steel stair platform from the west side of the building.
4. Perform exploratory archaeology (shovel test pits) of the building's exterior perimeter prior to excavation.
5. Install helical piles at north and east walls to stabilize existing foundations.
6. Install temporary shoring of first floor framing within crawl space.
7. Install continuous steel repair plates with adhesive anchors across cracks in existing concrete walls.
8. Remove loose trash/debris from inside building. Salvage historic artifacts such as mill equipment on upper floors. Retain existing mill equipment in lower level as is.
9. Cap and abandon existing well.
10. Demolish existing stone masonry footings at north and east foundation walls.
11. Demolish existing concrete mass at north side of crawl space.
12. Demolish and replace existing stair and cheek walls leading down into crawl space.
13. Excavate, install formwork, and place concrete for new reinforced concrete foundation walls and footings at north and east walls.
14. Demolish existing stone masonry foundation at south foundation wall.
15. Excavate, install formwork, and place concrete for new reinforced concrete foundation wall and footing at south wall.
16. Backfill excavations around north and east walls.

17. Repair holes in floors with plywood.
18. Remove and replace deteriorated basement doors and frame with new steel door and frame.
19. Remove any insect and bird nests.

Staff finds the revised proposal to be preferable to the proposal presented at the Preliminary Consultation. First, the majority of the work will be conducted on the exterior of the mill, below grade, leaving the crawlspace/basement accessible for its future use. At the Preliminary Consultation, the HPC indicated disappointment that filling the basement with concrete would eliminate the possibility of utilizing that space in the future. Second, this work could conceivably be reversed at some time in the future if necessary; whereas the concrete fill proposed in the Prelim was an irreversible treatment. Third, the proposed work will not interact with the wood posts, so there are no concerns about how the concrete will impact the long-term preservation of the posts. Because section drawings for the stairs and checkwall were not submitted with the application material, Staff recommends the HPC include a condition requiring the submission of those drawings before Staff releases the final approval documents. Final approval authority to verify this work is compatible with the character of the site can be delegated to Staff. Staff find this solution is appropriate and recommends the HPC approve the foundation stabilization and stair replacement under 24A-8(b)(1), (2), (3), (4), and (6); Standards 2 and 7; and the objectives of the *MARC Rail Communities Sector Plan*.

The Staff Archaeologists reviewed the proposal to perform exploratory archaeology and found the description of the proposed work identified in the proposal required additional specificity and recommended the HPC include a condition to the HAWP that:

- The applicant continues to consult with the Staff Archaeologist prior to ground disturbance along the foundation to identify areas that warrant archaeological testing and
- Complete archaeological testing as warranted through excavation units along the foundation intended to document historic construction and repair episodes.

With the recommended condition, Staff finds the proposed archaeology is consistent with Standard 8.

Staff notes the applicant also proposes to install a historic plaque on site after the foundation has been stabilized. The proposed language states:

BOYDS, MARYLAND, AND HOYLE'S MILL
 THE TOWN OF BOYDS ORIGINATED AS A CAMP FOR WORKERS BUILDING THE B&O
 RAILROAD METROPOLITAN BRANCH IN 1866-1873 UNDER RAILROAD CONTRACTOR
 JAMES A. BOYD. THE NEW RAILROAD STATION WAS NAMED FOR BOYD, WHO HAD
 SETTLED NEARBY ON A LARGE FARM. JUST DOWN THE ROAD WAS A COMMUNITY
 FOUNDED BY BLACK FAMILIES WHO HAD BEEN ENSLAVED ON FARMS IN THE AREA
 UNTIL 1864. THE HOYLE FAMILY HAD A WATER-POWERED GRAIN AND SAW MILL ON
 LITTLE SENECA CREEK FROM THE EARLY 1800S TO 1903. IN 1915, SMITH HOYLE OPENED
 A NEW GASOLINE-POWERED FLOUR MILL ACROSS FROM BOYD'S STATION, WHICH HE
 OPERATED UNTIL 1940.
 MARYLAND HISTORICAL TRUST
 MARYLAND TRANSIT ADMINISTRATION

One of the Staff historians reviewed the text of the proposed marker and recommended additional research related to the nearby postbellum community established by formerly enslaved families. Staff recommends the HPC add a condition to the approval of this HAWP that requires the applicant to continue to work with HPC Staff to refine the language of the marker including, but not limited to, the description and history of the African American community in Boyds.



Figure 4: View of the facade of Hoyle's Mill, 2023.

STAFF RECOMMENDATION

Staff recommends that the Commission **approve with three (3) condition** the HAWP application with final approval authority delegated to staff:

1. The applicant must consult with the Staff Archaeologist prior to ground disturbance along the foundation to identify areas that warrant archaeological testing. Complete archaeological testing as warranted through excavation units along the foundation intended to document historical construction and repair episodes. All final reports must be submitted to the Historic Preservation Office.
2. The applicant must continue to work with staff to refine the language on the historic marker, including, but not limited to, the description and history of the African American community in Boyds.
3. The applicant must submit revised drawings showing the new stairs and checkwalls showing elevation, section drawings, and labeling all materials, and showing final heights. Final approval authority to verify this work is compatible with the character of the site can be delegated to Staff.

under the Criteria for Issuance in Chapter 24A-8(b)(1), (2), (3), (4), and (6), having found that the proposal will not substantially alter the exterior features of the historic resource and is compatible in character with the district and the purposes of Chapter 24A;

and with the *Secretary of the Interior's Standards for Rehabilitation* #2, #7, and #8;

and with the objectives of the *MARC Rail Communities Sector Plan*;

and with the general condition that the applicant shall present an electronic set of drawings, if applicable, to Historic Preservation Commission (HPC) staff for review and stamping prior to submission for the Montgomery County Department of Permitting Services (DPS) building permits;

and with the general condition that final project design details, not specifically delineated by the Commission, shall be approved by HPC staff or brought back to the Commission as a revised HAWP application at staff's discretion;

and with the general condition that the applicant shall notify the Historic Preservation Staff if they propose to make **any alterations** to the approved plans. Once the work is completed the applicant will **contact the staff person** assigned to this application at 301-563-3400 or dan.bruechert@montgomeryplanning.org to schedule a follow-up site visit.



APPLICATION FOR HISTORIC AREA WORK PERMIT
HISTORIC PRESERVATION COMMISSION
301.563.3400

FOR STAFF ONLY:
HAWP#
DATE ASSIGNED

APPLICANT:

Name:
Address:
Daytime Phone:
E-mail:
City:
Zip:
Tax Account No.:

AGENT/CONTACT (if applicable):

Name:
Address:
Daytime Phone:
E-mail:
City:
Zip:
Contractor Registration No.:

LOCATION OF BUILDING/PREMISE: MIHP # of Historic Property

Is the Property Located within an Historic District? Yes/District Name
No/Individual Site Name

Is there an Historic Preservation/Land Trust/Environmental Easement on the Property? If YES, include a map of the easement, and documentation from the Easement Holder supporting this application.

Are other Planning and/or Hearing Examiner Approvals /Reviews Required as part of this Application? (Conditional Use, Variance, Record Plat, etc.?) If YES, include information on these reviews as supplemental information.

Building Number: Street:

Town/City: Nearest Cross Street:

Lot: Block: Subdivision: Parcel:

TYPE OF WORK PROPOSED: See the checklist on Page 4 to verify that all supporting items for proposed work are submitted with this application. Incomplete Applications will not be accepted for review. Check all that apply:

- New Construction
Addition
Demolition
Grading/Excavation
Deck/Porch
Fence
Hardscape/Landscape
Roof
Shed/Garage/Accessory Structure
Solar
Tree removal/planting
Window/Door
Other:

I hereby certify that I have the authority to make the foregoing application, that the application is correct and accurate and that the construction will comply with plans reviewed and approved by all necessary agencies and hereby acknowledge and accept this to be a condition for the issuance of this permit.

Signature of owner or authorized agent

Date

HAWP APPLICATION: MAILING ADDRESSES FOR NOTIFYING
[Owner, Owner's Agent, Adjacent and Confronting Property Owners]

| | |
|---|--|
| Owner's mailing address | Owner's Agent's mailing address |
| Adjacent and confronting Property Owners mailing addresses | |
| | |
| | |
| | |

Description of Property: Please describe the building and surrounding environment. Include information on significant structures, landscape features, or other significant features of the property:

Description of Work Proposed: Please give an overview of the work to be undertaken:

| | |
|-----------------------------------|----------------|
| Work Item 1: _____ | |
| Description of Current Condition: | Proposed Work: |

| | |
|-----------------------------------|----------------|
| Work Item 2: _____ | |
| Description of Current Condition: | Proposed Work: |

| | |
|-----------------------------------|----------------|
| Work Item 3: _____ | |
| Description of Current Condition: | Proposed Work: |

**HISTORIC AREA WORK PERMIT
CHECKLIST OF
APPLICATION REQUIREMENTS**

| | Required Attachments | | | | | | |
|--------------------------------|------------------------|--------------|---------------------|----------------------------|----------------|----------------|-----------------------------|
| Proposed Work | I. Written Description | 2. Site Plan | 3. Plans/Elevations | 4. Material Specifications | 5. Photographs | 6. Tree Survey | 7. Property Owner Addresses |
| New Construction | * | * | * | * | * | * | * |
| Additions/Alterations | * | * | * | * | * | * | * |
| Demolition | * | * | * | | * | | * |
| Deck/Porch | * | * | * | * | * | * | * |
| Fence/Wall | * | * | * | * | * | * | * |
| Driveway/Parking Area | * | * | | * | * | * | * |
| Grading/Excavation/Landscaping | * | * | | * | * | * | * |
| Tree Removal | * | * | | * | * | * | * |
| Siding/ Roof Changes | * | * | * | * | * | | * |
| Window/ Door Changes | * | * | * | * | * | | * |
| Masonry Repair/ Repoint | * | * | * | * | * | | * |
| Signs | * | * | * | * | * | | * |

MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION

HOYLE'S MILL STRUCTURAL STABILIZATION – 30% DESIGN RESUBMISSION

C. I. P. PROJECT 501915 SHA TRACKING NO. 22-AP-MO-022-XX

IT IS THE RESPONSIBILITY OF PERMITTEE/OWNER OF THIS SITE TO OBTAIN ALL REQUIRED PERMITS PRIOR TO ISSUANCE OF APPROVED SEDIMENT CONTROL PERMIT.

| TYPE OF PERMIT | REQD | NOT REQD | PERMIT NO. | EXPIRATION DATE | WORK RESTRICTION DATES |
|--|------|----------|-----------------|-----------------|------------------------|
| M.C.D.E.P. Floodplain District | x | | | | |
| WATERWAY/WETLANDS | | | | | |
| a. Corps of Engineers | | x | | | |
| b. M.D.E. | | x | | | |
| c. M.D.E. Water Quality Certification | | x | | | |
| M.D.E. Dam Safety | | x | | | |
| DPS Roadside Tree Protection Plan | x | | TBD | TBD | |
| N.P.D.E.S. NOTICE OF INTENT | x | | TBD | TBD | |
| M.C.D.P.S. STORMWATER MANAGEMENT | x | | 285472 | N/A | |
| M.C.D.P.S. SEDIMENT CONTROL | x | | 288386 | TBD | |
| FEMA LOMR (REQUIRED POST CONSTRUCTION) | | x | | | |
| D.P.S. BUILDING PERMIT | x | | TBD | TBD | |
| M.C.P.D.S. SEPTIC SYSTEM PERMIT | x | | TBD | TBD | |
| OTHERS: (PLEASE LIST) | | | | | |
| SHA ACCESS PERMIT | x | | 22-AP-MO-022-XX | | |

GENERAL NOTES

1. TRANSIT IMPROVEMENTS PROJECT, SITE CLEANUP AND GROUNDWATER REMEDIATION SHALL BE DONE UNDER A SEPARATE CONTRACT.
2. THE SPECIFICATIONS FOR THIS CONTRACT WILL BE THOSE OF THE LATEST EDITION OF THE MARYLAND DEPARTMENT OF TRANSPORTATION STATE HIGHWAY ADMINISTRATION, THE MARYLAND DEPARTMENT OF TRANSPORTATION STATE HIGHWAY ADMINISTRATION BOOK OF STANDARDS FOR HIGHWAY AND INCIDENTAL STRUCTURES, THE MARYLAND DEPARTMENT OF TRANSPORTATION STATE HIGHWAY ADMINISTRATION 2023 STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS, THE MARYLAND WASHINGTON SUBURBAN SANITARY COMMISSION (W.S.S.C.) STANDARDS, MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION STANDARDS, AND SOIL CONSERVATION SERVICE POND CONSTRUCTION SPECIFICATIONS FOR MARYLAND.
3. HORIZONTAL DATUM: NAD 83(1991) VERTICAL DATUM: NAVD 88.
4. INFORMATION CONCERNING UNDERGROUND UTILITIES WAS OBTAINED FROM AVAILABLE RECORDS, BUT THE CONTRACTOR MUST DETERMINE THE EXACT LOCATIONS AND ELEVATIONS OF THE LINES BY DIGGING TEST PITS BY HAND AT ALL UTILITY CROSSINGS, WELL IN ADVANCE OF TRENCHING. IF CLEARANCES ARE LESS THAN SHOWN OR SIX (6) INCHES, WHICHEVER IS LESS, CONTACT MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION'S PROJECT INSPECTOR AND THE APPROPRIATE UTILITY OWNER BEFORE PROCEEDING WITH CONSTRUCTION.
5. REPAIRS TO UTILITIES OR PROPERTY DAMAGE AS A RESULT OF THE CONTRACTOR'S NEGLIGENCE OR METHOD OF OPERATION MUST BE MADE AT THE CONTRACTOR'S EXPENSE AT NO ADDITIONAL COST TO THE COUNTY BEFORE PROCEEDING WITH CONSTRUCTION.
6. DISTURBED AREAS ADJACENT TO ESTABLISHED LAWNS SHALL BE SODDED. OTHER DISTURBED AREAS SHALL BE SEEDED AND MULCHED.
7. THE CONTRACTOR SHALL OBTAIN A ROADSIDE TREE PERMIT FOR ANY MAINTENANCE, TREATMENT, PLANTING, REMOVAL, OR ROOT CUTTING ON TREES WITHIN THE PUBLIC RIGHT OF WAY. PERMIT REQUIREMENTS MAY BE OBTAINED FROM THE DEPARTMENT OF NATURAL RESOURCES, MARYLAND FOREST, PARK AND WILDLIFE SERVICE, TELEPHONE 301-854-6060.
8. CONTACT THE WASHINGTON SUBURBAN SANITARY COMMISSION SYSTEM MAINTENANCE ENGINEER BEFORE EXCAVATING BENEATH OR IN THE VICINITY OF EXISTING WATER OR SEWER LINES. BACKFILL TO BE DONE UNDER SUPERVISION OF WSSC MAINTENANCE ENGINEER, CALL 301-206-9772.
9. CALL "MISS UTILITY" AT 1-800-257-7777 48 HOURS PRIOR TO THE START OF WORK. THE EXCAVATOR MUST NOTIFY ALL PUBLIC UTILITY COMPANIES WITH UNDERGROUND FACILITIES IN THE AREA OF PROPOSED EXCAVATION AND HAVE THOSE FACILITIES LOCATED BY UTILITY COMPANIES PRIOR TO COMMENCING EXCAVATION. THE EXCAVATOR IS RESPONSIBLE FOR COMPLIANCE WITH REQUIREMENTS OF CHAPTER 36A OF THE MONTGOMERY COUNTY CODE.

DEVELOPER'S/BUILDER'S CERTIFICATION

I HEREBY CERTIFY THAT ALL CLEARING, GRADING, CONSTRUCTION AND/OR DEVELOPMENT WILL BE DONE PURSUANT TO THIS PLAN AND THAT ANY RESPONSIBLE PERSONNEL INVOLVED IN THE CONSTRUCTION PROJECT WILL HAVE A CERTIFICATE OF ATTENDANCE OF A DEPARTMENT OF NATURAL RESOURCES APPROVED TRAINING PROGRAM FOR THE CONTROL OF SEDIMENT AND EROSION BEFORE BEGINNING THE PROJECT.

DATE _____

TIMOTHY H. CUPPLES, P.E., CHIEF
DIVISION OF TRANSPORTATION ENGINEERING

DESIGN CERTIFICATION

I HEREBY CERTIFY THAT THIS PLAN HAS BEEN PREPARED IN ACCORDANCE WITH THE "2011 MARYLAND STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL", MONTGOMERY COUNTY DEPARTMENT OF PERMITTING SERVICES EXECUTIVE REGULATIONS 5-90, 7-02AM AND 36-90, AND MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS AND TRANSPORTATION "STORM DRAIN DESIGN CRITERIA" DATED JUNE, 2014.

DATE _____

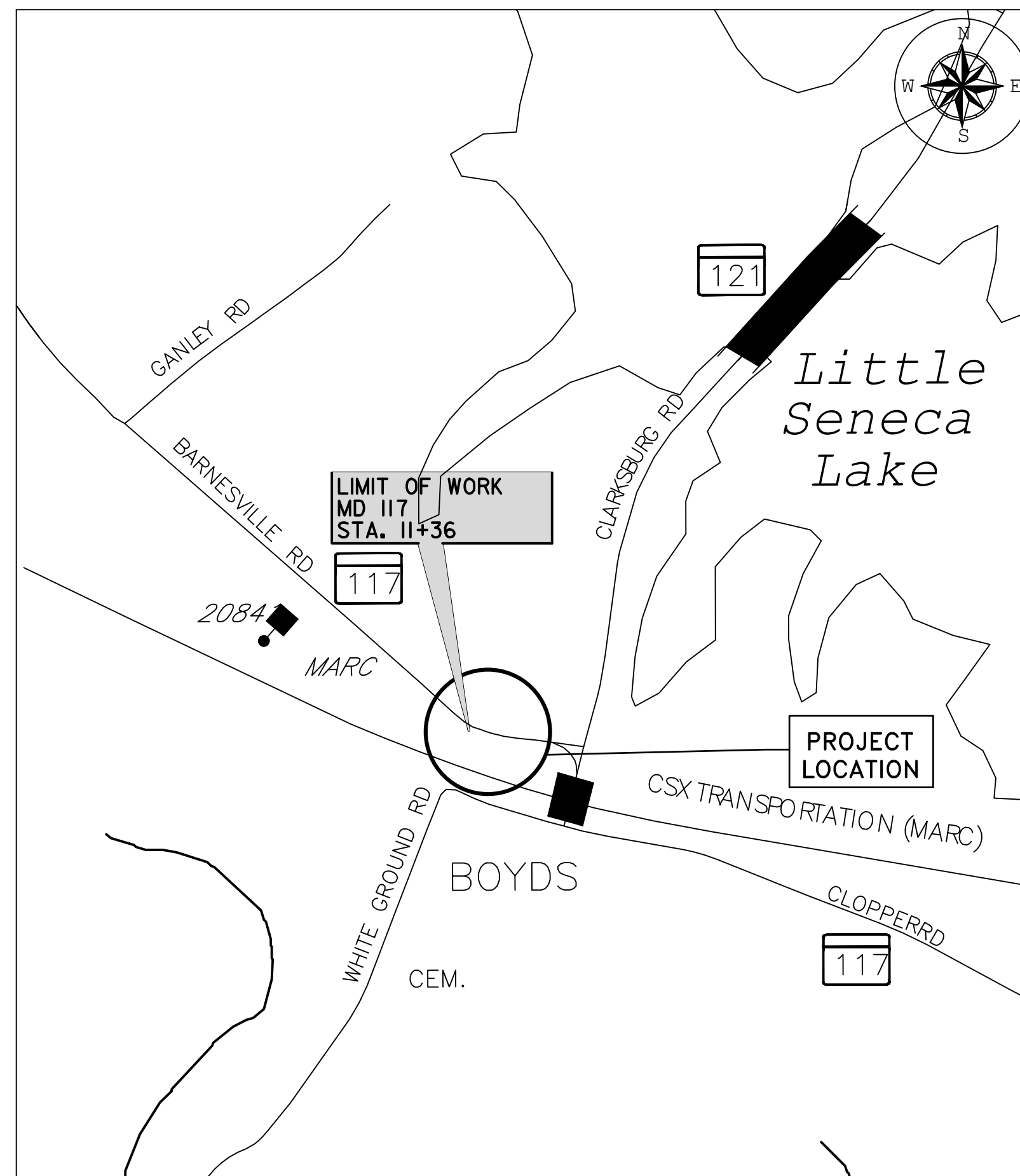
JASON D. COSLER, P.E.
MD REGISTRATION NO. 28467

CERTIFICATION OF QUANTITIES

I FURTHER CERTIFY THAT THE TOTAL AMOUNTS OF EXCAVATION AND FILL AS SHOWN ON THESE PLANS HAVE BEEN COMPUTED TO BE 4,500 CUBIC YARDS OF EXCAVATION AND 1,700 CUBIC YARDS OF FILL AND THAT THE TOTAL AREA TO BE DISTURBED AS SHOWN ON THESE PLANS HAS BEEN DETERMINED TO BE A MAXIMUM OF 55,965 SQUARE FEET OR 1.28 ACRES.

PAMELA H. DESTINO, P.E.
MD REGISTRATION NO. 42708

DATE _____



PROJECT LENGTH = 0.08 MILES MONTGOMERY COUNTY

VICINITY MAP
SCALE : 1" = 500'

| MD 117 (BARNESVILLE ROAD) DESIGN DATA | |
|---------------------------------------|-----------|
| DESIGN SPEED: | 40 M.P.H. |
| 2022 AADT: | 5,193 |

OWNER/ADDRESS:
MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
100 EDISON PARK DRIVE, 4TH FLOOR
GAITHERSBURG, MD 20878

CONTACT:
REBECCA PARK, P.E.
240-777-7263

PROFESSIONAL CERTIFICATION.
I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND, LICENSE NO. _____ EXPIRATION DATE: _____



| NO. | REVISION | DATE | BY |
|-----|----------|------|----|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

**MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
GAITHERSBURG, MARYLAND**

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section _____ Date _____

APPROVED

Chief, Division of Transportation Engineering _____ Date _____

Designed by: LH Drawn by: FIE Checked by: FAH

**TI-01 TITLE SHEET
HOYLE'S MILL
STRUCTURAL STABILIZATION
BOYDS, MARYLAND**

SCALE : NTS 24 JULY 2024

Project No. : 32207.003 SHEET 1 of 8

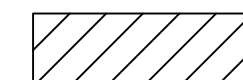
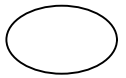




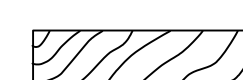
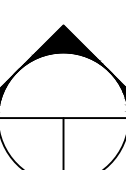
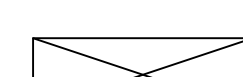
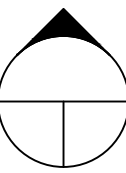
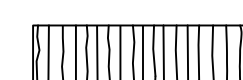
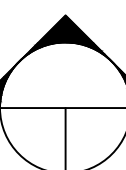


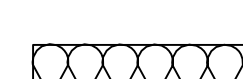
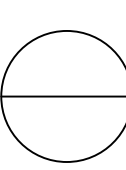
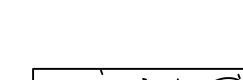
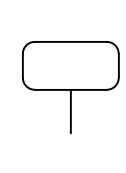

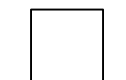

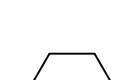


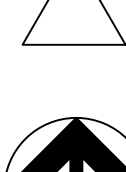
MCDPS-SC/SWM SHEET NO. 1 OF 8

288386

285472

ESD TO THE MEP - 3.397 OF, ON & Q, WAWR N/A
1-RENEWABLE PAYMENT
1-PROPRIETARY DEVICE

LEGEND

| | | | |
|---|---------------------------------|---|-----------------------------------|
|  | BRICK |  | DOOR NUMBER SYMBOL |
|  | CONCRETE MASONRY UNITS |  | WINDOW NUMBER SYMBOL |
|  | GYPSUM BOARD PARTITIONS |  | ROOM NUMBER SYMBOL |
|  | WOOD-FINISH GRADE |  | BUILDING SECTION SYMBOL |
|  | WOOD BLOCKING |  | WALL SECTION/ELEVATION SYMBOL |
|  | RIGID WALL/PERIMETER INSULATION |  | TITLE AND DETAIL REFERENCE SYMBOL |
|  | RIGID ROOF INSULATION |  | WALL/BUILDING SECTION SYMBOL |
|  | BATT INSULATION |  | TITLE AND DETAIL REFERENCE SYMBOL |
|  | CONCRETE |  | PARTITION TYPES |
|  | POROUS FILL |  | NUMBER-CONSTRUCTION NOTE |
|  | EARTH |  | NUMBER-DEMOLITION NOTE |
|  | METAL PATTERN |  | REVISION |
| | |  | NORTH ARROW (CONSTRUCTION NORTH) |

DRAWING INDEX

| SHEET NAME | SHEET NUMBER | DRAWING TITLE |
|------------|--------------|--|
| T1-01 | 1 | TITLE SHEET |
| G1-01 | 2 | INDEX, LEGEND AND ABBREVIATIONS |
| EC-01 | 3 | EXISTING CONDITIONS |
| EC-02 | 4 | DEMO PLAN |
| A1-01 | 5 | BASEMENT PLAN AND WINDOW DETAILS |
| S0-01 | 6 | STRUCTURAL GENERAL NOTES AND ABBREVIATIONS |
| S1-01 | 7 | BASEMENT PLAN AND FIRST FLOOR PLAN |
| S3-01 | 8 | BUILDING SECTIONS |

ABBREVIATIONS

| | | | | | |
|--------|--|--------|---|--------|---|
| ABV | ABOVE | F | FILLER | N | NORTH |
| AD | ACCESS DOOR (OR PANEL) | FC | FAN COIL UNIT | NA | NOT APPLICABLE |
| ADA | AMERICAN WITH DISABILITIES ACT | FD | FLOOR DRAIN OR FIRE DAMPER | NIC | NOT IN CONTRACT |
| ADD | ADDENDUM | FE | FIRE EXTINGUISHER ON BRACKET | NO | NUMBER |
| ADJ | ADJACENT | FEC | FIRE EXTINGUISHER CABINET | NOM | NOMINAL |
| AES | ABOVE EXISTING SLAB | FH | FLAT HEAD | NTS | NOT TO SCALE |
| AF | ACCESS FLOOR | FIRE T | FIRE TREATED | OA | OVERALL |
| AFF | ABOVE FINISHED FLOOR | FIN | FINISH OR FINISHED | OC | ON CENTER |
| AHU | AIR HANDLING UNIT | FIX | FIXTURE | OHD | OVERHEAD COILING DOOR |
| ALT | ALTERNATE | FL | FLASHING | OHG | OVERHEAD COILING GRILLE |
| ALUM | ALUMINUM | FLR | FLOOR | OPNG | OPENING |
| APPROX | APPROXIMATE | FR | FIRE RATED | OPP | OPPOSITE |
| ARCH | ARCHITECTURAL | FRC | FIBER-REINFORCED COATING | OZ | OUNCE |
| ATC | ACOUSTICAL TILE CEILING (CONCEALED SUSPENSION) | FT | FOOT OR FEET | PAV | PAVER TILE |
| AWP | ACOUSTICAL WALL PANEL | FTG | FOOTING | PC | PIECE |
| | | | | PF | PLASTIC FABRICATION |
| BD | BOARD | CA | GAUGE | PL | PLATE |
| BEN | BENCH | GALV | GALVANIZED | PLAM | PLASTIC LAMINATE |
| BETW | BETWEEN | GEN | GENERAL | PLAS | PLASTER |
| BLDG | BUILDING | GRD | GROUND | PREFAB | PREFABRICATED |
| BLKG | BLOCKING | GRT | GROUT | PRES T | PRESSURE TREATED |
| BM | BEAM | GVP | GYPSUM VENEER PLASTER | PT | PAINT |
| BOT | BOTTOM | GYPB | GYPSUM BOARD (WALL OR CEILING) | PTN | PARTITION |
| BR | BRICK | GYPBS | GYPSUM BOARD SHAFT-WALL ASSEMBLY | PVC | POLYVINYL CHLORIDE |
| | | | | | |
| C/C | CENTER TO CENTER | H | HEAD | QTY | QUANTITY |
| CAB | CABINET | HB | HORIZONTAL BLIND | R | RISER OR RADIUS |
| CEM | CEMENT | HDW | HARDWARE | RCP | REINFORCED CONCRETE PIPE |
| CER | CERAMIC | HM | HOLLOW METAL | RD | ROOF DRAIN OR ROUND |
| CI | CAST IRON | HOR | HORIZONTAL | REQ'D | REQUIRED |
| CG | CORNER GUARD | HP | HIGH POINT | REBAR | REINFORCING BAR |
| CJ | CONTROL JOINT | HR | HOUR | REINF | REINFORCED OR REINFORCING |
| CL | CENTERLINE | HT | HEIGHT | RESF | RESINOUS FLOORING |
| CLOS | CLOSET | HTR | HEATER | REQ | REQUIRED |
| CLG | CEILING | HVAC | HEATING, VENTILATING AND AIR CONDITIONING | RET | RETURN |
| CLR | CLEAR | HW | HOT WATER | REV | REVISION |
| CMU | CONCRETE MASONRY UNIT | | | RH | ROBE HOOK |
| CO | CLEAR OPENING | IN | INCH | RM | ROOM |
| COL | COLUMN | INSUL | INSULATION | RO | ROUGH OPENING |
| COMP | COMPACTED | INT | INTERIOR | RWR | RECESSED WASTE RECEPTACLE |
| CONC | CONCRETE | | | RV | ROOF VENT |
| CONSTR | CONSTRUCTION | | | RX | REMOVE EXISTING |
| CONT | CONTINUOUS | JT | JOINT | S | SILL, SOUTH OR SINGLE |
| CONV | CONVECTOR | L | LINTEL | SCH | SCHEDULE OR SCHEDULED |
| CR | COLD ROLLED | LG | LONG | SD | SOAP DISPENSER OR STORM DRAIN |
| CX | CONNECT TO EXISTING | LIN | LINOLEUM FLOOR COVERING | SECT | SECTION |
| | | LLV | LONG LEG VERTICAL | SF | SQUARE FOOT |
| D | DOUBLE | LOC | LOCATION | SFT | STRUCTURAL FACING TILE |
| DEG | DEGREE | LP | LOW POINT | SHT | SHEET |
| DEMO | DEMOLITION | LT | LIGHT | SIM | SIMILAR |
| DET | DETAIL | LTG | LIGHTING | SJ | STEEL JOIST |
| DIA | DIAMETER | LV | LOUVER | SND | SANITARY NAPKIN DISPOSAL |
| DIR | DIRECTORY | | | SOD | SECTIONAL OVERHEAD DOOR (STEEL; ALUMINUM: PLASTIC PANEL) |
| DN | DOWN | MACH | MACHINE | SPEC | SPECIFICATION |
| DO | DOOR OPENING | MAS | MASONRY | SP | STAND PIPE |
| DR | DOOR | MATL | MATERIAL | SSM | SOLID SURFACING MATERIAL |
| DS | DOWNSPOUT | MAX | MAXIMUM | STAT | STATIONARY |
| DWG | DRAWING | MET | METAL | STL | STEEL |
| | | MDF | MEDIUM DENSITY FIBERBOARD | STRUCT | STRUCTURAL OR STRUCTURE |
| E | EAST | MFB | MINERAL FIBER BLANKET | SYS | SYSTEM |
| EA | EACH | MECH | MECHANICAL | | |
| EFS | EXTERIOR FINISH SYSTEM | MET | METAL | T | TILE |
| EIFS | EXTERIOR INSULATION AND FINISH SYSTEM | MFR | MANUFACTURER | T&B | TOP & BOTTOM |
| EJ | EXPANSION JOINT | MH | MANHOLE | T&G | TONGUE & GROOVE |
| EL | ELEVATION | MIN | MINIMUM | | |
| ELEC | ELECTRIC OR ELECTRICAL | MISC | MISCELLANEOUS | | |
| EPS | EXPANDED POLYSTYRENE | MK | MARK | | |
| EPX | EPOXY | MO | MASONRY OPENING | | |
| EQ | EQUAL | MP | METAL PANEL | | |
| EQUIP | EQUIPMENT | MTD | MOUNTED | | |
| EST | ESTIMATE | MTL | METAL | | |
| EUH | ELECTRIC UNIT HEATER | | | | |
| EW | EACH WAY | | | | |
| EWV | ELECTRIC WATER COOLER | | | | |
| EWCA | ELECTRIC WATER COOLER - ACCESSIBLE | | | | |
| EXIST | EXISTING | | | | |
| EXP | EXPANSION OR EXPOSED | | | | |
| EXT | EXTERIOR | | | | |

\$\$\$\$\$NONSPEC\$\$\$\$\$DATE\$\$\$\$\$

PROFESSIONAL CERTIFICATION.
I HEREBY CERTIFY THAT THESE DOCUMENTS
WERE PREPARED OR APPROVED BY ME, AND
THAT I AM A DULY LICENSED PROFESSIONAL
ENGINEER UNDER THE LAWS OF THE STATE
OF MARYLAND, LICENSE NO. _____
EXPIRATION DATE: _____



| NO. | REVISION | DATE | BY |
|-----|----------|------|----|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

**MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
GAITHERSBURG, MARYLAND**

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section _____ Date _____

APPROVED

Chief, Division of Transportation Engineering _____ Date _____

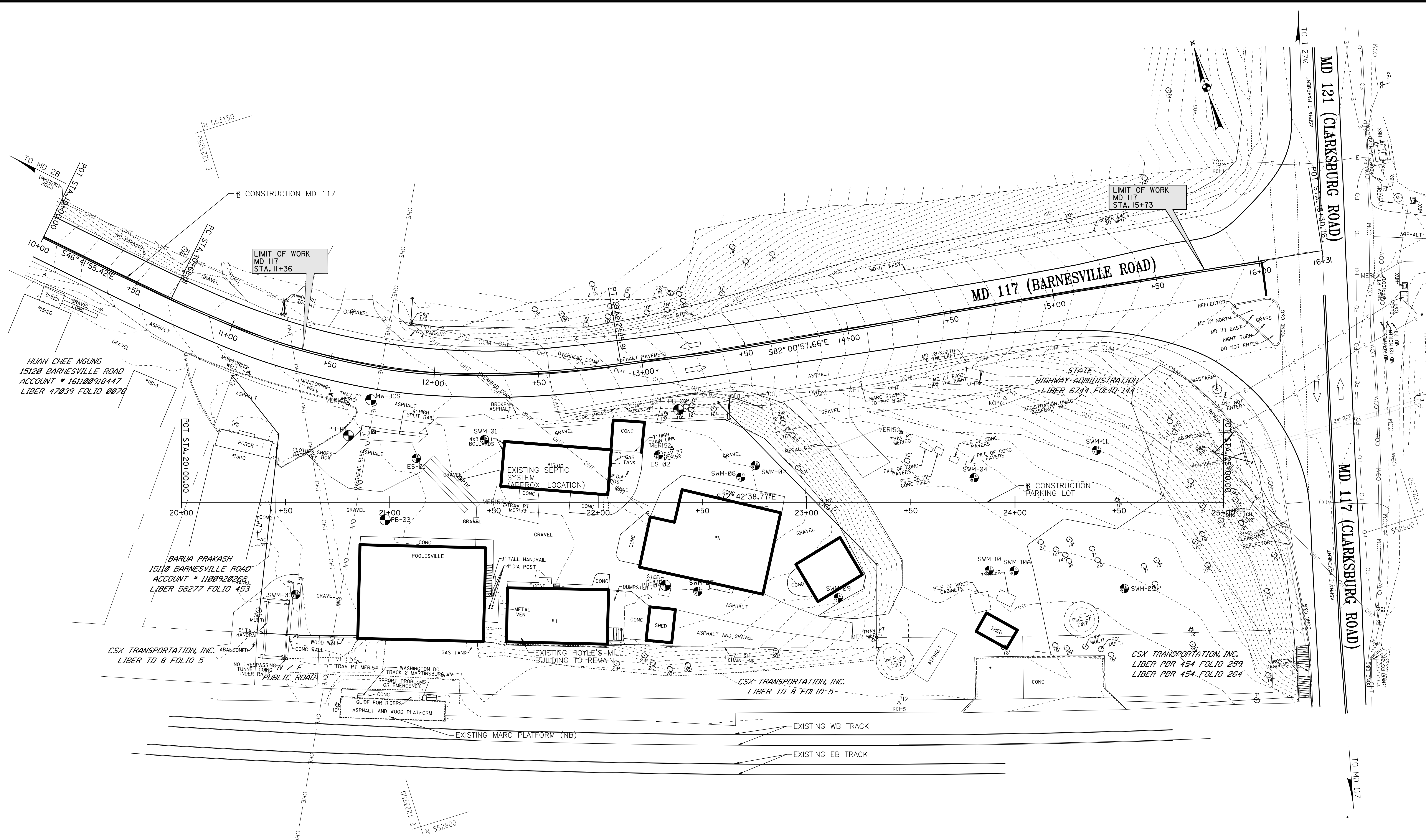
Designed by: LH Drawn by: FIE Checked by: FAH

G1-01 INDEX, LEGEND AND ABBREVIATIONS
**HOYLE'S MILL
STRUCTURAL STABILIZATION
BOYDS, MARYLAND**

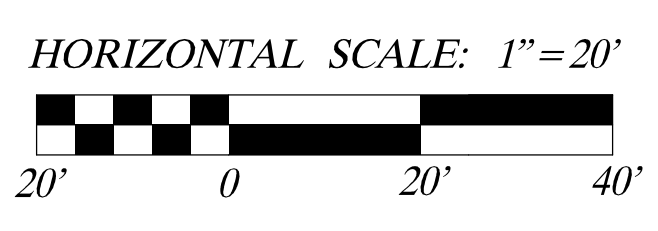
SCALE : NTS

24 JULY 2024

Project No. : 32207.003 SHEET 2 of 8



N:\32207-003\CADD\Structural\Stabilization\Arch\VEC-0001_Boys.dgn
 3/29/2024



PROFESSIONAL CERTIFICATION.
 I HEREBY CERTIFY THAT THESE DOCUMENTS
 WERE PREPARED OR APPROVED BY ME, AND
 THAT I AM A DULY LICENSED PROFESSIONAL
 ENGINEER UNDER THE LAWS OF THE STATE
 OF MARYLAND, LICENSE NO. _____
 EXPIRATION DATE: _____

Whitman, Requard & Associates, LLP
 801 South Caroline Street, Baltimore, Maryland 21231

| NO. | REVISION | DATE | BY |
|-----|----------|------|----|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

MONTGOMERY COUNTY
 DEPARTMENT OF TRANSPORTATION
 GAITHERSBURG, MARYLAND

RECOMMENDED FOR APPROVAL
 Chief, Transportation Planning and Design Section _____ Date _____
 APPROVED
 Chief, Division of Transportation Engineering _____ Date _____
 Designed by: AMU Drawn by: AMU Checked by: PHD

(FOR REFERENCE ONLY)
 EC-01 EXISTING CONDITIONS
HOYLE'S MILL
STRUCTURAL STABILIZATION
BOYDS, MARYLAND

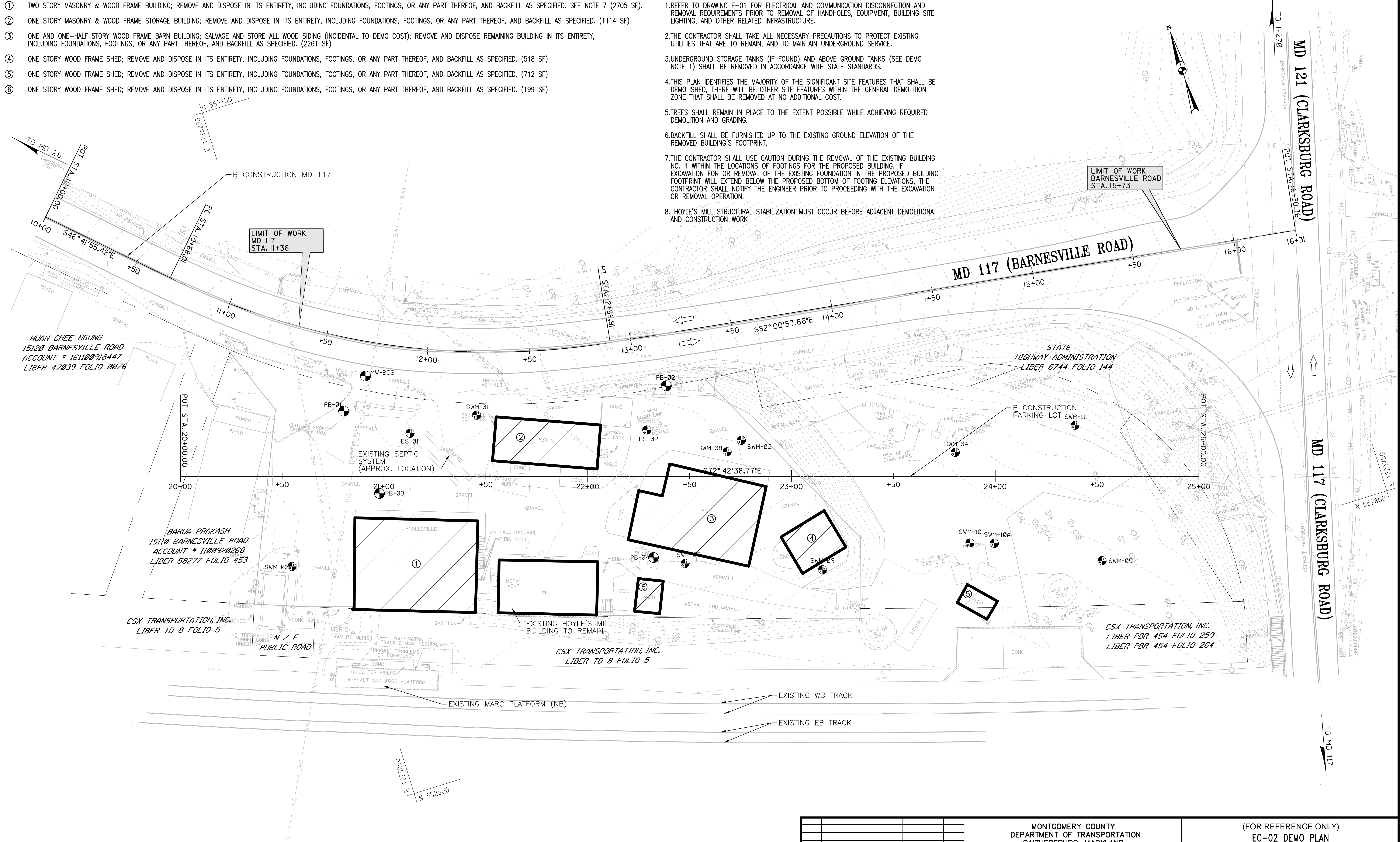
SCALE: 1" = 20'
 24 JULY 2024
 Project No. : 32207.003 SHEET 3 of 8

SITE DEMOLITION KEY NOTES - NOT IN CONTRACT AND REFERENCE ONLY

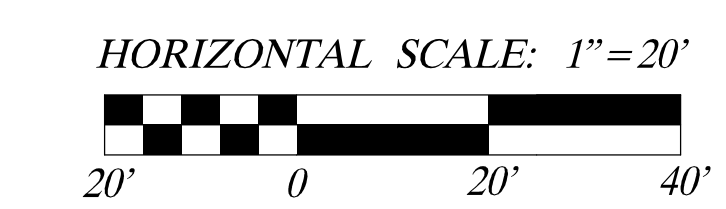
- ① TWO STORY MASONRY & WOOD FRAME BUILDING; REMOVE AND DISPOSE IN ITS ENTIRETY, INCLUDING FOUNDATIONS, FOOTINGS, OR ANY PART THEREOF, AND BACKFILL AS SPECIFIED. SEE NOTE 7 (2705 SF).
- ② ONE STORY MASONRY & WOOD FRAME STORAGE BUILDING; REMOVE AND DISPOSE IN ITS ENTIRETY, INCLUDING FOUNDATIONS, FOOTINGS, OR ANY PART THEREOF, AND BACKFILL AS SPECIFIED. (1114 SF)
- ③ ONE AND ONE-HALF STORY WOOD FRAME BARN BUILDING; SALVAGE AND STORE ALL WOOD SIDING (INCIDENTAL TO DEMO COST); REMOVE AND DISPOSE REMAINING BUILDING IN ITS ENTIRETY, INCLUDING FOUNDATIONS, FOOTINGS, OR ANY PART THEREOF, AND BACKFILL AS SPECIFIED. (2261 SF)
- ④ ONE STORY WOOD FRAME SHED; REMOVE AND DISPOSE IN ITS ENTIRETY, INCLUDING FOUNDATIONS, FOOTINGS, OR ANY PART THEREOF, AND BACKFILL AS SPECIFIED. (518 SF)
- ⑤ ONE STORY WOOD FRAME SHED; REMOVE AND DISPOSE IN ITS ENTIRETY, INCLUDING FOUNDATIONS, FOOTINGS, OR ANY PART THEREOF, AND BACKFILL AS SPECIFIED. (712 SF)
- ⑥ ONE STORY WOOD FRAME SHED; REMOVE AND DISPOSE IN ITS ENTIRETY, INCLUDING FOUNDATIONS, FOOTINGS, OR ANY PART THEREOF, AND BACKFILL AS SPECIFIED. (199 SF)

GENERAL DEMOLITION NOTES: - NOT IN CONTRACT AND FOR REFERENCE ONLY

1. REFER TO DRAWING E-01 FOR ELECTRICAL AND COMMUNICATION DISCONNECTION AND REMOVAL REQUIREMENTS PRIOR TO REMOVAL OF HANDHOLES, EQUIPMENT, BUILDING SITE LIGHTING, AND OTHER RELATED INFRASTRUCTURE.
2. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT EXISTING UTILITIES THAT ARE TO REMAIN, AND TO MAINTAIN UNDERGROUND SERVICE.
3. UNDERGROUND STORAGE TANKS (IF FOUND) AND ABOVE GROUND TANKS (SEE DEMO NOTE 1) SHALL BE REMOVED IN ACCORDANCE WITH STATE STANDARDS.
4. THIS PLAN IDENTIFIES THE MAJORITY OF THE SIGNIFICANT SITE FEATURES THAT SHALL BE DEMOLISHED, THERE WILL BE OTHER SITE FEATURES WITHIN THE GENERAL DEMOLITION ZONE THAT SHALL BE REMOVED AT NO ADDITIONAL COST.
5. TREES SHALL REMAIN IN PLACE TO THE EXTENT POSSIBLE WHILE ACHIEVING REQUIRED DEMOLITION AND GRADING.
6. BACKFILL SHALL BE FURNISHED UP TO THE EXISTING GROUND ELEVATION OF THE REMOVED BUILDING'S FOOTPRINT.
7. THE CONTRACTOR SHALL USE CAUTION DURING THE REMOVAL OF THE EXISTING BUILDING NO. 1 WITHIN THE LOCATIONS OF FOOTINGS FOR THE PROPOSED BUILDING. IF EXCAVATION FOR OR REMOVAL OF THE EXISTING FOUNDATION IN THE PROPOSED BUILDING FOOTPRINT WILL EXTEND BELOW THE PROPOSED BOTTOM OF FOOTING ELEVATIONS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER PRIOR TO PROCEEDING WITH THE EXCAVATION OR REMOVAL OPERATION.
8. HOYLE'S MILL STRUCTURAL STABILIZATION MUST OCCUR BEFORE ADJACENT DEMOLITION AND CONSTRUCTION WORK.



| LEGEND | |
|--------|------------------|
| | BUILDING REMOVAL |



PROFESSIONAL CERTIFICATION.
I HEREBY CERTIFY THAT THESE DOCUMENTS
WERE PREPARED OR APPROVED BY ME, AND
THAT I AM A DULY LICENSED PROFESSIONAL
ENGINEER UNDER THE LAWS OF THE STATE
OF MARYLAND, LICENSE NO. _____
EXPIRATION DATE: _____

WRA
Whitman, Requardt & Associates, LLP
801 South Caroline Street, Baltimore, Maryland 21231

| NO. | REVISION | DATE | BY |
|-----|----------|------|----|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
GAITHERSBURG, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section _____ Date _____

APPROVED

Chief, Division of Transportation Engineering _____ Date _____

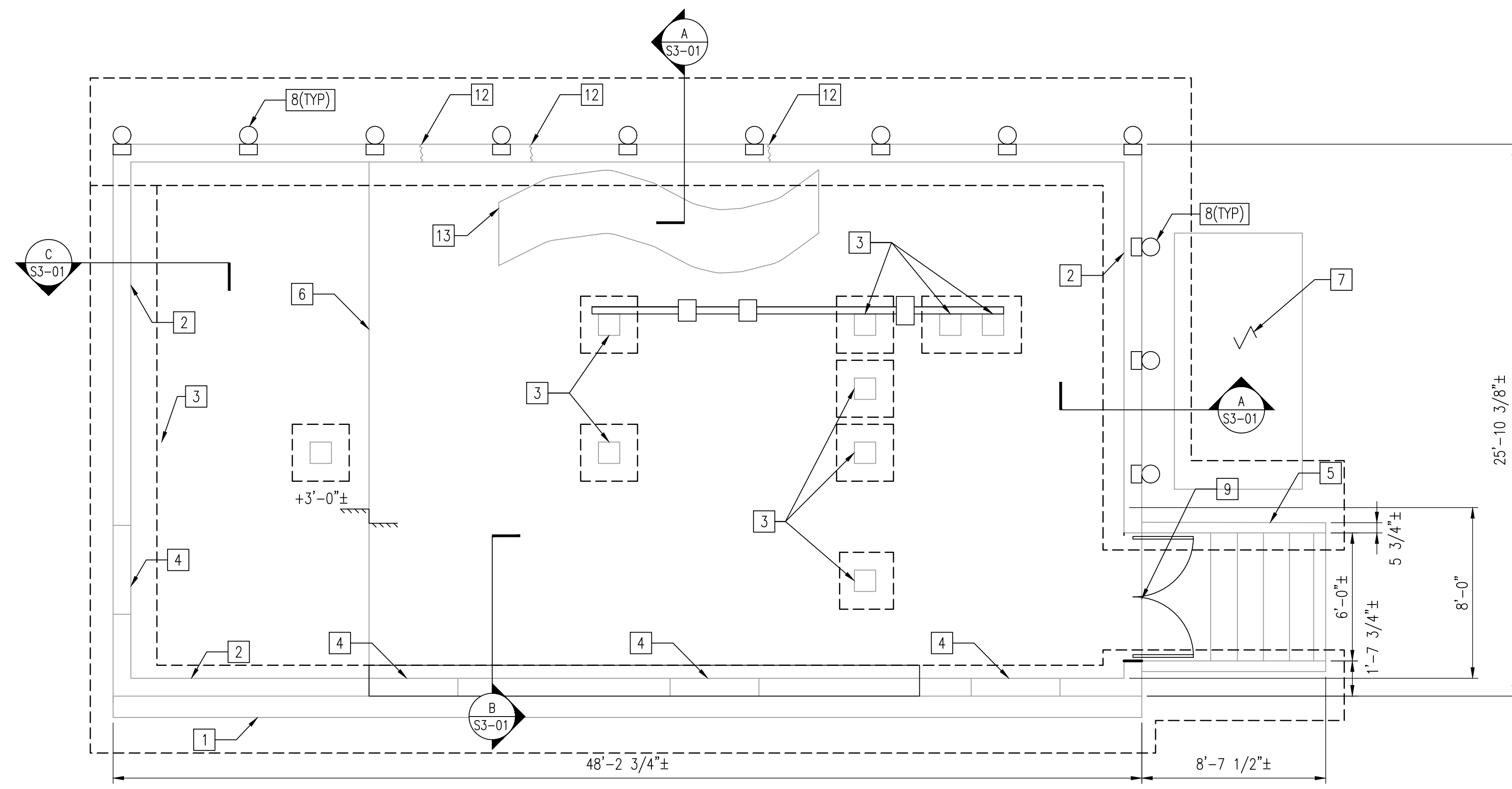
Designed by: AMU Drawn by: AMU Checked by: PHD

(FOR REFERENCE ONLY)
EC-02 DEMO PLAN
HOYLE'S MILL
STRUCTURAL STABILIZATION
BOYDS, MARYLAND

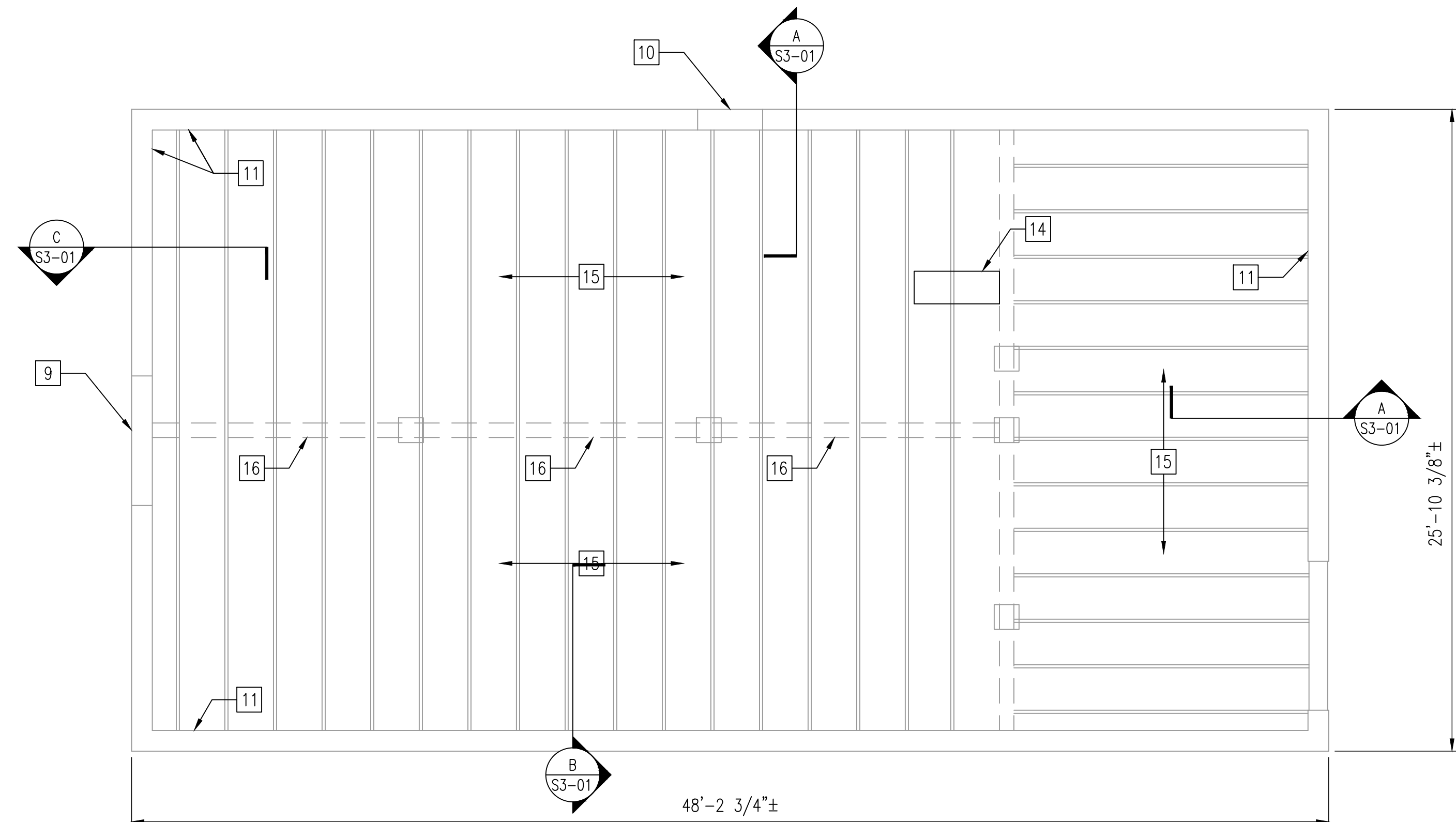
SCALE: 1" = 20'

24 JULY 2024

Project No.: 32207.003 SHEET 4 of 8

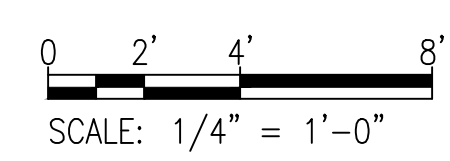


1 FOUNDATION PLAN
S1-01 SCALE: 1/4" = 1'-0"



2 FIRST FLOOR PLAN
S1-01 SCALE: 1/4" = 1'-0"

NOTE:
1. FRAMING LAYOUT SHOWN IS SCHEMATIC. CONTRACTOR TO VERIFY ACTUAL LOCATION OF WOOD FRAMING MEMBERS AND LOCATE OPENINGS TO AVOID CONFLICT WITH FRAMING.



GENERAL SHEET NOTES

- REFER TO SHEET S0-01 FOR STRUCTURAL GENERAL NOTES, BUILDING CODES AND STANDARDS, AND DESIGN LOADS.
- FIELD VERIFY DIMENSIONS, LOCATIONS AND ELEVATIONS SHOWN ON DRAWINGS FOR EXISTING STRUCTURES. BRING DISCREPANCIES TO THE ATTENTION OF THE ENGINEER BEFORE PROCEEDING WITH WORK.
- COORDINATE WORK WITH ARCHITECTURAL DRAWINGS.
- THE EXISTING STRUCTURE IS IN VERY POOR CONDITION, AND ACCESS TO THE BUILDING IS DANGEROUS. CONTRACTOR MUST PROVIDE TEMPORARY SHORING WITHIN THE BASEMENT AREA PRIOR TO COMMENCING WITH ANY OTHER WORK WITHIN THE BASEMENT.

X SHEET KEYNOTES

- EXISTING SITE RETAINING WALL ADJACENT TO EXTERIOR WALL TO REMAIN.
- EXISTING EXTERIOR CONCRETE WALL ON MASONRY FOOTINGS TO REMAIN.
- EXISTING WOOD BUILDING COLUMN TO REMAIN.
- EXISTING WINDOW OPENING. SEE ARCHITECTURAL DRAWINGS FOR MODIFICATIONS.
- DEMOLISH EXISTING STAIR TO BASEMENT AND REPLACE WITH SIMILAR NEW CONCRETE STAIR DESIGNED TO MEET CURRENT CODE WITH GALVANIZED STEEL HANDRAILS ON EACH SIDE.
- ELEVATION CHANGE IN EXISTING UNFINISHED BASEMENT FLOOR.
- DEMOLISH EXISTING CONCRETE PAD UNDER EXISTING METAL SHED.
- HELICAL PILES SPACED AT 6'-0" +/- TO SUPPORT EXISTING CONCRETE FOUNDATION WALL. HELICAL PILE DESIGN DELEGATED TO CONTRACTOR'S ENGINEER.
- EXISTING DOUBLE DOOR. SEE ARCHITECTURAL DRAWINGS.
- EXISTING DOOR.
- EXISTING EXTERIOR WOOD-FRAMED WALLS.
- EXISTING FULL-DEPTH, FULL-WIDTH CRACK IN EXISTING CONCRETE WALL. REPAIR WITH (2) 4" WIDE CONTINUOUS STEEL PLATES WITH ADHESIVE ANCHORS AT 12" EXTENDING TO ADJACENT PILES.
- DEMOLISH, REMOVE, AND DISPOSE OF EXISTING LARGE CONCRETE MASS (APPROXIMATELY 40" WIDE X 36" DEEP).
- PATCH HOLE IN FLOOR LOCATED IN FRONT OF EXISTING STAIR WITH PLYWOOD.
- EXISTING 1/2" THICK WOOD BOARDS ON 2"x8" WOOD JOISTS SPACED AT 22" +/- ON CENTER.
- EXISTING 7" WIDE X 9" DEEP WOOD GIRDERS.

\$\$\$NONSPECIFIC\$\$\$\$\$DATE\$\$\$\$\$

PROFESSIONAL CERTIFICATION.
I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND, LICENSE NO. _____ EXPIRATION DATE: _____

| NO. | REVISION | DATE | BY |
|-----|----------|------|----|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

MONTGOMERY COUNTY
DEPARTMENT OF TRANSPORTATION
GAITHERSBURG, MARYLAND

RECOMMENDED FOR APPROVAL

Chief, Transportation Planning and Design Section _____ Date _____

APPROVED

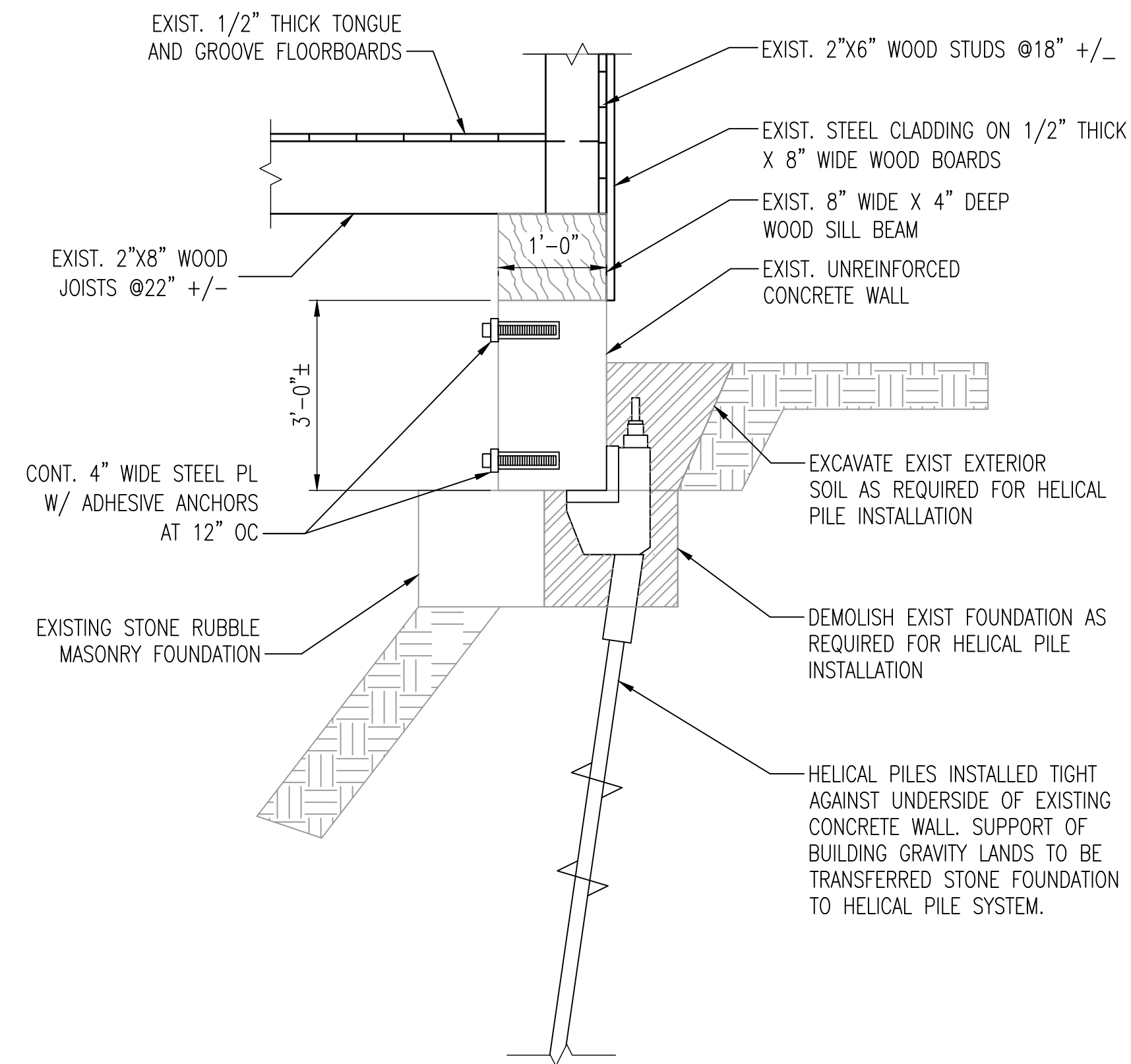
Chief, Division of Transportation Engineering _____ Date _____

Designed by: SA _____ Drawn by: SA _____ Checked by: BMB _____

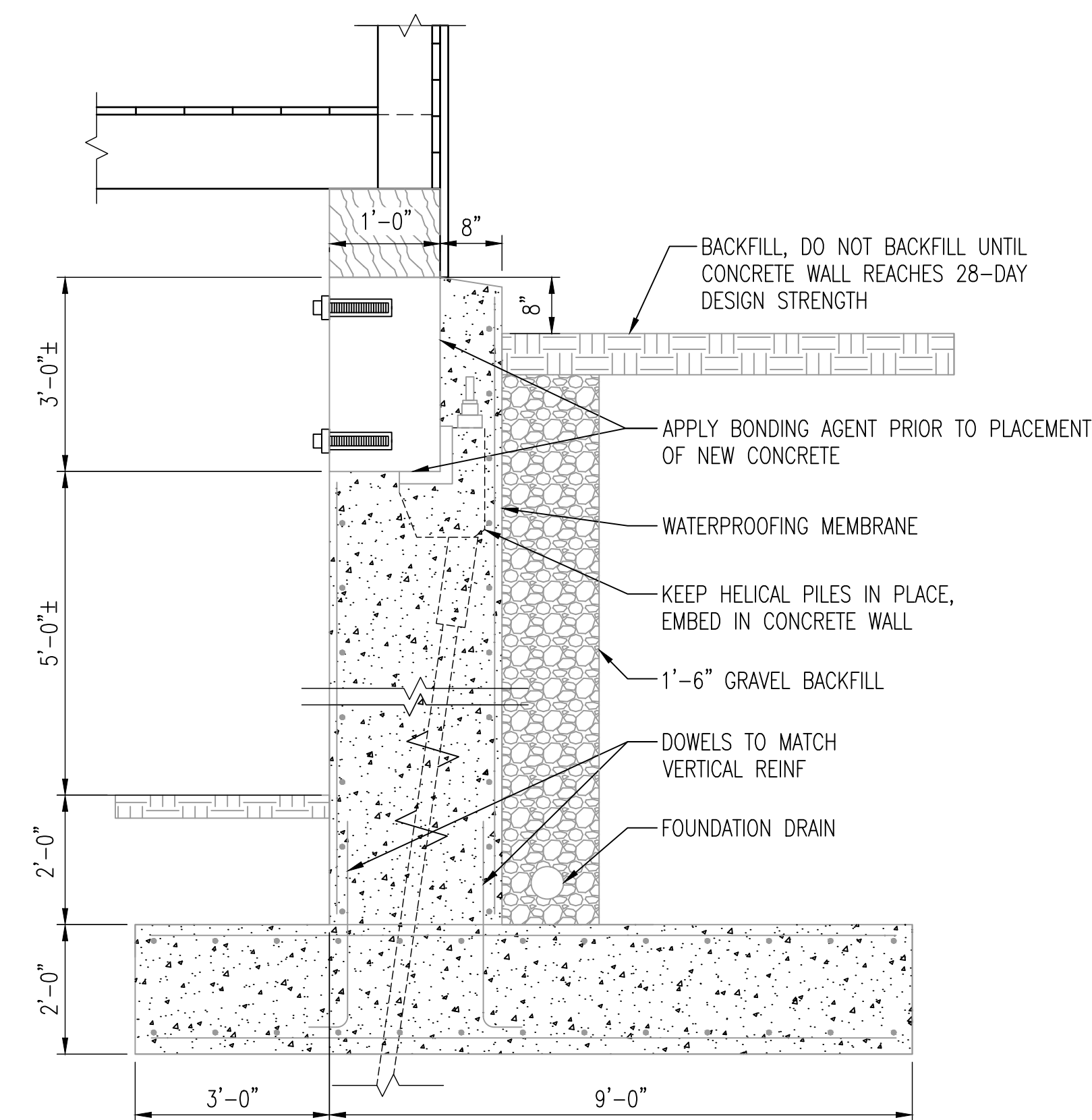
S1-01 BASEMENT PLAN AND FIRST FLOOR PLAN
HOYLE'S MILL
STRUCTURAL STABILIZATION
BOYDS, MARYLAND

SCALE: 1/4" = 1'-0" 24 JULY 2024

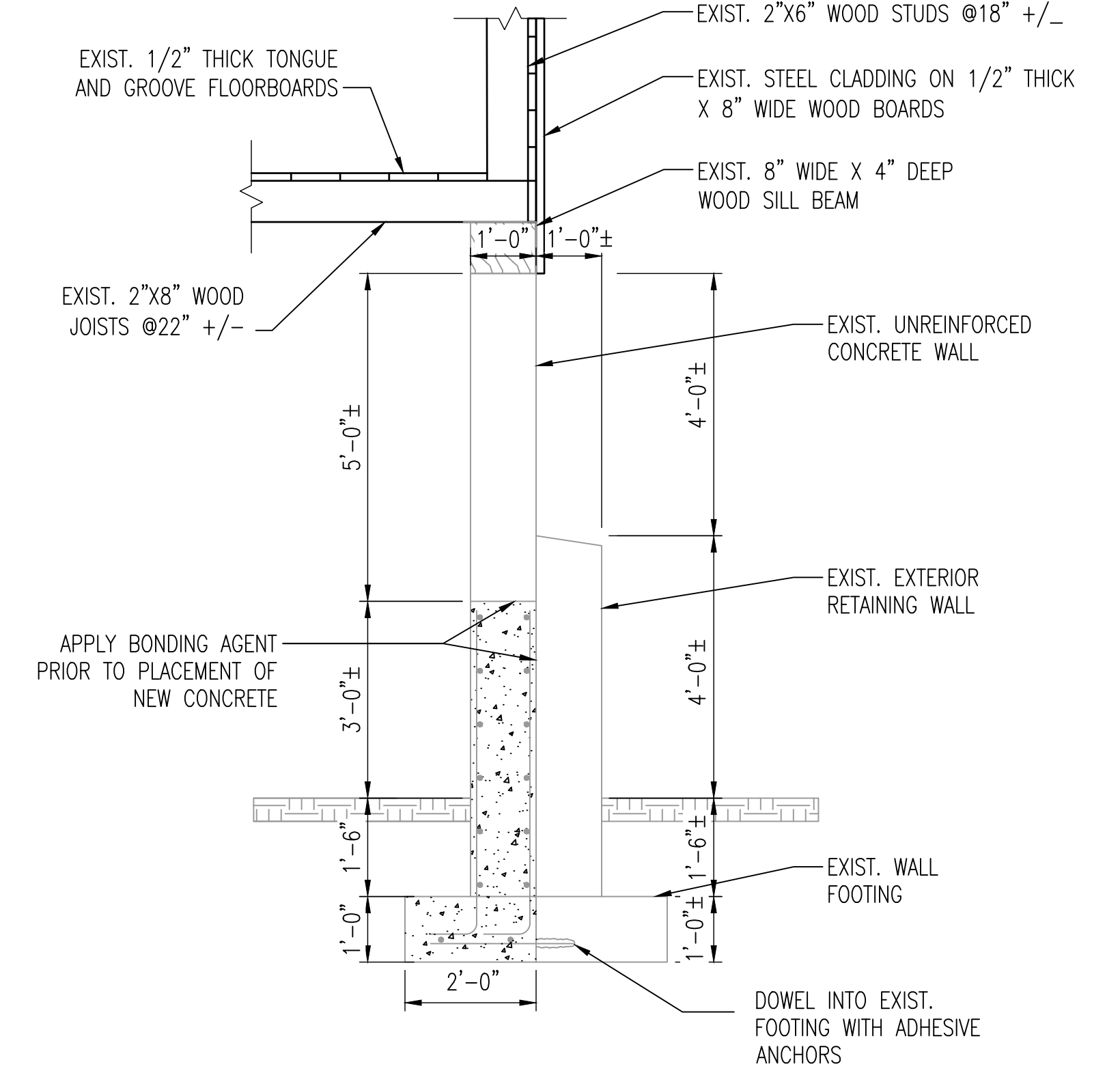
Project No.: 32207.003 SHEET 7 of 8



A SECTION (STAGE ONE)
S3-01 SCALE: 1/2" = 1'-0"

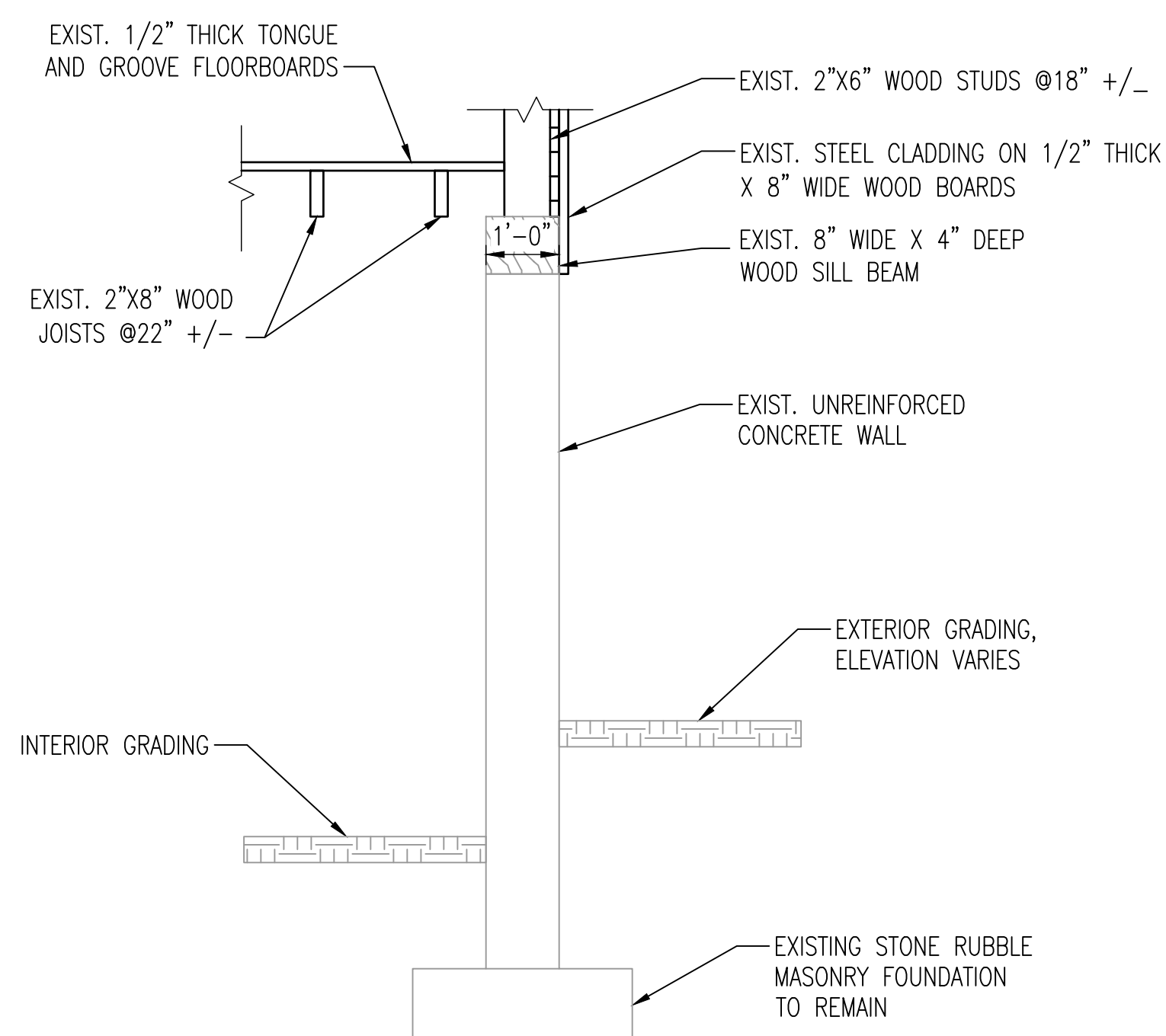


B SECTION (STAGE TWO)
S3-01 SCALE: 1/2" = 1'-0"



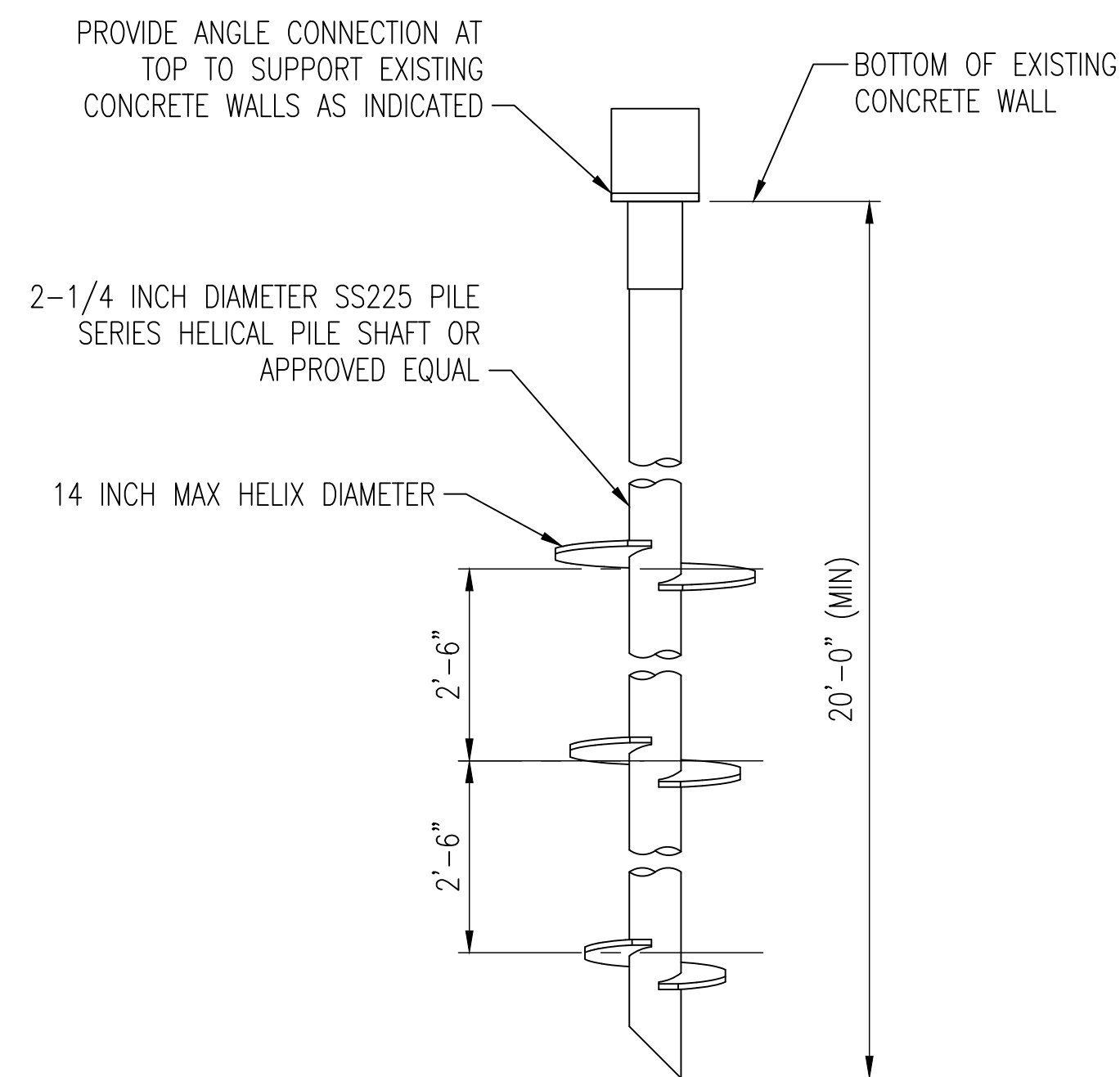
NOTE: PROVIDE TEMPORARY SHORING PRIOR TO COMMENCING DEMOLITION WORK NEEDED TO INSTALL NEW CONCRETE WALL AND FOUNDATION. TEMPORARY SHORING DESIGN BY CONTRACTOR.

C SECTION
S3-01 SCALE: 1/2" = 1'-0"



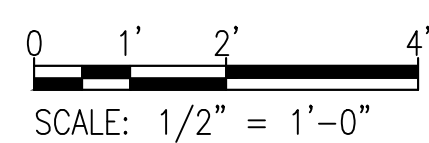
NOTE: THIS SECTION SHOWS ASSUMED EXISTING CONDITIONS. NO MODIFICATIONS TO THIS WALL ARE REQUIRED FOR STRUCTURAL STABILIZATION.

D SECTION
S3-01 SCALE: 1/2" = 1'-0"



NOTE: HELICAL PILE PRODUCTS, INSTALLATION EQUIPMENT, TOOLING AND PROCEDURES, AND TERMINATION CRITERIA MUST BE IN ACCORDANCE WITH HELICAL PILE NOTES ON SHEET S0-01.

TYPICAL HELICAL PILE
NOT TO SCALE



PROFESSIONAL CERTIFICATION.
I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND, LICENSE NO. _____ EXPIRATION DATE: _____



| NO. | REVISION | DATE | BY |
|-----|----------|------|----|
| | | | |
| | | | |
| | | | |
| | | | |

| MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION GAITHERSBURG, MARYLAND | |
|---|--------------|
| RECOMMENDED FOR APPROVAL | |
| Chief, Transportation Planning and Design Section | Date |
| APPROVED | |
| Chief, Division of Transportation Engineering | Date |
| Designed by: SA | Drawn by: SA |
| Checked by: BMB | |

| S3-01 BUILDING SECTIONS HOYLE'S MILL STRUCTURAL STABILIZATION BOYDS, MARYLAND | |
|--|--------------|
| SCALE: 1/2" = 1'-0" | 24 JULY 2024 |
| Project No.: 32207.003 | SHEET 8 of 8 |

\$\$\$\$\$DOWNSPEC\$\$\$\$\$DATE\$\$\$\$\$