



SILVER SPRING

DOWNTOWN AND ADJACENT COMMUNITIES PLAN



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 **Montgomery Planning**

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APPROVED AND ADOPTED JUNE 2022

Abstract

The Silver Spring Downtown and Adjacent Communities Plan contains the text and supporting maps for a comprehensive amendment to portions of the approved and adopted 2000 *Silver Spring Central Business District Sector Plan*, as amended; the 2000 *North and West Silver Spring Master Plan*, as amended; and the 2000 *East Silver Spring Master Plan*, as amended. It also amends *The General Plan (On Wedges and Corridors) for the Physical Development of the Maryland-Washington Regional District in Montgomery and Prince George's Counties*, as amended; the 2013 *Countywide Transit Corridors Functional Master Plan*; the 2018 *Master Plan of Highways and Transitways*, as amended; and the 2018 *Bicycle Master Plan*.

The Silver Spring Downtown and Adjacent Communities Plan envisions a downtown that is a strong commercial and economic hub, home to diverse arts and entertainment opportunities, an affordable place to live, and a regional destination with a unique community of small and independent businesses. The Plan's recommendations support a green and resilient downtown with a robust transit network, improved pedestrian and bicycle networks, new and improved parks and open spaces, and an increase in affordable and diverse housing options.

Master and sector plans convey land use policy for defined geographic areas and should be interpreted together with relevant countywide functional plans and county laws and regulations. Plans provide comprehensive recommendations for the use of public and private land and should be referred to by public officials and private individuals when making land use decisions. Public and private land use decisions that promote plan goals are essential to fulfilling a plan's vision.

Master and sector plans look ahead 20 years from the date of adoption, although they are intended to be revised every 10 to 15 years. Moreover, after a plan is adopted, circumstances will change, and the specifics of a plan may become less relevant over time. Plans do not specify all development possibilities. They often include illustrative sketches or diagrams intended to convey a sense of desirable future character rather than detailed recommendations for a particular design.

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The Maryland-National Capital Park and Planning Commission
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The Maryland-National Capital Park and Planning Commission

The Maryland-National Capital Park and Planning Commission is a bi-county agency created by the General Assembly of Maryland in 1927. The Commission's geographic authority extends to the great majority of Montgomery and Prince George's Counties; the Maryland-Washington Regional District (M-NCPPC planning jurisdiction) comprises 1,001 square miles, while the Metropolitan District (parks) comprises 919 square miles in the two counties.

The Commission is charged with preparing, adopting, and amending or extending *The General Plan (On Wedges and Corridors) for the Physical Development of the Maryland-Washington Regional District in Montgomery and Prince George's Counties*. The Commission operates in each county through Planning Boards appointed by the county government. The Boards are responsible for preparing all local plans, zoning amendments, subdivision regulations, and administration of parks.

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SILVER SPRING

DOWNTOWN AND ADJACENT COMMUNITIES PLAN

APPROVED AND ADOPTED JUNE 2022

M-NCPPC

Prepared by the Montgomery County Planning Department
MontgomeryPlanning.org

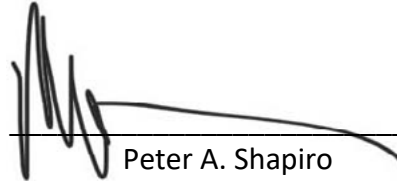
CERTIFICATION OF APPROVAL AND ADOPTION
SILVER SPRING DOWNTOWN AND ADJACENT COMMUNITIES PLAN

This Comprehensive Amendment to portions of the 2000 *Silver Spring Central Business District Sector Plan*, as amended; the 2000 *North and West Silver Spring Master Plan*, as amended; and the 2000 *East Silver Spring Master Plan*, as amended; being also an amendment to the *General Plan (On Wedges and Corridors) for the Physical Development of the Maryland-Washington Regional District in Montgomery and Prince George's Counties*, as amended; the 2013 *Countywide Transit Corridors Functional Master Plan*, as amended; the 2018 *Master Plan of Highways and Transitways*, as amended; and the 2018 *Bicycle Master Plan* has been approved by the Montgomery County Council, sitting as the District Council, by Resolution Number 19-1297 on May 26, 2022, and has been adopted by The Maryland-National Capital Park and Planning Commission by Resolution Number 22-18 on June 15, 2022, after duly advertised public hearings pursuant to the Land Use Article – Division II, of the Annotated Code of Maryland.

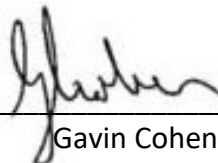
THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION



Casey Anderson
Chair



Peter A. Shapiro
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Gavin Cohen
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THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

6611 Kenilworth Avenue • Riverdale, Maryland 20737

MCPB NO. 22-051
M-NCPPC NO. 22-18

RESOLUTION

WHEREAS, The Maryland-National Capital Park and Planning Commission, by virtue of the Land Use Article of the Annotated Code of Maryland, is authorized and empowered, from time to time, to make and adopt, amend, extend and add to *The General Plan (On Wedges and Corridors) for the Physical Development of the Maryland-Washington Regional District Within Montgomery and Prince George's Counties*; and

WHEREAS, the Montgomery County Planning Board of The Maryland-National Capital Park and Planning Commission, pursuant to procedures set forth in the Montgomery County Code, Chapter 33A, held a duly advertised public hearing on December 2, 2021 on the Public Hearing Draft of *Silver Spring Downtown and Adjacent Communities Plan*, being also an amendment to *The General Plan (On Wedges and Corridors) for the Physical Development of the Maryland Washington Regional District in Montgomery County and Prince George's Counties*, as amended; the 2000 *Silver Spring Central Business District Sector Plan*, as amended; the 2000 *North and West Silver Spring Master Plan*, as amended; the 2000 *East Silver Spring Master Plan*, as amended; the 2013 *Countywide Transit Corridors Functional Master Plan*, as amended; the 2018 *Master Plan of Highways and Transitways*, as amended; and the 2018 *Bicycle Master Plan*.

WHEREAS, the Montgomery County Planning Board, after said public hearing and due deliberation and consideration, on January 6, 2022, approved the Planning Board Draft of the *Silver Spring Downtown and Adjacent Communities Plan*, recommended that it be approved by the Montgomery County Council sitting as the District Council for the portion of the Maryland-Washington Regional District lying within Montgomery County (the "Montgomery County District Council"), and forwarded it to the Montgomery County Executive for recommendations and analysis; and

Approved as to
Legal Sufficiency: /s/Emily Vaias
M-NCPPC Legal Department

MCPB NO. 22-051
M-NCPPC NO. 22-18

WHEREAS, the Montgomery County Executive reviewed and made recommendations on Planning Board Draft of the *Silver Spring Downtown and Adjacent Communities Plan* and forwarded those recommendations and analysis to the Montgomery County District Council on March 25, 2022 and April 29, 2022; and

WHEREAS, the Montgomery County District Council held a public hearing on February 17, 2022, wherein testimony was received concerning the Planning Board Draft of the *Silver Spring Downtown and Adjacent Communities Plan*; and

WHEREAS, the District Council, on May 26, 2022 approved the Planning Board Draft of the *Silver Spring Downtown and Adjacent Communities Plan* subject to the modifications and revisions set forth in District Council Resolution No. 19-1297.

NOW, THEREFORE BE IT RESOLVED, that the Montgomery County Planning Board and The Maryland-National Capital Park and Planning Commission do hereby adopt the said *Silver Spring Downtown and Adjacent Communities Plan*, together with the *General Plan (on Wedges and Corridors) for the Physical Development of the Maryland-Washington Regional District within Montgomery and Prince George's Counties*, as amended; the 2000 *Silver Spring Central Business District Sector Plan*, as amended; the 2000 *North and West Silver Spring Master Plan*, as amended; the 2000 *East Silver Spring Master Plan*, as amended; the 2013 *Countywide Transit Corridors Functional Master Plan*, as amended; the 2018 *Master Plan of Highways and Transitways*, as amended; and the 2018 *Bicycle Master Plan*; and as approved by the District Council in the attached Resolution No. 19-1297; and

MCPB NO. 22-051
M-NCPPC NO. 22-18

BE IT FURTHER RESOLVED, that copies of said *Silver Spring Downtown and Adjacent Communities Plan* must be certified by The Maryland-National Capital Park and Planning Commission and filed with the Clerk of the Circuit Court for both Montgomery and Prince George's Counties, as required by law.

This is to certify that the foregoing is a true and correct copy of Resolution No. 22-051 adopted by the Montgomery County Planning Board of The Maryland-National Capital Park and Planning Commission at its regular meeting held on Thursday, June 2, 2022, in Wheaton, Maryland on motion of Vice Chair Verma, seconded by Commissioner Cichy, with a vote of 5 to 0, Chair Anderson, Vice Chair Verma, and, Commissioners Cichy, Patterson, and Rubin, voting in favor of the motion.



Casey Anderson, Chair
Montgomery County Planning Board

CERTIFICATION

This is to certify that the foregoing is a true and correct copy of Resolution No. 22-18 adopted by the Maryland-National Capital Park and Planning Commission on motion of Commissioner Rubin, seconded by Commissioner Verma, with Chair Anderson, Vice Chair Shapiro, Commissioners Bailey, Cichy, Doerner, Geraldo, Patterson, Rubin and Verma voting in favor of the motion, with Commissioner Washington being absent for the vote, at its regular meeting held on Wednesday, June 15, 2022 via video-conference, and broadcast by the Department of Parks and Recreation, Prince George's County.



Asuntha Chiang-Smith Executive Director

Table of Contents

Chapter		Page
1	Introduction	1
	1.1. A Vision for Silver Spring 2040	2
	1.2. Plan Context	6
	1.3. History of Silver Spring	8
	1.4. Concept Framework	12
	1.5. Plan Goals	14
	1.6. Summary of Recommendations	15
2	Districts	17
	2.1. District Visions	18
	2.2. Connecting the Districts	28
	2.3. District Recommendations	37
3	Plan-Wide Recommendations	81
	3.1. Land Use and Zoning	82
	3.2. Economic Growth	89
	3.3. Housing	92
	3.4. Urban Design	96
	3.5. Parks Trails and Open Spaces	98
	3.6. Transportation	120
	3.7. Resilient Downtown	164
	3.8. Community Facilities	172
	3.9. Historic Resources	180
4	Implementation	195
	4.1. Zoning	196
	4.2. Capital Improvements Program	200
	4.3. Parks	202
	4.4. Historic Preservation Summary	203
	4.5. Partnerships	204

Figures, Maps & Tables

Figures

Figure 1. "What We've Heard" Graphic	4	Figure 31. Colesville Road (M-10): Spring Street to Fenton Street, Looking North (MR)	150
Figure 2. Key Components of the Green Loop	31	Figure 32. Colesville Road (M-10): Fenton Street to Georgia Avenue, Looking North (MR)	150
Figure 3. Existing Connections Under the Rail	32	Figure 34. Colesville Road (M-10): North Noyes Drive to Spring Street, Looking North (CR)	151
Figure 4. Proposed Connection from East-West Highway to Ripley District	34	Figure 33. Colesville Road (M-10): Georgia Avenue to the Sarbanes Transit Center, Looking North (MR)	151
Figure 5. Crossing Over the Rail	35	Figure 35. Colesville Road (M-10): Spring Street to Fenton Street, Looking North (CR)	152
Figure 6. Urban Design Illustrative Diagram	36	Figure 36. Colesville Road (M-10): Fenton Street to Georgia Avenue, Looking North (CR)	152
Figure 7. Ellsworth District Illustrative Diagram	39	Figure 37. Colesville Road (M-10): Georgia Avenue to the Sarbanes Transit Center, Looking North (CR)	153
Figure 8. Fenton Village District Illustrative Diagram	45	Figure 38. Colesville Road (M-10): Sarbanes Transit Center to East-West Highway, Looking North (CR)	153
Figure 9. Metro Center Illustrative Diagram	51	Figure 39. Colesville Road (M10): East-West Highway to 16th Street, Looking North (CR)	154
Figure 10. Ripley District Illustrative Diagram	56	Figure 40. Georgia Avenue (M-8): Spring Street to Colesville Road, Looking North (MR)	155
Figure 11. South Silver Spring Urban Design Illustrative Diagram	63	Figure 41. Georgia Avenue (M-8): Colesville Road to Wayne Avenue, Looking North	156
Figure 12. Downtown North Urban Design Illustrative Diagram	70	Figure 42. Georgia Avenue (M-8): Wayne Avenue to Blair Mill Road, Looking North (MR)	156
Figure 13. Falklands District Urban Design Illustrative Diagram	74	Figure 43. Georgia Avenue (M-8): Blair Mill Road to Eastern Avenue, Looking North (MR)	157
Figure 14. Adjacent Communities Urban Design Illustrative Diagram	77	Figure 44. Georgia Avenue (M-8): Spring Street to Colesville Road, Looking North (CR)	157
Figure 15. Housing Types in Silver Spring	93	Figure 45. Georgia Avenue (M-8): Colesville Road to Wayne Avenue, Looking North	158
Figure 16. Illustrative Concept for Gene Lynch Urban Park	105	Figure 46. Georgia Avenue (M-8): Wayne Avenue to Blair Mill Road, Looking North (CR)	158
Figure 17. Illustrative Concept for Jesup Blair Urban Recreational Park	109	Figure 47. Georgia Avenue (M-8): Blair Mill Road to Eastern Avenue, Looking North (CR)	159
Figure 18. Jesup Blair Park Experience Zones	111	Figure 48. 13th Street (B-3): Georgia Avenue to Eastern Avenue, Looking East	159
Figure 19. South Silver Spring Park Concept Diagram	113	Figure 49. 16th Street (M-9): Colesville Road to East-West Highway, Looking North	160
Figure 20. Standard High-Visibility Crosswalk Markings	127	Figure 50. Silver Spring Avenue (B-25): Fenton Street to Georgia Avenue, Looking East	160
Figure 21. 1st Avenue Extension Diagram (B-30)	135	Figure 51. Cameron Street (B-28): 2nd Avenue to Georgia Avenue, Looking East	161
Figure 23. Sligo Avenue to Silver Spring Avenue (B-32)	135	Figure 52. Burlington Avenue Bridge (M-20): Looking East	161
Figure 22. Thayer Avenue to Bonifant Street (B-31)	135	Figure 53. Fenton Street (A-264): Philadelphia Avenue to Wayne Avenue, Looking North	162
Figure 24. East-West Highway (M-20): 16th Street to Blair Mill Road, Looking North (Interim)	145	Figure 54. Blair Mill Road (B-25): Eastern Avenue to East-West Highway, Looking East	162
Figure 25. East-West Highway (M-20): Blair Mill Road to Blair Mill Way, Looking North (Interim)	146		
Figure 26. East-West Highway (M-20): Blair Mill Way to Georgia Avenue, Looking North (Interim)	146		
Figure 27. East-West Highway (M-20): 16th Street to Blair Mill Road, Looking North (Ultimate)	147		
Figure 28. East-West Highway (M-20): Blair Mill Road to Blair Mill Way, Looking North (Ultimate)	147		
Figure 29. East-West Highway (M-20): Blair Mill Way to Georgia Avenue, Looking North (Ultimate)	148		
Figure 30. Colesville Road (M-10): North Noyes Drive to Spring Street, Looking North (MR)	149		

Figure 55. 1st Street (B-1): Spring Street to Fenwick Lane, Looking North	163
Figure 56. 1st Street (B-30): Fenwick Lane to Cameron Street, Looking East.....	163
Figure 57. Nature Based Solutions	165
Figure 58. Rising Urban Temperatures.....	166
Figure 59. Plan Area Elementary Schools Enrollment Trend & Projection	175
Figure 60. Plan Area Middle Schools Enrollment Trend & Projection	175
Figure 61. Plan Area High Schools Enrollment Trend & Projection	175
Figure 62. Legacy Business Registry.....	191
Figure 63. Interpretation of Historic Sites.....	192

Tables

Table 1. Recommended Ellsworth District Zoning	41
Table 2. Recommended Fenton Village Zoning.....	47
Table 3. Recommended Metro Center Zoning.....	53
Table 4. Recommended Ripley District Zoning.....	58
Table 5. Recommended South Silver Spring Zoning.	66
Table 6. Recommended Downtown North Zoning.	72
Table 7. Recommended Falklands District Zoning.....	75
Table 8. Recommended Adjacent Communities Zoning.....	79
Table 9. Bikeway Network Recommendations.	124
Table 10. Street Classification and ROW Recommendations.....	136
Table 11. Former Operating Schools.....	178
Table 12. Capital Improvements Program.....	200
Table 13. Historic Preservation Implementation Summary.....	203

Maps

Map 1: Plan Boundary	7
Map 2: Concept Framework Plan	13
Map 3: Plan Districts.....	19
Map 4. The Green Loop.....	29
Map 5: New and Existing Connections Across the Rail	33
Map 6: Recommended Ellsworth District Zoning.....	40
Map 7: Recommended Fenton Village Zoning.....	46
Map 8: Recommended Metro Center Zoning	52
Map 9: Recommended Ripley District Zoning.....	57
Map 10: Recommended South Silver Spring Zoning	65
Map 11. Recommended Downtown North Zoning	71
Map 12. Recommended Falklands District Zoning	75
Map 13. Recommended Adjacent Communities Zoning	78
Map 14. Existing Zoning.....	84
Map 15. Recommended Zoning	85
Map 16. Existing Land Use.....	86
Map 17. Recommended Land Use.....	87
Map 18. Maximum Building Heights	88
Map 19. Existing Parks and Major Public Spaces.....	99
Map 20. Proposed Park Locations Approach Diagram	103
Map 21. Existing and Planned Bikeway Infrastructure.....	122
Map 22. Existing Sidewalk Gaps	128
Map 23. Recommended Protected Crossings	129
Map 24. Roadway Classifications (2018 Master Plan of Highways and Transitways)	134
Map 25. Parking Lot District and Transportation Management District Boundaries.....	142
Map 26. Existing Parking Facilities	143
Map 27. Right-of-Way Priority Planting Areas	168
Map 28. School Service Area Map, 2021-2022 School Year	174
Map 29. Former Operating Schools.....	178
Map 30. Historic Resources Listed in the <i>Master Plan for Historic Preservation</i>	183
Map 31. Historic Resources Listed in the <i>Locational Atlas and Index of Historic Sites</i>	185



Aerial view of downtown Silver Spring, 2022



1 Introduction

The future of downtown Silver Spring is successful, unique, diverse, green and vibrant.

1.1. A VISION FOR SILVER SPRING 2040

Downtown Silver Spring is a vibrant urban area that has undergone a significant transformation over the last 20 years. Today it is a center for arts and entertainment, business and housing that draws visitors and consumers from across the county and the region. Silver Spring is renowned for its uniqueness and diversity, as well as for its wealth of locally owned and ethnically diverse bars, breweries, restaurants, cafes and coffee shops.

The revitalization of Silver Spring was spurred by the 2000 *Silver Spring Central Business District Sector Plan* and related initiatives by the public and private sector. The 2000 plan came on the heels of a failed proposal to build a large mega-mall and indoor water park, an idea that drew opposition for many reasons – including concerns about traffic congestion and the scale of the project and its effect on its surroundings – but ultimately collapsed when it became clear that the development team behind the idea would not be able to secure private financing.

In the wake of the mega-mall proposal, county leaders regrouped and reconsidered what steps would be needed to spark change. In assessing the history of unsuccessful efforts that preceded the mega-mall proposal, they concluded that strong support from the public sector – including aggressive steps to assemble and facilitate redevelopment of parcels within the area roughly bounded by Georgia Avenue, Colesville Road, Wayne Avenue, and Spring Street – would be necessary to attract private investment in the downtown core. The resulting public-private partnership was in many ways tremendously successful, and it has brought people from all over the region to work, live, play and hang out in Silver Spring. Investment in civic spaces, the arts, and street-activating retail together with the relocation of Discovery Communications from Bethesda brought new energy and economic vitality to the central business district.

Shoppers on Ellsworth Street



While the first decade following adoption of the 2000 plan saw many positive changes in the blocks closest to the intersection of Colesville Road and Georgia Avenue, the momentum generated by the retail and restaurant development there had limited impact beyond the immediate vicinity. More recently, market demand for multifamily housing – with support from county government – has led to construction of several high-rise residential and mixed-use buildings along Georgia Avenue north of Colesville Road and south of Wayne Avenue. About 5,000 housing units, 20 percent of which are affordable, have been built in downtown Silver Spring, bringing many new residents to this growing urban center. Discovery's departure has highlighted the need for a more diversified base of office tenants to support the retail and restaurant sector as well as the county's tax base, but overall Silver Spring is well-positioned for the future.

This Sector Plan incorporates lessons learned from the last 20 years of redevelopment and reflects current planning approaches for the growth of successful urban areas. Urban areas are not static; previous generations were often fearful that urban growth would bring an unmanageable increase in population and congestion that would degrade the quality of life both downtown and in the surrounding neighborhoods. The experience of the last two decades in Silver Spring – and centuries of evolution of cities and other urban places throughout the world - have demonstrated that even intensively developed centers can accommodate more people, more development and even more parks and open spaces. The renewed interest in urban living also has shown the mutually reinforcing aspects of residential, retail, and office uses in creating what planners have taken to calling "complete communities" that provide a sense of energy and place while also reducing reliance on automobile travel and limiting the effects of growth on traffic congestion.

This Plan continues to support many of the goals emphasized by the previous plan while highlighting the importance of reducing driving in the downtown, mitigating the impacts of climate change, and celebrating the cultural diversity that is a hallmark of Silver Spring. The Plan recognizes that economic growth is promoted by zoning that allows development to respond to the market, and a strong public realm including streets, parks and open spaces that serve all users. The Plan also aims to reinforce downtown Silver Spring's identity as a regional destination for arts and culture. The Arts and Entertainment District draws people of all ages and backgrounds to the downtown for theater, music, films and art. This Sector Plan builds upon the previous plan by strengthening the existing successful components of Silver Spring, while setting forth a vision for a thriving commercial core, along with recommendations for the emerging areas of the downtown that will help Silver Spring continue to be a regional destination for the next 20 years.

In 2019, the Montgomery County Council passed the Racial Equity and Social Justice Act, which challenged the Planning Board to "consider the racial equity and social justice impact of a master plan." This Plan endeavors to understand the historic advantages and disadvantages that have affected residents in Silver Spring and include those who have been excluded from planning processes in the past. The plan has considered the impacts of the recommendations on racial equity and social justice. Equity is woven throughout the goals and recommendations of this Plan that strive to provide more equitable access to investment and civic resources.



Figure 1. "What We've Heard" Graphic

Though the COVID-19 pandemic posed unexpected challenges for engagement, the unusual circumstances allowed the planning team the opportunity to engage stakeholders in new and different ways, with an emphasis on transparency throughout the planning process. Staff connected with residents, community organizations, property owners, civic associations, condo boards, employers, business owners, students at Montgomery College, and county agency partners. Engagement was conducted primarily online and included many live, large virtual meetings, numerous small group online chats, and a web-based survey in several languages.

Plan visioning events included a series of interactive online workshops using virtual whiteboards and smaller focused virtual chats with individual planners from the team. For those who were unable to attend the live sessions, recordings and notes were made available on the website, along with an interactive online mapping tool, ReactMap, which allowed community members to submit comments about any location within the plan area. More than 150 community members participated in the visioning events and ReactMap received more than 500 comments. In the spring of 2021, the team also met with several stakeholders in person for site visits or walking tours.

Highlights of the engagement effort included attracting a mix of age ranges that reflected the plan area demographics, including a series of meetings with local Ethiopian community leaders, and the new experience of conducting an inclusive engagement process almost entirely online. Partnerships with existing community leaders proved valuable when reaching out to new groups that had not previously participated in a planning process.



Building on this engagement, the plan imagines a Silver Spring of the future that is diverse, distinctive, and thriving.

Downtown Silver Spring will continue to be a great place to work, do business, and enjoy the arts; it will remain home to small independent businesses, cutting-edge tech companies, hubs for science and research, educational institutions, and arts organizations that draw patrons from around the region. Downtown Silver Spring will maintain its uniqueness while remaining an affordable and attractive place to live for people of all ages and backgrounds. New open spaces will promote a healthier community for all who spend time in the downtown. The plan envisions an urban area that is better connected and characterized by green, climate-resilient, walkable streets that are safe and comfortable for everyone – pedestrians, bikers, transit riders, and drivers.

*Ice skating at Veterans Plaza (top),
Pride on the Plaza 2022 (middle),
United Therapeutics, Colesville Road and Spring Street (bottom).*

1.2. PLAN CONTEXT

Downtown Silver Spring is located just north of the Washington, D.C. border, centered around the intersection of Georgia Avenue and Colesville Road, and is surrounded by residential neighborhoods that are comprised primarily of single-family homes. The neighborhoods to the north, east, and west are all considered part of Silver Spring and are included within the *East Silver Spring Master Plan* and the *North and West Silver Spring Master Plan*, both from 2000.

This Plan expands the boundary from the previous Sector Plan for downtown Silver Spring to include both the area formerly known as the Central Business District and the "Adjacent Communities:" blocks from several residential neighborhoods primarily to the north and east of the downtown, located within a 10-minute/half-mile walkshed from either the Silver Spring Transit Center or the Purple Line station at the Silver Spring Library. The Plan area covers approximately 442 acres and is generally bound by Eastern Avenue to the south, 16th Street to the west, Spring Street to the north, and portions of the Seven Oaks-Evanswood and East Silver Spring neighborhoods to the east.

Intersection of East-West Highway and Colesville Road



Map 1: Plan Boundary



1.3. HISTORY OF SILVER SPRING¹

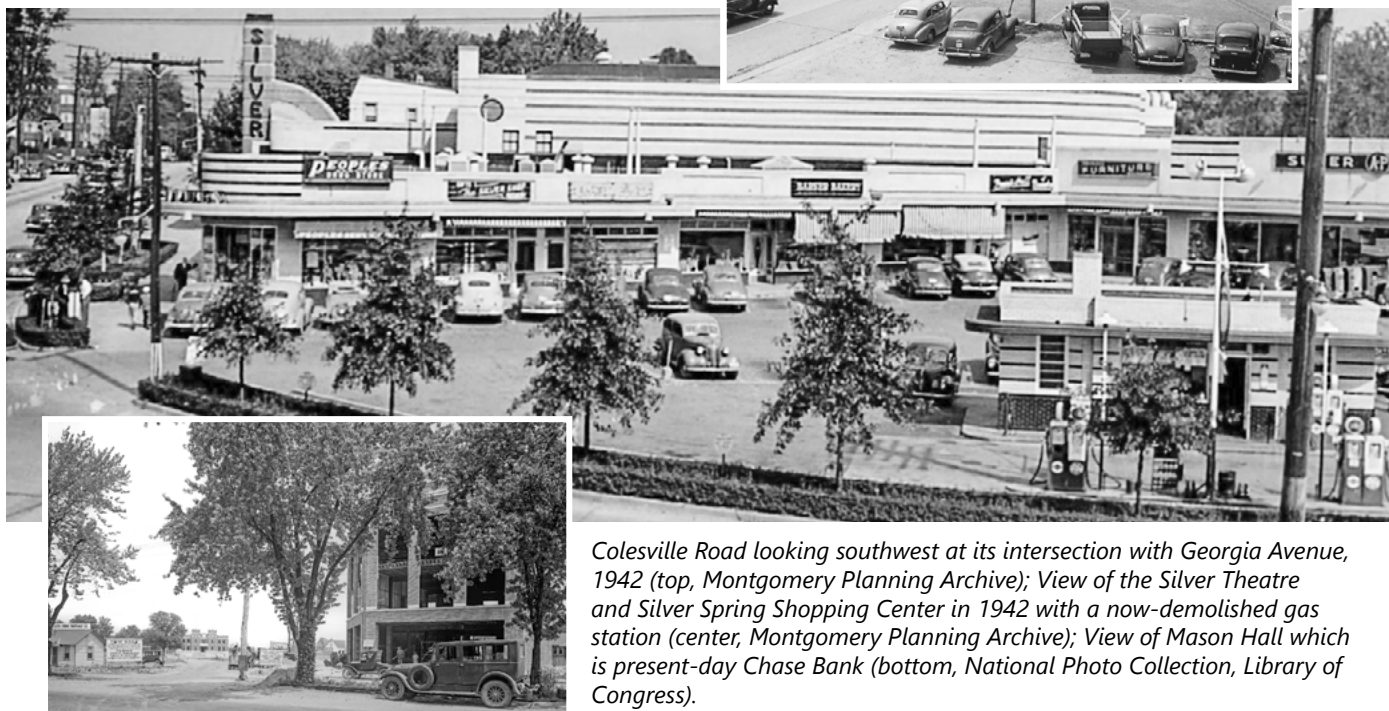
Indigenous people have lived in Montgomery County and the surrounding region for more than 10,000 years. The Piscataway and Nacotchtank tribes appear to have used the river valleys mostly for occupation and relied on the adjacent uplands for temporary camps associated with hunting and obtaining materials such as workable stone, until European colonization.

In the 1800s, Silver Spring consisted of a crossroads community and plantations, including that of Francis Preston Blair, built upon an enslaved Black labor force. The opening of the Metropolitan Branch of the Baltimore and Ohio Railroad (1873) and the Washington, Woodside, and Forest Glen Railway & Power Company (1897) allowed for more housing and commercial opportunities in the area, limited primarily to whites. At the start of the 20th century, developers of residential neighborhoods in Silver Spring often included racially restrictive covenants to bar Blacks (and people of other nationalities, ethnicities, and religious backgrounds) from the housing market. De jure and de facto segregation permitted Blacks to work in the

plan area but excluded them from owning or renting property within its boundaries.

The opening of the Silver Spring Shopping Center and Silver Theatre (1938) and Hecht's Store (1947), along with associated infrastructure improvements, transformed Silver Spring from a bedroom community to a regional retail center with offices, shops, hotels, and other businesses, and a successful manufacturing and light industrial area near the train station.

Segregation and discrimination in various forms continued to exclude non-white residents from many of the benefits of this growth.



Colesville Road looking southwest at its intersection with Georgia Avenue, 1942 (top, Montgomery Planning Archive); View of the Silver Theatre and Silver Spring Shopping Center in 1942 with a now-demolished gas station (center, Montgomery Planning Archive); View of Mason Hall which is present-day Chase Bank (bottom, National Photo Collection, Library of Congress).

¹ This narrative collates and builds upon the works of the Silver Spring Historical Society, historian David Rotenstein who explored and shed light on issues of equity in Silver Spring for the last decade, and EHT Traceries (a cultural resource management firm) which conducted a select architectural survey in 2020 and 2021 with the benefit of a Certified Local Government grant from the Maryland Historical Trust.

During the Civil Rights movement, county residents, Black and white, fought for fair access to housing and public accommodations. Notable individuals during this period include: Mary Betters who served as president of the local branch of the NAACP; J. Charles Jones who directed the Action Coordinating Committee to End Segregation in the Suburbs (ACCESS); David Scull who led the County Council to pass the Fair Housing Ordinance; Morris Milgram who built the first integrated apartment housing in greater Silver Spring; and Roscoe Nix who challenged business owners and held the county accountable for upholding its public accommodations law.



Beltway Marchers led by J. Charles Jones, head of the Action Coordinating Committee to End Segregation in the Suburbs (ACCESS), embark on a 66-mile march around the Capital Beltway in June 1966 (Washington Star Collection, D.C. Public Library).

In the 1960s, Silver Spring's dominance as a regional shopping center declined with the opening of suburban malls. However, the arrival of new industries, such as financial institutions, increased federal services and reliance on contractors, and the shift of federal office space away from Washington, D.C. led to the rebranding of the plan area as a regional office destination, with several federal agencies establishing office buildings downtown. This office space, along with ample county-owned parking and accommodating zoning led to the construction of several high-rise apartment buildings in the downtown. Office development in Silver Spring waned in the 1970s. Many pinned their hopes for the revitalization of Silver Spring on the opening of the Red Line of the Metro operated by the Washington Metropolitan Area Transit Authority (WMATA). Early plans for the line first sited the Silver Spring station on Georgia Avenue. Developers and the business community, however, successfully lobbied the County Council and WMATA to approve the station near its present-day location to service a proposed, but never realized mixed-use, high-rise redevelopment of the Falkland garden apartments. The station served

as the terminus of the line when it opened in 1978, but it failed to spur revitalization. In part, the station's isolation from the Georgia Avenue and Colesville Road intersection dampened any immediate effects on the existing commercial and business districts.

The presence of the Metro station did eventually lead to the construction of high-rise office buildings. Regional developer Lloyd Moore built 1100 Wayne Avenue, the first consequential office tower in nearly a decade, in 1983. Developers then built a series of offices within the triangular area bound by Colesville Road, Georgia Avenue, and 2nd/Wayne Avenue. This included the Metro Plaza complex completed in 1987.

The burgeoning office environment did not coincide with a revival of the local retail market. The closing of many of the mid-sized retailers followed by the Silver Spring Theatre (1984), Hecht's Department Store (1987), and J.C. Penney's (1989) left the area with a definite void. The commercial vacancies, however, were filled by the establishment of a rich, ethnically diverse, local business community that continues today.

Montgomery County and local developers started to plan for the revitalization of Silver Spring in the mid-1980s. The first two proposals, the Silver Triangle and the American Dream, failed to be realized due to community opposition, extensive litigation, and inability to secure tenants or financing. Regional developer Lloyd Moore introduced the Silver Triangle proposal that included an enclosed mall with two department stores, a hotel, and four office buildings in 1988. The Silver Spring-Takoma Traffic Coalition led by Pat Singer and historic preservationists objected to the scale, traffic congestion, and demolition of historic resources including the Silver Spring Theatre and Shopping Center required by the proposal. After years of public hearings and approvals, Montgomery County and Lloyd Moore severed an agreement to build the mall in 1995. That same year, Triple Five proposed the American Dream, an urban entertainment mall with a wave pool, ice skating rink, indoor roller coaster, IMAX, hotel, restaurants, and retailers. Montgomery County entered a memorandum of understanding with the developers, but as with the Silver Triangle proposal, the plan faced community opposition and failed to secure necessary financing.

In 1997, Foulger-Pratt and the Peterson Company proposed the successful “Town Center” plan, later renamed “Downtown Silver Spring.” The project included a retail core with street-level entrances, public plaza, civic building, office building, and parking garage. The redevelopment plan included the restoration of the Silver Spring Shopping Center and Theatre.

While there are many immigrant communities in the plan area, the Ethiopian community has had particular success in Silver Spring and the downtown is a center for Ethiopian life and culture in the region. In the late 1990s, Ethiopians moved from Washington, D.C., to Silver Spring and Alexandria, Virginia. Ethiopian food became a defining element of downtown with at least a dozen current businesses operating in Fenton Village. Other organizations such as the Ethiopian Community Center of Maryland opened an office in Silver Spring to assist immigrants and further foster a sense of community. Since 2000, revitalization efforts envisioned by the *Silver Spring Central Business District Sector Plan* have seen the redevelopment of the Downtown Silver Spring retail core, the establishment of the Arts and Entertainment District including the reopening of the historic Silver Theatre, and the opening of the Discovery corporate headquarters. With this, the former light industrial areas west of Georgia Avenue have been redeveloped with multi-family housing and the Paul Sarbanes Transit Center has been completed at the Silver Spring Metro station. The 2000 Sector Plan also recommended several civic and community facilities that draw people to downtown Silver Spring today including the Civic Building and Veterans Plaza and the Silver Spring Library. In the coming years, both this Plan and the Purple Line light-rail system connecting Silver Spring to Bethesda and New Carrollton will create new opportunities for growth and development in the downtown.

Aerial view of Silver Spring looking north, 1964. Georgia Avenue is the arterial road in the center of the photograph. Visible is the present-day Georgia Avenue railroad underpass constructed in 1948. (Montgomery Planning Archives)





Historical photograph of Georgia Avenue, looking north from intersection with Bonifant Street (top, National Archives); current photo of Georgia Avenue looking north from Wayne Avenue (bottom).

1.4. CONCEPT FRAMEWORK

At the close of the year-long engagement and visioning effort with stakeholders, the Concept Framework Plan was developed to identify the basic components of the present-day urban fabric in downtown Silver Spring along with initial concepts for the future including:

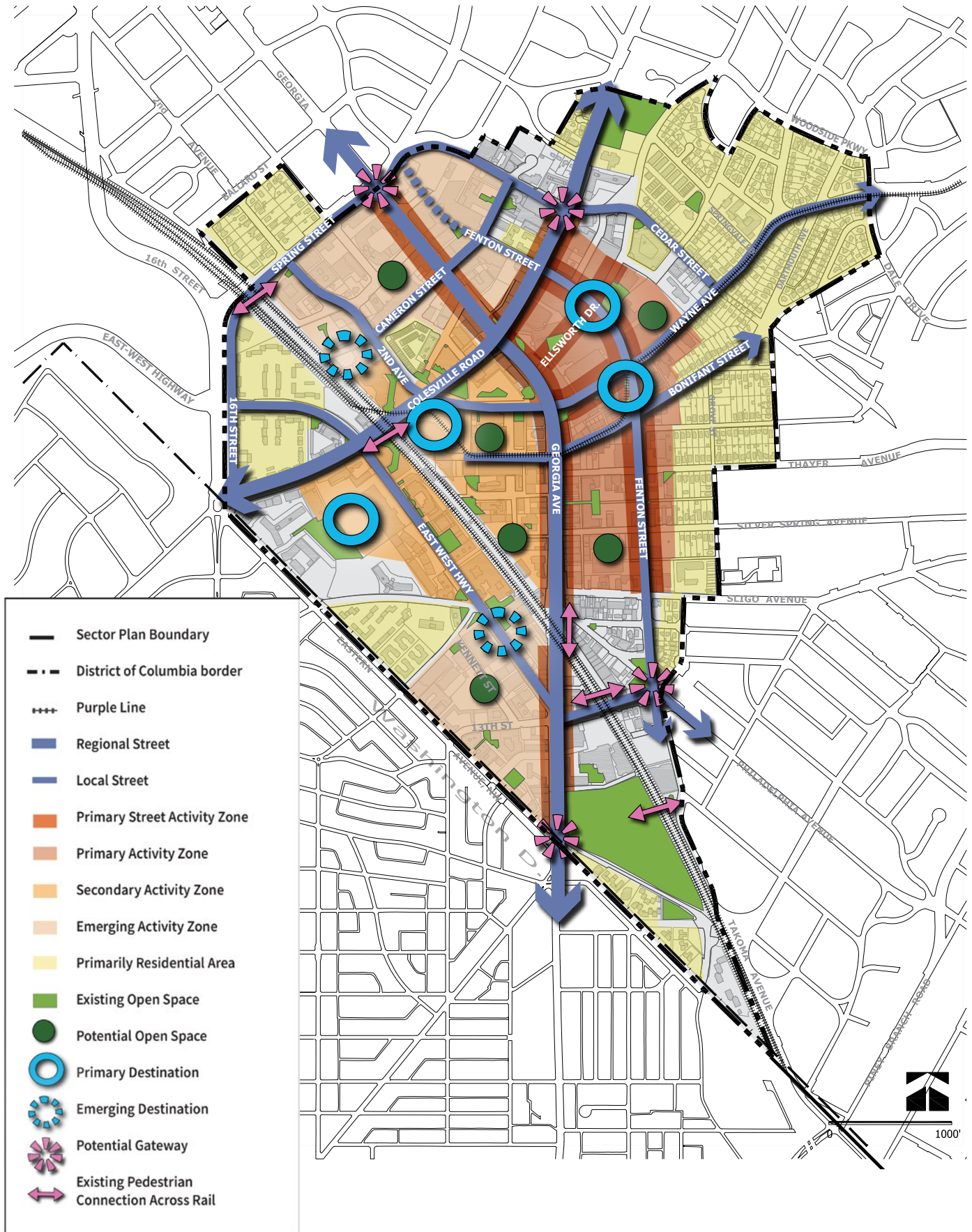
- Activity zones
- Residential areas
- Destinations
- Parks and open spaces

The Concept Framework Plan identifies the primary activity zone in downtown Silver Spring centered on Ellsworth Street, Fenton Street, and Fenton Village. Even though the transit center and the mixed-use development at the Blairs are identified as existing destinations, the areas surrounding these nodes are not as vibrant as the primary zone. In addition, the Concept Framework Plan addresses the challenge of improving existing connections across the Metro system, and the opportunity to create gateway experiences into Silver Spring, particularly at the border with Washington, D.C. The recommendations in this Plan strive to address these challenges and support the emerging areas of downtown Silver Spring in meeting their full potential.

Public engagement at Bike to Work Day in Veterans Plaza, 2022



Map 2: Concept Framework Plan



1.5. PLAN GOALS

The Plan is focused on strengthening the existing success of Silver Spring and supporting growth and development in the emerging areas of the downtown. The themes of diversity, connectivity, resiliency and community health emerged as key values of the Plan, and all Plan goals and recommendations pursue equitable economic and civic outcomes by supporting one or more of these themes:

DIVERSITY

- Strengthen the unique retail market in Silver Spring with its diverse mix of independent stores, restaurants, and arts and entertainment experiences.
- Provide more diverse housing types and preserve existing affordable units to serve individuals and families of all ages, incomes, and backgrounds.
- Celebrate the past, present, and future of the diverse cultures that are part of the Silver Spring community.

CONNECTIVITY

- Enhance and reinforce the pedestrian and bicycle networks throughout the Plan area to reduce automobile dependence.
- Create a green network of priority streets and public open spaces that connects neighborhoods and meets the needs of both people and the environment.
- Safely connect communities across the Metrorail tracks to increase accessibility to all downtown neighborhoods.

RESILIENCY

- Support sustainable and lasting economic growth and resiliency in and around the commercial core through flexible zoning, development incentives, and public realm improvements.
- Reduce impacts of climate change through sustainable strategies for buildings and streetscapes.

COMMUNITY HEALTH

- Create more active recreation opportunities through parks and public spaces, as well as opportunities for social gathering and enjoyment of nature.
- Provide a healthier, cooler, more comfortable pedestrian experience along streets and in open spaces.



1.6. SUMMARY OF RECOMMENDATIONS

- Establish the Civic Improvement Fund (CIF) to allow all Commercial/Residential (CR) zoned properties to obtain additional density if needed to meet maximum building heights. The fund would support a world-class arrival experience at the transit center, a new bridge connection across the rail tracks, and strategic civic and streetscape improvements.
- Increase heights throughout the downtown to encourage redevelopment.
- Implement a Design Advisory Panel and design review for all Optional Method development projects.
- A vision and recommendations for each district to redevelop key sites and build on its unique character and assets.
- A Green Loop to increase connectivity by expanding and unifying access for all residents to green, safe, and inviting sidewalks, bikeways, parks, and open spaces.
- Create numerous new and enhanced parks and open spaces, including Jesup Blair Park.
- Support expansion of bio-science, technology, and education uses to increase employment opportunities in the downtown.
- Update the zoning code to create flexibility for future development, to increase affordable housing, support small business growth, and realize the Sector Plan goals.

8250 Georgia Avenue



View of construction in the Ripley District, from Georgia Avenue.

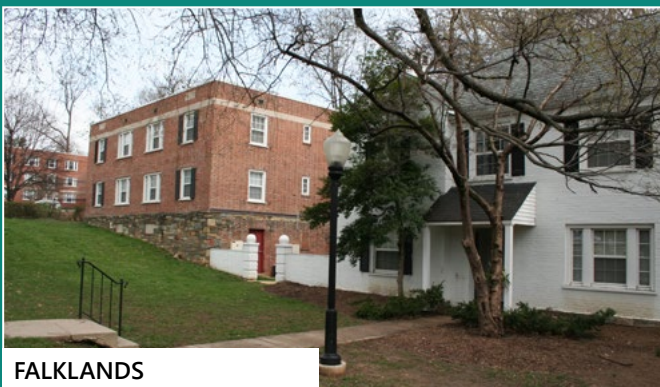
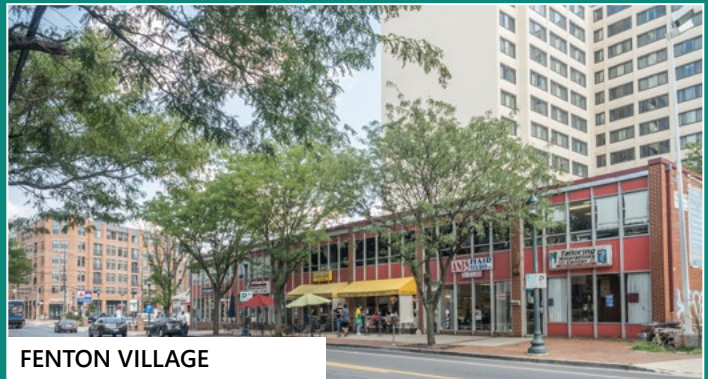


2 Districts

The Plan is divided into eight districts, each with its own unique vision, goals and recommendations.

2.1. DISTRICT VISIONS

Silver Spring is home to several distinct neighborhoods within the downtown and the surrounding residential area. Each neighborhood, or district, has its own unique characteristics that contribute to the diversity of Silver Spring. The Plan has an individual vision for each district that, when linked together, comprise the overarching vision for Silver Spring in 2040 described in Section 1.1.

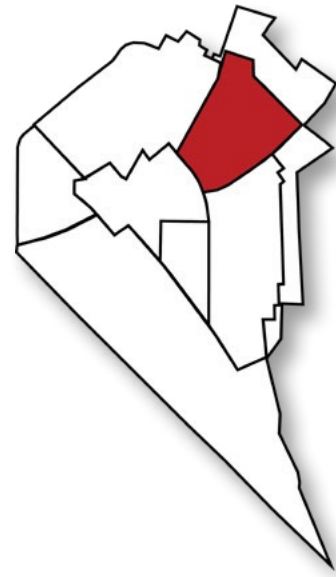


Map 3: Plan Districts



ELLSWORTH

The Ellsworth District is the heart of Silver Spring. It is the primary activity center where people from all over the county and the region come to shop, eat, enjoy entertainment, and just hang out. Veterans Plaza and Ellsworth Place are some of the most successful public spaces in the area; festivals, music events, and a farmers' market regularly take place here.



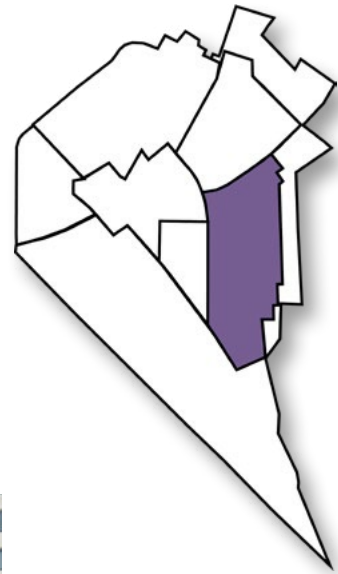
The Ellsworth District will continue to be the active heart of downtown Silver Spring, with a greener public realm and exciting flexible public spaces that connect Georgia Avenue to Cedar Street.



Ellsworth Drive at Fenton Street (top), event at Veterans Plaza (bottom).

FENTON VILLAGE

Fenton Village is a much-loved destination in Silver Spring — it is the place in the downtown that best celebrates the diversity that is one of the hallmarks of this urban area. It is characterized by smaller scale retail, a mix of residential types, and a strong streetscape.



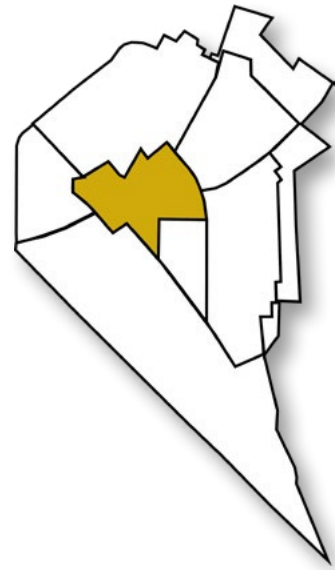
Fenton Village will build on and sustain its diversity with new community open spaces, new development at an appropriate scale to support small businesses, and an expanded presence for the arts.



*Local businesses on Fenton Street (top),
Storefronts on Georgia Avenue (bottom).*

METRO CENTER

The area around the Paul S. Sarbanes Transit Center is the commercial center of Silver Spring and will continue to host some of the tallest buildings and the densest development in the downtown.



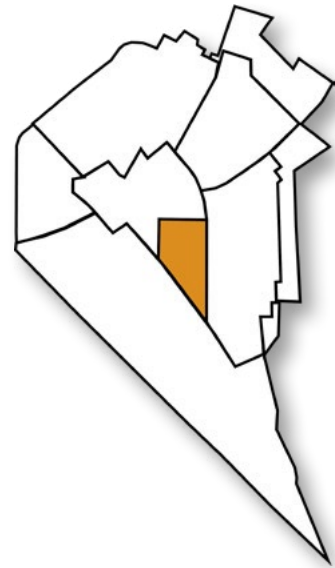
The Metro Center District will welcome visitors with a reimagined hub of activity with the highest-intensity commercial development in the downtown, world-class public space, and safe and inviting connections to surrounding districts.



*Upper level of the Transit Center (top),
Inventa Towers (bottom).*

RIPLEY

Over the last decade the Ripley area has emerged as a micro-neighborhood within the downtown. This is where people live to be close to everything, with parcels under development today and several more key opportunity sites.



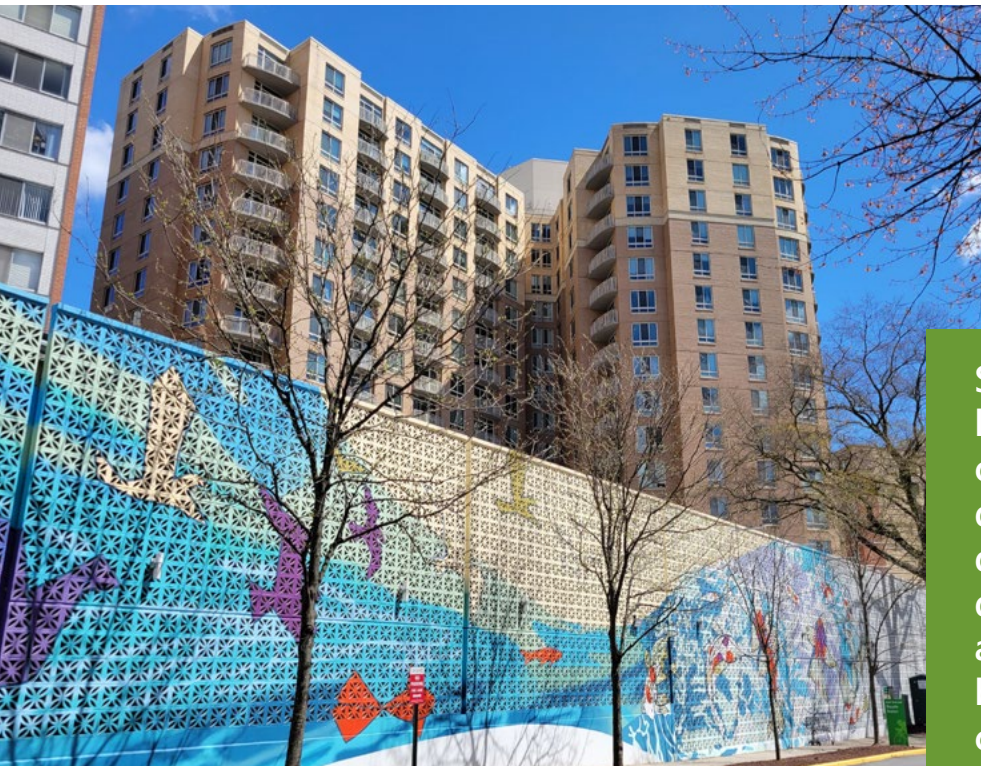
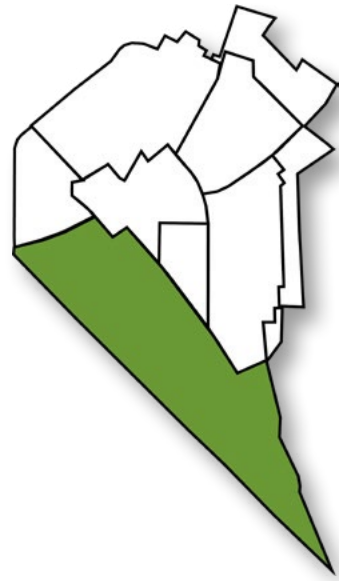
Ripley will be a vibrant complete multi-family residential district steps from the Metro, Purple Line, and Metropolitan Branch Trail, with a new central open space and a pedestrian connection to South Silver Spring across the Metrorail/CSX tracks.



View of the Ripley District from Georgia Avenue (top), Bonifant-Dixon Garage as it crosses over Bonifant Street (bottom).

SOUTH SILVER SPRING

South Silver Spring is a large district with a great diversity of housing types, commercial and educational uses, and public spaces, including Jesup Blair Park. The rail tracks separate this district from the rest of downtown.



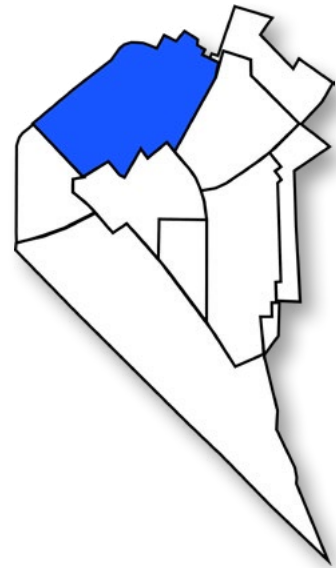
South Silver Spring will leverage its unique array of assets to become a destination for mixed-use development to serve its diverse community, and an enhanced connector between the developing centers along Georgia Avenue, from the Walter Reed Campus in D.C. to the core of downtown Silver Spring. South Silver Spring will become a destination within the downtown, with the renovation of Jesup Blair Park and the opportunity for Montgomery College to expand its presence in this neighborhood.



The Blairs (top), Acorn Park (bottom).

DOWNTOWN NORTH

The area north of Colesville Road includes a mix of housing types, commercial, institutional, and civic uses, as well as retail and entertainment uses, including the Fillmore concert venue.



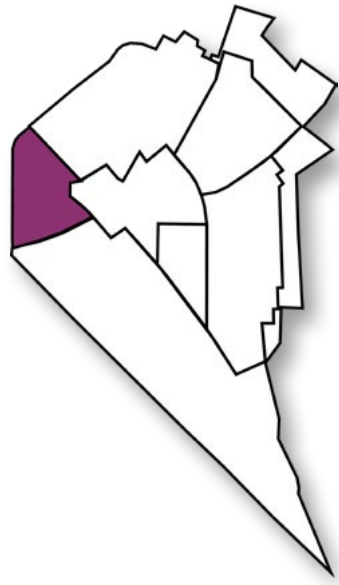
Downtown North will build on the connection between the state-of-the-art South County Regional Recreation and Aquatic Center and the world-renowned United Therapeutics campus to create a unique district focused on health and health sciences.



United Therapeutics campus facing Spring Street (top), Tastee Diner on Cameron Street (bottom).

FALKLANDS

The historic Falklands apartments hold a foundational place in the development of Silver Spring and have provided first homes to generations of residents.



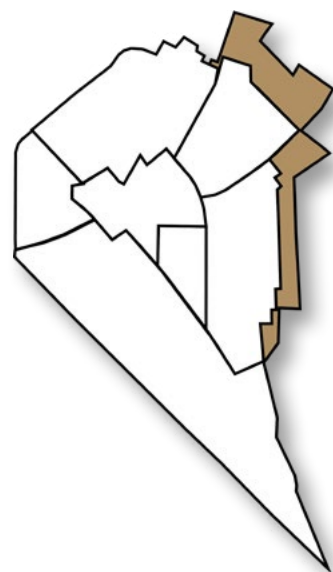
The Falklands District will retain its historic fabric of affordable housing south of East-West Highway while embracing the opportunity for higher-density mixed-use development north of East-West Highway.



Photos of the Falkland Chase apartments

ADJACENT COMMUNITIES

These blocks include portions from the established neighborhoods of Woodside Park, Seven Oaks-Evanswood and East Silver Spring.



Consistent with the Plan goal of housing diversity, the Adjacent Communities can include a greater variety of housing types, fully integrated into the existing neighborhood fabric, to allow a wider range of residents to enjoy the valued proximity to the downtown and nearby transit options.



*Images from the Adjacent Communities.
Ellsworth Drive (top), Easley Street (bottom)*

2.2. CONNECTING THE DISTRICTS

While each district has its own distinct vision and features, together they form the thriving urban area of downtown Silver Spring. As each district offers different opportunities to experience Silver Spring, a strong, well-connected, green, comfortable, and safe public space network that connects the districts is essential to the success of this downtown.

2.2.1. Goal

Connect the neighborhoods of Silver Spring via a green, resilient, comfortable, consistent multi-modal network, focused primarily on local streets that will improve quality of life, offer opportunities for building community, create an attractive environment for businesses and employers, and improve the environmental resiliency of a dense urban area and support walking and biking around the downtown. Pursue opportunities to provide connections across the Metrorail/CSX tracks.

2.2.2. The Green Loop

Recommendation:

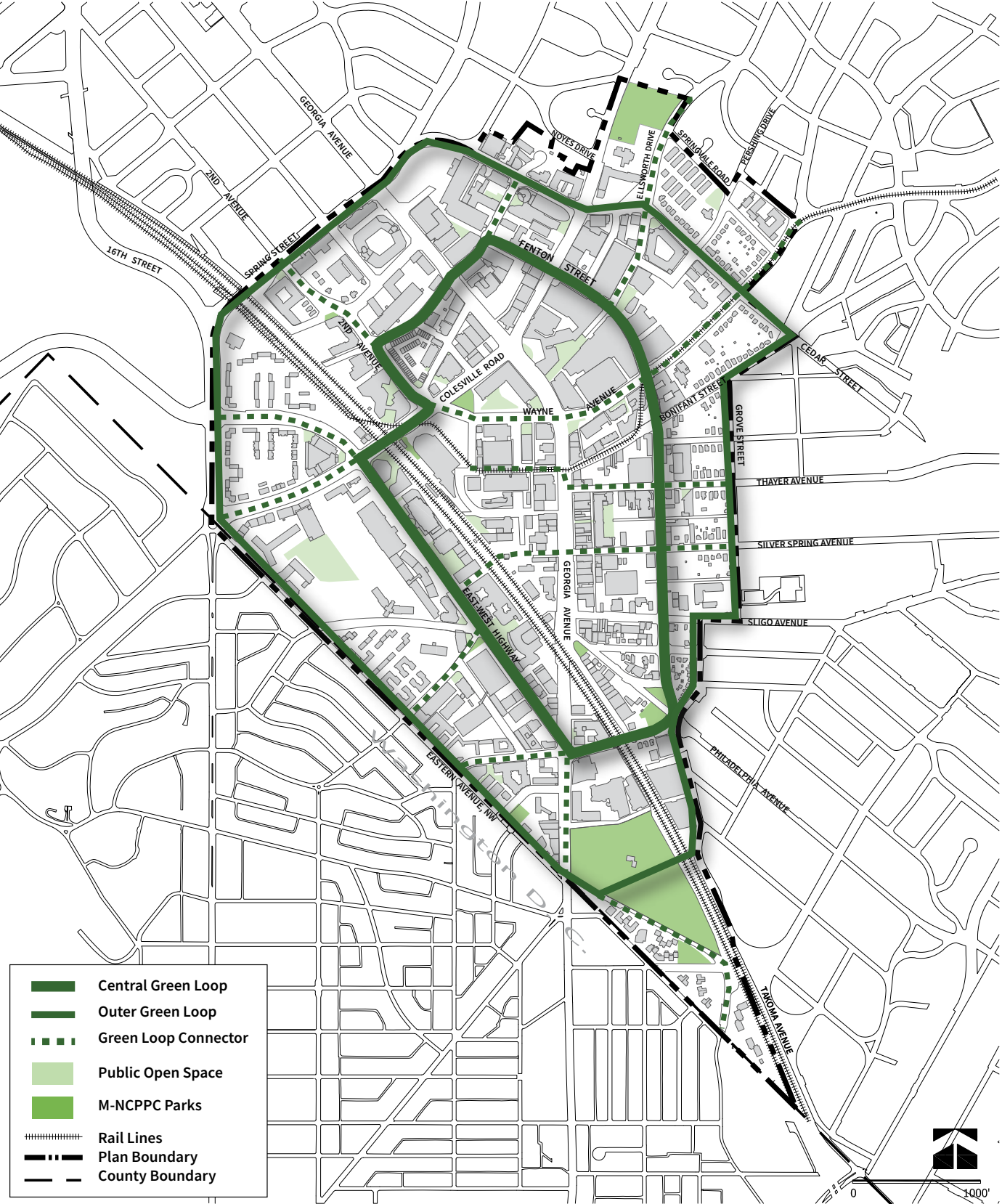
The Green Loop is comprised of a Central Loop, an Outer Loop, and “connectors” that bridge between the two loops. The Central Loop is the primary component of the Green Loop. It connects Fenton Street, Cameron Street, 2nd Avenue, East-West Highway and Burlington Ave in a “loop” that moves through and around the central districts in the Plan. Many of the primary public spaces in Silver Spring are located along the Central Loop.

At the periphery of the Plan area is a secondary Outer Loop. This loop will primarily serve those who are coming to the downtown from the surrounding neighborhoods and is connected to the majority of the peripheral parks and open spaces of Silver Spring.

Linking the Central and Outer Loops are Connectors that allow a smooth transition between the two loops by providing multi-modal streets that are shaded and comfortable for residents, workers, and visitors to access parks and public spaces in downtown Silver Spring.

The Green Loop supports the Plan themes of diversity, connectivity, resiliency and community health. This system will connect the unique neighborhoods of the downtown, and link between existing and proposed parks and open spaces. The Green Loop will promote walking and biking as accessible and healthy ways to travel in and around downtown Silver Spring. A safe and continuous pedestrian and bicycle network connecting to numerous key opportunity sites will help support future development. In addition, the green infrastructure within the right-of-way will help mitigate impacts of climate change and contribute to cooler streets in the downtown.

Map 4. The Green Loop



Elements

The Plan recommends the following elements for incorporation into the Green Loop. As not all rights-of-way will be able to accommodate all of the elements listed below, elements should be prioritized consistent with the recommendations for streets found in Section 3.6 Transportation and the environmental recommendations in Section 3.7 Resilient Downtown.

- Sidewalks
- Bicycle lanes
- Canopy trees with deep soil panels
- Stormwater management within the right-of-way
- Cool surfaces
- Native, stratified and drought-tolerant plant vegetation
- Wayfinding
- Smart street elements including but not limited to:
 - Electric vehicle charging stations
 - Solar metering and lighting

The Design Guidelines that will accompany the Sector Plan will include Cool Streets Recommendations for several elements above, including cool surfaces and stormwater management practices within a right-of-way.



East-West Highway (left), Fenton Street (right)



Figure 2. Key Components of the Green Loop
 Sidewalks, canopy street trees, separated bike lanes, stormwater treatment and solar-powered street lighting.

2.2.3. Connecting Across the Rail

The Silver Spring Metro Station is one of the busiest on the Red Line. The CSX tracks transport goods through the region and are shared by MARC and Amtrak. However, while the Metro station is integral to the success of this downtown, the rail tracks are a barrier that divides Silver Spring, making it difficult to connect South Silver Spring to the rest of the districts via a pedestrian network.

Currently there are five points where a pedestrian can cross over or under the rail, but those points are either at the transit center at Colesville Road, or at the southern end of the downtown, and include existing connections that many users experience as unsafe. To truly connect the different neighborhoods of Silver Spring, connecting across the railroad must be easier and safer. The Plan recommends both new connections and improvements to existing connections.

Existing Connections

Recommendations:

- Ensure accessibility and mobility for pedestrians (of all abilities) and bicyclists should any bridge require reconstruction or substantial repair in the future. Reconstruction and replacement presents a new opportunity to improve access, comfort and safety.
- Spring Street Bridge:
 - Provide one-way separated bicycle lanes on each side of the bridge per the Bicycle Master Plan.
- Georgia Avenue Underpass:
 - Provide additional lighting in the pedestrian tunnel so pedestrians can see all the way to the end of the tunnel even after dark.
 - Consider additional engaging contributions from local artists along the wall of the tunnel.
 - Evaluate the tunnel for other safety improvements, such as rear-view mirrors or other visual aids.
- Burlington Avenue Bridge:
 - Provide a wider sidewalk on both the north and south sides of the street to increase pedestrian safety at this crossing.
- Colesville Road Underpass:
 - Provide additional lighting on both the north and south sides of the Colesville Road underpass to increase safety and visibility.
 - Enhance the underpasses with art from local artists and/or wayfinding concepts that will transform these tunnel-like connections into key components of the Metro Center district pedestrian network and the arrival experience at the transit center.
- Pedestrian bridge at Montgomery College/Jesup Blair Park
 - Should the Montgomery College bridge ever be reconstructed, it should be redesigned and built to allow the use of bicycles for adequate access to the Metropolitan Branch Trail and upcoming Fenton Street separated bike lanes.

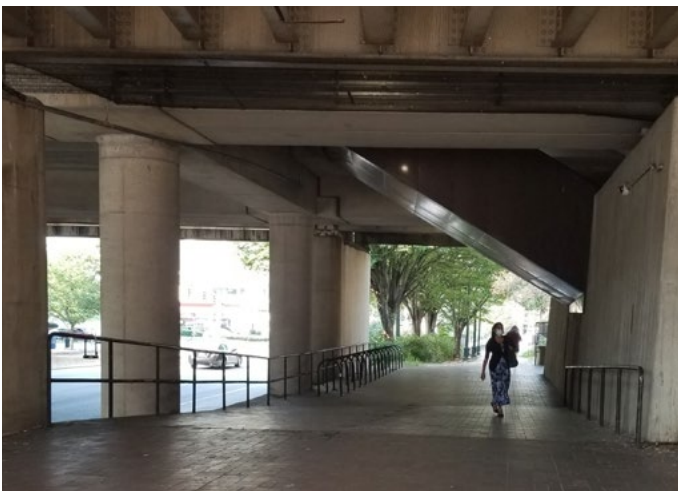
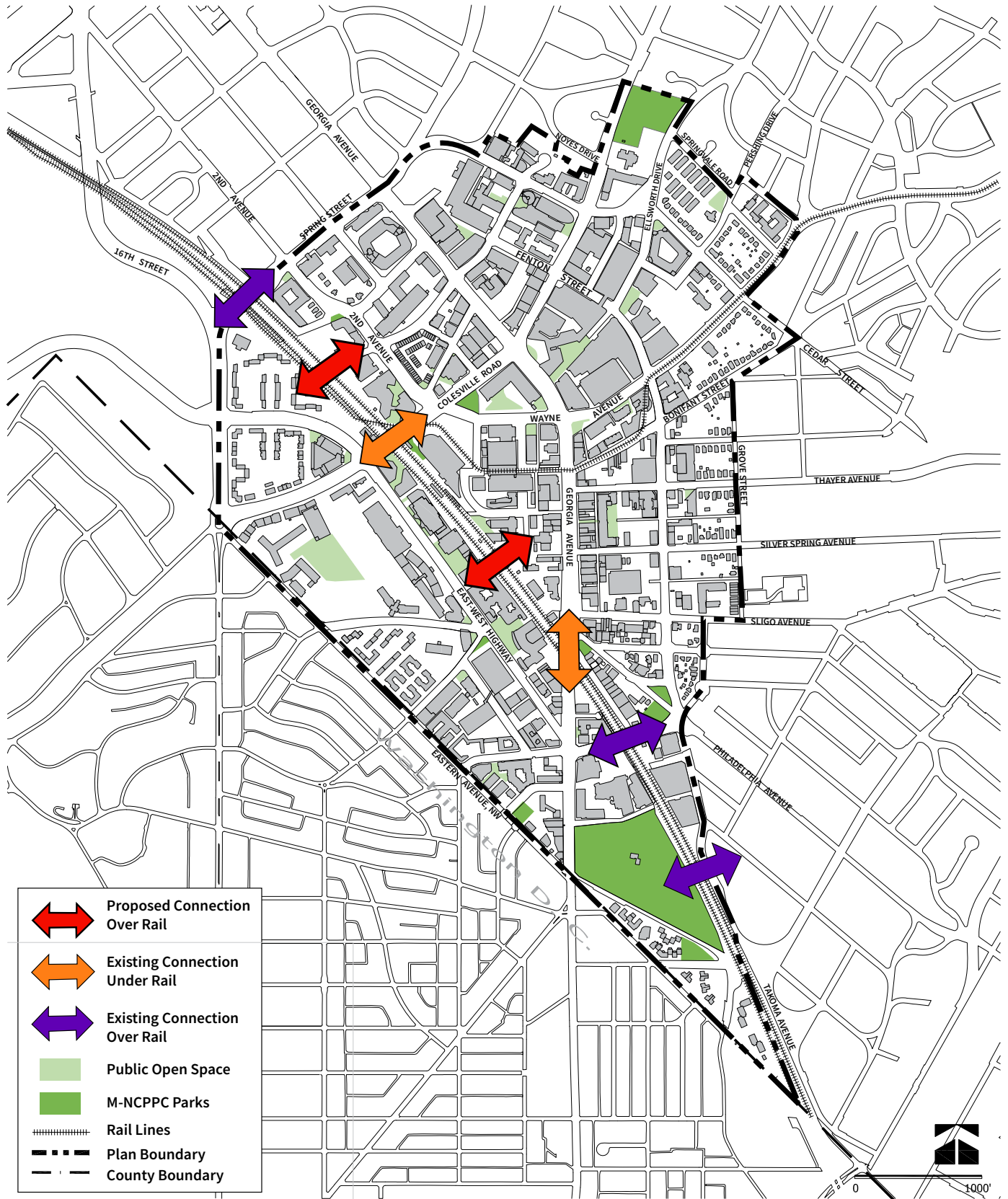


Figure 3. Existing Connections Under the Rail
Colesville Road Underpass (left), Georgia Avenue Underpass (right)

Map 5: New and Existing Connections Across the Rail



New Connections

Recommendations:

- All new rail connections (over or under) should be designed with best practices for access, comfort, safety, and mobility to address the needs of bicyclists and pedestrians who walk and/or roll.
- Provide a new pedestrian and bicycle connection across the rail from East-West Highway to the western terminus of Silver Spring Avenue in the Ripley District.
- The Plan proposes this new connection be located between 1215 East-West Highway (The Bennington) and 1201 East-West Highway (The Silverton), crossing the rail and connecting on the east side of the rail at the intersection of Dixon Street and Silver Spring Avenue in the Ripley District. The Plan envisions a landscaped bridge connection with adequate shade and views to the north and south. The bridge would land at an open space in the Ripley District that connects to Silver Spring Avenue. This connection would provide easy and safe access from South Silver Spring to the Metro Center area or Fenton Village. This bridge would also provide a connection to the Metropolitan Branch Trail, which provides a bicycle connection to Union Station in Washington, D.C. to the south and will connect with the Capital Crescent Trail at the Transit Center to the north of this connection.
- The Plan also recommends that if the northern Falklands parcel is redeveloped as a mixed-use site, consider the feasibility of creating a connection across the rail from the Falklands District to the Downtown North District.
- At the time of redevelopment, property adjacent to any proposed new connection shall coordinate with the Montgomery County Department of Transportation (MCDOT) to define the right-of-way or easement requirements to achieve the connection in the event of a publicly built connection, such as ensuring unfettered public access and limiting physical, visual, and noise encroachment. Should the connection be built outside of the Capital Improvement Program, conditions of approval may stipulate hours of access, ease of navigation and convenience, ADA accessibility, visibility and obviousness of the route, wayfinding, etc. In the event the applicant is not constructing the connection, the applicant must work with MCDOT to define what preparations need to be made to the site such that the connection can be constructed in the future. This may include but is not limited to defined stubs that can be later expanded into a future tunnel or bridge.



Figure 4. Proposed Connection from East-West Highway to Ripley District



Figure 5. Crossing Over the Rail
 Park Union Bridge in Colorado Springs (top - www.dezeen.com); conceptual image of new proposed bridge connection in Silver Spring (bottom).

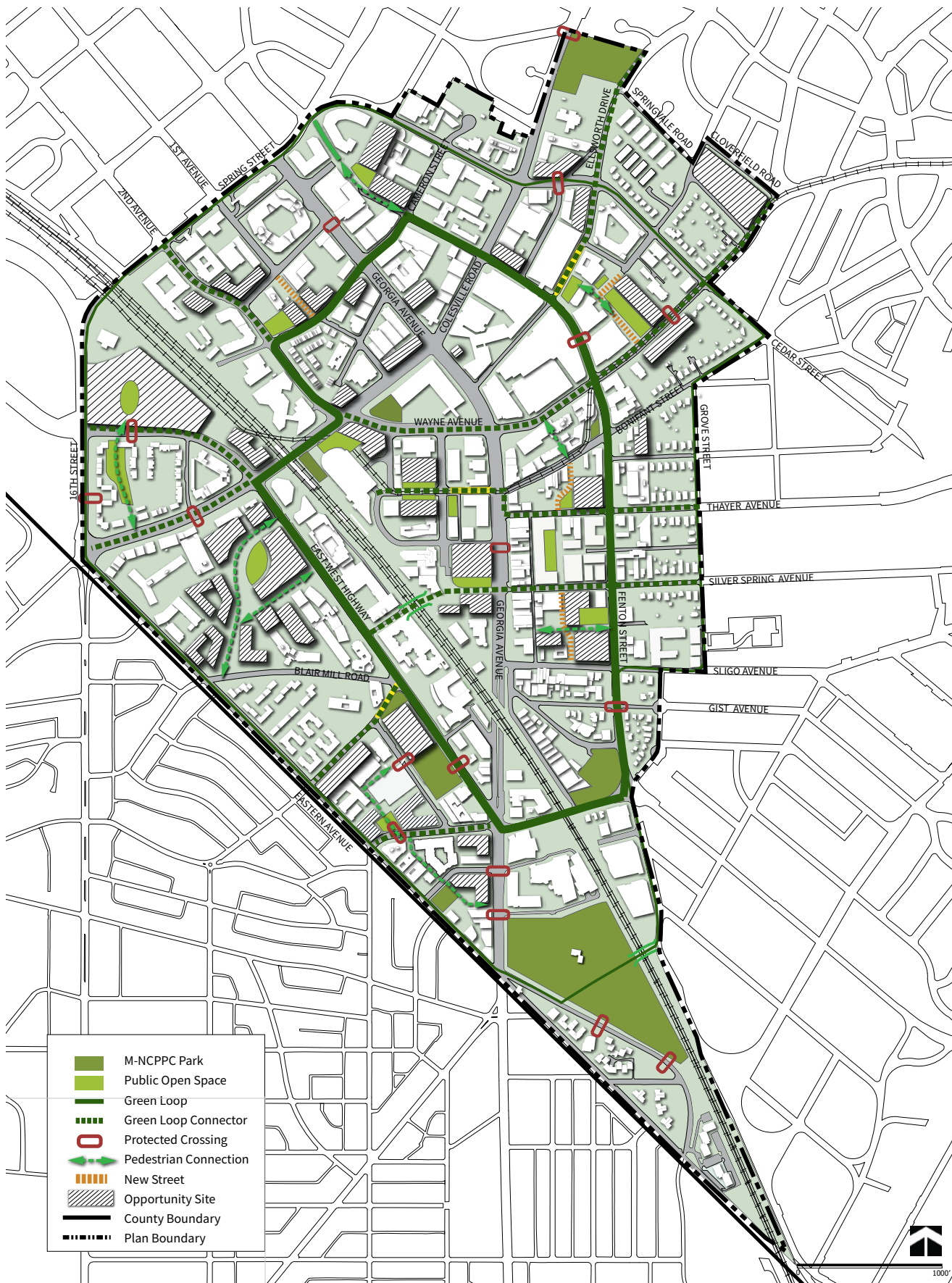


Figure 6. Urban Design Illustrative Diagram

2.3. DISTRICT RECOMMENDATIONS

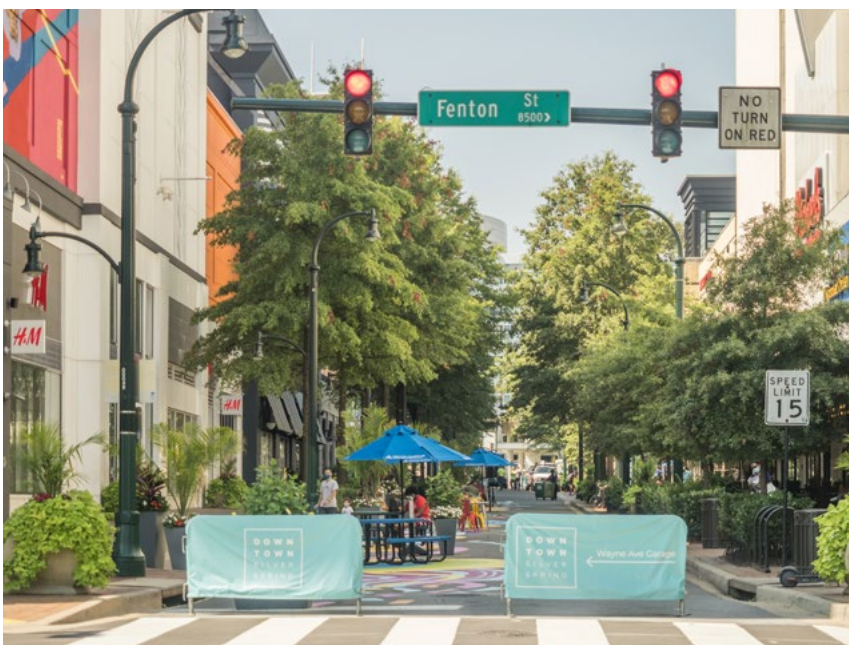
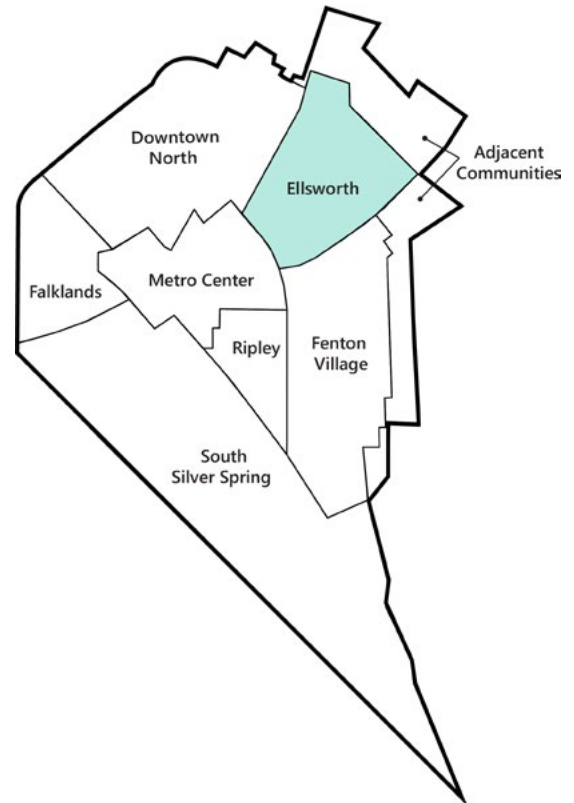
2.3.1. Ellsworth District

Vision

The Ellsworth District will continue to be the active heart of downtown Silver Spring, with a greener public realm and exciting flexible public spaces that connect Georgia Avenue to Cedar Street.

Goals:

- New development should strengthen the Civic Building and Veterans Plaza area as a primary center of activity and gathering for residents and visitors to Silver Spring.
- Improve the pedestrian connection to Ellsworth Place from the intersection of Colesville Road and Georgia Avenue.
- Improve the existing public spaces and pedestrian realm by expanding the area where vehicle access is limited.
- Provide additional green public space through new development opportunities.
- Redevelop remaining opportunity sites in the district, maintaining the predominant retail and commercial development pattern.



Ellsworth Drive at Fenton Street (left), Whole Foods (right)

Recommendations:

Urban Design

- New tall buildings along Georgia Avenue and Colesville Road should have a clearly differentiated base that relates to the pedestrian scale, with substantial variation in the building massing, facade and materials. Towers should step back above the base along streets, open spaces and through-block connections in a way that distinctly differentiates the tower from the building base. Refer to the Design Guidelines that accompany this Plan.
- Establish Ellsworth Drive between Fenton Street and the exit from the Towne Square Garage as a commercial shared street that can be closed for a farmer's market and other festivals and outdoor events as an expansion of Veterans Plaza. Vehicle traffic would be limited to loading and emergency vehicles, and street parking could be removed or limited.

Parks and Public Spaces

- Support continued work with the Maryland Historical Trust on appropriate improvements to the parking lot at the Silver Spring Shopping Center to better draw pedestrians through the lot into the Ellsworth Place pedestrian mall.
- Provide a Neighborhood Green with the redevelopment of the parking lot at the Whole Foods/Ace Hardware retail development.



St. Michaels Catholic Church on Wayne Avenue

Opportunity Sites

- *Parking Lot at Whole Foods/Ace Hardware:* The Plan recommends redeveloping this surface parking lot with a mix of uses, structured parking, and a green open space connecting Veterans Plaza to Wayne Avenue.
- *8807 Colesville Road:* The Plan recommends redevelopment of this gateway site with the greatest height along Colesville Road and stepping down along Spring Street towards Ellsworth Drive. Redevelopment of this site should consider the views of this site from Colesville Road and Spring Street.
- *Ellsworth Drive and Spring Street:* The former home of the National Ready Mixed Concrete Association, this now-vacant site will partner with the Citron multi-family development as the residential gateway for the district. The scale of the development should respond to both the taller buildings on the west side of Spring Street as well as the single-family buildings across the intersection.
- *8551 Fenton Street:* Refer to Section 3.9.3 in the Historic Resources section for additional information about this property.
- *Parking Lot at Silver Spring Shopping Center:* Refer to Section 3.9.2 in the Historic Resources section for an appropriate approach for improving this parking lot.

Zoning

- Equalize commercial and residential density values in CR zones for maximum flexibility in future redevelopment.
- Rezone existing EOF properties to CR.



Figure 7. Ellsworth District Illustrative Diagram

Map 6: Recommended Ellsworth District Zoning



Table 1. Recommended Ellsworth District Zoning

Map Number	Existing Zoning	Recommended Zoning	Justification
1A	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-300	Increase flexibility for future mixed-use development.
1B	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-300	Increase flexibility for future mixed-use development.
1C	CR-3.0 C-2.0 R-2.75 H-145 T	CR-3.0 C-3.0 R-3.0 H-260	Increase flexibility for future mixed-use development.
1D	CR-3.0 C-2.0 R-2.75 H-145 T	CR-6.0 C-6.0 R-6.0 H-260	Proposed zoning brings parcel into conformance.
2A	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
2B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-6.5 C-6.5 R-6.5 H-260	Proposed zoning brings parcel into conformance.
2C	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
3A	EOF 3.0 H-100	CR-3.0 C-3.0 R-3.0 H-150	Update to CR zone from pre-2014 EOF zone and increased height for flexibility.
3B	EOF 3.0 H-100	CR-3.0 C-3.0 R-3.0 H-100	Update to CR zone from pre-2014 EOF zone.
4A	CR-5.0 C-4.0 R-4.75 H-75 T	CR-5.0 C-5.0 R-5.0 H-165	Increase flexibility for future mixed-use development.
4B	CR-3.0 C-2.0 R-2.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
4C	CR-5.0 C-4.0 R-4.75 H-75 T	CR-5.0 C-5.0 R-5.0 H-135	Increase flexibility for future mixed-use development.
5	CR-1.5 C-1.0 R-1.5 H-60 T	CR-1.5 C-1.5 R-1.5 H-70	Proposed zoning brings parcel into conformance.
6A	CR-1.5 C-1.0 R-1.5 H-90 T	CR-1.5 C-1.5 R-1.5 H-110	Increase flexibility for future redevelopment.
6B	CR-3.0 C-2.0 R-2.75 H-145 T	CR-3.0 C-3.0 R-3.0 H-175	Increase flexibility for future mixed-use development.
6C	CR-1.5 C-1.0 R-1.5 H-60 T	CR-1.5 C-1.5 R-1.5 H-70	Increase flexibility for future mixed-use development.

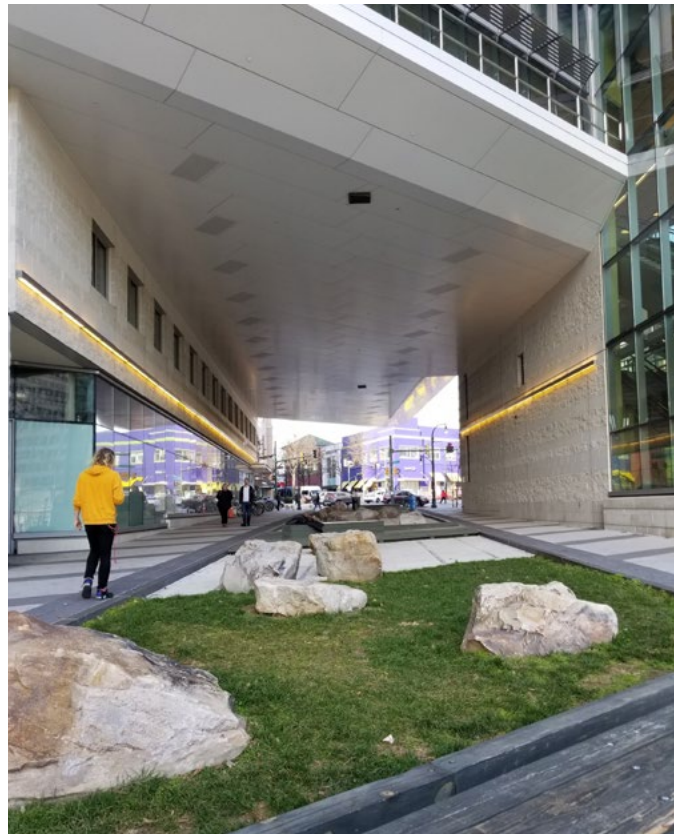
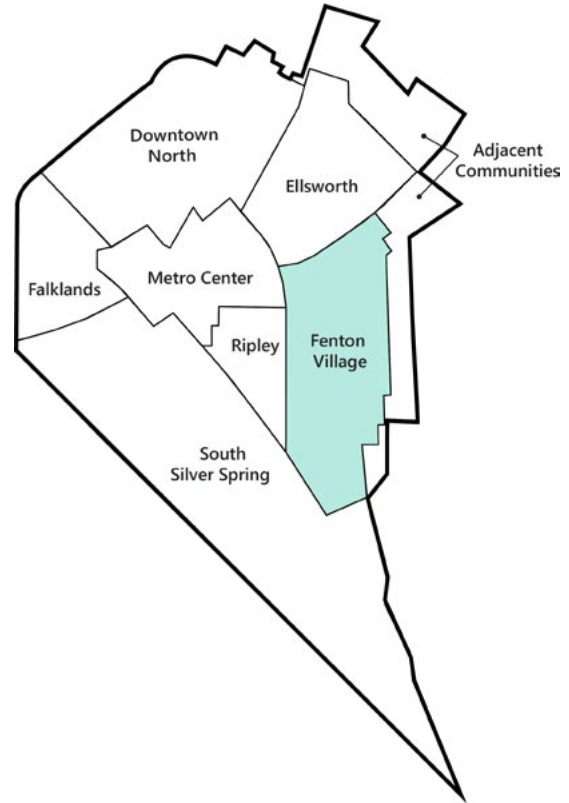
2.3.2. Fenton Village District

Vision:

Fenton Village will build on and sustain its diversity with new community open space, new development at an appropriate scale to support small businesses, and an expanded presence for the arts.

Goals:

- Maintain zoning that provides low-rise development on Georgia Avenue and Fenton Street, and taller development in the middle of the block.
- Provide a compatible transition in building form and height from Fenton Village to East Silver Spring.
- Redevelop opportunity sites with mixed-use development with retail or commercial spaces on the ground floor appropriate for local and independent retailers.
- Enhance Fenton Street as the main street for the district by preserving and improving opportunities for both active retail and arts and entertainment venues and organizations.
- Retain a full-service grocery store in the district.
- Promote the redevelopment of public parking lots and garages in collaboration with the Parking Lot District's goals.
- Encourage residential development that provides a mix of unit types and sizes for people of all ages, levels of income and household size.
- Provide new outdoor community gathering space in Fenton Village and link this new open space to the Green Loop segment along Fenton Street.
- Build on relationships with nearby Artspace Silver Spring and Montgomery College to further expand opportunities for public art and arts-related uses.



Future Purple Line Station at the Brigadier General Charles E. McGee Library



Independent businesses in Fenton Village

Recommendations:

Urban Design

- Consistent with the Fenton Village Overlay Zone, buildings should step back above the base to maintain a low-rise character along the street.
- Retail bays should be small enough to house small local retailers to preserve the economic diversity typical of Fenton Village.
- New development interior to the blocks between Fenton Street and Georgia Avenue should be composed of buildings that are divided into smaller components, instead of one large, monolithic structure.
- Transform Bonifant Street into a pedestrian-friendly retail corridor along the Purple Line, with sidewalk cafes and street trees.
- Implement the Green Loop on Fenton Street.
- Provide public through-block connections throughout the district to enhance walkability and connectivity within long or large blocks.

Parks and Public Spaces

- Expand the existing Fenton Street Urban Park into a cohesive neighborhood gateway park that is directly connected to the Green Loop, should property in this area become available. Additional facilities should encourage active recreation and social gathering.
- Provide a consolidated, green community-focused space of approximately half an acre but no less than one-third of an acre along Fenton Street as part of any future redevelopment of Public Parking Garage 4 and adjacent sites.

Opportunity Sites

- *Public Parking Garage 4:* Encourage the redevelopment of Parking Garage 4 and surrounding properties through a public-private partnership with the Parking Lot District. If the redevelopment of this site includes the removal of the existing parking structure, the Plan recommends that this large block be divided via a new north-south connection that accommodates both pedestrians and vehicles. This connection could provide loading and service connections for the new development. In addition, the Plan recommends an east-west through-block pedestrian connection along with redevelopment. A consolidated green

community-focused open space of approximately half an acre but no less than one-third of an acre should be located at this site, fronting on Fenton Street. This open space could be an opportunity to celebrate local artists and the diversity of Fenton Village. This site is also large enough that it may provide a unique opportunity to consider urban agriculture facilities, either at the ground or as part of a green roof concept.

- *County Parking Lot 29:* The Plan recommends redevelopment of this surface parking lot with a mix of uses compatible with the adjacent residential development.
- *Safeway grocery site and adjacent parcels:* The Plan recommends the redevelopment of the existing Safeway grocery site and the adjoining sites including County Parking Lot 38 for mixed-use development. Maintaining a full-size grocery store in Fenton Village is very important for access to food. For any redevelopment, vehicular access to the site for parking and/or loading should not be from Fenton Street; the frontage along Fenton Street should have active ground-floor uses. A new north-south street that aligns with the north-south connection at the block to the south is recommended for this site.

Zoning

- Revise the Fenton Village Overlay Zone with minor updates as presented in the Implementation section. These include zoning text changes proposed to support small businesses.
- Maintain zoning pattern that allows for a transition between the commercial corridor of Fenton Street and the residential neighborhood of East Silver Spring.
- Equalize commercial and residential density values in CR zones for maximum flexibility in future redevelopment.



Figure 8. Fenton Village District Illustrative Diagram

Map 7: Recommended Fenton Village Zoning

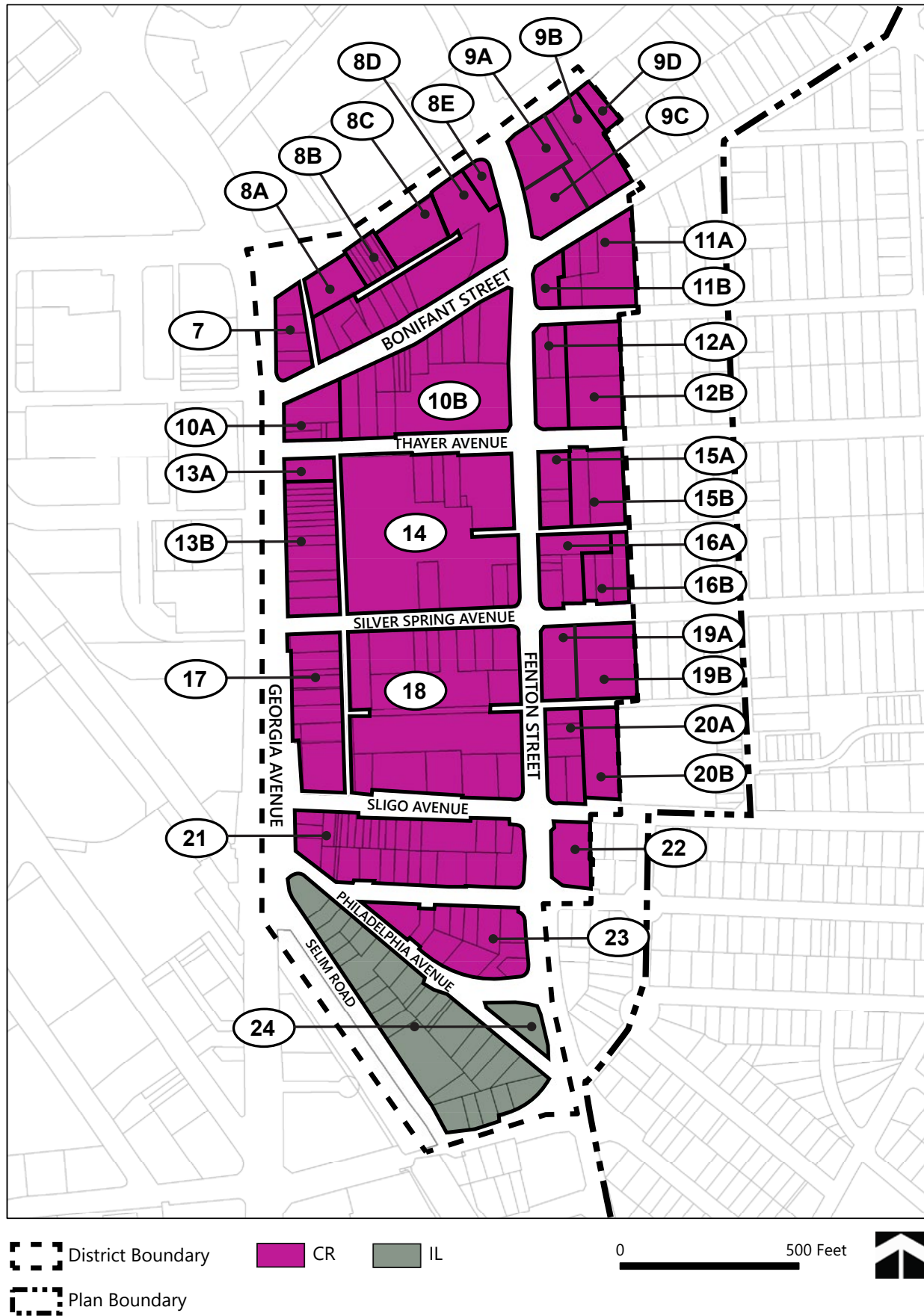


Table 2. Recommended Fenton Village Zoning

Map Number	Existing Zoning	Recommended Zoning	Justification
7	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
8A	CR-5.0 C-4.0 R-4.75 H-145 T	CR-7.0 C-7.0 R-7.0 H-175	Proposed zoning brings parcel into conformance.
8B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-175	Increase flexibility for future mixed-use development.
8C	CR-5.0 C-4.0 R-4.75 H-145 T	CR-7.0 C-7.0 R-7.0 H-175	Proposed zoning brings parcel into conformance.
8D	CR-3.0 C-2.0 R-2.75 H-110 T	CR-3.0 C-3.0 R-3.0 H 130	Increase flexibility for future mixed-use development.
8E	CR-3.0 C-2.0 R-2.75 H-75 T	CR-3.0 C-3.0 R-3.0 H-90	Increase flexibility for future mixed-use development.
9A	CR-3.0 C-2.0 R-2.75 H-75 T	CR-3.0 C-3.0 R-3.0 H-90	Increase flexibility for future mixed-use development.
9B	CR-1.5 C-1.0 R-1.5 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
9C	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-90	Consolidate split zoned properties under one zone.
9D	R-60	CR-3.0 C-3.0 R-3.0 H-70	Rezone to CR for future mixed-use development.
10A	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
10B	CR-3.0 C-2.0 R-2.75 H-110 T	CR-3.0 C-3.0 R-3.0 H-130	Increase flexibility for future mixed-use development.
11A	CR-1.5 C-1.0 R-1.5 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase allowable density near high-capacity transit.
11B	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
12A	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
12B	CR-1.5 C-1.0 R-1.5 H-60 T	CR-1.5 C-1.5 R-1.5 H-70	Increase zoning flexibility. Silver Spring Tower remains non-conforming.
13A	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
13B	CR-3.0 C-2.0 R-2.75 H-110 T	CR-3.0 C-3.0 R-3.0 H-130	Increase flexibility for future mixed-use development.
14	CR-3.0 C-2.0 R-2.75 H-110 T	CR-5.0 C-5.0 R-5.0 H-130	Increase flexibility for future mixed-use development.
15A	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.

Map Number	Existing Zoning	Recommended Zoning	Justification
15B	CR-1.5 C-1.0 R-1.5 H-60 T	CR-1.5 C-1.5 R-1.5 H-70	Increase flexibility for future mixed-use development.
16A	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
16B	CR-1.5 C-1.0 R-1.5 H-60 T	CR-1.5 C-1.5 R-1.5 H-70	Increase flexibility for future mixed-use development.
17	CR-3.0 C-2.0 R-2.75 H-110 T	CR-3.0 C-3.0 R-3.0 H-130	Increase flexibility for future mixed-use development.
18	CR-3.0 C-2.0 R-2.75 H-110 T	CR-3.0 C-3.0 R-3.0 H-130	Increase flexibility for future mixed-use development.
19A	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
19B	CR-1.5 C-1.0 R-1.5 H-60 T	CR-1.5 C-1.5 R-1.5 H-70	Increase flexibility for future mixed-use development.
20A	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
20B	CR-1.5 C-1.0 R-1.5 H-60 T	CR-1.5 C-1.5 R-1.5 H-70	Increase flexibility for future mixed-use development.
21	CR-3.0 C-2.0 R-2.75 H-110 T	CR-3.0 C-3.0 R-3.0 H-130	Increase flexibility for future mixed-use development.
22	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
23	CR-3.0 C-2.0 R-2.75 H-110 T	CR-3.0 C-3.0 R-3.0 H-130	Increase flexibility for future mixed-use development.
24	IL-1.0 H-50	IL-1.0 H-50	Confirm existing zoning.



Thayer and Spring Apartments in Fenton Village

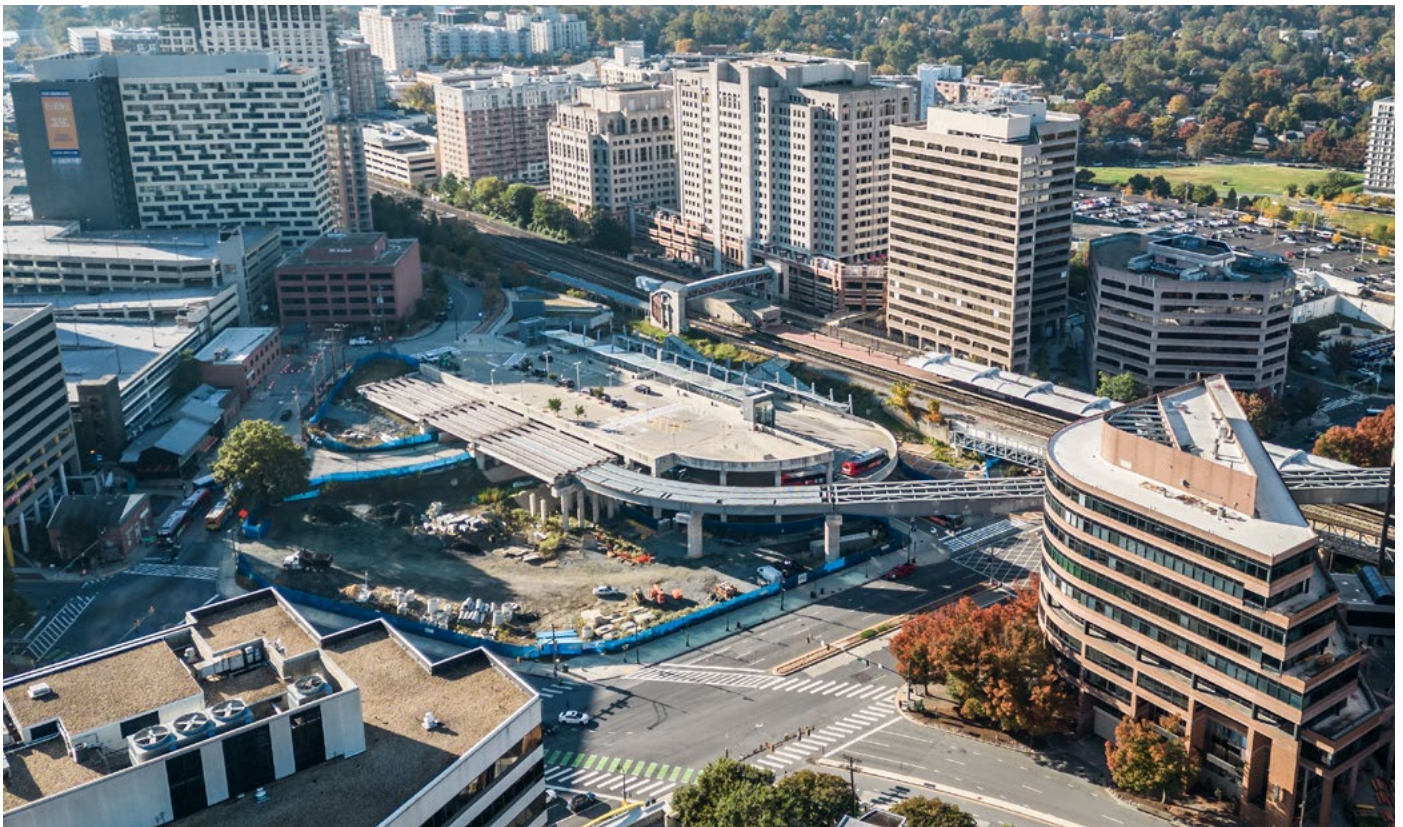
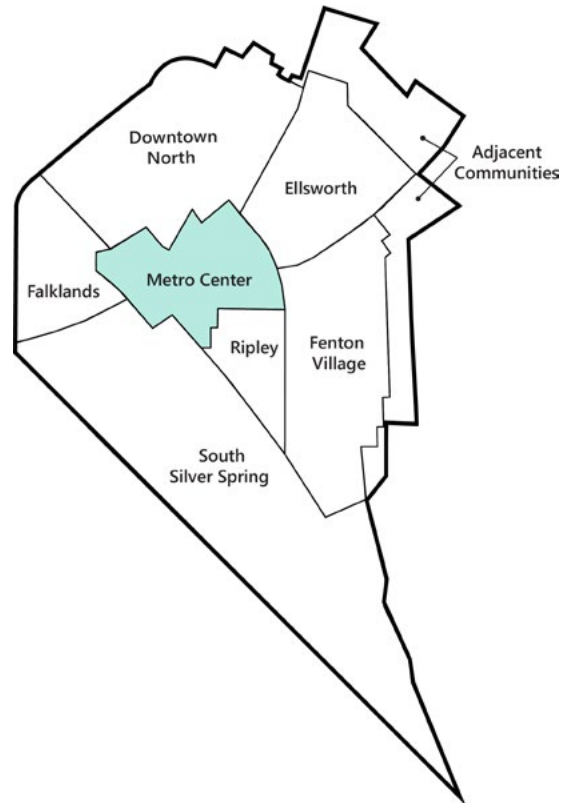
2.3.3. Metro Center District

Vision:

The Metro Center District will welcome visitors with a reimagined hub of activity with the highest-intensity commercial development in the downtown, world-class public space, and safe and inviting connections to surrounding districts.

Goals:

- Confirm the Metro Center District as the commercial center of Silver Spring by encouraging redevelopment of opportunity sites, including county parking garages and surface lots, with spaces for a diversity of tenants.
- Create a world-class destination arrival experience for all users of the transit center.
- Enhance the pedestrian experience from the Transit Center to Ellsworth and other surrounding districts.



Aerial view of the Transit Center

Recommendations:

Urban Design

- New tall buildings along Colesville Road should have clearly differentiated bases that relates to the pedestrian scale, with substantial variation in the building massing, facade and materials. Towers should step back above the base along streets, open spaces and through-block connections in a way that distinctly differentiates the tower from the building base. Refer to the Design Guidelines that accompany this Plan.
- For building massing, comply with the Design Guidelines that will accompany this Plan to address the street-level pedestrian experience for the taller buildings allowed in this district.
- Sponsor an invited charrette and design competition for the creation of a world-class arrival experience at the transit center.
- Improve the Metrorail and Purple Line underpasses with art, lighting, wayfinding, and other interventions to make the pedestrian environment safer and more inviting.
- Enhance pedestrian connections via the Green Loop from the Transit Center to surrounding districts.

Parks and Public Spaces

- Expand and reinforce the open space connection between the arrival experience at the transit center, the recently redeveloped spaces along Wayne Avenue - including Gene Lynch Park - to Ellsworth Drive, and beyond to the Civic Building.

Opportunity Sites

- *Transit Center Development Site:* Design a signature building or buildings at the intersection of Colesville Road and Wayne Avenue and consider integrating the development with the Transit Center. The new development should activate the corner at Colesville Road and Wayne Avenue with a ground-floor use appropriate to the center of an urban area. This building should be architecturally significant and a landmark for Silver Spring. As this site is constrained, consider providing no parking or developing this site along with part of the Bonifant/Dixon garage site. The Plan recommends that the maximum allowed building height on this site be 360 feet, with Planning Board approval.

- *County Parking Garages 5/55 (Bonifant/Dixon Garage):* In alignment with the Parking Lot District's goals for this site, redevelop this garage that is located in both the Metro Center district and the Ripley District. The Plan recommends that the upper-level connections over Bonifant Street and Dixon Avenue be removed, which will create two large redevelopment sites and a small site that may be appropriate for an open space (see Ripley District recommendations). The redevelopment of the parcel located in the Metro Center district (north side of Bonifant Street) could maintain a large parking garage that could provide both public and private parking or could provide additional support for bus operations at the Transit Center as needed. This site is a prominent, visible site from Colesville Road and the Transit Center and would be appropriate for a low base of several stories with an articulated tower set back above the base. The ground floor experience on this site should respond appropriately to the Purple Line tracks that will occupy all of Bonifant Street in this location. The Plan recommends that the maximum allowed building height on this site be 360 feet, with Planning Board approval.
- *Colesville Road and Georgia Avenue Northwest corner:* A hotel is currently approved on this key site. The Plan recommends expanding the development site to include the adjacent two-story commercial property for a signature mixed-use development.
- *Colesville Road and East-West Highway Northwest Corner:* This site is located immediately adjacent to the northern Metro Station entrance but is significantly constrained by the WMATA substation central to the site. The Plan recommends continuing to pursue relocation of the substation to promote a signature development at this location. If the substation is not able to be relocated, it may be incorporated into the redevelopment of the site. The Plan recommends incorporating or connecting this site to the arrival experience at the transit station.
- *8407 Colesville Road:* This site is currently under-utilized and is home to a McDonald's.

Zoning

- Equalize commercial and residential density values in CR zones for maximum flexibility in future redevelopment.

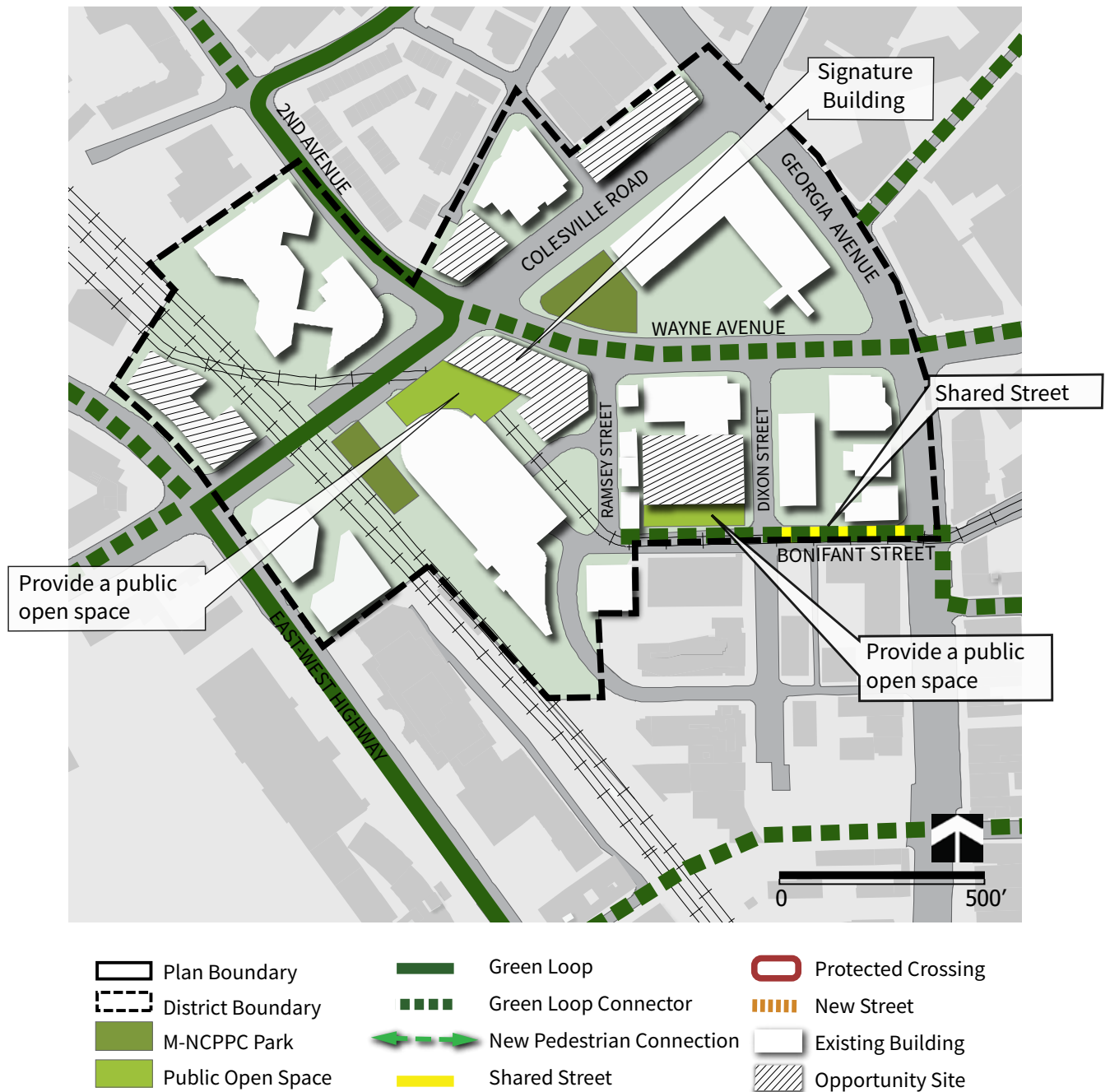


Figure 9. Metro Center Illustrative Diagram

Map 8: Recommended Metro Center Zoning

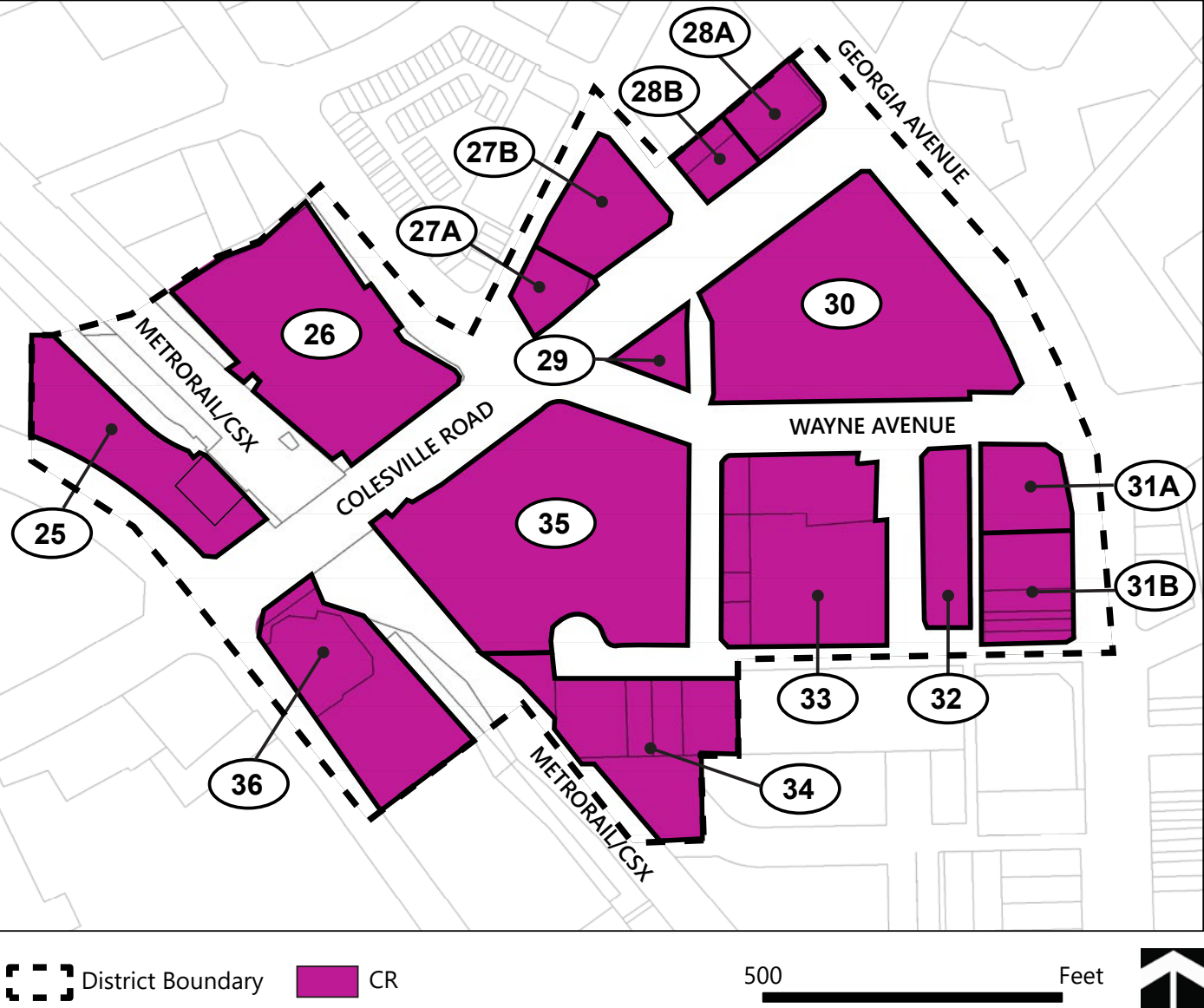


Table 3. Recommended Metro Center Zoning

Map Number	Existing Zoning	Recommended Zoning	Justification
25	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-300	Increase flexibility for future mixed-use development.
26	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-300	Increase flexibility for future mixed-use development.
27A	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
27B	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-300	Increase flexibility for future mixed-use development.
28	CR-5.0 C-4.0 R-4.75 H-145 T	CR-8.0 C-8.0 R-8.0 H-300	Increase flexibility for future mixed-use development.
29	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-300	Increase flexibility for future mixed-use development.
30	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-300	Increase flexibility for future mixed-use development.
31A	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-300	Increase flexibility for future mixed-use development.
31B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
32	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-300	Increase flexibility for future mixed-use development.
33	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-360	Increase flexibility for future mixed-use development, particularly around the Transit Center.
34	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-300	Increase flexibility for future mixed-use development.
35	CR-5.0 C-4.0 R-4.75 H-145 T	CR-8.0 C-8.0 R-8.0 H-360	Increase flexibility for future mixed-use development, particularly around the Transit Center.
36	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-300	Increase flexibility for future mixed-use development.

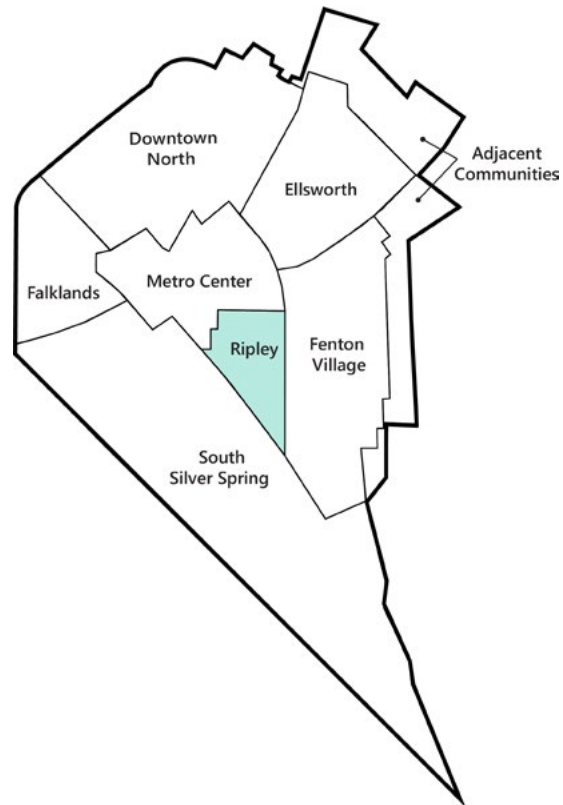
2.3.4. Ripley District

Vision:

Ripley will be a vibrant complete multi-family residential district steps from the Metro, Purple Line, and Metropolitan Branch Trail, with a new central open space and a pedestrian connection to South Silver Spring across the Metrorail.

Goals:

- Encourage redevelopment of under-utilized sites, including those on Georgia Avenue, and existing public parking garages into mixed-use projects with active ground floor uses.
- Increase the housing stock in this primarily residential district with a diverse mix of multi-family unit sizes at all levels of affordability.
- Improve the pedestrian connection and visitor arrival experience at the Purple Line station at Bonifant Street and Ramsey Street at the upper level of the transit center.
- Create a new open space in the Ripley District for active recreation and social gathering opportunities.
- Connect the Ripley District across the Metrorail/CSX tracks to South Silver Spring.



Ripley II under construction in the Ripley District

Recommendations:

Urban Design

- New tall buildings along Georgia Avenue should have clearly differentiated bases that relates to the pedestrian scale, with substantial variation in the building massing, facade and materials. Towers should step back above the base along streets, open spaces and through-block connections in a way that distinctly differentiates the tower from the building base. Refer to the Design Guidelines that accompany this Plan.
- Create a sense of arrival with a welcoming pedestrian connection along Bonifant Street to Georgia Avenue to the Purple Line station at the upper level of the Transit Center.
- Connect the Ripley District to South Silver Spring via a landscaped pedestrian bridge over the train tracks, with infrastructure for pedestrians and cyclists.
- Introduce a mid-block crossing at Georgia Avenue and Mayor Lane to connect the Ripley District with Fenton Village.
- Implement the Green Loop concept along Silver Spring Avenue so that street, along with the proposed pedestrian bridge, will function as a connector to the Central Loop.

Parks and Public Spaces

- Create a Civic Green at the center of the Ripley District per the recommendations of the 2010 *Silver Spring Central Business District Green Space Plan Guidelines* and 2018 *Energized Public Spaces Functional Master Plan*, that will be an important focal point and social gathering space for this neighborhood. It will connect to the proposed pedestrian bridge over the Metrorail/CSX tracks between the South Silver Spring District and the Ripley District.
- Create an urban recreational park at the parcel currently occupied by the stair tower of Parking Garage 5/55 on the south side of Bonifant Street to the east of Dixon Avenue, adjacent to an existing alley. This park can have small scale active recreation activities to serve the residents of the Ripley District.

Opportunity Sites

- *Parking Garages 5/55 (Bonifant/Dixon Garage)*: In alignment with the Parking Lot District's goals for this site, redevelop this garage that is located in both the Metro Center district and the Ripley District (refer to Metro Center District recommendations above). Provide a linear green space with a continuous pedestrian connection from the Purple Line station at the Transit Center to Georgia Avenue and the center of the downtown. This green space will also serve as a buffer from the rail tracks that will occupy all of Bonifant Street between the station and Dixon Avenue. Currently the garage bridges both over Bonifant Street and an alley to the east of Bonifant Street. The Plan recommends that the redevelopment of this garage eliminate these bridges. The development site in the Ripley District should respond to both the need for a safe, green, inviting pedestrian connection along Bonifant Street to Georgia Avenue for commuters and visitors, and the opportunity for a tall, architecturally distinctive tower in such a prominent location proximate to the transit center. The Plan recommends that the maximum allowed building height on this site be 360 feet, with Planning Board approval.
- *Block of Ripley Street, Dixon Avenue, and Georgia Avenue*: This block consists of several potential development sites along Georgia Avenue. If there is an opportunity for these parcels to redevelop together, The Plan recommends this site to include a central open space, potentially connected across Ripley Street to the 8230 Georgia Avenue site.
- *8126 Georgia Avenue*: This long narrow site has frontage on Dixon Avenue Extended and Georgia Avenue. The Plan recommends mixed-use redevelopment of the site with access from Dixon Avenue.
- *8230 Georgia Avenue (Sherwin-Williams site)*: This site is significantly constrained by the Master-Planned right-of-way for Ripley Street between Dixon Avenue and Georgia Avenue, the dedication of which would significantly reduce the developable area. Streetscape improvements and other public use facilities should not further significantly reduce the developable area. Previous efforts to incorporate this site into adjacent developments were unsuccessful.

Zoning

- Equalize commercial and residential density values in CR zones for maximum flexibility in future redevelopment.

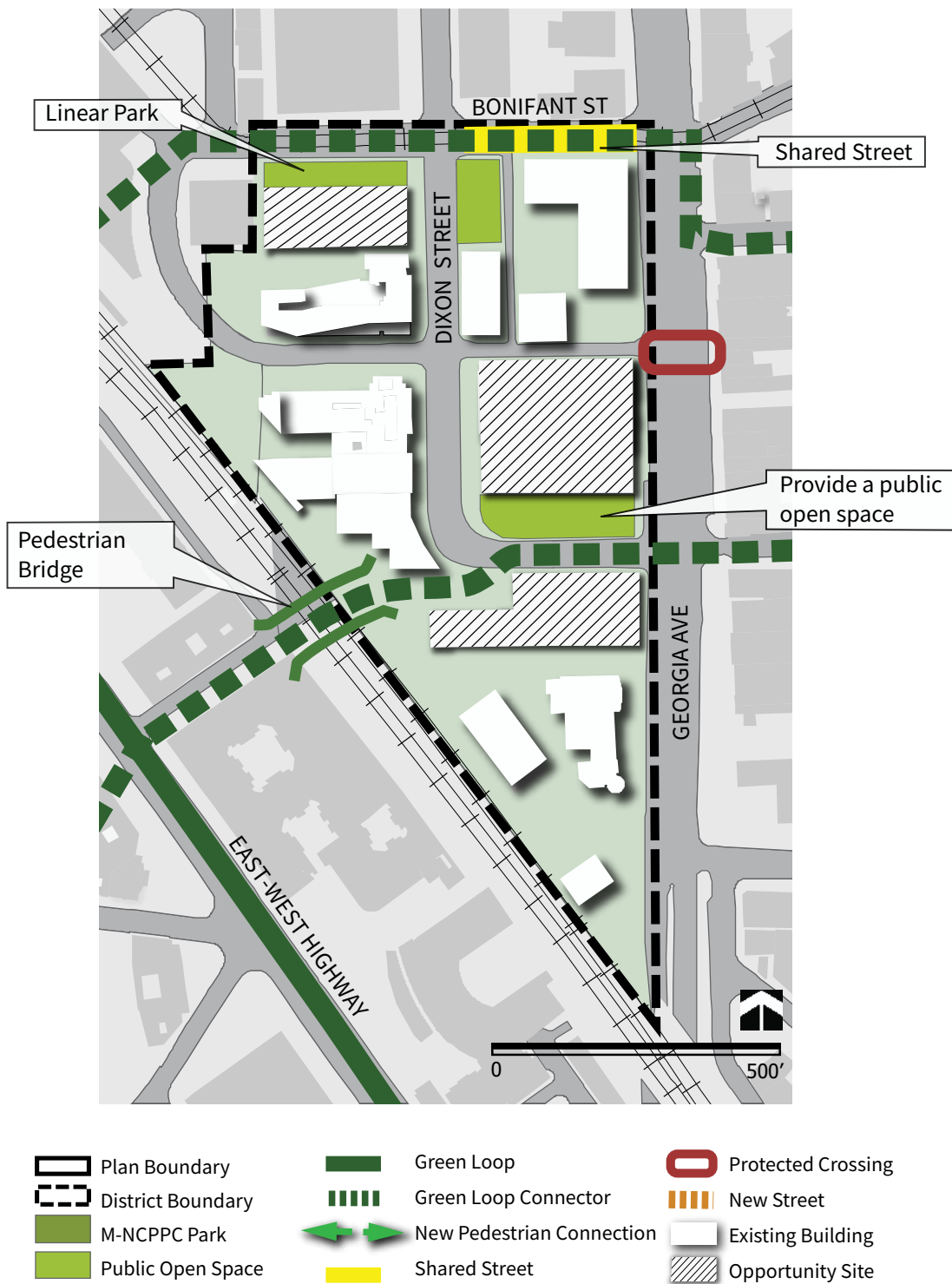


Figure 10. Ripley District Illustrative Diagram

Map 9: Recommended Ripley District Zoning

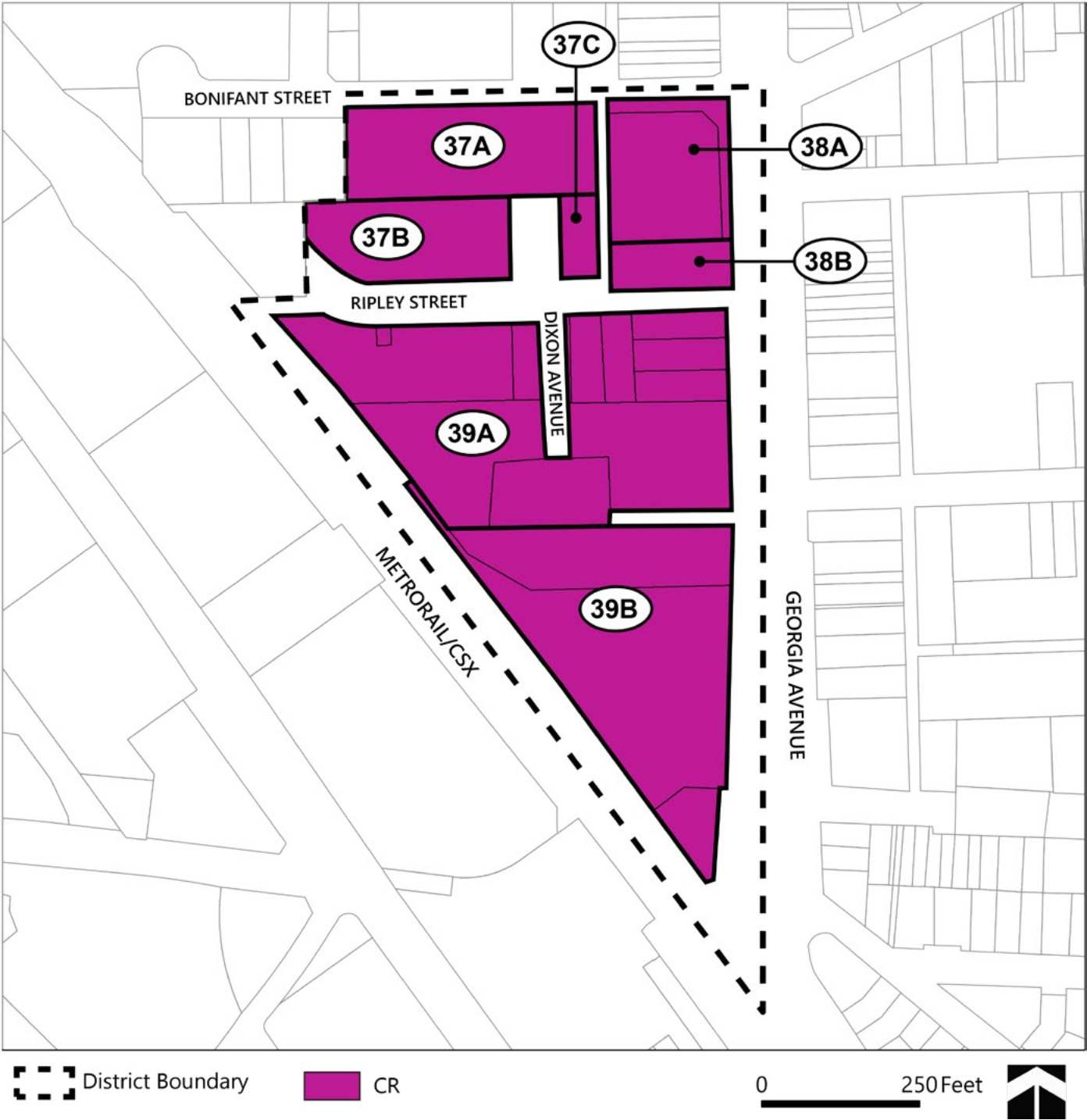


Table 4. Recommended Ripley District Zoning

Map Number	Existing Zoning	Recommended Zoning	Justification
37A	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-360	Increase flexibility for future mixed-use development, particularly near the Transit Center.
37B	CR-5.0 C-4.0 R-5.0 H-200 T	CR-5.0 C-5.0 R-5.0 H-300	Increase flexibility for future mixed-use development.
37C	CR-5.0 C-4.0 R-5.0 H-200 T	CR-5.0 C-5.0 R-5.0 H-300	Increase flexibility for future mixed-use development.
38A	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-300	Increase flexibility for future mixed-use development.
38B	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-300	Increase flexibility for future mixed-use development.
39A	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-300	Increase flexibility for future mixed-use development.
39B	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-300	Increase flexibility for future mixed-use development.
39C	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-300	Increase flexibility for future mixed-use development.
39D	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-240	Increase flexibility for future mixed-use development.

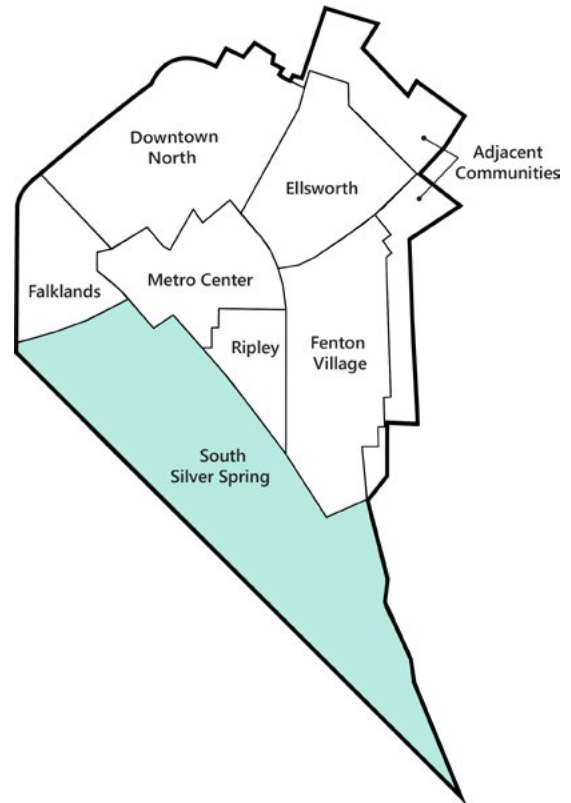
2.3.5. South Silver Spring District

Vision:

South Silver Spring will leverage its unique array of assets to become a destination for mixed-use development to serve its diverse community, and an enhanced connector between the developing centers along Georgia Avenue, from the Walter Reed campus in Washington, D.C. to the core of downtown Silver Spring.

Goals:

- Encourage redevelopment of under-utilized parcels throughout South Silver Spring by adjusting the existing zoning for increased flexibility of uses.
- Support redevelopment of opportunity sites to provide a mix of housing options for people of all ages, income levels, and household size.
- Strive for no net loss of affordable housing in the event of redevelopment of naturally occurring affordable housing.
- Collaborate with the Takoma Park/Silver Spring campus of Montgomery College to expand educational and science and technology programs in the district.
- Provide new public open space in South Silver Spring to support existing and future residents of this neighborhood.
- Reconnect and reimagine Jesup Blair Park to the downtown via a visionary renovation of the park. This park should be both a gateway to Silver Spring from Washington, D.C. and a unique destination in the downtown.
- Support the redevelopment of the Blairs per the approved preliminary master plan for the site.



The Veridian Apartments at 1133 East-West Highway

Recommendations:

Urban Design

- New tall buildings along Georgia Avenue should have clearly differentiated bases that relates to the pedestrian scale, with substantial variation in the building massing, facade and materials. Towers should step back above the base along streets, open spaces and through-block connections in a way that distinctly differentiates the tower from the building base. Refer to the Design Guidelines that accompany this Plan.
- Redevelopment sites along or near to Eastern Avenue should step down toward the residential neighborhood across Eastern Avenue in Washington, D.C.
- Redevelopment sites along East-West Highway and 13th Street should provide some active ground-floor uses such as retail or other community amenities.
- Implement the Green Loop concept along East-West Highway as a key component of the Central Loop.
- Create a new mid-block crossing on Georgia Avenue at Montgomery College for improved access to the campus.

Parks and Public Spaces

- Provide a new urban recreational park in South Silver Spring per the recommendations of the 2010 *Silver Spring Central Business District Green Space Plan Guidelines* and 2018 *Energized Public Spaces Functional Master Plan*. The vision for this park is an active recreation space that can serve as a complement to the historic, contemplative setting of Acorn Park just up the street that will also be renovated. This park will also provide a green space to support the retail and food services establishments along East-West Highway.
- Renovate Jesup Blair Park to create a unique open place that includes social, active and contemplative experiences throughout its 14 acres. The new Jesup Blair Park will be a gateway and a regional destination that promotes an active lifestyle and offers the unique historical and cultural setting of a special park designated on the *Master Plan for Historic Preservation*. Renovate the historic Jesup Blair House for appropriate community uses that complement the park programming. Refer to Parks and Public Spaces Recommendations in Section 3.5.

- Support the existing community garden use at 7980 Georgia Avenue until the parcel is redeveloped.
- Support the approved project plan for a mixed-use infill redevelopment, found in the Blairs Master Plan, that includes several new public open spaces in the northern part of South Silver Spring west of East-West Highway. This includes a large civic space and smaller complementary spaces with active recreation opportunities.



Acorn Park (top), looking north on East-West Highway (bottom left), independent businesses along Eastern Avenue (bottom right).

Opportunity Sites:

- *7980 Georgia Avenue*: The Plan recommends redevelopment of this site as a mixed-use development along Georgia Avenue. Consider incorporating a community garden use as part of the open space on this parcel. Improve connections in this area of South Silver Spring by creating a through-block connection.
- *8040 13th Street/Days Inn*: This is a key opportunity site in South Silver Spring and a strong site for mixed-use development with active ground floor uses along 13th street, including retail. This site should include a public open space and a pedestrian a connection through the site to the to the proposed South Silver Spring Park along Kennett Street. Heights should step down towards Eastern Avenue as identified in the Design Guidelines. This Plan recommends coordination with the Parking Lot District regarding the potential use of the existing surface lot and parking garage as a parking resource for any redevelopment.
- *Montgomery College*: The Plan recommends working with the college to explore and develop opportunities to expand the campus program in the South Silver Spring District.
- *8045 Kennett Street (Caldor Building)*: The Plan recommends the adaptive re-use of this building, possibly as an educational facility or for future employment.
- *8001 Newell Street (Self-Storage)*: The Plan recommends that redevelopment of this site transition in height to the garden apartment buildings across Newell Street and the single-family homes across Eastern Avenue Northwest in Washington, D.C.
- *7996 Georgia Avenue*: The Plan recommends redevelopment of this key site at Georgia Avenue and East-West Highway for institutional/educational uses to support Montgomery College across Georgia Avenue, or other mixed-use development.
- *Blairs Master Plan sites*: Support the approved project plan for a mixed-use infill redevelopment that includes multiple parcels in South Silver Spring west of East-West Highway, including the redevelopment of the existing Blair Park Shopping Center.



8045 Kennett Street (top), Self-Storage Facility at 8001 Newell Street (middle), The Pearl at the Blairs (bottom).



Figure 11. South Silver Spring Urban Design Illustrative Diagram

Garden Apartments

- South Silver Spring is home to several garden style apartment complexes built predominantly in the 1930s and 1940s that provide a significant source of naturally occurring affordable housing for the Silver Spring community. Like many aging multi-family properties discussed in previous master plans, these garden facilities may require significant renovation, rehabilitation, or redevelopment in the coming decades. In the event of redevelopment, these properties should strive for no net loss of affordable housing. No net loss is where naturally occurring affordable housing units are replaced with an equivalent number of new income-restricted affordable housing units through redevelopment.
- 7719-7725 Eastern Avenue (Blair Park Apartments): This Plan recommends rezoning to allow flexibility and redevelopment that would support future affordable housing development.
- 7701-7705 Eastern Avenue; 805-809 Juniper Street; 7700-7705 Blair Road (Blair Park Gardens): Recommendation to support a future application for a CRT Floating Zone with residential density of up to 4.0 FAR, that strives for no net loss of affordable housing.

- 7603-7615 Eastern Avenue (Eastern Avenue Apartments): Recommendation to support a future application for a CRT Floating Zone with residential density of up to 4.0 FAR, that strives for no net loss of affordable housing.
- 8000-8012 Eastern Drive; 8001-8009 Eastern Drive; 8033- 8039 Eastern Avenue; 8000-8004 Blair Mill Road; 1300-1302 Blair Mill Road (Rock Creek Springs): Recommendation to support a future application for a CRT Floating Zone with residential density of up to 4.0 FAR, that strives for no net loss of affordable housing.
- 8001-8031 Eastern Avenue (Spring Garden Apartments): Recommendation to support a future application for a CRT Floating Zone with residential density of up to 4.0 FAR, that strives for no net loss of affordable housing.

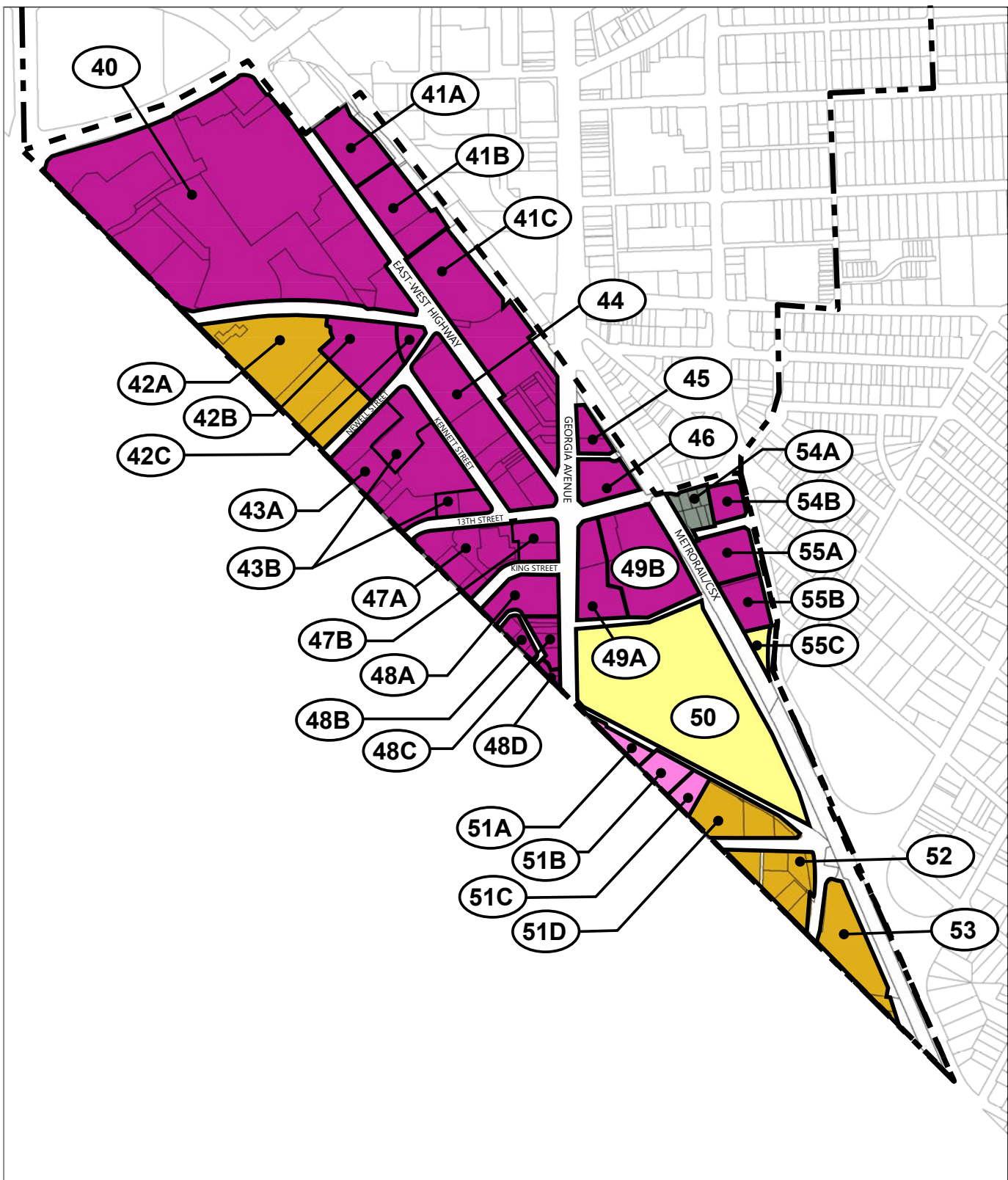
Zoning

- Equalize commercial and residential density values in CR zones for maximum flexibility in future redevelopment.
- Correct zoning to bring existing non-compliant parcels into conformance.



Spring Garden Apartments

Map 10: Recommended South Silver Spring Zoning



District Boundary
 Plan Boundary

CR
 CRT
 IL
 R-60
 R-10

0 1,000 Feet

Table 5. Recommended South Silver Spring Zoning

Map Number	Existing Zoning	Recommended Zoning	Justification
40	CR-5.0 C-0.5 R-5.0 H-200 T	CR-5.0 C-5.0 R-5.0 H-240	Increase flexibility for future mixed-use development.
41A	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-300	Increase flexibility for future mixed-use development.
41B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
41C	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-240	Increase flexibility for future mixed-use development.
42A	R-10	R-10	Confirm current zoning for existing affordable housing.
42B	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
42C	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
43A	CR-3.0 C-2.0 R-2.75 H-125 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
43B	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
44	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
45	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
46	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
47A	CR-3.0 C-2.0 R-2.75 H-125 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development; one property remains non-compliant.
47B	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
48A	CR-3.0 C-2.0 R-2.75 H-125 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
48B	CR-3.0 C-2.0 R-2.75 H-125 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
48C	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.

Map Number	Existing Zoning	Recommended Zoning	Justification
48D	CR-3.0 C-2.0 R-2.75 H-125 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
49A	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
49B	IM-2.5 H-50	CR-3.0 C-3.0 R-3.0 H 110	Rezone to CR from IM to allow future mixed-use development.
50	R-60	R-60	Confirm existing zoning; existing park.
51A	CRT-2.25 C-1.5 R-0.75 H-75	CRT-2.25 C-2.25 R-2.25 H-75	Increase flexibility for future mixed-use development.
51B	R-10	CRT-3.0 C-0 R-3.0 H 75	Rezoned to CR to provide flexibility for future development.
51C	CRT-2.25 C-1.5 R-0.75 H-75	CRT-2.25 C-2.25 R-2.25 H-75	Increase flexibility for future mixed-use development.
51D	R-10	R-10	Confirm current zoning for existing affordable housing.
52	R-10	R-10	Confirm current zoning for existing affordable housing.
53	R-10	R-10	Confirm current zoning for existing affordable housing.
54A	IL-1.0 H-50	IL-1.0 H-50	Confirm existing zoning.
54B	IM-2.5 H-50	CR 2.5 C-2.5 R-2.5 H-55	Rezone to CR to provide flexibility for future development.
55A	IL-1.0 H-50	CR-2.5 C-2.5 R-2.5 H 55	Rezone to CR to provide flexibility for future development.
55B	IM-2.5 H-50	CR-2.5 C-2.5 R-2.5 H 55	Rezone to CR to provide flexibility for future development.
55C	R-60	R-60	Confirm existing zoning.

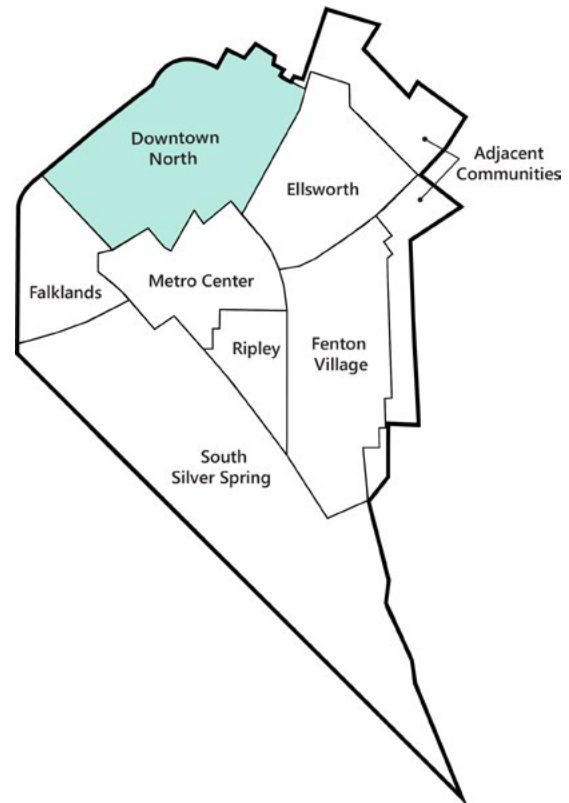
2.3.6. Downtown North District

Vision:

Downtown North will build on the connection between the state-of-the-art South County Regional Recreation and Aquatic Center and the world-renowned United Therapeutics campus to create a unique district focused on health and health sciences.

Goals:

- Encourage redevelopment of under-utilized parcels including existing public parking garages and surface lots by increasing zoning flexibility and incentivizing additional height and FAR where appropriate.
- Connect the east and west sides of the Downtown North District via Cameron Street. The Plan envisions Cameron Street as a new Complete Street, with neighborhood-supporting retail and community amenities.
- Promote infill development on either side of Georgia Avenue north of Colesville Road.
- Improve the road network in this district by extending existing connections or creating new connections to divide super blocks and create a more cohesive urban fabric.
- Support the expansion of life-sciences and other bio-tech related industry facilities in the district.
- Promote active ground floor uses in all redevelopment projects, whether retail, community-use, or arts focused.



Georgia Avenue in the Downtown North District

Recommendations:

Urban Design

- New tall buildings along Georgia Avenue should have clearly differentiated bases that relates to the pedestrian scale, with substantial variation in the building massing, facade and materials. Towers should step back above the base along streets, open spaces and through-block connections in a way that distinctly differentiates the tower from the building base. Refer to the Design Guidelines that accompany this Plan.
- At infill redevelopment sites along Georgia Avenue provide active ground floor uses.
- Implement the Green Loop concept along Cameron Street between Fenton Street and 2nd Avenue as a key component of the Central Green Loop. Cameron Street between Fenton Street and Spring Street should be a Green Loop connector with some elements of the Green Loop implemented as feasible.
- Implement Green Loop elements along Spring Street as part of the secondary Outer Loop.
- Increase public open spaces and community gathering opportunities in this district through development projects on existing opportunity sites.
- Improve pedestrian crossings on Georgia Avenue north of Colesville Road.
- Provide public through-block connections throughout the district to enhance walkability and connectivity between long or super blocks.

Parks and Public Spaces

- Create an Urban Recreational Park proximate to Cameron Street and 2nd Avenue to support the increased level of activity and number of occupants in this area as a result of the new mixed-use project that includes residential towers over a new County Recreation Center. This park's direction connection to the Green Loop via Cameron Street will complement other active facilities in the Plan, including the Recreation and Aquatic Center across the street.

Opportunity Sites

- *Parking Garage 7:* Redevelop this existing garage in collaboration with the Parking Lot District. Extend 1st Avenue south through the site to align with Ramsey Avenue where it intersects with Cameron Street. The Plan recommends that any new development at this location should include neighborhood-serving retail on the ground floor and incorporate a public open space that is accessible to all (see Urban Recreational Park recommendation above).
- *Parking Garage 2:* This garage can also be redeveloped in collaboration with the Parking Lot District. The Plan recommends continuing Fenton Street north through the site as a pedestrian and bicycle connection to align with the through-block connection under construction at the 8787 Georgia Avenue site. Active ground floor uses should front on both Cameron Street and Spring Street.
- *Additional opportunity sites in this district include:* 8615 Georgia Avenue, 8676 Georgia Avenue, 8600-8606 2nd Avenue, 1323 Fenwick Lane, and 1327 Fenwick Lane.

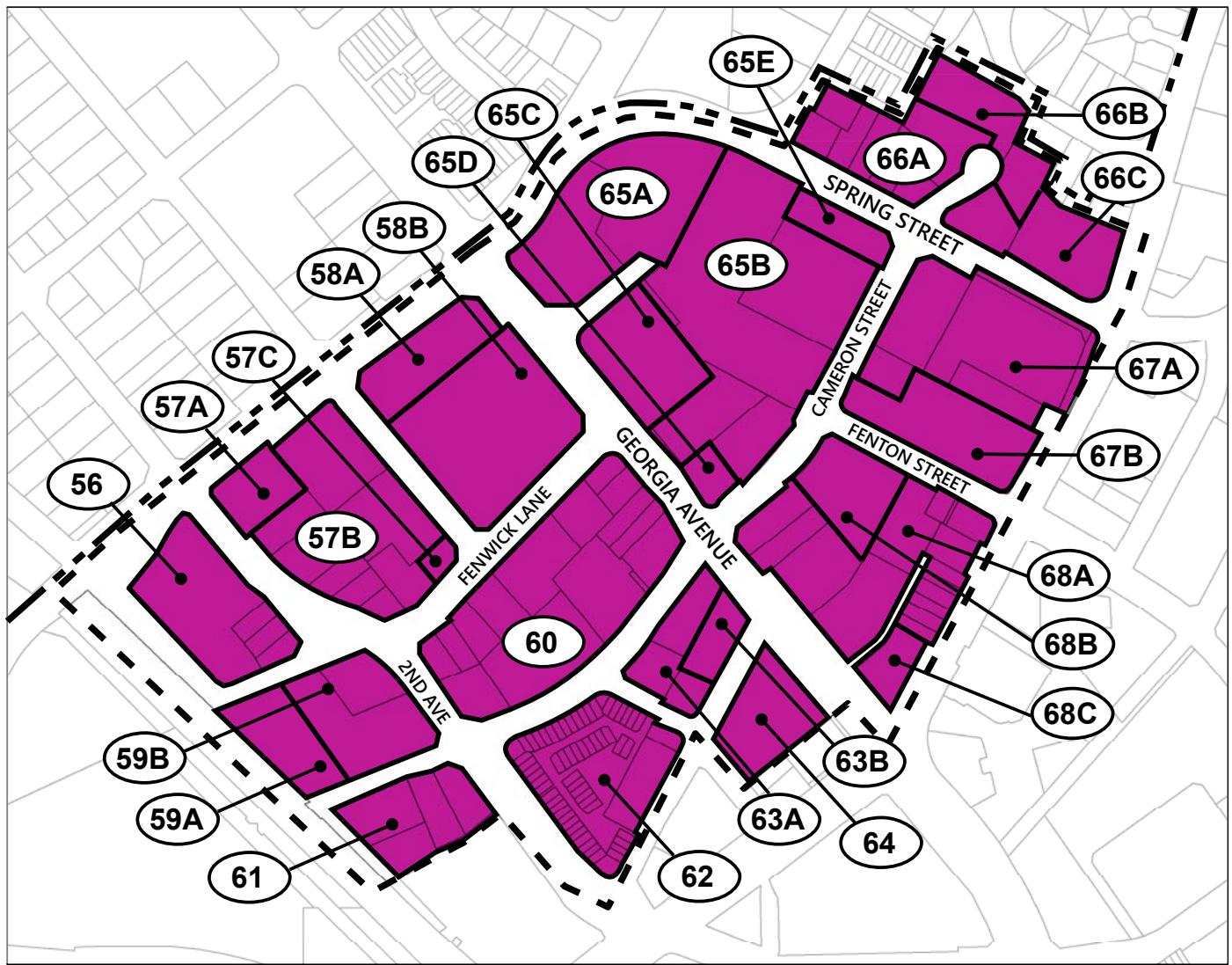
Zoning

- Equalize commercial and residential density values in CR zones for maximum flexibility in future redevelopment.
- Correct zoning to bring existing non-compliant parcels into conformance.



Figure 12. Downtown North Urban Design Illustrative Diagram

Map 11. Recommended Downtown North Zoning



--- District Boundary

CR

0 500 Feet



... Plan Boundary

Table 6. Recommended Downtown North Zoning

Map Number	Existing Zoning	Recommended Zoning	Justification
56	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
57A	CR-3.0 C-1.25 R-3.0 H-145 T	CR-3.0 C-3.0 R-3.0 H-175	Increase flexibility for future mixed-use development.
57B	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
57C	CR-3.0 C-2.0 R-2.75 H-90 T	CR-5.0 C-5.0 R-5.0 H-175	Increase flexibility for future mixed-use development.
58A	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-175	Increase flexibility for future mixed-use development.
58B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-6.75 C-6.75 R-6.75 H-175	Adjust zoning to bring parcel into conformance.
59A	CR-3.0 C-2.0 R-2.75 H-90 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development. Consolidate split zoning.
59B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
60	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
61	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
62	CR-5.0 C-1.0 R-5.0 H-200 T	CR-5.0 C-5.0 R-5.0 H-300	Increase flexibility for future mixed-use development.
63A	CR-5.0 C-1.0 R-5.0 H-200 T	CR-5.0 C-5.0 R-5.0 H-300	Increase flexibility for future mixed-use development.
63B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
64	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-300	Increase flexibility for future mixed-use development.
65A	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
65B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development; resolve split zoning.
65C	CR-5.0 C-4.0 R-4.75 H-145 T	CR-6.0 C-6.0 R-6.0 H-260	Adjust zoning to bring parcel into conformance.
65D	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
65E	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-165	Increase flexibility for future mixed-use development.
66A	EOF-3.0 H-100	CR-3.0 C-3.0 R-3.0 H-100	Update to CR zone from pre-2014 EOF zone.
66B	R-60	CR-3.0 C-3.0 R-3.0 H-100	Consolidate split zone properties under one zone
66C	EOF-3.0 H-100	CR-3.0 C-3.0 R-3.0 H-100	Update to CR zone from pre-2014 EOF zone.
67	CR-3.0 C-0.75 R-3.0 H-145 T	CR-3.0 C-3.0 R-3.0 H-260	Increase flexibility for future mixed-use development.
68A	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-260	Increase flexibility for future mixed-use development.
68B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-6.0 C-6.0 R-6.0 H-260	Adjust zoning to bring parcel into conformance.
68C	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-300	Increase flexibility for future mixed-use development.

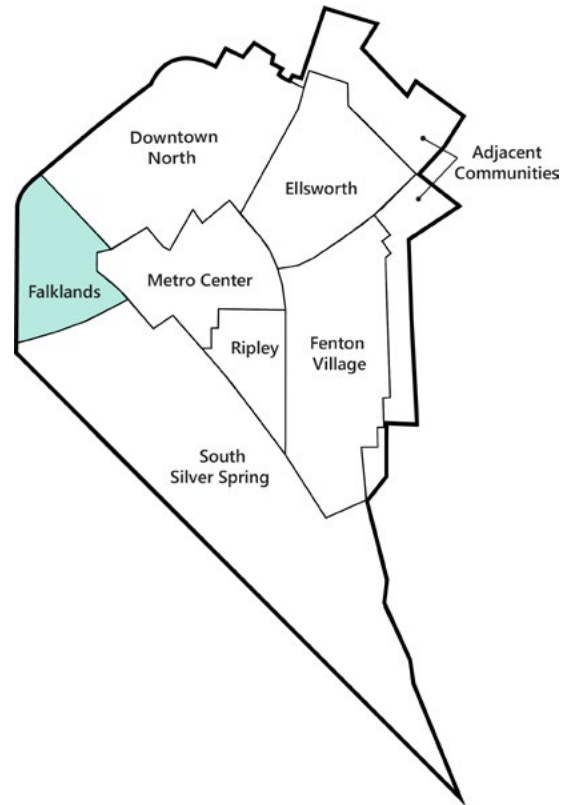
2.3.7. Falklands District

Vision:

The Falklands District will retain its historic fabric of affordable housing south of East-West Highway while embracing the opportunity for higher-density mixed-use development north of East-West Highway.

Goals:

- Preserve the historically significant Falklands South parcel just south of East-West Highway as a sylvan retreat of naturally occurring affordable housing in the middle of a dense urban environment.
- Encourage appropriate redevelopment of the northern portion of the Falklands while striving for no net loss of market rate affordable housing on this site to support the Plan goal of housing diversity.
- Maintain and protect the existing public realm of the Falklands, including significant green cover, a mature tree canopy, and an existing stream.
- Provide public open space and access to existing stream valley in the southern parcel of the Falklands.



Recommendations:

Urban Design

- Redevelop the Falkland property on the north side of East-West Highway. The redevelopment should be designed with a variety of building heights, with an internal circulation pattern that honors and retains the existing landscaped character of the site.
- If the northern Falklands parcel is redeveloped as a mixed-use site, consider the feasibility of creating a connection across the rail from the Falklands District to the Downtown North District.

Parks and Public Spaces

- Implement the recommendation from the 2010 *Silver Spring Central Business District Green Space Guidelines* for a renovated stream valley in the southern portion of the Falklands parcel between East-West Highway and Colesville Road.
- Connect the renovated stream valley in the southeast quadrant to a consolidated public use space on the north parcel that is green and clearly public in nature.



Opportunity Sites

- *Northern Parcel of Falklands Apartments:* This parcel is bounded by 16th Street to the west, East-West Highway to the south and the Metrorail/CSX tracks to the east. This property has an approved site plan for a mixed-use project and the Plan supports the redevelopment of this parcel.

Zoning

- Equalize commercial and residential density values in CR zones for maximum flexibility in future redevelopment.



Figure 13. Falklands District Urban Design Illustrative Diagram

Map 12. Recommended Falklands District Zoning

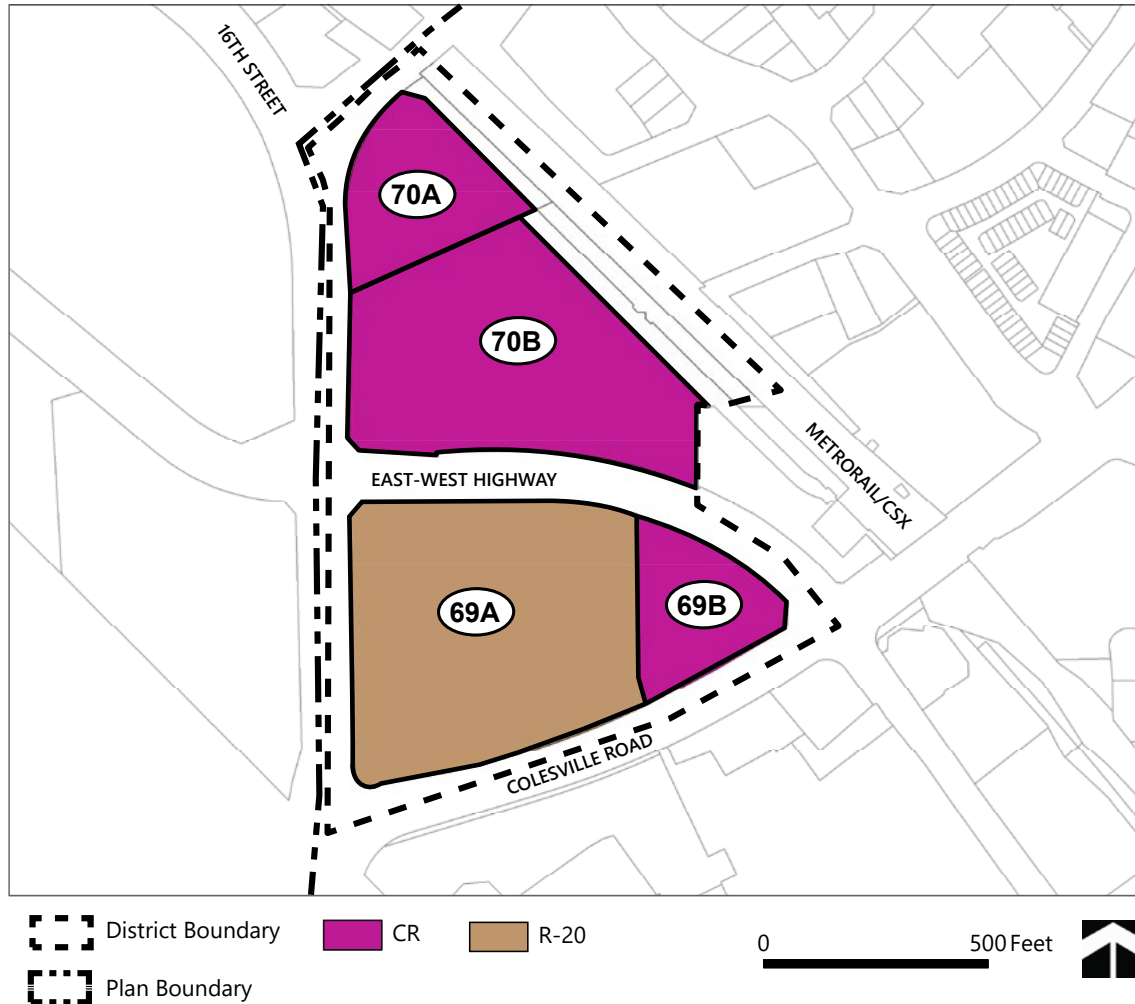


Table 7. Recommended Falklands District Zoning

Map Number	Existing Zoning	Recommended Zoning	Justification
69A	R-20	R-20	Confirm existing zoning; historic property.
69B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-6.0 C-6.0 R-6.0 H-175	Adjust zoning to bring parcel into conformance.
70A	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
70B	CR-3.0 C-0.75 R-3.0 H-145 T	CR-3.0 C-3.0 R-3.0 H-175	Increase flexibility for future mixed-use development.

2.3.8. Adjacent Communities

Vision:

Consistent with the Plan goal of housing diversity, the Adjacent Communities can include a greater variety of housing types, fully integrated into the existing neighborhood fabric, to allow a wider range of residents to enjoy the valued proximity to the downtown and nearby transit options.

Goals:

- Maintain these neighborhoods as primarily residential and preserve the mature tree canopy found along many streets.
- Encourage a greater diversity of housing types.
- Preserve existing naturally occurring affordable housing in this district.

Recommendations:

Urban Design

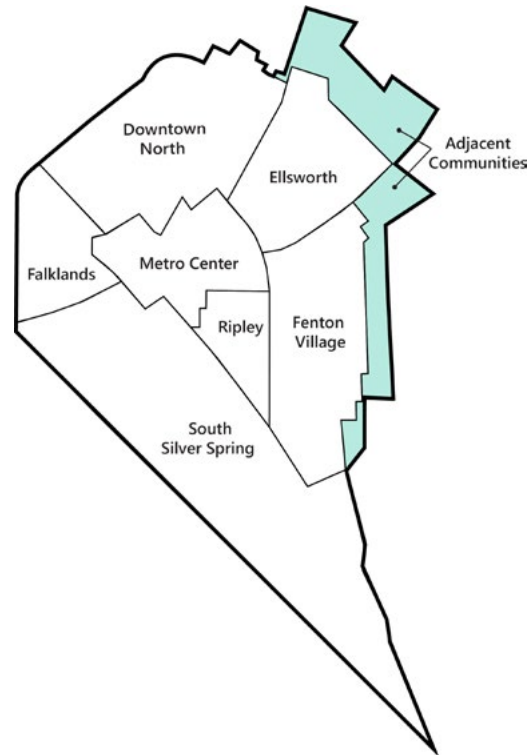
- All new buildings in these neighborhoods should be compatible in scale with the surrounding development, regardless of building type.
- Maintain mature tree canopy by continuing to plant and replace street trees as needed.
- Implement as many Green Loop elements as possible along the following streets that will be Green Loop Connectors into the downtown:
 - Ellsworth Drive
 - Bonifant Street

Parks and Public Spaces

- Ellsworth Urban Park will continue to serve as a key destination in this district; the Plan recommends renovating this park, particularly along the frontage on Colesville Road.

Opportunity Site

- *Block including 8505 Springvale Road and 620 Pershing Drive:* This site currently includes a facility for seniors. The Plan proposes a rezoning from R-60 to CRT to support the potential future redevelopment of the full site for multifamily housing.



Zoning

- Confirm all existing zoning with the exception of the mapped areas shown and described in the table below.
- Convert parcels zoned RT-12.5 to THD as shown in the map and the table.
- Rezone R-60 parcels on blocks in East Silver Spring that include CR parcels in Fenton Village to CRN 0.75 C-0 R-0.75 H-40 as shown in the map and described in the table. This includes the block south of Wayne Avenue and north of Bonifant Street.
- 8901 Colesville Road (former Silver Spring Library site): The current zoning is R-60 with a project currently approved for this site. If the approved project is not realized on this site, this location could be considered for an alternate use and would be appropriate for a Commercial/Residential floating zone. Any potential future use beyond what is currently approved should align and coordinate with the adjacent park.
- Block including 8505 Springvale Road and 620 Pershing Drive: This block currently includes a senior housing facility. Proposed rezoning to CRT for future flexibility for multifamily and/or senior housing.
- 8808 Colesville Road: This property is currently zoned R-60 but is being used as a commercial property. The Plan recommends this property as a candidate for a CRN floating zone of CRNF-0.75 C-0.75 R-0.75, H-40.



Figure 14. Adjacent Communities Urban Design Illustrative Diagram

Map 13. Recommended Adjacent Communities Zoning



Table 8. Recommended Adjacent Communities Zoning

Map Number	Existing Zoning	Recommended Zoning	Justification
71	R-60	R-60	Confirm existing zoning.
72A	R-60	R-60	Confirm existing zoning.
72B	RT-12.5	THD	Update to THD from pre-2014 zone.
73A	R-60	R-60	Confirm existing zoning.
73B	RT-12.5	THD	Update to THD from pre-2014 zone.
74	R-60	R-60	Confirm existing zoning.
75	R-60	CRT-1.5 C-0 R-1.5 H-65	Rezone to allow increased flexibility for multifamily development.
76A	R-60	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
76B	R-60	CR-3.0 C-3.0 R-3.0 H-70	Rezone to CR for flexibility for future mixed-use development.
77A	R-30	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
77B	R-60	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
77C	CRN-0.5 C-0.5 R-0.25 H-35	CRN-0.75 C-0.5 R-0.75 H-40	Rezone to match adjacent overall FAR while maintaining commercial density that allows the existing use.
78A	R-60	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
78B	R-20	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
79A	R-20	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
79B	R-60	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
80	R-60	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
81	R-60	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
82	CRNF-1.25 C-0.25 R-1.0 H-65	CRN-1.25 C-0.25 R-1.0 H-65	Confirm existing zoning.
83	R-60	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
84	R-60	R-60	Confirm existing zoning.



View of Station Square from Wayne Avenue



3

Plan-Wide Recommendations

Goals and recommendations to help downtown Silver Spring achieve its full potential.



3.1. LAND USE AND ZONING

This Plan envisions Silver Spring in 2040 as a thriving, mixed-use downtown that continues to be a regional center where people of all ages and stages in life want and can afford to live, work, and play. The recommendations in the sections below support and reinforce the themes of the Plan: diversity, connectivity, resiliency, community health, and equity.

The Sector Plan land use recommendations:

- Promote a diverse mix of housing types throughout the Plan area, with an estimated 11,000 new multifamily residential units in the downtown.
- Incentivize approximately 44,000 jobs in downtown Silver Spring, a 50 percent increase from what is currently existing.
- Forecast up to 46,300 people, double the existing population within the Plan area.

3.1.1. Goals

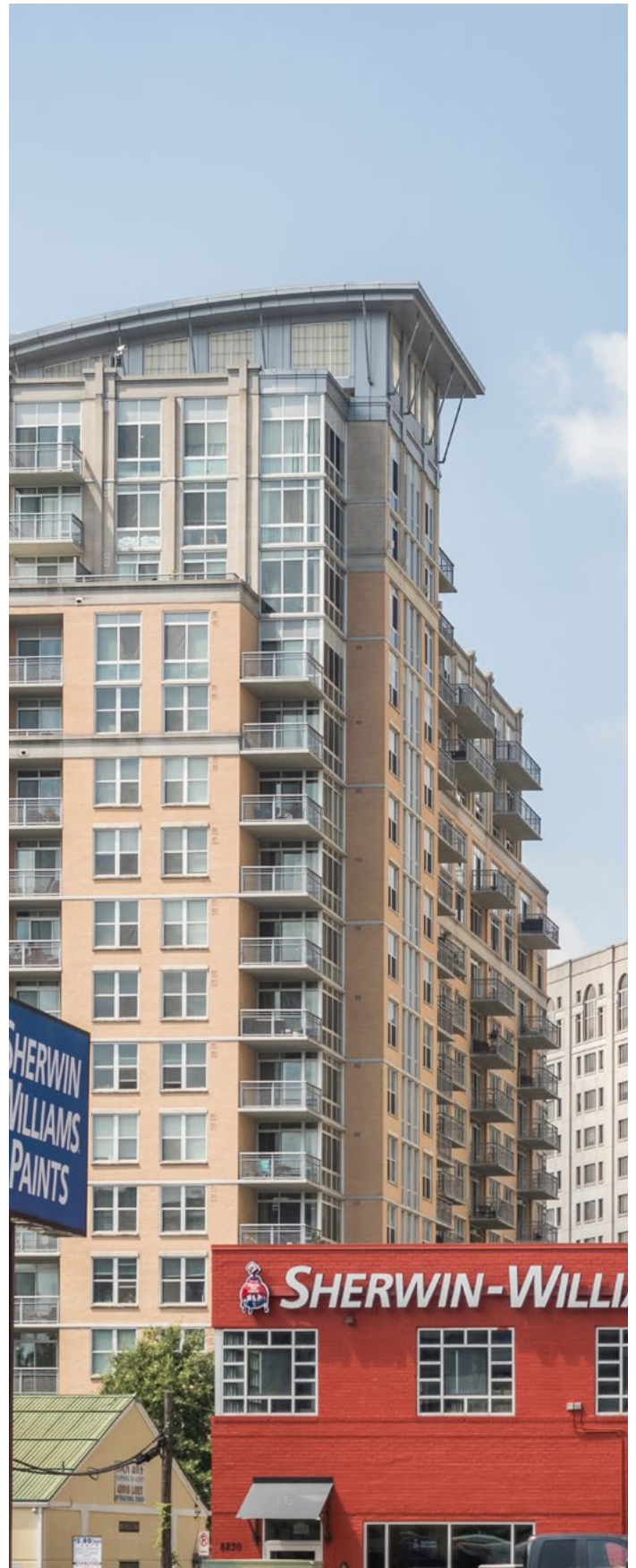
- Encourage redevelopment of under-utilized sites throughout the Plan area and create opportunities for properties to obtain additional height and density where appropriate.
- Increase flexibility in CR zoning so future development can respond to market conditions.
- Create new parks to address current lack of places for community gathering and active recreation.
- Strive for no net loss of affordable housing throughout the Plan area.
- Maintain existing light industrial zoning to support community-serving auto-repair and related small businesses.
- Encourage growth of existing retail nodes and corridors.
- Promote the development of diverse housing types in the Adjacent Communities district as recommended by this Plan.
- Preserve identified historical and cultural resources.

Aerial view of Colesville Road looking south



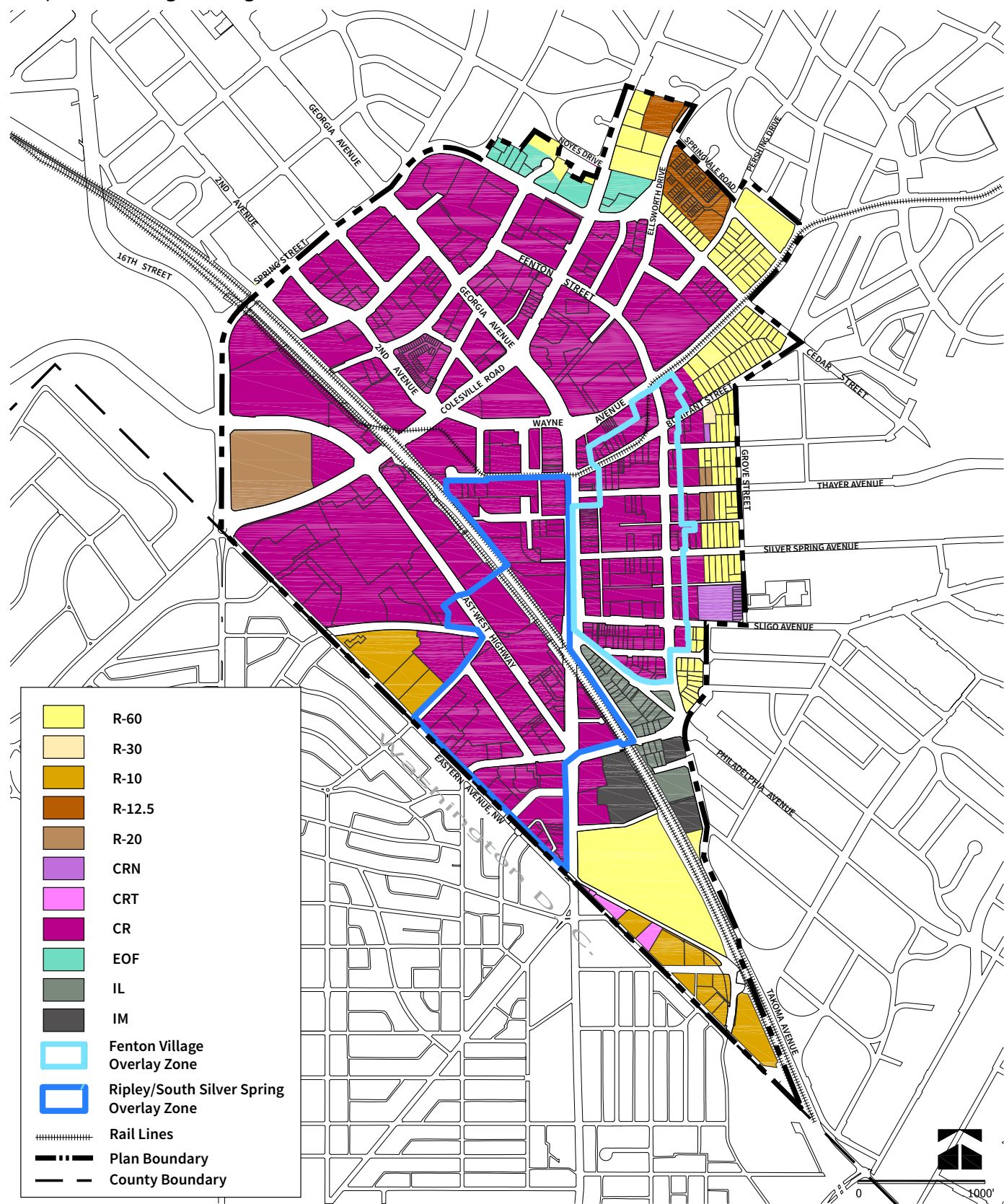
3.1.2. Recommendations

- Civic Improvement Fund
 - Any CR property in the Plan Area may obtain additional density necessary to reach the mapped maximum building height by making a contribution to the Civic Improvement Fund (CIF). CIF contributions will be made only for density above the total mapped maximum density for the site, not achieved through density-averaging, or for providing MPDUs above the required minimum, up to the approved maximum height. Refer to Section 4.1.2. Implementation.
- Increase heights on CR properties in the downtown to encourage redevelopment. The blocks surrounding the Transit Center should permit heights up to 360 feet, with Planning Board approval. Refer to the Design Guidelines that accompany this Plan.
- Update the Fenton Village Overlay Zone by adjusting the eastern boundary and revising its text to better meet the goals of the Overlay Zone and to provide clarity. Refer to Section 4.1.3. Implementation for further detail.
- Remove the Ripley/South Silver Spring Overlay Zone. The Design Guidelines for this Sector Plan will address compatibility with buildings that are along Eastern Avenue confronting single-family homes in Washington, D.C.
- For Optional Method development projects required to provide public open space on a site not recommended for a new public open space in the Sector Plan, the Plan requires contribution to a recommended park preferably in the same district, unless the Planning Board determines the public open space will significantly contribute to the public open space network proposed in this Sector Plan. Refer to Section 4.1.12. Implementation.
- Rezone all parcels in the Plan area as shown in the zoning maps and tables included in this document.
- Confirm the existing zoning for all parcels within the Plan boundary except those where zoning changes are proposed as shown on Map 16 Proposed Zoning and in the maps and tables in the Districts section above.

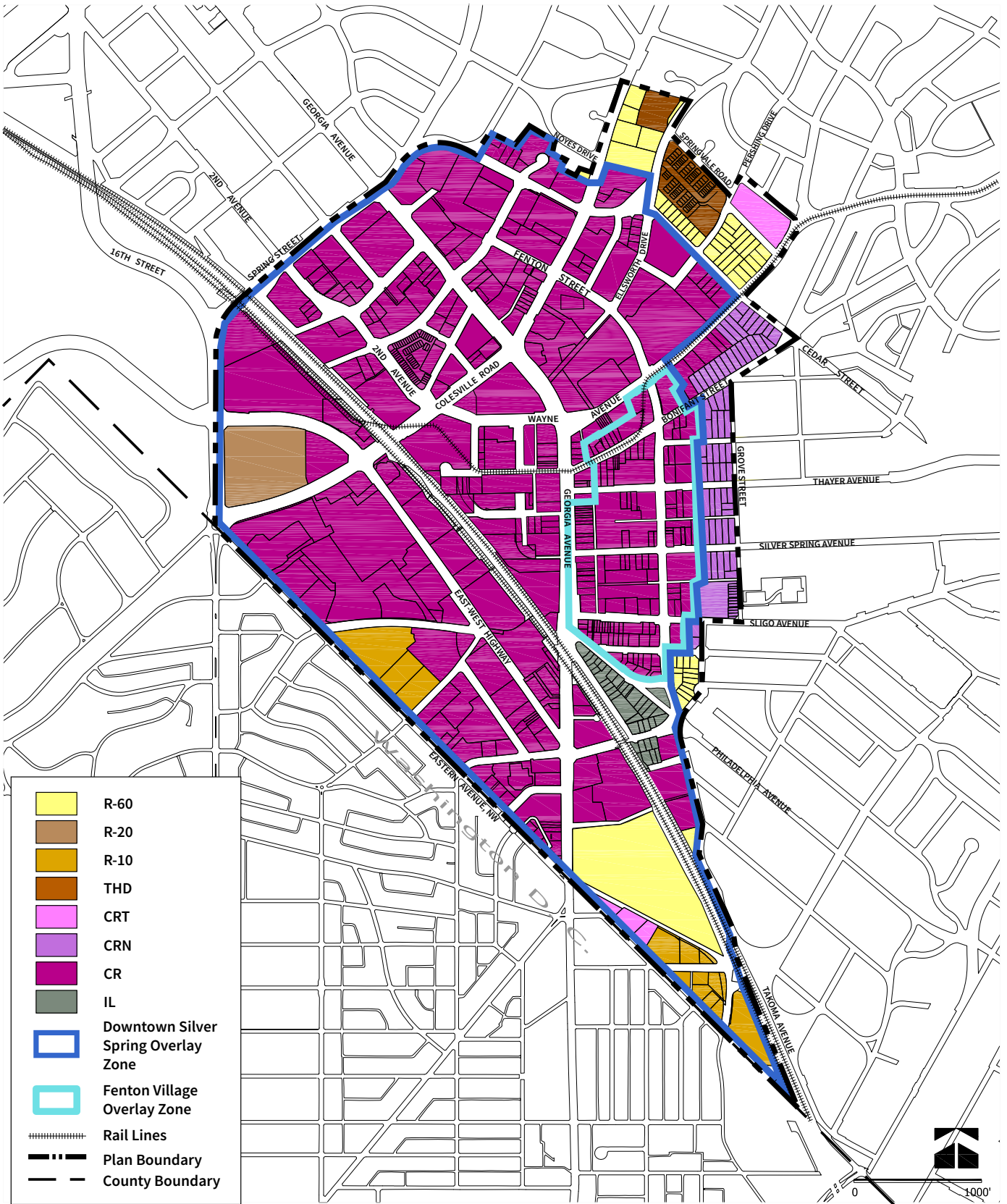


View of the Ripley District from Georgia Avenue

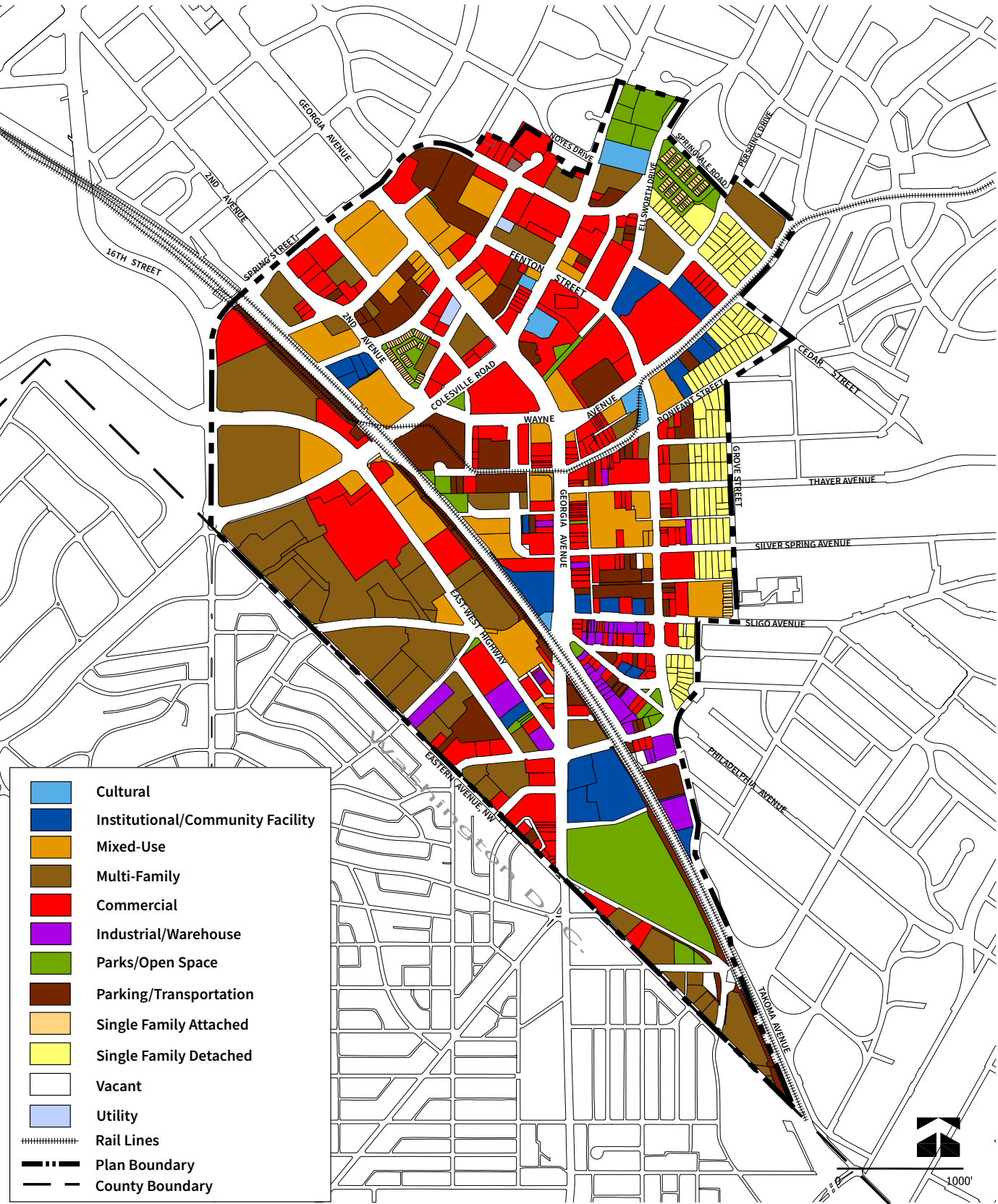
Map 14. Existing Zoning



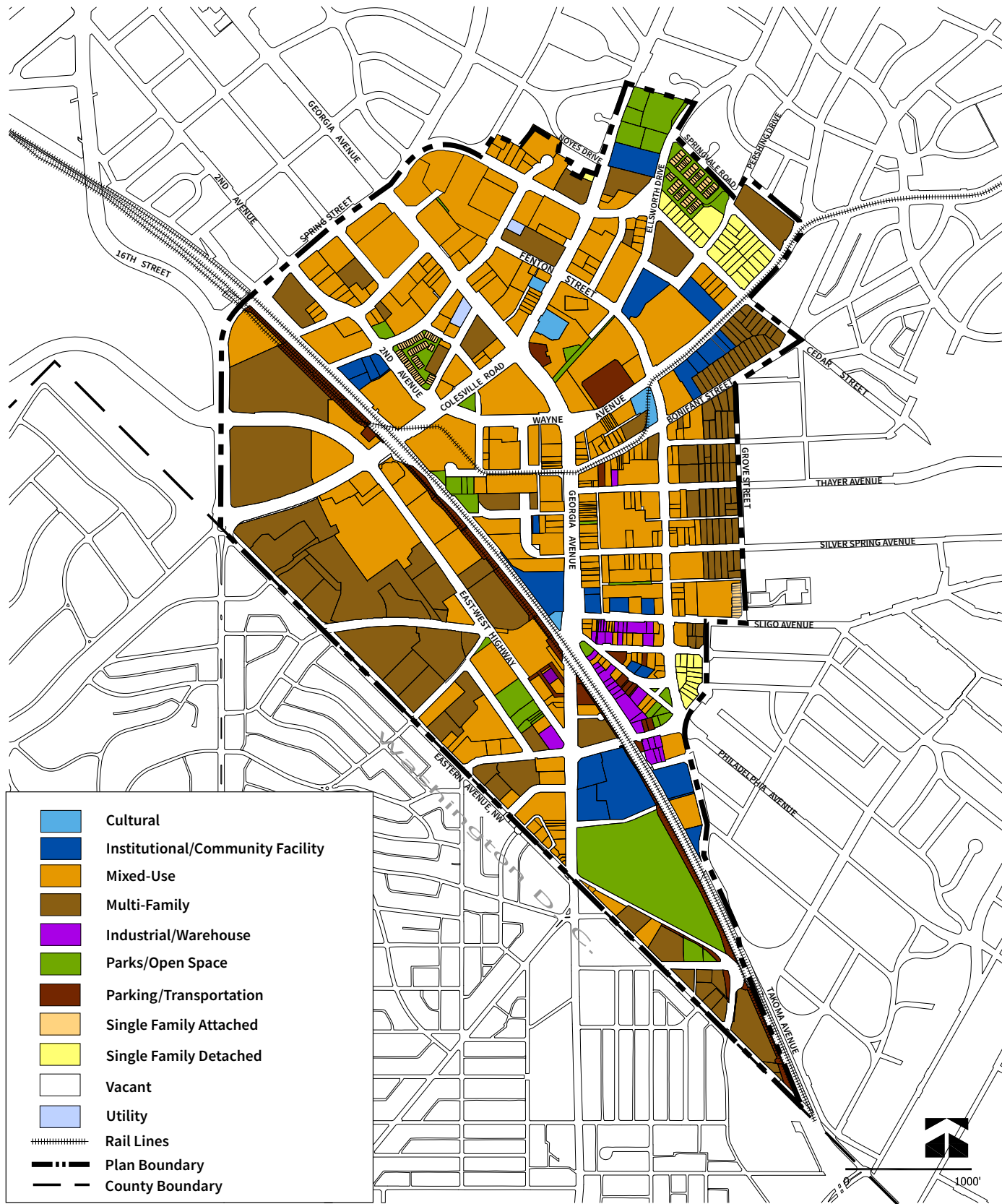
Map 15. Recommended Zoning



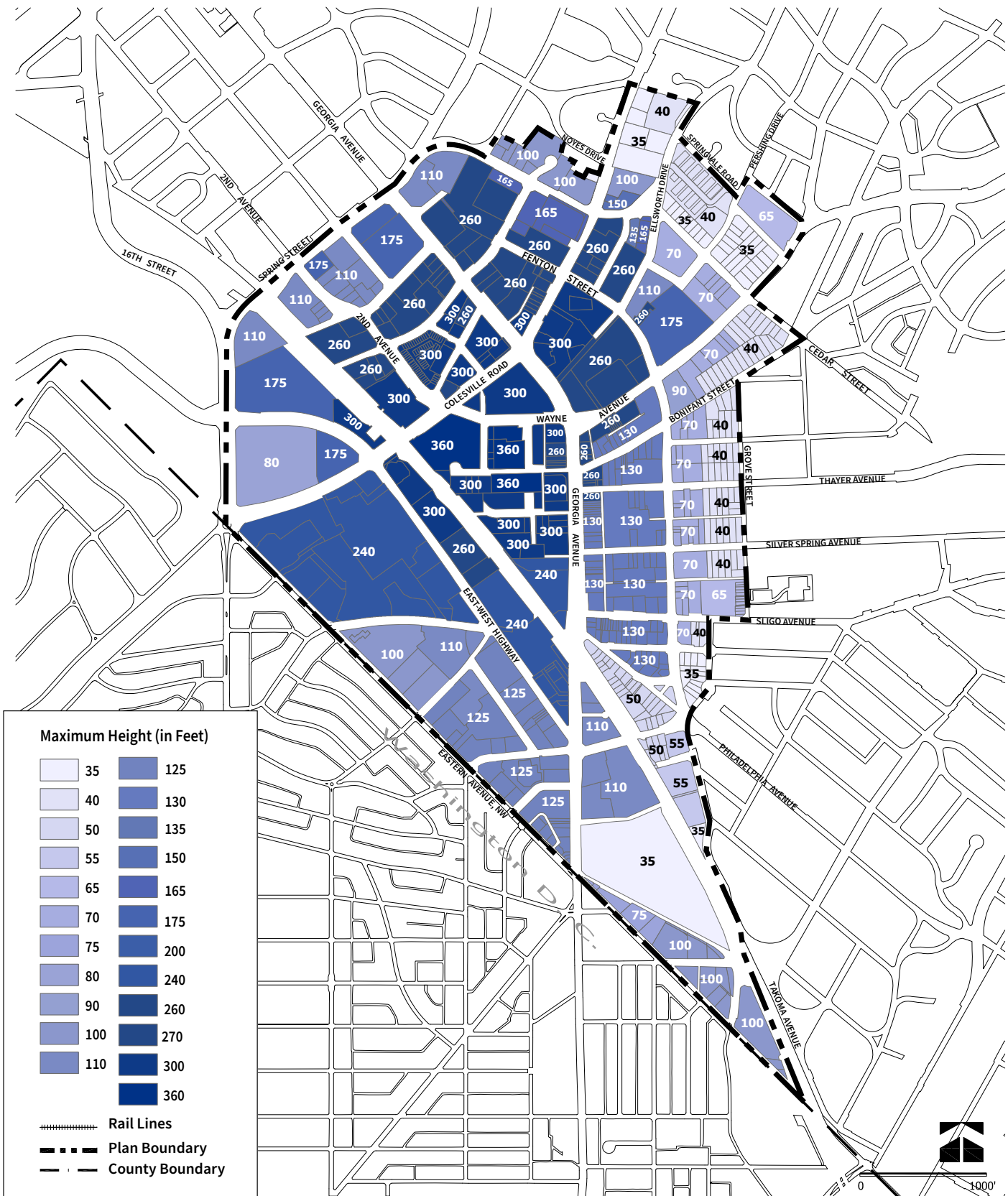
Map 16. Existing Land Use



Map 17. Recommended Land Use



Map 18. Maximum Building Heights



3.2. ECONOMIC GROWTH

Over the last 20 years, downtown Silver Spring has become a key economic engine for the eastern half of Montgomery County. It is home to arts and entertainment venues, a diverse retail landscape, major employers, new start-ups and cutting-edge science and technology companies. Over the last decade, over six million sf of new construction has shaped the streetscapes and the skyline, and the Plan envisions this growth continuing into the future. With green, walkable streets and a transit connection to all regional centers and train and air travel hubs, downtown Silver Spring will be a premier location for businesses large and small, new, or established.

Over the last ten years, downtown Silver Spring has seen significant growth in high-quality residential development, construction of a new regional recreation and aquatic center, and the expansion of the United Therapeutics bio-tech campus. But it has not attracted as many new office users despite access to transit, high quality urban form and numerous civic amenities. Retailers in downtown Silver Spring depend on the office market for daytime sales, accentuating the need for policies and interventions that attract additional jobs to fill the empty office space in the downtown before new office space will be built.

Downtown Silver Spring boasts a vibrant retail market with a sizable cluster of diverse and independent retailers, with a number of shops owned by members of or serving minority/migrant communities. Most community members that engaged in the development of this Plan cited the vibrant and diverse mix of retail as a key component of downtown Silver Spring's unique character and a major reason why consumers continue coming to Silver Spring.

A parallel study by Montgomery Planning, *Retail in Diverse Communities Study*, documented these retailers and proposed several interventions and policies to strengthen and support the cluster. The study found that most small retail businesses in Silver Spring rent their space and are vulnerable to increases of already high rents, and that many retailers reported property owners and absentee landlords who make little to no investment in their properties.

This Sector Plan envisions a future in which downtown Silver Spring is among the region's premier office markets, has continued to grow its profile as a unique retail destination, and has a diverse base of high-quality jobs in numerous industries, making it resilient to evolving economic conditions and an attractive place to work and do business. The Sector Plan recommendations to improve and enhance the public realm including the creation of the Green Loop, along with increased heights and flexible zoning are designed to achieve this vision.

United Therapeutics, Project 242T



3.2.1. Goals

- Make Silver Spring into a premier office market.
- Improve the public realm to make downtown Silver Spring more attractive for businesses, retailers, and consumers.
- Strengthen the unique retail market in Silver Spring with its diverse mix of independent stores and restaurants.
- Encourage economic growth that will be sustainable into the future.
- Focus on strengthening the retail and business economy in Silver Spring by attracting large and small new employers and retailers, and by supporting the businesses that have been in the downtown for years.

3.2.2. Recommendations

In addition to the zoning recommendations to encourage the development of underutilized property, and the recommendations to improve the public realm, the Plan recommends:

- Encourage the conversion of vacant office spaces to other uses (short term or long term), including residential (condominiums or rentals), hotels/hospitality centers, healthcare facilities, laboratory/research facilities, education facilities, and nonprofit service centers.
- Promote new ground-floor retail to established retail nodes or corridors where there is already existing retail and significant pedestrian activity. For those locations that are not adjacent to or on a block with existing retail consider alternative strategies for activating the ground floor, such as an art gallery/venue, community meeting space, educational or medical use.
- Encourage ground-floor spaces in new development that are divided into smaller bays appropriately sized for small businesses where feasible and when consistent with other Plan recommendations.
- Consider an incentive for property owners with underutilized street-level retail spaces to build and fit out the space for a retail incubator managed by a mission-oriented non-profit organization in which entrepreneurs can try new retail concepts.
- Create an incentive that provides a subsidy for new retail businesses to complete the fit-out of 2,000 square foot or smaller retail spaces.
- Explore creating a Minority Business Designation Program in coordination with the Legacy Business Program recommended in Section 3.9.7.
- The Plan recommends developing a wayfinding and branding plan that highlights the unique multicultural destinations in the downtown. Coordinate an engagement effort with the international business community to explore the designation of Fenton Village as an international or multicultural business district.

Opposite Page:

*Small businesses along Georgia Avenue (top),
Shops at the Blairs Shopping Center (middle),
Sligo Artspace, a live-work community in
Fenton Village (bottom).*



3.3. HOUSING

In 2019, the Metropolitan Washington Council of Governments (MWCOC) established regional housing targets to address a growing supply gap and affordability issues in the region. To meet our housing goals and obligations, the county needs an additional 10,000 housing units by 2030 to meet future housing demand from population and job growth. This is beyond the existing 31,000 housing units already forecasted through the completed MWCOC forecast process. In this context, this Sector Plan allows for the creation of approximately 11,000 new multifamily units in the downtown. This Plan specifically builds upon the recommendations of the 2000 Plan that emphasized high density housing in the downtown and the importance of a thriving residential community in a successful mixed-use urban area.

The vision for housing laid out in the Silver Spring Downtown and Adjacent Communities Plan is consistent with the 2011 *Housing Element of the General Plan* and builds upon a foundation of progressive housing policy laid out in several previous master plans, including the *Bethesda Downtown Plan*, the *Veirs Mill Corridor Master Plan*, and the *Forest Glen/Montgomery Hills Sector Plan* that strives to increase housing production, promote housing diversity, and preserve existing naturally occurring affordable housing. This Plan acknowledges the deep disparities in wealth and homeownership that were shaped by a legacy of discriminatory lending practices, restrictive covenants, and single-family zoning. The impacts of these policies are still being felt today and the Plan strives to address some of these inequities in the recommendations below.

This Plan recommends prioritizing providing a range of unit types for a diversity of households, including families, seniors, and persons with disabilities, to allow more diverse households to take advantage of Silver Spring's amenities. The existing rental units in the downtown are predominately smaller bedroom units.

Within the downtown area are more than 35 multi-unit residential buildings containing more than 12,000 units. Approximately 43 percent of all units in multi-unit residential buildings are currently naturally occurring affordable housing, meaning they are affordable to households earning incomes below 80 percent of the Washington, D.C. metropolitan region's area median income (AMI). At 80 percent AMI in 2020, these units would be affordable to households of three earning around \$90,720 a year for a household of three.

This Plan aims to balance the preservation of existing naturally occurring affordable housing with the production of new housing, which will result in the creation of new Moderately Priced Dwelling Units (MPDUs). The Plan seeks to achieve this through rezoning select properties or by supporting future applications for Floating Zones that prioritize the replacement of existing naturally occurring affordable housing with new income-restricted affordable housing.



Figure 15. Housing Types in Silver Spring
Clockwise from top - Duplex, townhomes, single-family home, triplex.

3.3.1. Goals

The housing goals for the plan are guided by the following policies and practices found in previous plans including the *2011 Housing Element of the General Plan*, and recent master plans. The intent of this Plan is to ensure it is in sync with current best practices in planning and housing policy. The goals as aligned with the Housing Element of 2011 include:

- Build the majority of new housing in transit-oriented locations and near jobs and employment centers.
- Encourage and support neighborhood diversity with a range of unit sizes, types, and occupancy (including rental and ownership options) to serve individuals and families of all ages, incomes and backgrounds. The range of housing types to be encouraged and supported may include Personal Living Quarters and/or micro units; “missing middle” housing types such as tiny houses, cottages, duplexes, multiplexes, and small apartment buildings; shared housing, co-housing, cooperative housing, and accessory dwelling units (ADUs).
- Create mixed-use neighborhoods with local small retail businesses and basic services within walking distance of housing.
- Encourage housing cooperatives, faith-based organizations, neighborhood housing groups, and employers to use their existing property or to purchase land and buildings for the production and preservation of housing affordable to households with low and moderate incomes
- Make housing affordable to low-, moderate-, and middle-income households a priority.
- Provide underused and strategically located surplus public properties for housing, including units affordable to low- and moderate-income households, at a higher percentage than required in the MPDU program and using best design practices. Property that is designated as parkland is not considered surplus.

Example of multifamily housing



3.3.2. Recommendations

- Require all Optional Method of Development projects to provide a minimum of 15 percent moderately priced dwelling units (MPDUs) or other DHCA-equivalent affordable housing.
- Preserve existing, naturally occurring affordable housing where possible, striving for no net loss of affordable housing in the event of redevelopment.
- Publicly owned properties should be encouraged to provide a minimum of 30 percent MPDUs, with 15 percent affordable to households earning at the standard MPDU level of 65-70 percent or less of Area Median Income (AMI) and 15 percent affordable to households earning less than 50 percent of AMI.
- Provide a range of unit types for a diversity of households, including families, seniors, and persons with disabilities.
- Support partnerships among public, private, and philanthropic institutions, when possible, to assist in the development of affordable housing.
- Increase infill housing opportunities in office buildings with high vacancy rates and other underused properties by setting commercial and residential densities the same in CR zones.
- The Plan also recognizes the need and supports development of permanent supportive housing for the homeless in the Plan area.



Elizabeth Square, a mixed-use development currently under construction with a significant amount of affordable housing via a public-private partnership.



Eastern Village Cohousing, a co-op on Eastern Avenue.



Missing middle housing in Bethesda.

3.4. URBAN DESIGN

3.4.1. Building Form

Downtown Silver Spring contains a diversity of building forms, from one- and two-story retail along Colesville Road, Georgia Avenue, and throughout Fenton Village, to mid- and high-rise office and apartment buildings, as well as the house-scale buildings of the Adjacent Communities. New development in each of the districts should respond to these varied conditions.

Goals:

- Encourage future growth in downtown Silver Spring that takes advantage of additional allowable height while considering street-level experience, views and building form.
- New development should be compatible with the urban form and scale of the immediate surrounding neighborhood context.
- Design tall buildings with bases appropriate for active ground floor uses and architecturally interesting towers.
- Create a walkable environment where buildings frame the public realm and the lower floors of the building respond to the pedestrian environment at the ground floor.

Recommendations:

- Prepare Design Guidelines to define Design Excellence for new development throughout the plan area.
- Create a Design Advisory Panel (DAP) that allows community participation and is representative of the Plan area. Design Review will be required for all Optional Method Development Projects in the Plan Area.
- Encourage innovative building form and allow flexibility in design. Vary tower heights, setbacks, and building materials.
- Limit bulky towers and consider architectural features, building articulation, solar building orientation, and access to light and air.
- Implement noise mitigation strategies for new development to comply with the county's noise ordinance.
- In the Adjacent Communities, new development should be compatible in massing and form to the surrounding context.

Montgomery College along Georgia Avenue



3.4.2. Public Realm

The public realm includes all outdoor space that is publicly accessible. This includes streets, parks, open space, sidewalks, alleys, and plazas. A green, maintained, well-connected, walkable, safe, multi-modal public realm is essential to the success of an urban area.

Goals:

- Introduce new public spaces in downtown Silver Spring that strengthen each district, contribute to community identity, and improve access to public spaces for all.
- Improve existing public spaces to contribute to community health and connectivity.
- Focus on the existing local streets as neighborhood “spines” and convert these to resilient complete streets that will improve the pedestrian and bicycle network, help to cool the downtown, and manage storm water runoff.
- Activate vacant spaces (both publicly and privately owned) through temporary placemaking strategies. These tactical moves can help spur long-term public realm transformation and improve public life.

Recommendations:

- Implement the Green Loop concept on the streets indicated in Section 2.2.
- Encourage public and private property owners to consider tactical urbanism as a form of placemaking, including, but not limited to, the following activation strategies:
 - Temporary Plazas are public open spaces that are created out of underutilized space on or adjacent to existing streets or open spaces. They can transform empty pavement into lively gathering spaces. These plazas can host events, pop-up retail kiosks, or local food trucks.
 - Temporary Parks use a part of the street, usually one or two parallel parking spaces, as public space in which people can gather and socialize. They are composed of seating, planters, tables, and at times, platforms that separate the people from the surrounding pavement. They can be temporary and experimental or seasonal.
 - Outdoor Eateries typically take place in front of storefronts either directly adjacent to the building wall, or along the curb. This intervention is feasible where the sidewalk is wide enough to accommodate both the eating area and adequate pedestrian access. Outdoor eating areas can also occupy a closed parking or travel lane; this was successfully implemented throughout Montgomery County, including along the east side of Georgia Avenue, in 2020 during the Covid-19 pandemic.
- Activate spaces with placemaking efforts, including public art and programming, that celebrate and highlight the diverse cultures of downtown Silver Spring.

Outdoor eating in downtown Bethesda



3.5. PARKS TRAILS AND OPEN SPACES

The Plan envisions a community where all residents, visitors, and workers will enjoy a network of welcoming multi-use parks, trails, and public spaces. Parks and public spaces in urban areas are centers for community experiences and interaction, and improve the quality of life for community members of all ages, races, cultures, income levels, and physical abilities.

The goals and recommendations for this plan were developed in concert with policy guidance from previous and ongoing plans including the 2010 *Silver Spring Central Business District Green Space Guidelines*, the 2017 *Parks, Recreation, and Open Space Plan* (PROS Plan), and the 2018 *Energized Public Spaces Functional Master Plan* (EPS Plan), in addition to input from the community who voiced the importance of parks and public spaces to their quality of life.

The goals and recommendations below support the Plan's overall themes of diversity, connectivity, resiliency, and community health.

3.5.1. Goals:

- Encourage physical activity by providing safe and convenient access to a multi-functional, comfortable network of parks, public spaces, and trails connecting people to jobs, centers of activity, and nature.
- Facilitate social interaction by developing parks that offer opportunities for impromptu interactions with neighbors as well as organized social gatherings.
- Steward the environment within the urban context by integrating sustainable strategies to adapt to and mitigate climate change while maintaining a commitment to environmental stewardship.
- Promote economic prosperity by creating fun and appealing park facilities and programs that energize Silver Spring, while celebrating its rich history and vibrant culture.

Map 19 Key:

Existing Parks and Major Public Spaces

M-NCPPC Parks

1. Ellsworth Urban Park
2. Gene Lynch Urban Park*
3. Silver Spring Transit Plaza
4. Acorn Urban Park
5. Philadelphia Avenue Urban Park
6. Fenton Street Urban Park
7. King Farm Community Garden
8. Jesup Blair Local Park
9. Juniper Blair Neighborhood Park

Non-M-NCPPC Public Spaces

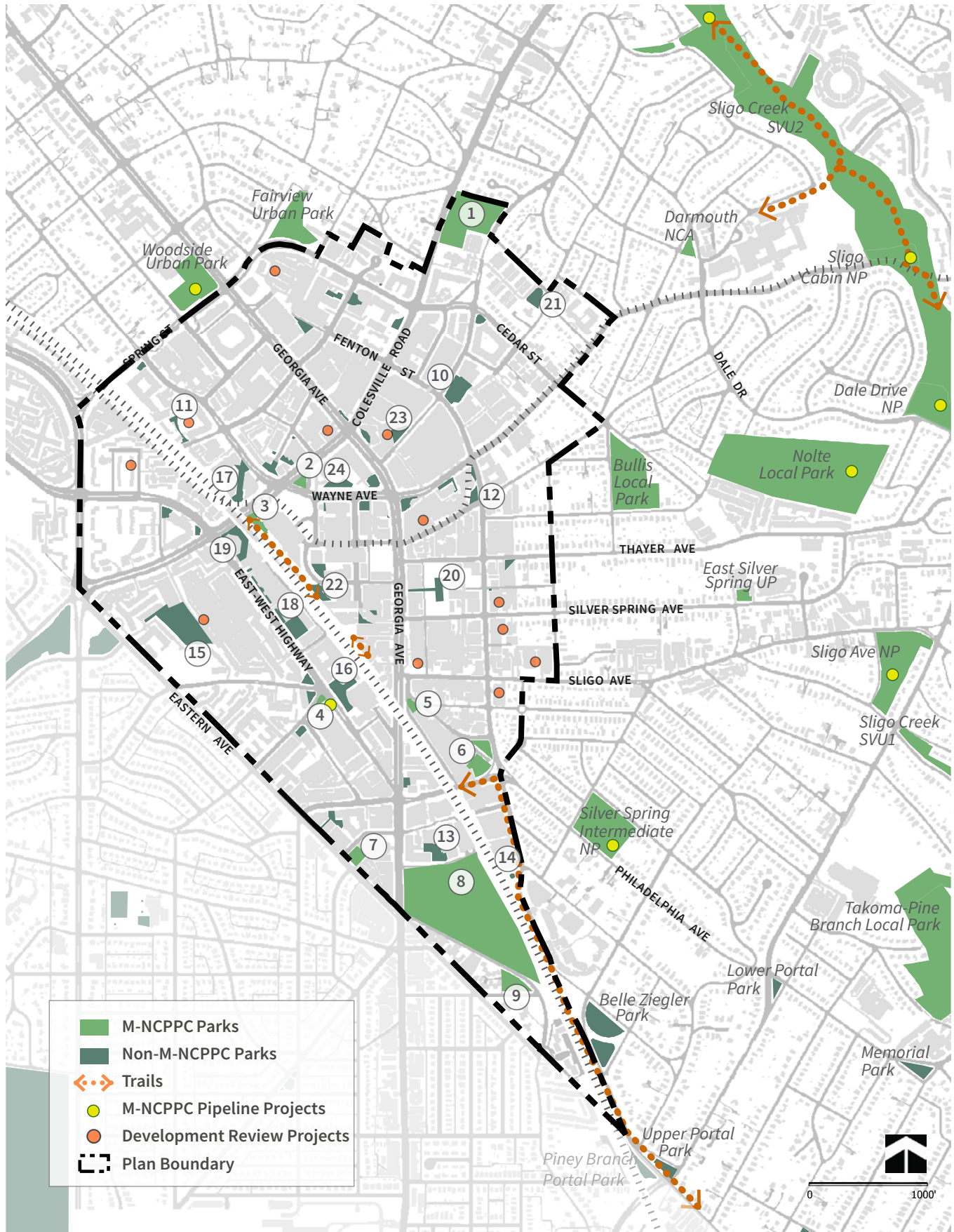
10. Veterans Plaza
11. South County Regional Recreation & Aquatic Center
12. Silver Spring Public Library
13. Montgomery College Plaza
14. Montgomery College Community Garden

Privately-Owned Public Spaces (10 out of 42 > than 1/4 acre)

15. The Blairs
16. The Silverton/Silver Spring Gateway
17. Silver Spring Metro Plaza
18. NOAA/Metro Center
20. Studio Plaza
21. Chelsea Court
22. 1050 Ripley Street
23. Silver Plaza
24. Discovery Civic Green

* WMATA owns the property;
M-NCPPC has an easement on the property

Map 19. Existing Parks and Major Public Spaces



3.5.2. Recommendations:

The recommendations of this Plan create an urban, social, active, and healthy community for all, and will strengthen the role of parks and recreation regarding physical activity, social connections, environmental stewardship, economic competitiveness, and racial equity.

Equity

- Promote a fair and equitable distribution of attractive, safe, and fun parks and public spaces as shared civic spaces with facilities and programming open to people of all ages, race, culture, income, and abilities.
- Add park facilities and programs that promote physical activity and social interaction.
- Equitably distribute park experiences across the various districts of the Plan. Parks should be located near transit, accessible via walking and biking, and surrounded by active building frontages. See Section 3.5.4 for proposed park locations.
- Design parks to be multi-functional. Create new and/or renovate existing parks and public spaces to accommodate multiple needs, including recreation, education, community-building, and environment stewardship within the urban context. Consider co-location, adaptive reuse, joint programming, and shared use of land, buildings, and facilities whenever possible to optimize limited available land.
- Promote facilities that celebrate cultural and historic aspects of the community, and that encourage social connection, and a sense of pride and ownership of shared spaces.
- Expand the urban tree canopy coverage and pervious surfaces in both publicly and privately owned parks and public spaces. Promote watershed connectivity through education and best stormwater management practices. Refer to Section 3.7. Resilient Downtown.
- Promote the creation of local organizations to support major parks and public spaces. Engage stakeholders early in the planning, programming and design of parks and public spaces to collaborate on creative solutions and to ensure that ultimate park designs reflect community needs and identity. Work with the community and schools to develop early advocacy programs and activities to encourage nature appreciation, education, and stewardship.

Green Loop Connectivity

- Implement the proposed Green Loop to connect existing and proposed parks and public spaces with other land uses inside the Plan and the surrounding region. Promote walking and biking to these places along comfortable, sustainable, safe and shaded roads, sidewalks and trails. The Plan recommends improving signage and wayfinding of parks, public spaces and trails within Silver Spring.

Creative Implementation

- Consider short-term/temporary solutions and “pop-up” programming that reflect community identity within temporary and/or interim parks. Consider empty lots, surface parking areas or other opportunity sites adjacent to the Green Loop as potential pilot sites.



Movie night at Jesup Blair Park



Ellsworth Park (Montgomery Parks)



Woodside Urban Park (Montgomery Parks)

3.5.3. Equity in Parks and Public Spaces Implementation

Silver Spring was identified as an implementation priority area through the mapping of Experience Improvement Areas (EIAs) of the Energized Public Spaces (EPS) Plan) with the overlay of the Equity Focus Areas (EFAs) from Montgomery Planning's Equity Focus Areas Analysis. Recommendations described in this Plan will help prioritize where and how to spend limited public funds where the highest need is identified.

- Experience Improvement Areas: The EIA map shows areas where low experience scores are spatially clustered. Experience scores were determined by calculating the ratio and scoring of accessible public space experiences to the number of people capable of walking to them from a specific location. See more information on this map on Appendix D.
- Equity Focus Areas: The EFA map identifies census tracts that have high concentrations of lower-income people of color, and those residents who also have limited English language skills. See more information on this map on Appendix D.

Goal:

Prioritize underserved communities in the distribution of park resources and contribute to the efforts initiated by the County Council on racial equity and social justice.

Recommendations:

- Prioritize and identify opportunity sites and test scenarios to measure the impact of additional development on park facilities to better address distribution of parks resources.
- Utilize EFAs and EIAs as tools to prioritize funding and implementation for parks and public spaces projects.

3.5.4. Proposed Park Locations and Park Recommendations

The 2017 and 2022 PROS Plans and the 2018 EPS Plan state that each area master plan should recommend an interconnected system of parks that achieve multiple objectives whose size, primary and supporting functions should be directly proportional to the projected density and land use patterns of the community.

The ongoing 2022 PROS Plan will be revising the park classification system categories associated with the Proposed Park Locations shown in Figure 21. The categories of the proposed park recommendations are based on the 2017 PROS Plan Parks Classification categories: Civic Green, Plaza, Urban Recreational Park, etc.

All urban parks should be strategically located to allow access by walking and biking to and from public transit and other complementary land uses to support the goals of encouraging physical activity, facilitating social connection, accessing nature, and promoting economic prosperity.

For the Sector Plan Area:

- Opportunities for active, contemplative, and social gathering experiences within parks
- Central civic spaces emphasizing social gatherings
- Interconnected system of sidewalks and trails to connect parks and open spaces through the proposed Green Loop

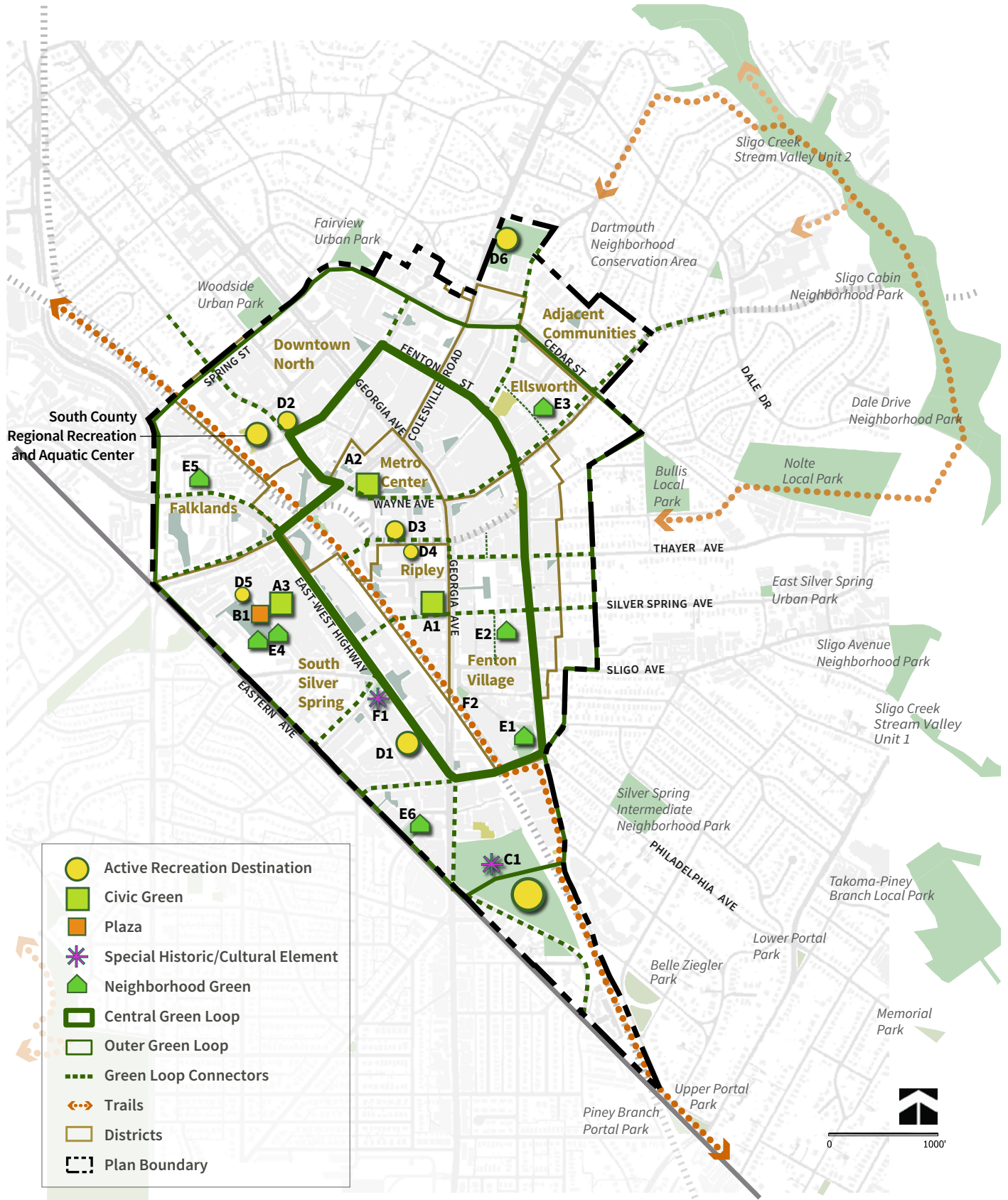
For each Urban District: Recreational amenities that can be accessed by walking or biking.

Map 21 identifies proposed locations for parks and public spaces in the Plan area. The map illustrates how the proposed green space network is connected to the Green Loop. The designation of active recreation destinations, Civic Green, Plaza, Neighborhood Greens, etc., highlights the major function of each open space but does not limit other experiences. All parks should provide social, active, and nature-based experiences to the extent possible.

In addition, the park recommendations also incorporate community input, previous studies, legacy infrastructure, localized needs, location of Experience Improvement Areas (EPS Plan methodology), Equity Focus Areas and other factors to deliver the “right parks in the right places.”

The current naming of the proposed parks and public spaces is subject to change and will be defined during the implementation phase of the project.

Map 20. Proposed Park Locations Approach Diagram



A. CIVIC GREEN

Civic Greens are spaces that emphasize social gathering. They are flexible gathering spaces for informal or large special events. Civic Greens may support open air markets, concerts, festivals, and/or special events. A large central lawn may be the focus, often accompanied by a plaza. Adjacent spaces provide complementary uses such as gardens, water features and/or shade structures. Supporting functions might include opportunities for physical activity and nature enjoyment.



Bryant Park, NYC (Wikipedia)

A1. Create: Ripley District Civic Green

Location: Ripley District
Likely ownership: POPS

This Civic Green will be an important focal point and social gathering space for the Ripley District. Its central location within the Plan area and along the Central Green Loop will serve the highest density commercial and residential areas. Along with the proposed bridge over the rail it will serve as a gateway between East-West Highway in South Silver Spring and Georgia Avenue in the Ripley District. This space will may include a central lawn and will be flexibly designed to accommodate a variety of experiences such as community events, unstructured recreation, physical activity, contemplation, and interaction with nature. This space should include a balance of sunny and shaded areas. Interactive installations with educational components and/or visitor amenities could be explored.

Recommended Size: 0.5 acre minimum; 1.5 acres ideal

A2. Create: Gene Lynch Civic Green (currently under-construction)

Location: Metro Center District
Ownership: MCDOT/M-NCPPC

This Civic Green will be an important focal point and social gathering space for the Metro Center District. Its location across from the Transit Center will function as a gateway to the commercial core of downtown Silver Spring. The main programming of this space will be flexible to accommodate community events and unstructured recreation with a central lawn area enclosed by lush plants and trees. A variety of seating options will be provided in both sunny and shaded areas, along with an amphitheater space and bench swings.

Size: 0.25 acres

A3. Create: Blair Park/The Terrace (currently approved under The Blairs Master Plan)

Location: South Silver Spring District
Likely ownership: POPS

This Civic Green will be an important focal point and social gathering space for the South Silver Spring District. The Blairs Master Plan describes this space as the heart of the Blairs development defined by retail storefronts, outdoor dining, and building entrances. The main programming of this space will be flexible to accommodate community events, unstructured recreation on a central lawn, and places for contemplation of nature. It will have a balance of sunny and shaded areas and will connect to the proposed Sonny's Park through a pedestrian path designed in a switchback pattern to provide maximum accessibility. Opportunities for interactive installations with educational components, visitor amenities, and active recreation will be explored.

Size: ~ 0.95 acre

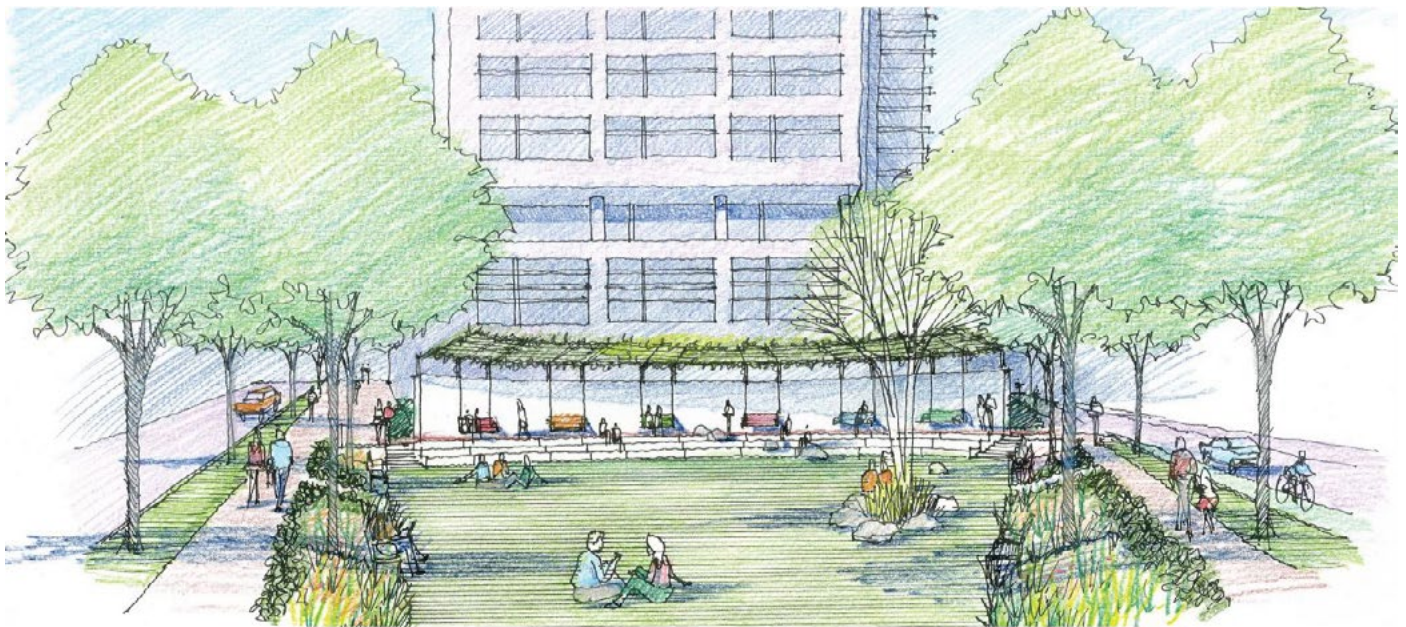


Figure 16. Illustrative Concept for Gene Lynch Urban Park (Montgomery Parks)

B. PLAZA

Plazas often complement Civic Greens, as these spaces also emphasize social gathering. Plazas typically have a central hardscape area rather than a central lawn and are often located near transit stops and/or commercial and high-density residential areas. Plazas should face streets and building frontages to maximize pedestrian use and exposure and have access to sunlight. They may support activities including open air markets, concerts, festivals, and special events.

B1. Create: Sonny's Park (currently approved under The Blairs Master Plan)

Location: South Silver Spring District

Likely ownership: POPS

This plaza space will be located at the southern end of the proposed Blair Park/The Terrace Civic Green and will be the primary social gathering space for the lower escarpment of the development.

Size: ~ 0.4 acre



Veterans Plaza, Silver Spring (Silver Spring Civic Building)

C. COUNTYWIDE URBAN RECREATION PARK

A Countywide Urban Recreational Park responds to the recreational needs of surrounding neighborhoods and districts and provides spaces for a variety of activities. Athletic fields, playing courts, picnic areas, dog parks, sitting areas and flexible grassy open spaces all may be found in this type of park. Programming might include farmer's markets, outdoor exercise classes, and/or community yard sales, etc. There should be a safe area for vehicular drop-offs and integrated accessible parking for those who cannot walk to the park. Physical activity is the main emphasis of this park, but social and contemplative opportunities should also be considered.

C1. Renovate: Jesup Blair Park

Location: South Silver Spring District

Ownership: M-NCPPC

This is a regional gateway urban park to downtown Silver Spring. This renovated urban recreational park will be a countywide destination that promotes an active lifestyle and opportunities to gather. It offers the unique historical and cultural setting of a special park designated on the *Master Plan for Historic Preservation*.

Current Size: ~ 14.2 acres

At 14.2 acres, Jesup Blair Local Park is the largest park within the Sector Plan area and is designated on the *Master Plan for Historic Preservation*. Community members have stated that this park is underutilized and that it needs facilities and landscape improvements and additional recreational experiences. Access to the park presents additional challenges, as it is located away from downtown Silver Spring and separated from the walkable urban fabric by physical barriers, such as railroad tracks and fences. Georgia Avenue and Blair Road are busy roads that border the park and are lacking safe pedestrian crossings.



Mission Dolores Park, San Francisco (kochla.com)

Concept Plan

In response to community input and previous redevelopment recommendations from the 2018 EPS Plan, the Parks Department is developing a concept plan for this park concurrent with the sector plan process. The concept plan will establish a framework for the redevelopment of the park into an active, regional destination with amenities that appeal to a broad range of interests and will be further refined and developed through the facility planning process.

The following recommendations improve physical and visual access to the park:

- Implement connections to the Green Loop at the southern tip of the proposed Outer Loop as shown on Map 4. At the border with Washington, D.C., this park will be an important gateway in welcoming people to Maryland.
- Expand connectivity/visibility and presence of the park along Georgia Avenue by pursuing a partnership with the DC Office of Planning to discuss opportunities to develop the corner sites along Blair Road and Georgia Avenue with active building frontages along the park, and open viewsheds to the park along Georgia Avenue.
- Improve wayfinding and signage in and around the park by collaborating with Montgomery College to improve visibility of the entrance to the park along Fenton Street at the pedestrian bridge. Install signs along the Metropolitan Branch Trail and below the pedestrian bridge with distances to the park to direct users to this entrance. Consider partnering with the owner of the adjacent storage facility to install a public art mural to provide more visibility to the entrance ramp area and encourage people to cross the bridge to the park. Consider partnering with organizations to create additional art installations across the rail to attract attention to the location of the park from Fenton Street.
- Consider implementing traffic calming features along Blair Road to make it more safe, accessible, and comfortable. Work with SHA/MCDOT to add new and renovate existing pedestrian crossings in these locations: at both ends of the park along Georgia Avenue to improve the pedestrian access and experience across this busy road and connect to the small businesses along that stretch of Georgia Avenue; at Blair Road at the edge of the park boundary to allow pedestrians safe crossing to the park entrances along Blair Road. If development patterns along and surrounding Blair Road change in the long term, and traffic calming measurements are already in place, consider working with MCDOT and SHA to study the feasibility of a closure of Blair Road along the perimeter of Jesup Blair Park on weekends and/or weekdays. It would allow expansion of the proposed active zone at the southern portion of the park and would allow direct connection to Juniper Blair Neighborhood Park across the street expanding the zone for recreation and social interactions.
- Work with SHA/MCDOT to consider art treatment of the two pedestrian crossings along Georgia Avenue as well as improvements on links to the privately owned public spaces along Jesup Blair Drive to mark the park entrances and transition to other pedestrian destinations.
- Consider removal and/or reduction of the perimeter fence around the park to make the park feel more welcoming.



Entrance to Jesup Blair Park from Georgia Avenue at Jesup Blair Drive

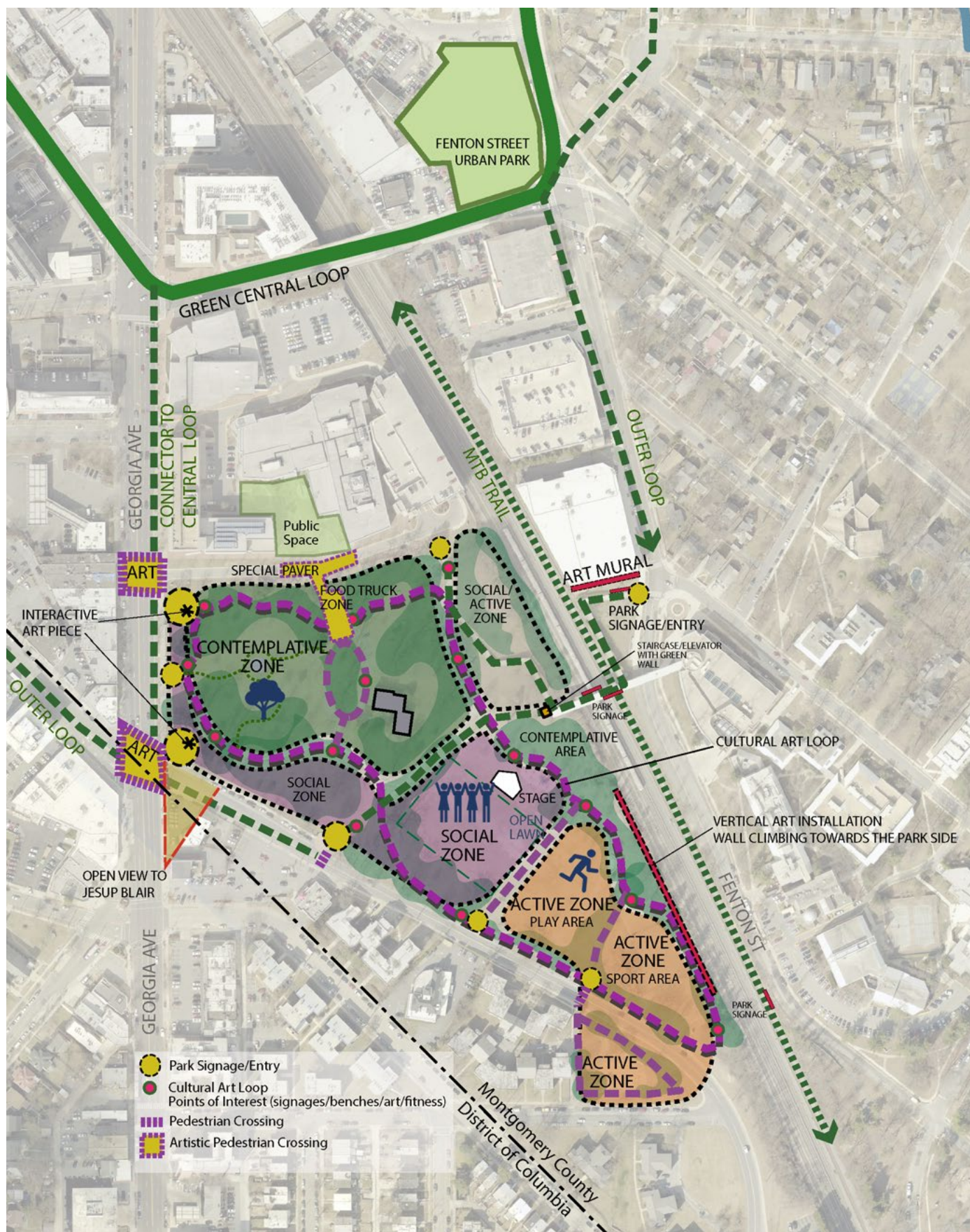


Figure 17. Illustrative Concept for Jesup Blair Urban Recreational Park

The following recommendations promote social connection, diversity, community health, identity and sense of ownership of the park:

- Promote robust programming for the park with activities and events that can appeal to a diverse audience and will encourage people to return to explore the park on their own.
- Encourage multi-use spaces such as lawn, play areas and courts instead of specialized uses. The existing central soccer field would continue to work as a central lawn space and multi-age play area. The transformation of this local park into a countywide destination park will allow its facilities and spaces to serve a much larger and diverse audience during large events. The park infrastructure should also consider the daily or weekly use of local residents. Consider including space for community-scale events such as performances and farmer's markets. Allow space for kiosks and community bulletin boards, cafes, or street vendors.
- Create a variety of experience zones inside this large park emphasizing active, social and contemplative areas, making the park a destination place with many things to do for different age groups, interests, and abilities.
 - Balance the amount of active, contemplative, and social experiences offered.
 - Inside the contemplative zone consider going beyond the traditional passive uses by introducing active programs such as yoga, tai-chi, and other activities that would be enhanced by the natural setting of this zone including a restored Jesup Blair House.
 - The social zone can accommodate a variety of uses such as café, lawn area, stage/ amphitheater, dog park, etc. The active zone may accommodate multi-age play areas, multi-use courts, zip line, skate park, and other activities.
- Promote partnerships that celebrate cultural aspects of the community through public art. Explore temporary installations (especially along Georgia Ave) and permanent pieces of art in the park. A great example to consider is the synergy between a café in the park and a sculpture area.
- Create a signature internal active/recreational Cultural Art Loop Trail with mileage markers, historic signage, art, benches, a mix of shaded and sunny areas and fitness equipment to integrate the existing

and future proposed recreational facilities and encourage physical exercise.

- Renovate the historic Jesup Blair House for appropriate community uses that complement the park programming. Identify a future partner who will help activate the contemplative zone to attract users to the park, gain community support to steward the unique historic setting, and enjoy the many activities in the park.
- Improve connectivity of the historic house and surrounding setting to the rest of park through the implementation of the cultural art loop, and other improvements that will activate the frontages along Georgia Avenue, Blair Road and Jesup Blair Drive to invite people into the park.
- Consider interactive design solutions to enhance the relationship between people and elements of the natural environment. Use a diverse palette of plant and tree species, including native plants, that provide different colors, form, and textures throughout all the seasons. Apply a diverse range of plants to minimize potential expansive destruction from pests or harsh climate conditions.
- Friends of Jesup Blair Park - engage the community to support this park, perhaps through a local organization. Create a framework for an inclusive and collaborative approach to promote recreational opportunities to all.
- Consider extending the operating hours of this park beyond sunset and introduce lighting to safely extend the use of the park.



Jesup Blair House



CONTEMPLATIVE ZONE



SOCIAL ZONE



ACTIVE ZONE

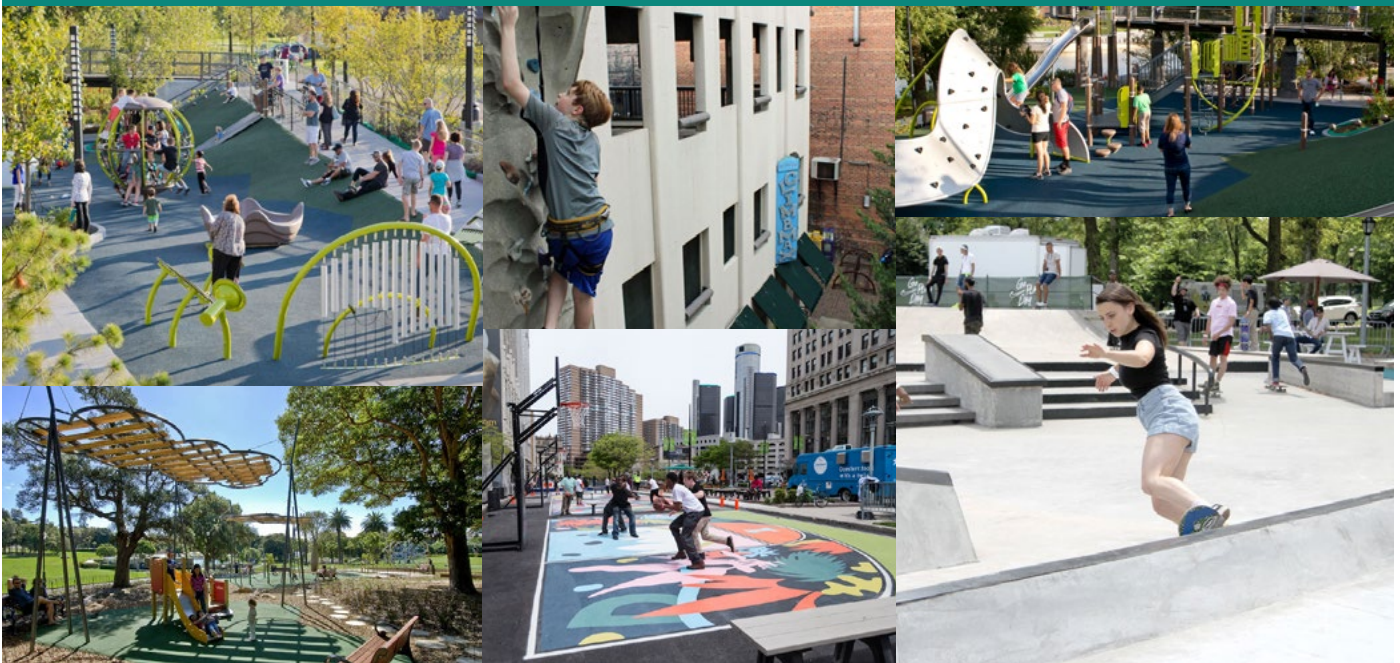


Figure 18. Jesup Blair Park Experience Zones

D. COMMUNITY USE URBAN RECREATION PARK

Community Use Urban Recreational Parks serve the immediate neighborhood. This type of park is primarily programmed for physical activity, with supporting programming for social and contemplative opportunities. Typical facilities include smaller sport courts, skating spots, lawns, playgrounds, or similar neighborhood recreation facilities. Sizes vary considerably depending on the land available and community size served.

D1. Create: South Silver Spring Park

Ownership: M-NCPPC

This park will serve as a through-block connection between East-West Highway and Kennett Street at the current location of the National Tire and Battery site. This park will be a key local active recreation destination in South Silver Spring that will serve the surrounding blocks. The direct connection to the Central Green Loop will link this location to other recreational parks proposed in this Plan. Montgomery Parks acquired one of the two parcels for this park in 2021. Its proximity to Acorn Urban Park will promote a synergy of park user experiences between the historic and contemplative setting of Acorn Park to the more active and social-driven space on the new proposed urban recreational park. Consider working with MCDOT to create a connection via the existing sidewalks between these two parks that would benefit the park users and the local businesses nearby. Consider including a seating area with tables that could support the businesses along East West Highway. Recreational amenities that could be considered for this park include multi-use courts, small open lawn, picnic area, multi-age play area, dog run, exercise stations, etc. Consider potential partnerships with adjacent properties to align park facilities with neighborhood needs, including opportunities for public art, such as murals, on existing building walls facing the site.

Size: ~1.62 acres



A'Beckett Urban Square, Melbourne, Australia (John Gollings)

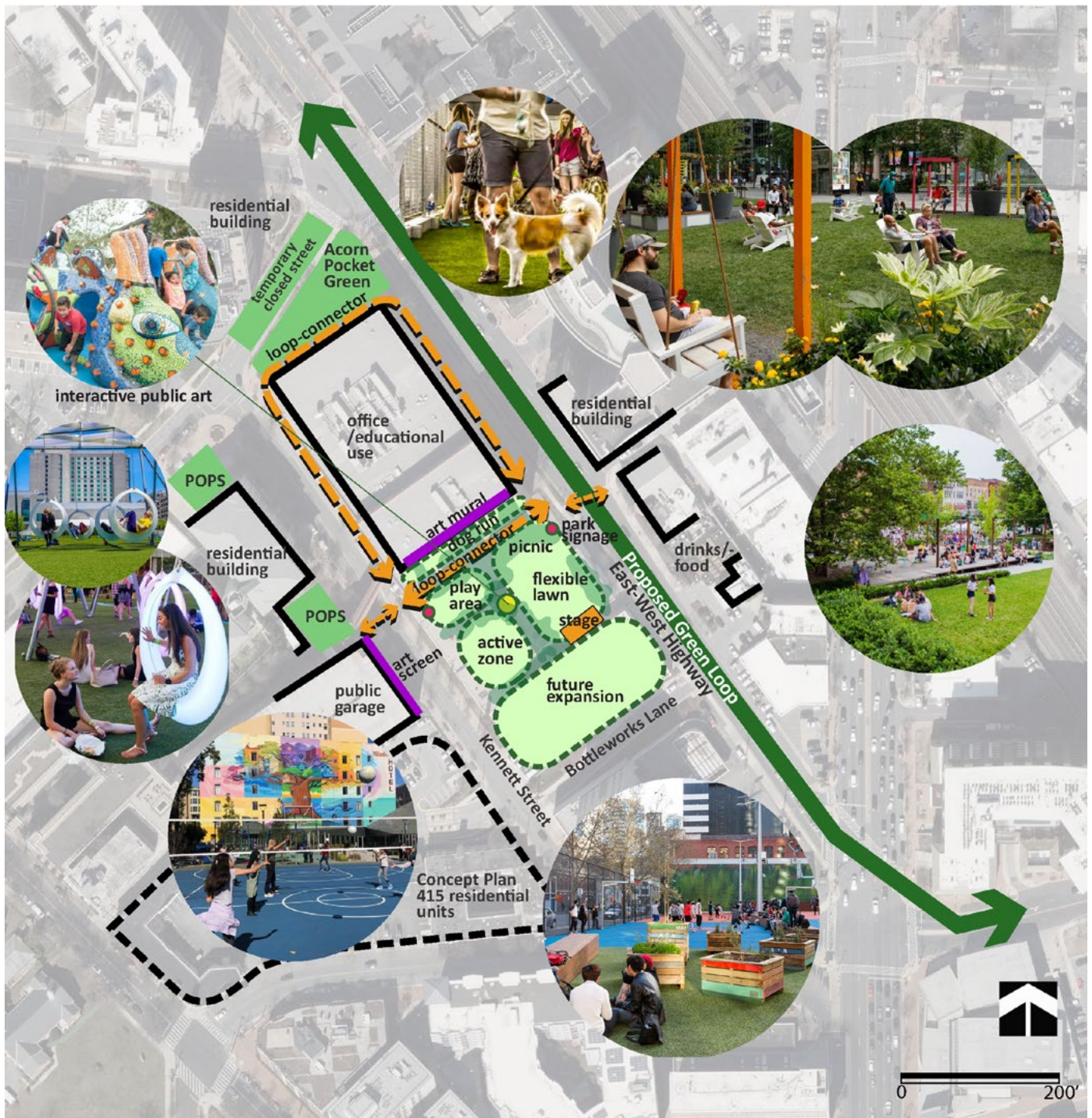


Figure 19. South Silver Spring Park Concept Diagram

D2. Create: Downtown North Park

Location: Downtown North District
Likely Ownership: POPS

This park will be a key active outdoor recreation destination in the Downtown North District, and will be created along with the redevelopment of the Cameron Street parking garage site as proposed in the Plan. It is located along the Central Green Loop and will connect to complementary active facilities elsewhere in the Plan as well as to the indoor active recreational experiences offered by the South County Regional Recreation and Aquatic Center across the street (currently under construction).

Recommended Size: minimum 0.5 acre

D3. Create: Metro Center Park

Location: Metro Center District
Likely Ownership: POPS

This park will be a key active recreation destination in the Metro Center District. Its location along the Central Green Loop will connect it to complementary facilities at other recreational parks proposed in this Plan. This park will be created along with the redevelopment of the Bonifant-Dixon Parking Garage site and may be located on a lower rooftop. The park's facilities may include multi-use courts and play areas.

Recommended Size: minimum 0.5 acre

D4. Create: Bonifant Park

Location: Ripley District

Likely Ownership: POPS

This park will be adjacent to new development in the Ripley District. It will focus on alternative forms of active recreation at a smaller scale such as game tables and basketball hoops. It will serve as a local destination for active experiences in the center of the downtown.

Recommended Size: minimum 0.1 acre

D5. Create: Fitness Park (currently approved under The Blairs Master Plan)

Location: South Silver Spring
Likely Ownership: POPS

This park will be an active place as part of the full build-out of the Blairs Master Plan. Located off of Portal Drive and alongside an existing surface parking lot, this park will have outdoor fitness equipment for adults and children as well as a sitting area for rest.

Proposed Size: ~ 0.22 acre

D6. Renovate/Repurpose: Ellsworth Park

Location: Adjacent Communities
Ownership: M-NCCPC

This park is an important active recreation destination in the Adjacent Communities District. Further renovation of reclaimed space from a brick house will expand awareness of the presence of the park along Colesville Road. Consider programming that is year-round. Currently, the presence of a dog park in this location makes it a regional destination. As more dog parks/runs are developed, this park will more likely serve as a local destination. Improve pedestrian crossing across Colesville Road at the north edge of the park.

Size: 3.6 acres



Dog Park at Ellsworth Urban Park (Montgomery Parks)

E. NEIGHBORHOOD GREEN

A Neighborhood Green is a flexible space that supports social connections, physical activities and access to nature. This type of park provides spaces for informal gatherings, lunchtime relaxation, or small special event gatherings. A lawn, play area, shaded seating, pathways, and wooded areas are typical features.



Lakelands Clubhouse Park at the Kentlands (Montgomery Parks)

E1. Renovate/Expand: Fenton Street Park

Location: Fenton Village District
Ownership: M-NCPPC

This park will be a gateway to the Fenton Village District. As adjacent properties become available, consolidate and expand this park into a cohesive neighborhood park that will promote and support the diverse character of this district. Encourage programming that supports active recreation and social gathering. The direct connection to the Central Green Loop will enhance the synergy of this park with other locations. Consider keeping and/or expanding the community garden function already on site.

Size: ~ 1.75 acres

E2. Create: Fenton Village Park

Location: Fenton Village District
Likely ownership: POPS

This park is envisioned as a community gathering space for Fenton Village. The community will have a place to come together and celebrate the diverse identities and cultures of the neighborhood, as well as a place to sit and enjoy food from the many local eateries in Fenton Village. This site may be large enough to consider urban agriculture facilities, either at the ground or as part of a green roof concept in alignment with the recommendations found in Section 3.7. Resilient Downtown.

Recommended Size: between 0.3 and 0.5 acre

E3. Create: Ellsworth District Park

Location: Ellsworth District
Likely ownership: POPS

This park will complement and connect to Veterans Plaza on the site of the existing Whole Foods parking lot. It will be created as part of the redevelopment of the eastern side of the parking lot to meet or exceed open space requirements. Provide green space and/or recreational amenities to complement Veterans Plaza. Provide a mix of shaded and sunny areas with a row of tree canopies facing the main entries and facades of proposed development.

Recommended Size: minimum 0.5 acre

E4. Create: Rachel Carson, Blair Stomping Ground, The Mews, and Lucy's Landing Park inside the Blairs Development (currently approved under The Blairs Master Plan)

Location: South Silver Spring

Likely ownership: POPS

These parks will be a connected system of green open spaces focused on contemplative and social experiences inside the full future build-out of the proposed Blairs Master Plan. From the approved master plan: "Rachel Carson Park makes the final through connection to the intersection of Blair Mill Road and Eastern Avenue, completing an open space axis that bisects the site to provide a clear pedestrian network for residents and patrons within both escarpments to the core of the central business district and Metrorail station. Rachel Carson Park is designed to have unique landscape elements including linear bioswales for stormwater management, an architecturally interesting pedestrian bridge, and terraces designed as sitting areas. North of Rachel Carson Park along Blair Mill Road, Blair Stomping Ground is a publicly accessible dog park with clear visibility from Blair Mill Road. It is intended to provide a social space for dogs and their owners. West of Rachel Carson Park along Eastern Avenue, The Mews is a linear space designed to connect Eastern Avenue to the Private Street C. The Mews will provide an additional connection for pedestrians through the site and enhance the accessibility for the public into interior spaces and the upper escarpment. Lucy's Landing is a second, smaller dog park also intended for residents and the public to ease any overcrowding that might occur at Blair Stomping Ground."

Proposed Size: 1 acre (all four parks combined)

E5. Create: Falkland Park & Connect: Falkland Stream Restoration to north parcel

Location: Falklands District

Ownership: POPS

The Plan envisions a visual and physical connection between the stream valley renovation in the southeast quadrant of the Falklands to a consolidated public use space on the north parcel that is green and clearly public in nature. Consider adding wayfinding signage to address the entries to these publicly accessible spaces. This would be realized along with the redevelopment of the Falkland north parcel.

Size: minimum 0.5 acre of consolidated open space

E6. Create/Develop: King Street Park

Location: South Silver Spring

Ownership: POPS (currently leased to M-NCPPC for the use of the community garden on site)

This park will be a neighborhood social gathering place. Consider retaining the existing community garden at the western end of the site at 7980 Georgia Avenue until the parcel is redeveloped. Upon redevelopment consider including a community garden program as part of the open space. Connect this park to the proposed through-block connection on this site and to the Outer Green Loop.

Size: 0.2 – 0.4 acre

F. POCKET GREEN

This smaller park will allow for “pauses” with a landscaped setting along the streets between larger parks within the parks and public spaces network. Research has suggested that smaller breaks during the work schedule increases productivity and health, especially mental health.

Pocket Greens are particularly important in areas where commercial activities are taking place. Due to its small scale, the space should be highly visible from the street and include protection from the wind, traffic and noise allowing workers to take a lunch or coffee break while enjoying a contemplative environment with pedestrians strolling by. They can provide seating, areas to eat or read, and small play areas or game areas. This park will serve nearby residents, workers, and visitors.

F1. Renovate: Acorn Park

Location: South Silver Spring
Ownership: M-NCPPC

The existing Acorn Park is a contemplative historic Pocket Green. Its proximity to the proposed South Silver Spring Park will promote a synergy of park experiences: from the historic and contemplative setting of Acorn Park to the more active and social-driven space on the new proposed recreational park. Acorn Park could sustain more park activation than the existing condition, possibly in partnership with a nearby privately owned public space (POPS). In summer 2020, as part of its Open Streets program, MCDOT closed Newell Street between Kennett Street and East-West Highway to expand recreational opportunities to residents and businesses. The street closure allows users of Acorn Park to enjoy a larger play area increasing park usage and stewardship of its historic asset. In Section 3.6.5 Roadways this street segment is recommended to be a shared street in the future.

F2. Develop: Philadelphia Park

Location: Fenton Village District
Ownership: M-NCPPC

Develop a Pocket Green connected to the Metropolitan Branch Trail.

Size: 0.18 acre



JF Collins Park, Philadelphia, PA (Montgomery Parks)

Temporary/Interim Park

A temporary park is a type of park created for a certain period of time in a location that is not currently planned as future public space. There is generally a beginning and an end time established.

An interim park is implemented in a short time frame on a site that is actively being designed for a future permanent public space. Owners and/or county staff can use these short-term facilities to collect information on programming and functions to inform the design of the long-term public space.

Both temporary and interim parks can provide opportunities to partner with local businesses and non-profits to engage with the community in creative ways to deliver places for social gathering, active or contemplative experiences while building a sense of community and belonging at the location. When feasible, new parks should consider implementation of temporary or interim parks.

For park implementation strategies and park ownership recommendations, see Section 4.3.



Placemaking event in the Randolph Hills neighborhood, 2018. (Montgomery Parks)

3.5.6. Evaluating the Proposed Parks and Public Spaces System

In 2018 the Parks Department completed the 2018 EPS Plan. This Plan's methodology identifies areas with the highest need for parks and open spaces and recommends opportunities to increase the amount and quality of parks and open space in those communities. The EPS Plan methodology brings a data-based approach to the sector plan process by providing a method of measuring whether the proposed new park facilities will meet the demand.

Existing Conditions Baseline vs. Scenario Testing Results

Silver Spring, along with other downtown areas, has a high supply of walkable recreational experiences. However, due to the physical character/size/location of most of the public spaces, there are more social gathering experiences inside the Plan's boundary, but fewer opportunities for active experiences. Access to active experiences increases out of the center of downtown since most of the larger parks that contain traditional active facilities are located at the periphery of the Plan.

That index – experience points per person – in a downtown area has a different context than in suburban and rural areas. In downtown areas such as Silver Spring, there is access to parks and also access to shops, restaurants, businesses, and schools. One experience point per person can mean access to multiple experiences, which are shared with more people than a similar experience would be in a more suburban area. This is expected since public spaces are platforms for social connection, especially in urban areas. This index allows us to check a balance of supply and demand as redevelopment continues in this area.

Sector Plan Results

When the proposed recommendations for Silver Spring were tested against the existing conditions, there was a significant improvement in the overall park needs and an increase in the supply of much-needed active recreational experiences. Overall, the Plan recommendations improve the distribution of active, contemplative, and social gathering experiences across the downtown and the Plan. See Appendix D for more details.

Opposite page: Wheaton Local Park



3.6. TRANSPORTATION

Downtown Silver Spring is a well-connected urban area with continuous sidewalks and a growing network of bike lanes. The area also offers a robust transit network centered around the Sarbanes Transit Center with service provided via the WMATA Metrorail Red Line, the MARC Train, and several bus transit lines including WMATA Metrobus, Montgomery County Ride-On bus, VanGo Circulator, FLASH Bus Rapid Transit, and the upcoming Purple Line light rail. The community is accessible by three major highways: East-West Highway (MD 410), Colesville Road (US 29 and MD 384) and Georgia Avenue (MD 97). This Plan identifies remaining gaps in the transportation network, connectivity opportunities, safety improvements for all users, and next steps.



Paul S. Sarbanes Transit Center

3.6.1. Goals

- Expand connectivity within and between the districts by crossing the rail tracks, implementing the Green Loop, and advancing the 2018 *Bicycle Master Plan*.
- Implement new transit alternatives by dedicating travel lanes for Bus Rapid Transit. Additional opportunities to enhance active transportation networks (non-auto driver travel modes) will be given the highest priority.
- Enhance roadway accommodation for all users and eliminate conflict points.
- Support walking/rolling, biking, and transit as viable and attractive travel modes and not just alternatives to driving.
- Prioritize safety and health for all transportation modes at roadway and street crossings to reaffirm and uphold the county's commitment to eliminate traffic-related deaths and severe injuries countywide by 2030.
- Make all public pathways including sidewalks, trails, and street crossings, ADA accessible in accordance with current best practices.
- Support a constrained parking policy for a downtown well-served by transit.
- Increase non-auto driver mode share within the Sector Plan area.

3.6.2. Bicycle Network

The 2013 *Countywide Transit Corridors Functional Master Plan* established Bicycle and Pedestrian Priority Areas (BiPPAs), which are geographic areas where bicycle and pedestrian traffic enhancements are prioritized over other transportation modes. Downtown Silver Spring is designated as a BiPPA and this Plan supports the recommendations outlined in the Functional Master Plan.

This Plan recommends new bikeways and reconfirms the bikeways recommendations of the 2018 *Bicycle Master Plan*. Protected intersections are the preferred treatment at all intersections (except very minor intersections) where at least one street is recommended to have a side path, separated bike lane, buffered bike lane, or conventional bike lane. Any section of bikeway that could be achieved by a road diet will require a more detailed inter-agency operational analysis before implementation. A summary of the new bikeways recommended as part of this Plan is included below.



Dockless Bike at the Silver Spring Metro Station

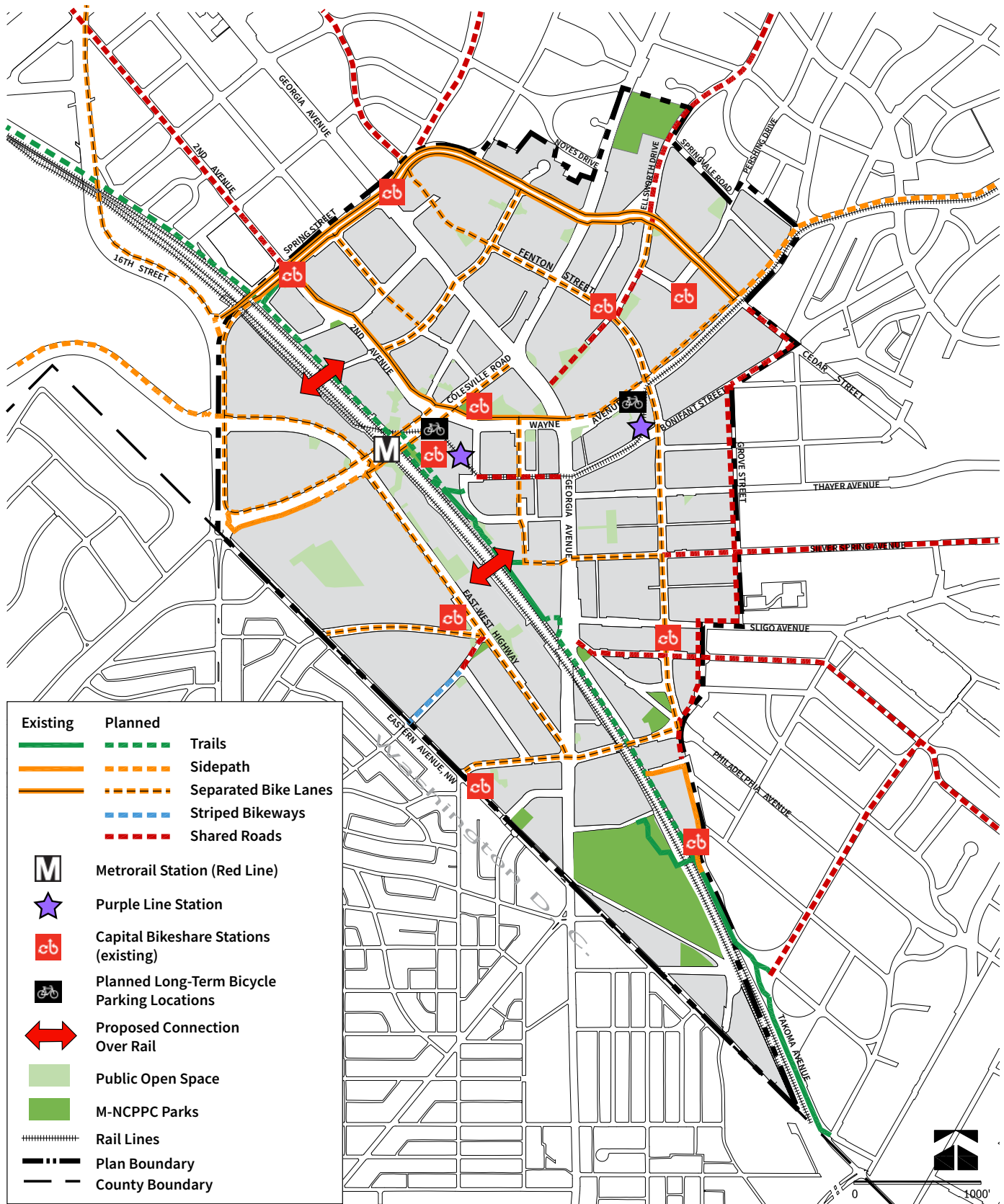
Recommendations:

- Provide one-way separated bike lanes or conventional bike lanes along Silver Spring Avenue between Georgia Avenue (US 29 / MD 97) and Fenton Street.
- Provide separated bike lanes along Blair Mill Road between Eastern Avenue and East-West Highway (MD 410).
- New downtown Streets will have designated bikeways:
 - Between Sligo Avenue and Silver Spring Avenue
 - Between Thayer Avenue and Bonifant Avenue
 - 1st Street Extended Bikeway
- If the Montgomery College bridge should ever need to be reconstructed, its replacement should be constructed with additional width to allow the use of bicycles for adequate access to the Metropolitan Branch Trail and upcoming Fenton Street separated bike lanes.
- Bicyclists should be permitted to use existing trails like the outer loop of Jesup Blair Park for transportation and recreational cycling.
- Implement a road diet to reallocate space for separated bike lanes along East-West Highway, 13th Street, and others in the South Silver Spring District which are necessary to improve southbound bicycle trips to destinations within Washington, D.C., especially the former Walter Reed site. Coordinate with the District Department of Transportation (DDOT) to identify opportunities to continue safe and comfortable cycling routes beyond Eastern Avenue.



Bicyclist on Spring Street

Map 21. Existing and Planned Bikeway Infrastructure



Bicycle Parking

This Sector Plan reaffirms the recommendation of short term and long-term bicycle parking in the *Bicycle Master Plan*. To keep pace with the emerging network of trail and separated bike lanes in downtown Silver Spring, investments are needed in short-term and long-term bicycle parking.

Recommendations:

- Install high-quality, long-term bicycle parking at all WMATA Metrorail Red Line stations and at the higher demand MARC, future Purple Line, and Bus Rapid Transit (BRT) stations to increase the numbers of bicyclists traveling to these transit hubs. Long-term bicycle parking at these and other high-demand areas require improved surveillance either with technological strategies or by relocating the parking in high-visibility areas that can be monitored. The recommended amount of bicycle parking spaces to be provided directly adjacent to each transit station in downtown Silver Spring are as follows:
 - Silver Spring Transit Center:
 - Long-term spaces: 600 recommended (0 provided today)
 - Short-term spaces: 170 recommended (158 provided today)
 - Silver Spring Library Purple Line Station
 - Long-term spaces: 40 recommended (0 provided today)
 - Short-term spaces: 10 recommended (42 provided today)
- Provide bicycle repair stations and air pumps at all long-term parking facilities.
- Install charging stations for electric bikes with improved, public long-term bicycle parking facilities.
- Encourage creative, artistic installations for bicycle parking with the caveat that they must either be marked or intuitively designed as a bicycle parking facility and meet the county's Bicycle Parking Design Standards.



Bicycle Repair Station Examples from Williamstown, MA (left) and Stratford-Upon-Avon train station in England (right).

Bikeshare

Bikeshare is intended to provide a convenient way to bicycle for short trips (one to three miles). Therefore, the success of docked bikeshare systems is tied directly to the proximity of its stations. The county's bikeshare system is well-established within downtown Silver Spring. Expanding the system in the Plan area would serve both the residents and the visitors from nearby neighborhoods. An expansion of bikeshare should be directly tied to new development, within an overall network of stations distributed throughout downtown. Bikeshare stations should also be timed to open with the bikeway recommendations in the Sector Plan.

Micromobility

In 2019, the county expanded its micromobility pilot program featuring dockless e-bikes and e-scooters. Downtown Silver Spring currently has locations for corrals along Fenton Street, Ellsworth Drive, and Thayer Avenue to support the dockless program. Micromobility continues to grow within the Plan area. More corrals should be provided so they are widely and conveniently available and riders learn to see them as an easy way to park the devices safely, conveniently, and in a way that does not hinder pedestrian access. Corrals should be built in accordance with MCDOT location and design specifications, including concrete pads, u-racks, scooter racks, lighting and charging capability for both e-scooters and e-bikes.

Recommendation:

- Install new and additional micromobility corrals in underutilized parking facilities, within available right-of-way, and near civic gathering spaces such as Veterans Plaza and the forthcoming South County Regional Recreation and Aquatic Center.

Table 9. Bikeway Network Recommendations

Bicycle network recommendations are summarized in the table below. New recommendations made in this Plan are shown in **bold**.

Street	From	To	Facility Type	Bikeway Type
Burtonsville to Silver Spring Breezeway				
Ellsworth Drive	Georgia Avenue	Fenton Street	Shared Road	Shared Street
	Fenton Street	Veterans Plaza	Shared Road	Shared Street
	Veterans Plaza	Cedar Street	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
	Cedar Street	Ellsworth Court	Shared Road	Neighborhood Greenway
Capital Crescent Trail Breezeway				
Capital Crescent Trail	Spring Street	Silver Spring Transit Center	Trail	Off-Street Trail
Glenmont to Silver Spring Breezeway				
Fenton Street Extended	Spring Street	Cameron Street	Trail	Off-Street Trail
Fenton Street	Cameron Street	Ellsworth Drive	Separated Bikeway	Separated Bike Lanes (Two-Way, West Side)
Metropolitan Branch Trail Breezeway				
Metropolitan Branch Trail	Silver Spring Transit Center	Takoma Avenue	Trail	Off-Street Trail
Additional Recommendations				
13th Street	District of Columbia	Georgia Avenue (MD 97)	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
16th Street (MD 390)	Spring Street	District of Columbia	Separated Bikeway	Separated Bike Lanes (Two-Way, East Side)
2nd/Wayne Avenue	Spring Street	Colesville Road (MD 384)	Separated Bikeway	Separated Bike Lanes (Two-Way, East Side)
	Colesville Road (MD 384)	Cedar Street	Separated Bikeway	Separated Bike Lanes (Two-Way, North Side)
	Cedar Street	Cloverfield Road	Separated Bikeway	Sidewalk (North Side)
Bonifant Street	Dixon Avenue	Georgia Avenue	Shared Road	Shared Street
Burlington Avenue (MD 410)	Georgia Avenue (MD 97)	Fenton Street	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)

Street	From	To	Facility Type	Bikeway Type
Cameron Street	Spring Street	2nd Avenue	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
Colesville Road (MD 384)	16th Street (MD 390)	Draper Lane	Separated Bikeway	Separated Bike Lanes (Two-Way, North Side) and Sidepath (South Side)
	Draper Lane	East West Highway (MD 410)	Separated Bikeway	Separated Bike Lanes (Two-Way, Both Sides)
	East West Highway (MD 410)	Sarbanes Transit Center Entrance	Separated Bikeway	Separated Bike Lanes (Two-Way, Both Sides)
	Sarbanes Transit Center Entrance	Georgia Avenue (MD 97)	Separated Bikeway	Separated Bike Lanes (Two-Way, Both Sides)
Dixon Avenue	Wayne Avenue	Georgia Avenue (MD 97)	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
East-West Highway (MD 410)	16th Street (MD 390)	Colesville Road (MD 384)	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
	Colesville Road (MD 384)	Georgia Avenue (US 29)	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
Fenton Street	Ellsworth Drive	King Street	Separated Bikeway	Separated Bike Lanes (Two-Way, West Side)
Metropolitan Branch Trail/King Street (Interim)	Railroad Tracks	Fenton Street	Separate Bikeway	Sidepath (South Side)
Metropolitan Branch Trail/Fenton Street (Interim)	King Street	New York Avenue	Separated Bikeway	Sidepath (West Side)
Newell Street	East West Highway (MD 410)	Kennett Street	Shared Road	Shared Street
	Kennett Street	Eastern Avenue	Striped Bikeway	Conventional Bike Lanes
Philadelphia Avenue/Gist Avenue	Selim Road	Woodbury Drive	Shared Road	Neighborhood Greenway
Selim Road	Philadelphia Avenue	Metropolitan Branch Trail	Separated Bikeway	Sidepath (West Side)
Silver Spring Avenue	Georgia Avenue	Fenton Street	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
	Fenton Street	Grove Street	Shared Road	Priority Shared Lane Markings
Spring Street/Cedar Street	16th Street (MD 390)	Wayne Avenue (MD 594-A)	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
Blair Mill Road	Eastern Avenue	East-West Highway (MD 410)	Separated Bikeway	Separated Bike Lanes (Two-Way South Side)
1st Street	Spring Street	Fenwick Lane	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
1st Avenue Extension (B-30)	Fenwick Lane	Ramsey Avenue	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)

3.6.3. Pedestrian Network

This Plan is focused on a safe and walkable downtown Silver Spring. Pedestrian facilities should be designed for all users, such that families, children, and persons with disabilities can safely and comfortably navigate them. Pavement materials and utilities should not create tripping hazards or pinch points. Crosswalks should be marked and highly visible to ensure stop-compliance from motorists. All traffic signals should incorporate audible cues and these treatments and facilities should be consistently installed throughout the Plan area.

The MCDOT report *Planning and Designing Streets to be Safer and More Accessible for People with Vision Disabilities* and the *Complete Streets Design Guide* (CSDG) emphasize the importance of high-

visibility crosswalks at all intersections. Per the CSDG, “continental crosswalks are the primary type of marking used at intersections and uncontrolled crossings in Montgomery County.” All crosswalks in downtown Silver Spring should be marked with the high-visibility continental pattern. The “Continental Pattern” consists of a series of wide stripes parallel to the curb for the length of the crossing. The continental marking significantly increases the visibility of a crosswalk to oncoming traffic thus, making crossing the street safer and more comfortable for all users.



Continental Crosswalk Pattern (pedbikeimages.org/Charles_Zegeer)

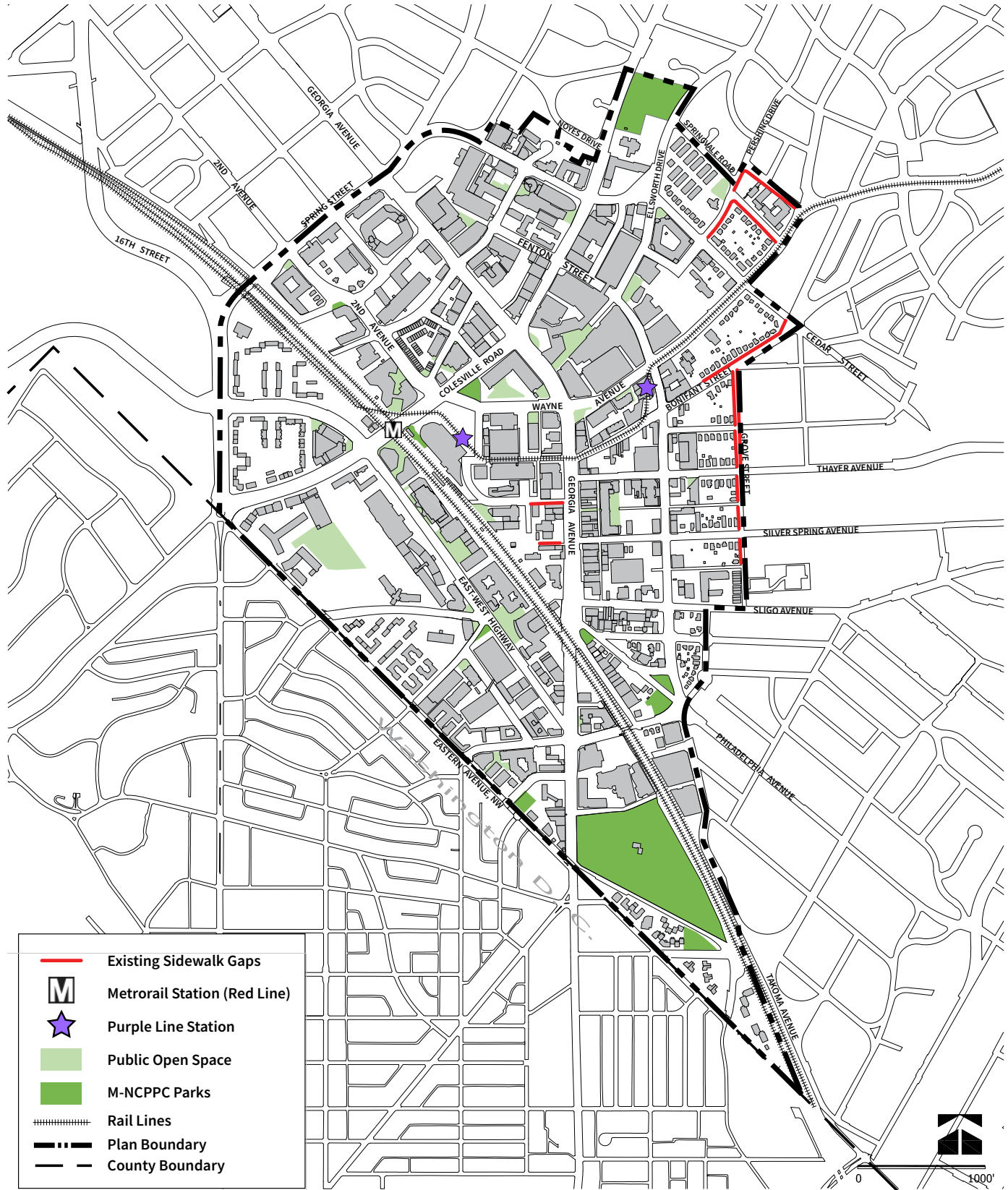


Figure 20. Standard High-Visibility Crosswalk Markings
([www.pedbikeimages.org/Charles Zegeer](http://www.pedbikeimages.org/Charles_Zegeer))

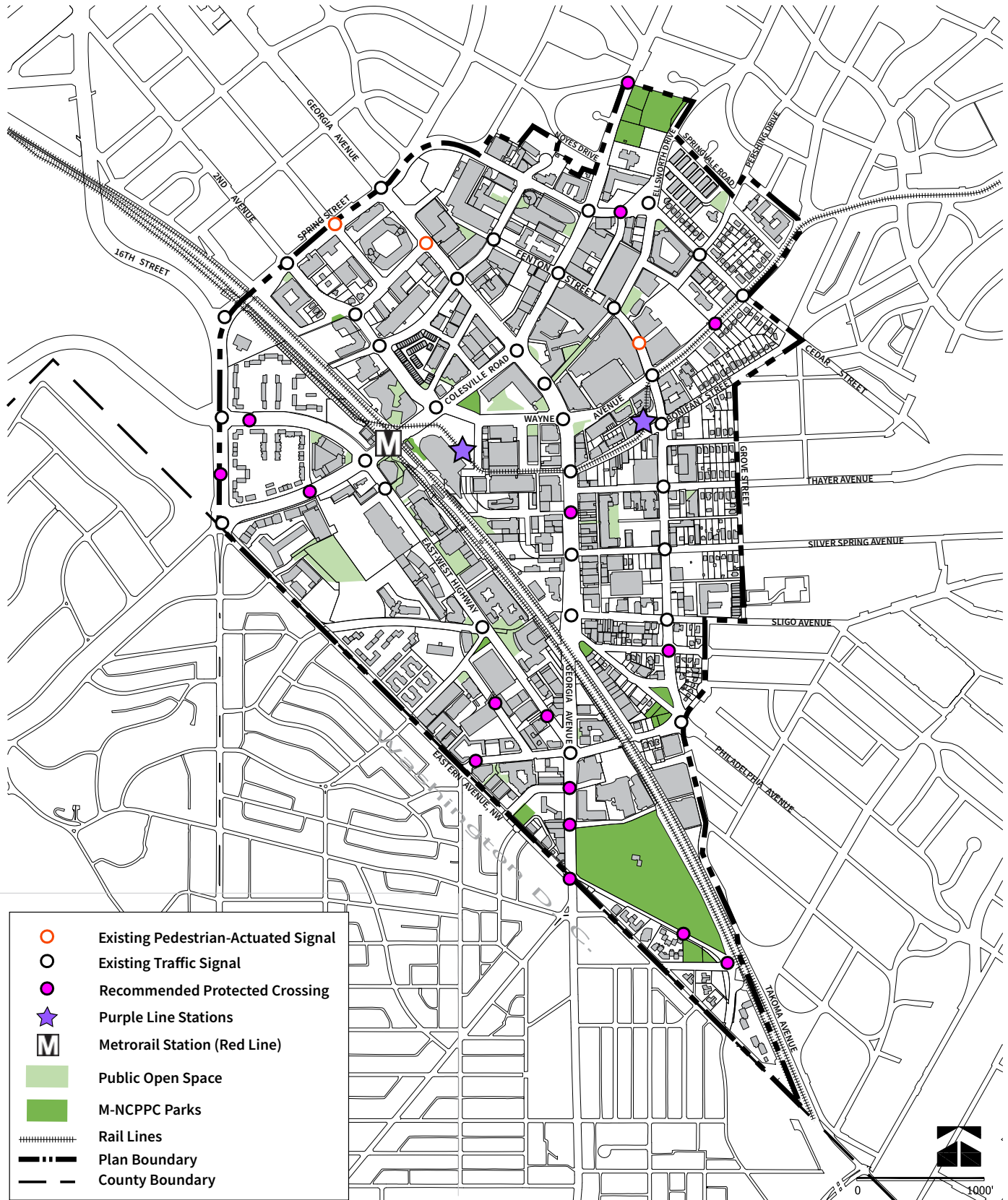
Recommendations:

- Upgrade all intersections with high-visibility continental crosswalk markings for all pedestrian approaches.
- Provide sidewalks on both sides of the street to provide safe pedestrian connections throughout the adjacent communities and within downtown Silver Spring. Refer to Map 23 for gaps in the sidewalk network. The design of sidewalks should be consistent with the Silver Spring Streetscape Standards. Options for cool sidewalk materials to reduce surface temperatures should be studied during the development of the Cool Streets Recommendations that will be included in the Design Guidelines that will accompany this Plan.
- Maximize safety by increasing and improving the quality of pedestrian-scale lighting provided along roadways, walkways, bicycle facilities, crosswalks, and pedestrian underpasses.
- Install new protected crossings to provide safer crossing conditions closer together and where pedestrians and bicyclists naturally want to cross in conformance with the *Complete Streets Design Guide*. The intersections identified for new protected crossings are noted in Map 24. Future study is required to determine the most appropriate traffic control device. Potential treatments include a full traffic signal, a high activity walk signal (HAWK), a rectangular rapid flashing beacon (RRFB), a stop sign and other measures.
- As discussed in Section 2.2 Connecting the Districts, this Plan recommends two landscaped pedestrian and bicycle crossings of the Metrorail/CSX tracks.
- Ensure that every public right-of-way within the Sector Plan area will be ADA accessible. This applies to public sidewalks, trails, and street crossings.
- Address ADA deficiencies within the limits of all BiPPA (Bicycle and Pedestrian Priority Area) projects, even if the scope of the project is a bicycle-facility improvement. Street trees are a high priority but should not compromise the safety or utility for the nonmotorized transportation network.
- Create the following new pedestrian connections:
 - Fenton Street to Mayor Lane (along with the redevelopment of Parking Lot 4 site in Fenton Village)
 - Bonifant Street to Wayne Avenue

Map 22. Existing Sidewalk Gaps



Map 23. Recommended Protected Crossings



3.6.4. Transit

The Sector Plan area offers a robust transit network for people to get to and from work, home, school, and other activities. When the Purple Line is completed, downtown Silver Spring will be connected via multiple rail and local and regional bus routes to Washington, D.C., Prince George's County and destinations within Montgomery County south of the Beltway. The Plan recommendations focus on dedicated travel lanes for bus transit entering into and exiting from the Sarbanes Transit Center to support the new Bus Rapid Transit (BRT) service and improving local bus transit facilities within the Plan area.

Recommendations:

- This Plan confirms BRT stations and routes for the Georgia Avenue South Bus Rapid Transit Route (Corridor 2) in the 2013 *Countywide Transit Corridor Functional Master Plan*.
- Increase transit reliability by studying the feasibility of reallocating existing travel lanes to transit use on Georgia Avenue and Colesville Road within the Sector Plan area.
- Upgrade existing and future bus stops within downtown Silver Spring to include shelters at all stops with real-time bus arrival displays.
- Extend the Silver Spring Metrorail south mezzanine to cross over the railroad tracks to provide a direct connection to the MARC platforms and improve connections not currently included in the Purple Line project.
- Study opportunities to expand the VanGo Circulator service coverage to include Jesup Blair Park and Woodside Urban Park.
- Explore ways to coordinate transit service between the various local and regional transit systems and provide a seamless connection between the former Walter Reed Army Base site in Washington, D.C. and downtown Silver Spring. Such services could include Montgomery County Ride-On, the VanGo Circulator, WMATA Metrobus and the proposed Walter Reed Army Base Site shuttle.
- Evaluate the feasibility of increasing rapid transit service along Georgia Avenue to and from Jesup Blair Park.



FLASH Bus Rapid Transit in downtown Silver Spring.

3.6.5. Roadways

The Plan area has a well-connected road network of high- and low-volume streets that provide access to and within this vibrant community. This Plan focuses on ensuring safety for all users, especially pedestrians, and further improving connectivity and circulation throughout the road network. At the time of this Plan's approval, the 2021 *Complete Streets Design Guide* (CSDG), a countywide policy developed jointly by the Planning Department and the Montgomery County Department of Transportation (MCDOT) has been approved, but the CSDG recommendations to updating County Code Chapter 49 have not been codified. For this reason, this plan includes roadway classifications per the 2018 *Master Plan of Highways and Transitways* and the County's Rode Code as well as the CSDG in anticipation of updates to the County Road Code.



Intersection of Wayne Avenue and Colesville Road at the Transit Center

Recommendations:

- Expand the street grid by installing new roadway connections. The recommended new Downtown Streets described below will include designated pathways for pedestrians and cyclists. Safety and utility for pedestrian and bicycle pathways and crossings will have the highest priority when determining space allocation within the right-of-way. Street trees should be allocated adequate space in which to thrive and expand the tree canopy.
 - Extend 1st Avenue south to connect to Cameron Street (to line up with Ramsey Avenue) to improve connectivity within the local street network and increase local circulation throughout the corridor. An alley is also recommended to consolidate and better manage loading for development within this block.
 - Create a new street connecting Bonifant Street to Thayer Avenue. This connection will improve connectivity within the corridor from Bonifant Avenue to Sligo Avenue. This street could be private or dedicated to public right-of-way. The design of the street should match that of Private Street A that was built as part of the Studio Plaza project, which connects Thayer Avenue and Silver Spring Avenue.
 - As part of the redevelopment of the Parking Lot 4 site and adjacent parcels (if the existing parking structure is removed), create a new street through the existing block from Sligo Avenue to Silver Spring Avenue that aligns with the north-south mid-block connection to the north. This street, designed to accommodate both pedestrians and vehicles, could be private or dedicated to public right-of-way.

- Designate the existing street segments listed below as shared commercial streets. A shared street is defined in the Draft Complete Streets Design Guide “a space that is shared by people using all modes of travel.”
 - Bonifant Street (Ramsey Street to Georgia Avenue)
 - Ellsworth Drive (Fenton Street to Veterans Plaza)
 - Newell Street (Kennett Street to East West Highway)
 - Shared streets are designed to create an environment that encourages low vehicle speeds and prioritizes pedestrians. Shared Streets are often curbside, providing pedestrians with freedom of movement and creating optimal spaces for special events. They can support a variety of land uses, including commercial, entertainment, dining, and residences. Shared Streets should include strategically defined edges and zones, and unique paving materials where feasible. Designs should allow for flexibility, so that streets can be easily closed to automobile traffic for events and reconfigured to support a wide range of social and cultural functions. Streetscape elements must facilitate navigation by pedestrians with vision disabilities, as shared streets allow free-form movement through all spaces for pedestrians and bicyclists. Tactile surfaces should indicate pedestrian-only zones and safe crossings.
- Improve safety on Colesville Road, north of Spring Street by removing the dynamic lane operation. Further study is needed to determine the final number of travel and dedicated bus lanes, as well as other roadway facilities at intersections.
- Encourage installation of new alleys within the Plan area with redevelopment to reduce conflicts with freight and parking operations.
- Relocate existing utilities underground and install new utilities underground.
- Evaluate the potential to apply the shared street treatment to Blair Road, in the event that future redevelopment of Jesup Blair Park spurs land use and development changes in the blocks surrounding the park.
- Identify strategies to calm traffic on Georgia Avenue within the Plan area, especially the southern segments between Ripley Street and Eastern Avenue, to improve safety for all users traveling along and across the corridor in coordination with the Maryland Department of Transportation State Highway Administration (MDOT SHA). Strategies could include narrowing lane widths, installing tree panels within the street buffers, installing curb extensions, lowering speed limits, etc.

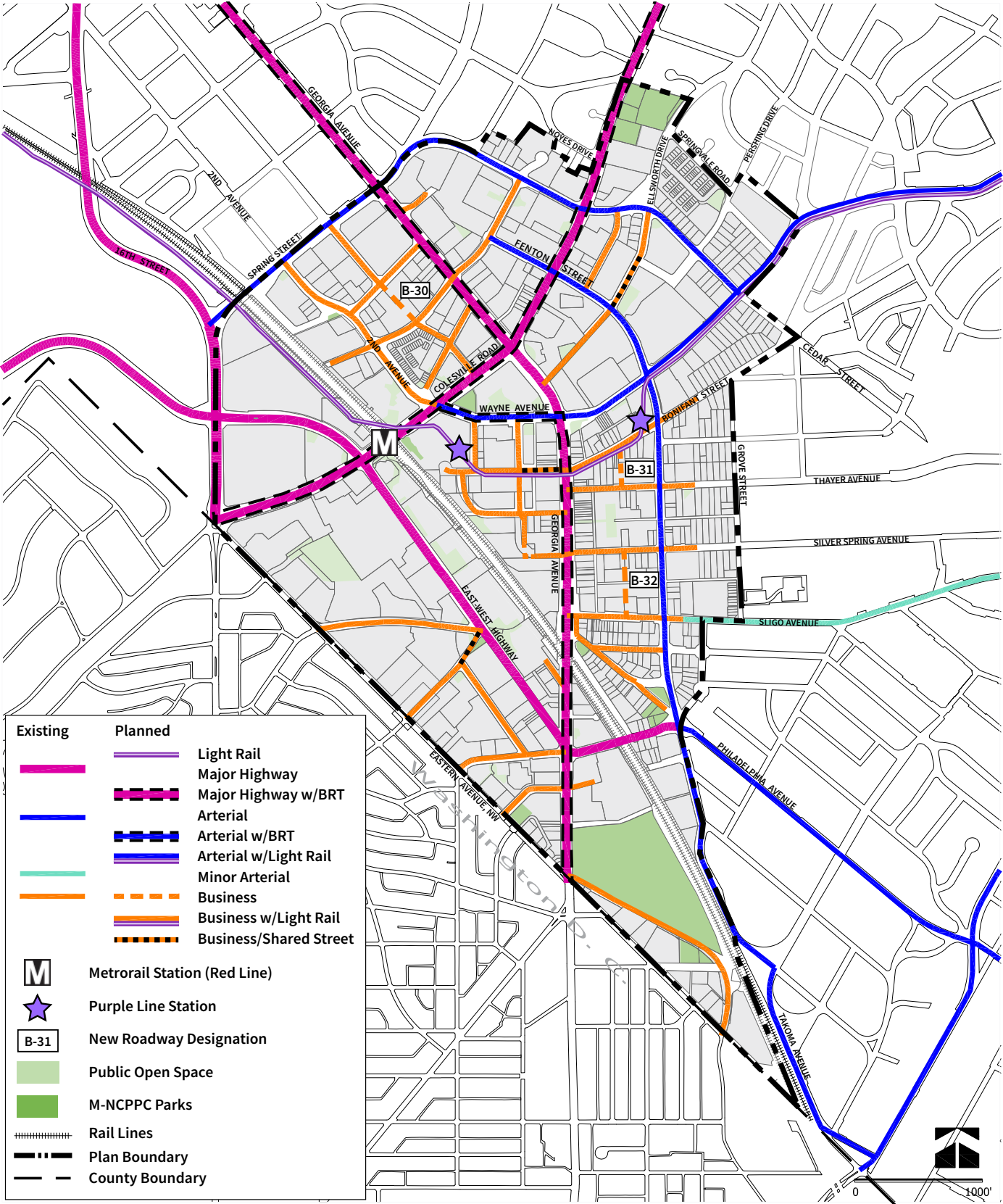


Accessible Shared Street graphic (Toole Design Group).



A shared street is a street where pedestrians, bicyclists, vehicles and even transit safely share the same space. The Wharf, Washington, D.C. (top), Wall Street, Asheville, NC (middle, asheville.gov), tramway in Montpellier, France (bottom, cactuslanguage.com).

Map 24. Roadway Classifications (2018 Master Plan of Highways and Transitways)



New Streets Diagrams:

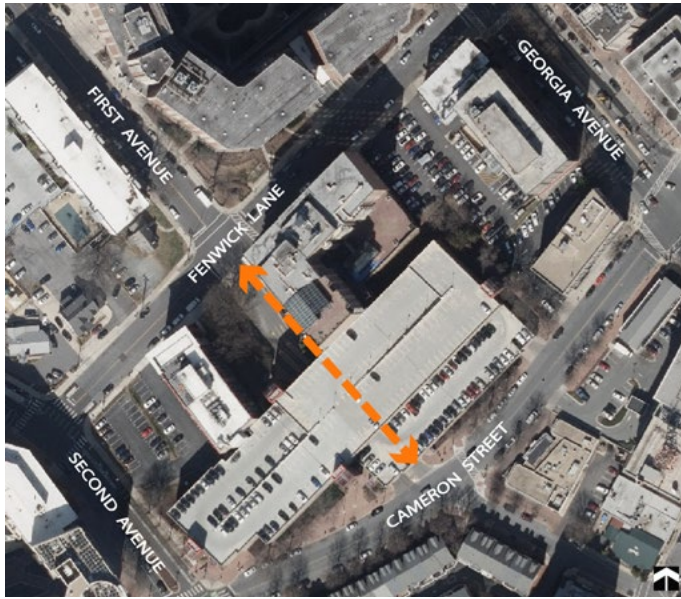


Figure 21. 1st Avenue Extension Diagram (B-30)



Figure 22. Thayer Avenue to Bonifant Street (B-31)

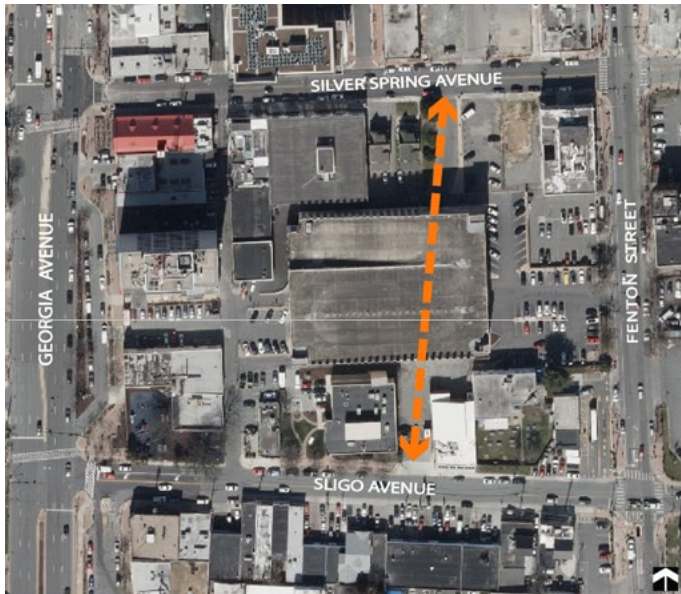


Figure 23. Sligo Avenue to Silver Spring Avenue (B-32)

Table 10. Street Classification and ROW Recommendations

Roadway	From	To	Functional Classification	Complete Streets Design Guidelines Street Type	Master Planned ROW (Minimum)	Existing Lanes	Planned Lanes	BRT Type
Major Highway								
Georgia Ave (M-8)	Spring Street	Colesville Road	Major Highway	Downtown Boulevard	126'	6D	4D + 2T	Dedicated
	Colesville Road	Wayne Avenue	Major Highway	Downtown Boulevard	120'	6-6D	6-6D	None
	Wayne Avenue	Sligo Avenue	Major Highway	Downtown Boulevard	125'-140'	6-6D	4-4D + 2T	Dedicated
	Sligo Avenue	Blair Mill Road	Major Highway	Downtown Boulevard	125'-140'	6-6D	4-4D + 2T	Dedicated
	Blair Mill Road	Eastern Avenue	Major Highway	Downtown Boulevard	125'	6-6D	4-4D + 2T	Dedicated
16th Street (M-9)	Colesville Road	East-West Highway (MD 410)	Major Highway	Downtown Boulevard	120'	6D	4D	None
	East-West Highway	630' South of Lyttonsville Road	Major Highway	Town Center Boulevard	120'	6D	4D	None
Colesville Road (M-10)	SS Transit Station Entrance	Eastern Avenue/16th Street	Major Highway	Downtown Boulevard	125'	6	4 + 2T	Dedicated
	Georgia Avenue (MD 97)	SS Transit Station Entrance	Major Highway	Downtown Boulevard	125'	6	4 + 2T	Dedicated
	Fenton Street	Georgia Avenue	Major Highway	Downtown Boulevard	100'	5-6	3-4 + 2T	Dedicated
	Noyes Drive	Fenton Street	Major Highway	Downtown Boulevard	120'	5-6	3-4 + 2T	Dedicated
East-West Highway (M-20)	16th Street	300' East of 16th Street	Major Highway	Downtown Boulevard	118'	4	4	None
	300' East of 16th Street	Blair Mill Road	Major Highway	Downtown Boulevard	110'	4	3	None
	Blair Mill Road	Blair Mill Way	Major Highway	Downtown Boulevard	90'	2-4	3	None
	Blair Mill Way	Georgia Avenue	Major Highway	Downtown Boulevard	84'	2-4	3	None
Burlington Ave (M-20)	Georgia Ave	Fenton St	Major Highway	Downtown Street	80'	3	3	None
Arterial								
Spring St (A-263)	16th St (MD 390)	630' west of Second Ave	Arterial	Downtown Street	100'	4D	2D	None
	630' west of Second Ave	First Ave	Arterial	Downtown Street	100'	2D	2D	None
	First Ave	Georgia Ave	Arterial	Downtown Street	100'	2D	2D	None
	Georgia Ave	Fairview Rd	Arterial	Downtown Street	100'	2D	2D	None

Roadway	From	To	Functional Classification	Complete Streets Design Guidelines Street Type	Master Planned ROW (Minimum)	Existing Lanes	Planned Lanes	BRT Type
Spring St (A-263)	Fairview Rd	Cameron St	Arterial	Downtown Street	80'	2	2	
	Cameron St	Colesville Rd	Arterial	Downtown Street	100'	2	2	None
	Colesville Rd	Ellsworth Dr	Arterial	Downtown Street	80'	4-4D	2	None
	Ellsworth Dr	Wayne Ave (MD 594-A)	Arterial	Downtown Street	80'	2	2	None
Fenton St (A-264)	Cameron St	Takoma Avenue	Arterial	Downtown Street	80'	2	2	None
Wayne Ave (A-76)	Colesville Rd (MD 384)	Georgia Ave (MD 97)	Arterial	Downtown Street	120'	4D	4	Mixed Traffic
	Georgia Ave (US 29)	Cedar Street	Arterial	Downtown Street	80'	4-4D	4	None
	Cedar St	Sligo Creek Pkwy	Arterial with planned light rail	Neighborhood Connector	80'	4	2 + 2T	None
Minor Arterial								
Sligo Ave (MA-35)	Approx. 149' east of Fenton St	Piney Branch Rd	Minor Arterial	Neighborhood Connector	50'	2	2	None
Business								
1st Ave (B-1)	Spring St	Fenwick Ln	Business	Downtown Street	70'	2	2	None
2nd Ave (B-2)	Spring St	Cameron St	Business	Downtown Street	80'	2	2	None
	Cameron St	Colesville Rd (MD 384)	Business	Downtown Street	105'	4	4	None
13th St (B-3)	Georgia Ave	Eastern Ave (D.C. Line)	Business	Downtown Street	80'	2	2	None
Apple Ave (B-4)	CSX Railroad	Second Ave	Business	Downtown Street	60'	2	2	None
Blair Mill Rd (B-5)	Blair Mill Way	Georgia Ave	Business	Downtown Street	60'	2	2	None
Blair Rd (B-6)	Georgia Ave (MD 97)	CSX Railroad	Business	Downtown Street	60'	2	2	None
Bonifant St (B-7)	CSX Railroad	Ramsey Avenue	Business with planned light rail	Downtown Street	70'	2	2	None
	Georgia Avenue	Fenton St	Business with planned light rail	Downtown Street	70'	2	2 + 2T	None
	Fenton St	Approx. 488' east of Fenton St	Business	Downtown Street	80'	2	2	None

Roadway	From	To	Functional Classification	Complete Streets Design Guidelines Street Type	Master Planned ROW (Minimum)	Existing Lanes	Planned Lanes	BRT Type
Dixon Ave Extended (B-10)	Ripley St	Silver Spring Ave	Business (Planned)	Downtown Street	80'	N/A	2	None
Draper Ln (Proposed) (B-11)	Colesville Rd	Blair Mill Rd	Business (Planned)	Downtown Street	60'	N/A	2	None
Ellsworth Dr (B-12)	Fenton Street	450' west of Fenton St	Business	Downtown Street	70'	2	2	None
	Cedar Street	Veteran's Plaza	Business	Downtown Street	70'	2	2	None
Fenwick Ln (B-13)	Georgia Ave	Second Ave	Business	Downtown Street	80'	2	2	None
Fidler Ln (B-14)	Second Ave	Georgia Ave	Business	Downtown Street	Varies	2	2	None
Gist Ave (B-15)	Philadelphia Ave	Fenton St	Business	Downtown Street	70'	2	2	None
Kennett St (B-16)	Newell St	13th St	Business	Downtown Street	60'	2	2	None
King St (B-17)	Eastern Ave (D.C. Line)	50' east of Georgia Ave and From B & O Railroad to Albany Av	Business	Downtown Street	60'	2	2	None
Newell St (B-18)	Eastern Ave (D.C. Line)	Kennett Street	Business	Downtown Street	70'	2	2	None
Planning Pl (B-19)	Georgia Ave	Silver Spring Parking Lot # 2	Business	Downtown Street	60'	2	2	None
Philadelphia Ave (B-20)	Selim Rd	Fenton St	Business	Downtown Street	70'	2	2	None
Ramsey Ave (B-21)	Cameron St	Colesville Rd (MD 384)	Business	Downtown Street	54'	2	2	None
	Wayne Ave	Bonifant St	Business	Downtown Street	70'	2	2	None
Ripley St (B-22)	Georgia Ave	Bonifant Street	Business	Downtown Street	70'	2	2	None
Roeder Rd (B-23)	Fenton St	Cedar St	Business	Downtown Street	60'	2	2	None
Selim Rd (B-24)	Sligo Ave	Philadelphia Ave	Business	Downtown Street	70'	2	2	None
Blair Mill Rd (B-25)	Eastern Ave (D.C. Line)	East-West Hwy (MD 410)	Business	Downtown Street	70'	2	2	None
Silver Spring Ave (B-25)	Georgia Ave	Approx. 280' east of Fenton St	Business	Downtown Street	70'	2	2	None

Roadway	From	To	Functional Classification	Complete Streets Design Guidelines Street Type	Master Planned ROW (Minimum)	Existing Lanes	Planned Lanes	BRT Type
Sligo Ave (B-26)	Fenton St	Approx. 149' east of Fenton St	Business	Downtown Street	80'	2	2	None
	Georgia Ave	Fenton St	Business	Downtown Street	70'	2	2	None
Thayer Ave (B-27)	Fenton St	Approx. 288' east of Fenton St	Business	Downtown Street	70'	2	2	None
	Georgia Ave	Fenton St	Business	Downtown Street	60'	2	2	None
Cameron St (B-28)	Georgia Ave	Spring St	Business	Downtown Street	75'	2	2	None
	Second Ave	Georgia Ave	Business	Downtown Street	74'	2	2	None
Dixon Ave (B-29)	Wayne Ave	Ripley St	Business	Downtown Street	80'	2	2	None
1st Ave Extension (B-30)	1st Avenue	Ramsey Avenue	Business	Downtown Street	70'	2	2	None
New street name TBD (B-31)	Bonifant Street	Thayer Avenue	Business	Downtown Street	70'	2	2	None
New street name TBD (B-32)	Silver Spring Avenue	Sligo Avenue	Business	Downtown Street	70'	2	2	None
Shared Streets								
Bonifant St (B-7)	Ramsey Avenue	Georgia Avenue	Business with planned light rail	Shared Street	40-70'	2	2	None
Ellsworth Dr (B-12)	Veterans Plaza	Fenton Street	Business	Shared Street	70'	2	2	None
Newell St (B-18)	Kennett Street	East-West Hwy (MD 410)	Business	Shared Street	70'	2	2	None

Table Notes:

1. The recommended target speed for Downtown Boulevards is 25 mph. All other Street Types identified within the Sector Plan are, per the *Complete Streets Design Guide* should be designed with a target speed of 20 mph.
2. On Downtown Streets and Neighborhood Connectors, safety and utility for pedestrians and bicyclists will have the highest priority when determining space allocation within the right-of-way. Street trees should be allocated adequate space in which to thrive and expand the tree canopy.
3. Minimum rights-of-way do not include lanes for turning, parking, acceleration, deceleration, or other purposes auxiliary to through travel. Additional rights-of-way may also be needed to accommodate master planned bicycle and transit facilities, including protected intersections, the envelopes of transit stations, and pedestrian crossing refuges.

3.6.6. Transportation Analysis

In the fall of 2020, the County Council adopted a new *Growth and Infrastructure Policy* (GIP) that focuses on two primary tasks:

- Identify opportunities to incorporate the county's Vision Zero travel safety objectives into the Local Area Transportation Review process
- Reintroduce a policy area-level-review to evaluate a master plan's balance between transportation capacity and land-use travel demand

The policy area-level metrics to evaluate the transportation adequacy of master plans are composed of five transportation system performance metrics. These metrics and how they are derived and interpreted are briefly described below.

Accessibility is defined as the number of jobs that can be reached in the Washington, D.C. metropolitan region within 45 minutes by auto and by transit at the time of buildout. Adequacy is achieved if the master plan improves average accessibility² for the Plan area relative to the currently adopted master plan.

Travel time is defined as the average time by auto and by transit, considering all trip purposes during all times on a weekday at time of buildout. Adequacy is achieved if the master plan improves average travel time for the Plan area relative to the currently adopted master plan.

Vehicle miles traveled (VMT) per capita is defined as the sum of the weekday VMT from trips that both start and end within the Plan area and half the weekday VMT from trips that either start or end within the Plan area. Adequacy is achieved if the plan improves (i.e., reduces) average VMT per capita for the Plan area relative to the currently adopted plan.

Non-auto-driver mode share (NADMS) is defined as the non-auto-driver mode share for the journey to work

in the Plan area. This is the meaning of the measure in current master plans, the 2020-2024 GIP,³ and the goals used by the county regulating transportation demand management. Adequacy is achieved if the plan confirms the relevant pre-established journey-to-work NADMS goal for the Plan area.

Low-stress bicycle accessibility is defined as the percentage of potential bicycle trips that can be accommodated on a low-stress (LTS-2)⁴ bikeway network. Adequacy is achieved if the plan meets or improves the average for the percentage for the county at the time of buildout.

Transportation System Performance Metrics

The transportation performance metrics pertaining to accessibility, travel time, and VMT per capita analyzed for the year 2045 adopted plan scenario (i.e. the 2000 Silver Spring CBD Sector Plan) and the year 2045 proposed plan scenario (this Sector Plan) are generally equivalent. This indicates that this Sector Plan achieves transportation adequacy for these metrics at buildout.

The projected NADMS results for the currently adopted and proposed plans are 61 percent for both. These estimates substantially exceed the pre-established 50 percent NADMS goal prescribed for employees in the Silver Spring CBD Transportation Management District (TMD) area; therefore, this result indicates that this Sector Plan achieves adequacy for this metric at buildout.

As previously stated, the low-stress bicycle accessibility metric is derived from the application of the Planning Department's Bicycle Travel Demand Model. Using this tool, this Plan's recommendations are projected to increase year 2045 countywide connectivity from 79.9 percent to 80.6 percent. Low-stress connectivity in the Silver Spring CBD Policy Area is projected to increase from 66 percent to 73 percent. These results indicate that this Plan achieves adequacy for this metric at buildout.

²Traffic analysis zone-level, population-weighted average

³<https://montgomeryplanning.org/wp-content/uploads/2020/11/20210101-Text-of-the-2020-2024-Growth-and-Infrastructure-Policy-with-Maps.pdf>

⁴LTS-2 is defined as bicycle travel network "appropriate for most adults" or "appropriate for most children". (Consistent with the approach for Objective 2.1 of Bicycle Master Plan – "Countywide Connectivity.")

3.6.7. Transportation Demand Management (TDM)

The purpose of Transportation Demand Management (TDM) is to encourage the use of alternatives to single-occupancy vehicles, including walking/rolling, bicycling, transit, and telework. The application of TDM's is an important strategy in managing intersection congestion within in area. Success in implementing TDM strategies is determined by establishing and monitoring the Non-Auto Driver Mode Share (NADMS). The NADMS measures the percentage of non-drivers arriving at a destination within a defined area during the peak period. Currently, most of the downtown Silver Spring area is located within a Transportation Management District (TMD) boundary that encourages TDM strategies and monitors its progress.

Under the prior Sector Plan, the NADMS goal in downtown Silver Spring was 46 percent. Over the previous years, downtown Silver Spring has been exceeding the goal by eight percent, which indicates that approximately 54 percent of commuters arrive to work by means other than single occupancy vehicles. Downtown Silver Spring is expected to continue exceeding the current NADMS goal once the recommendations outlined in this plan are implemented, the Purple Line opens, and the increase of teleworking opportunities within the Plan area continues.

Recommendations:

- Continue to encourage TDM strategies within the Silver Spring TMD.
- Expand the NADMS goal to apply to all commuters, both employees commuting into the TMD for jobs, and residents of the TMD commuting from their homes to jobs. Increase the NADMS goal to a Blended Average of 65 percent for both groups. This goal may be re-evaluated and amended in the future as part of the Growth and Infrastructure Policy.
- This Plan recommends that the TMD boundary be expanded to align with the Parking Lot District (PLD) boundary (refer to Map 25).

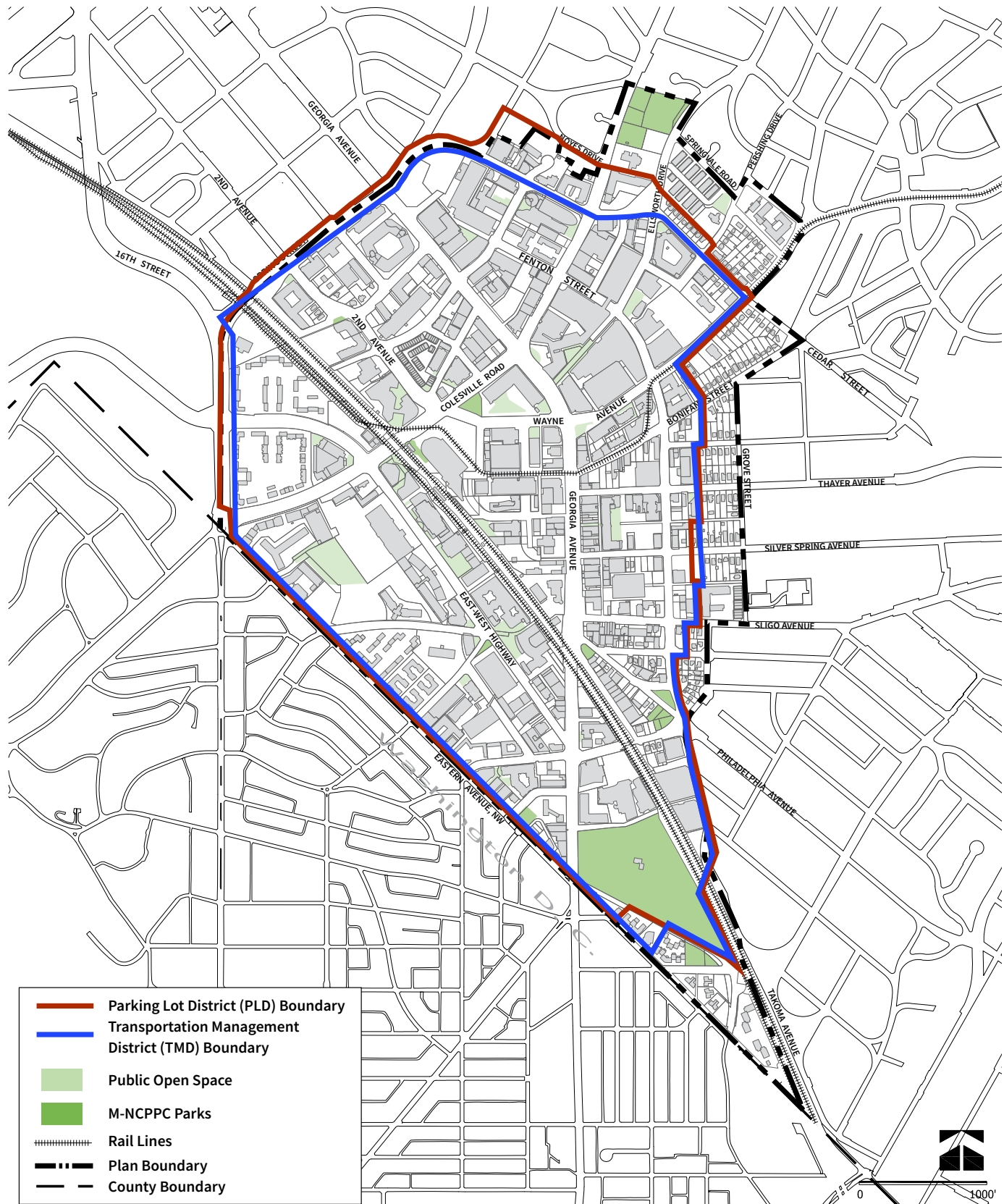
3.6.8. Parking and Loading

Downtown Silver Spring is located within MCDOT's Parking Lot District (PLD) program boundaries. The 2014 Zoning Ordinance in effect today allows properties within the PLD to provide fewer than the minimum required parking spaces, effectively allowing properties to not provide parking on site. MCDOT manages, maintains, and operates 10 parking garages and three surface parking lots with a total capacity of 9,692 and 1,100 metered parking spaces within downtown Silver Spring, as shown on Map 26. There are also 12 private garages that provide parking spaces accessible to the general public. The blocks within the Adjacent Communities District within the Plan boundary are not included in the PLD; parking is managed in these blocks through MCDOT's Residential Parking Permit Areas (RPPA) program.

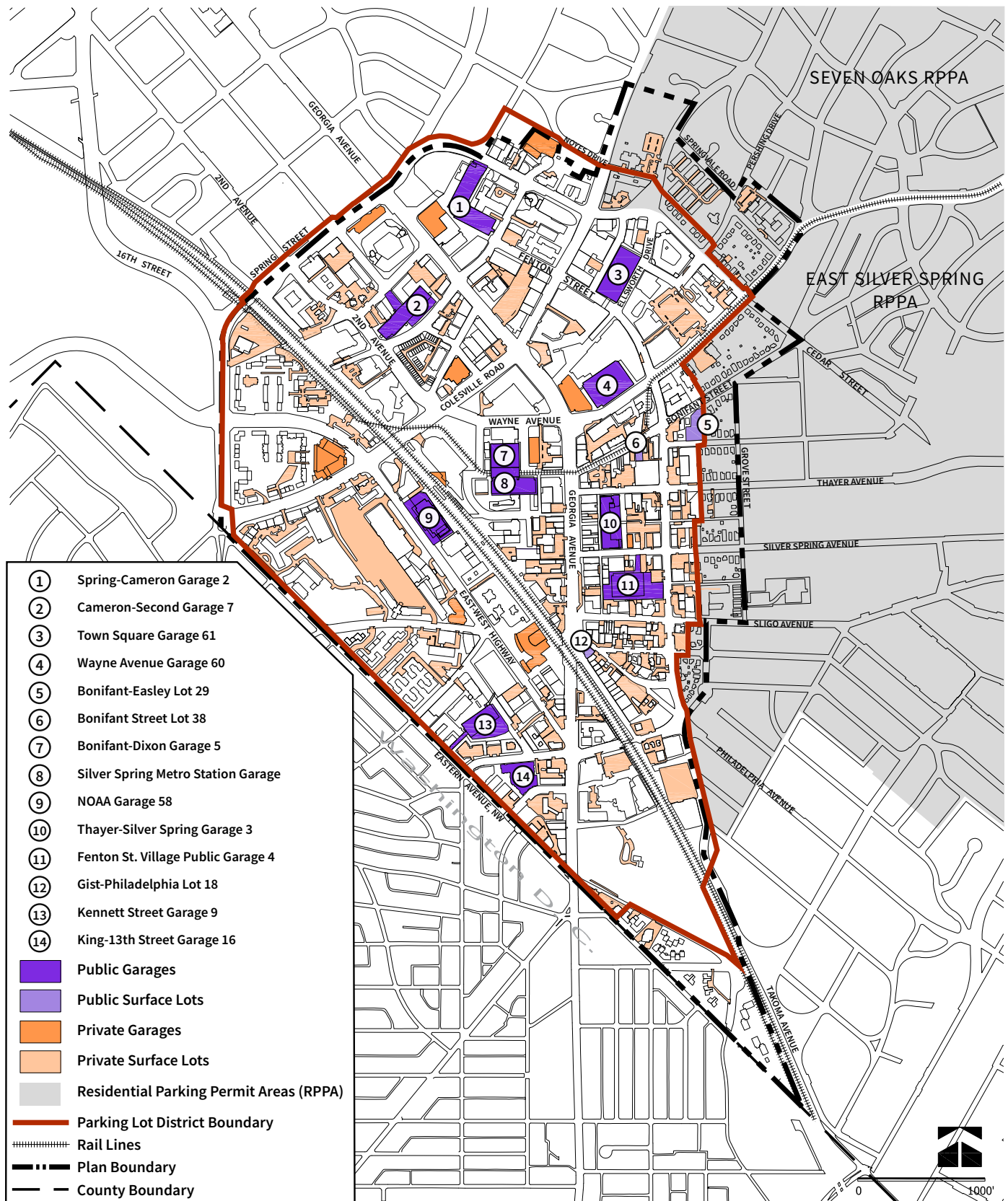
Recommendations:

- Promote a constrained parking policy appropriate for an urban area that is well-served by transit and is easily traversed on foot or by bicycle. The Plan supports redevelopment projects providing less than the required minimum parking for all areas of the plan.
- Existing and future underutilized garages should continue to be explored as redevelopment opportunities while ensuring adequate public parking is provided.
- Implement the recommendations of Montgomery Planning's forthcoming Urban and Loading Management Study, which will address the many competing demands within the curb lanes.
- Future study of options to improve accessibility in the Plan area.

Map 25. Parking Lot District and Transportation Management District Boundaries



Map 26. Existing Parking Facilities



3.6.9. Street Cross Sections

Cross sections were developed for street segments within the Sector Plan boundary that are recommended for dedicated transit lanes or designated bikeways that would necessitate road diets or other right-of-way reallocations. A cross section was also developed for new street B-30 (1st Street Extended).

These cross sections incorporate policy and design guidance from the *Complete Streets Design Guide*. The intent of providing these cross sections is to offer conceptual direction for future Capital Improvement Projects (CIP) and development regulatory applications, demonstrating that there is sufficient master-planned right-of-way to meet the objectives of the *Complete Streets Design Guide*.⁵ Further study of traffic operations will be necessary, and therefore the ultimate cross section may differ from what is recommended in the Sector Plan. For example, dedicated Bus Rapid Transit (BRT) lanes are recommended on Colesville Road and Georgia Avenue. Two sets of cross sections for each corridor from north to south were developed that envision either curb running or median running alignments of the bus rapid transit lanes. Future studies of traffic operations will determine which alignment is preferred (or if a new hybrid would be more feasible) and will refine the geometric design of the right-of-way

The street sections are organized in the following manner:

- East-West Highway sections
 - Interim Conditions
 - Ultimate Conditions
- Colesville Road Sections
 - Median-Running BRT
 - Curb-Running BRT
- Georgia Avenue Sections
 - Curb-Running BRT
 - Median-Running BRT
- Additional Street Sections

⁵ The Sector Plan confirms all minimum master-planned rights-of-way widths in the 2018 Master Plan of Highways and Transitways. The Sector Plan is not making any recommendations to widen what is currently master planned.

East-West Highway Sections

Designated bicycle lanes are recommended on East-West Highway. The Sector Plan recommends fitting the bicycle facilities into the right-of-way with a road diet, which reallocates a travel lane to the bikeway and street buffer. This road diet may be achieved with a CIP project or redevelopment. If the CIP project is implemented first, an interim condition is envisioned that fits the separated bikeway within the existing curb widths (Interim). As redevelopment occurs, the curbs can be relocated inward, and the bike lane can be relocated to the streetscape (Ultimate).

Interim Condition: Figures 24 - 26

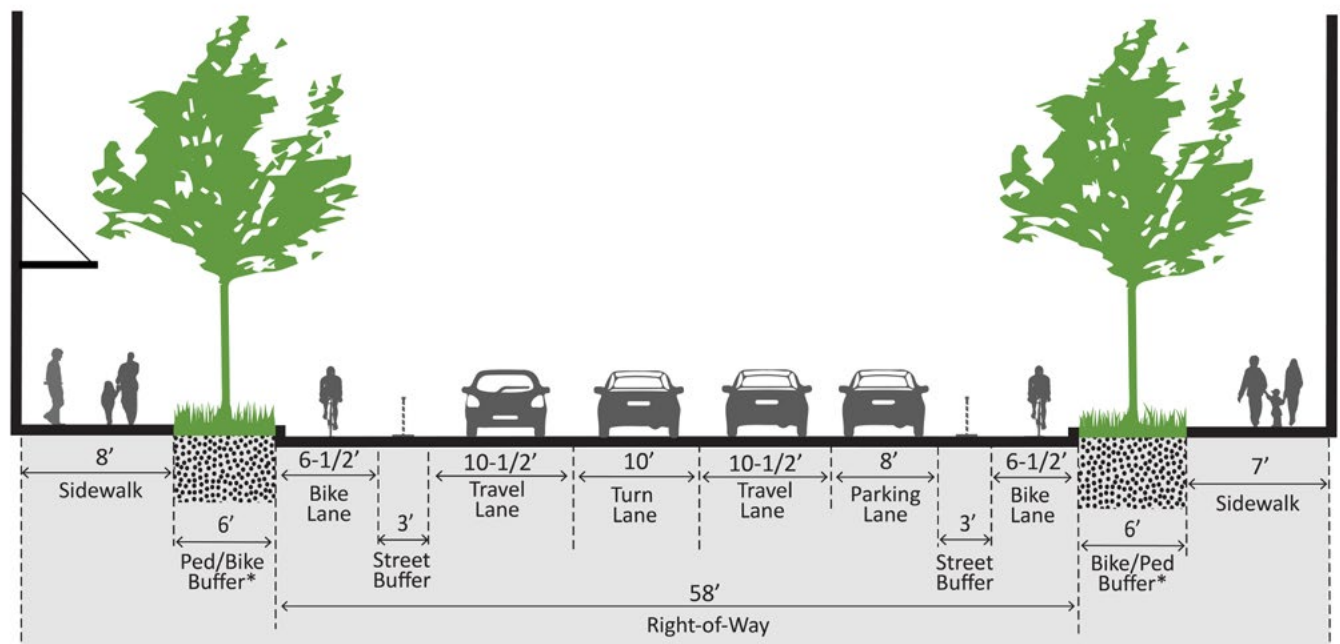


Figure 24. East-West Highway (M-20): 16th Street to Blair Mill Road, Looking North (Interim)

* Buffer with stormwater management to employ best practices

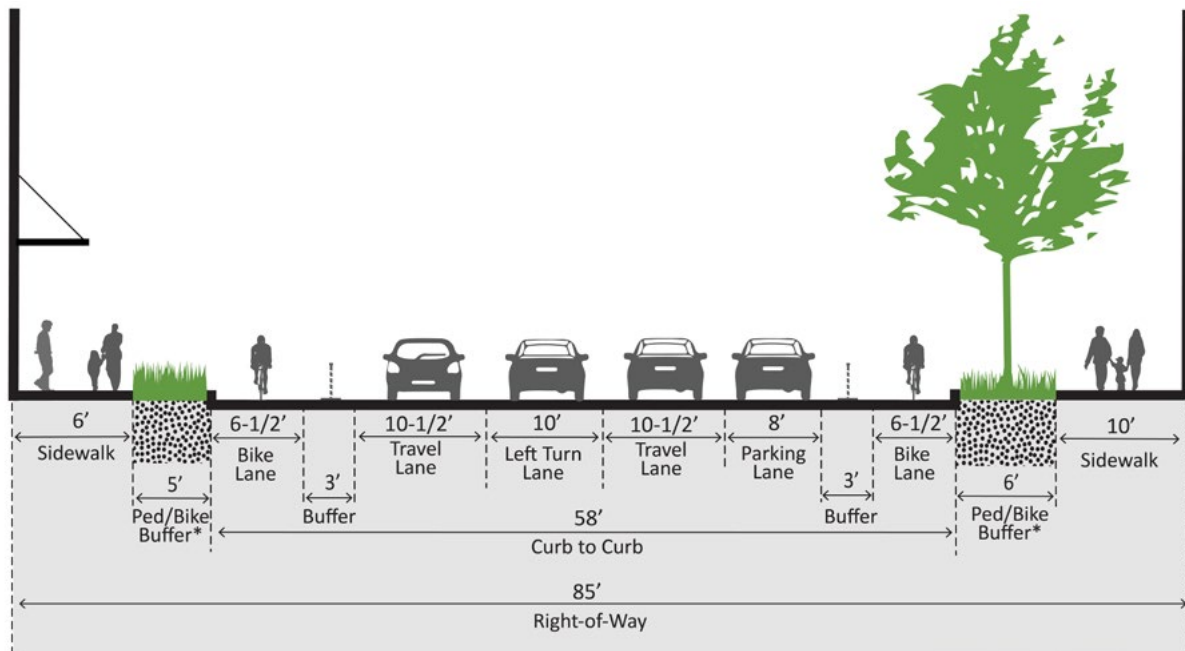


Figure 25. East-West Highway (M-20): Blair Mill Road to Blair Mill Way, Looking North (Interim)

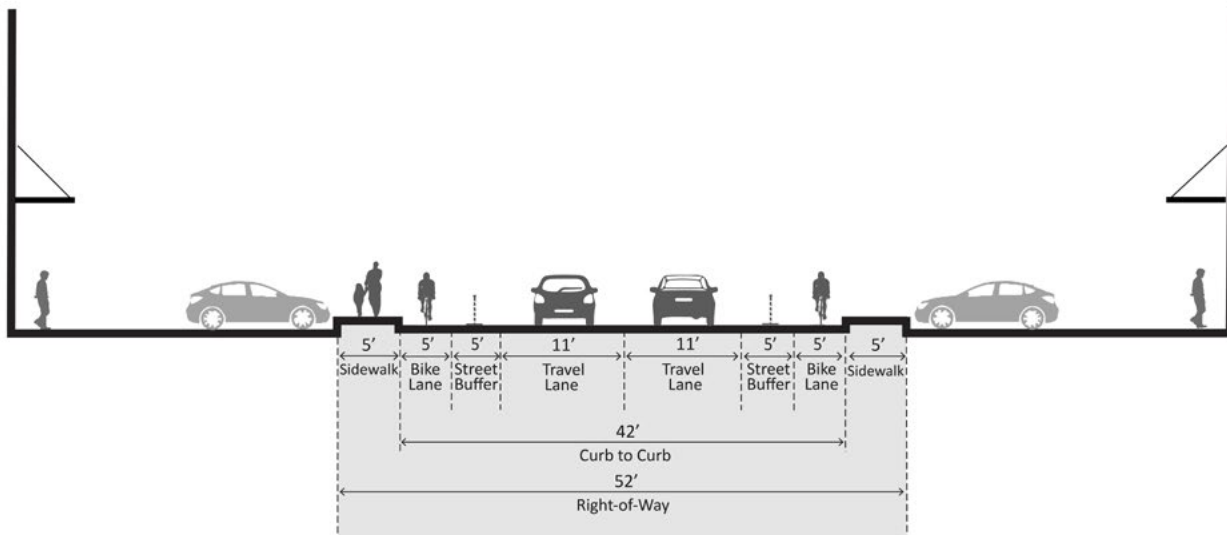


Figure 26. East-West Highway (M-20): Blair Mill Way to Georgia Avenue, Looking North (Interim)

* Buffer with stormwater management to employ best practices

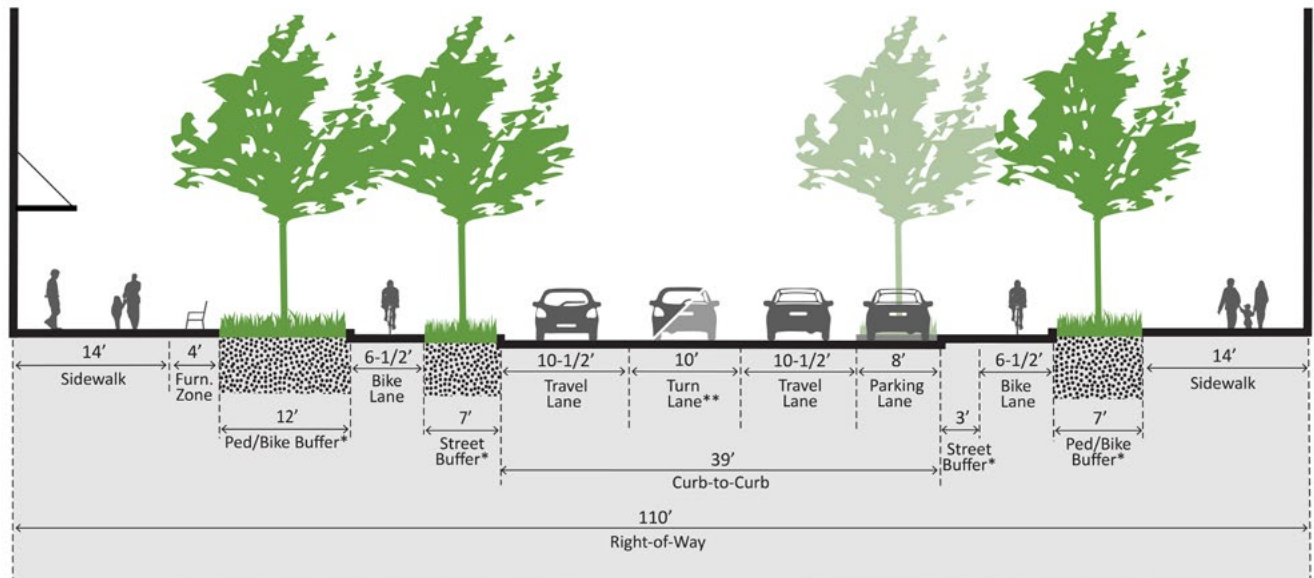


Figure 27. East-West Highway (M-20): 16th Street to Blair Mill Road, Looking North (Ultimate)

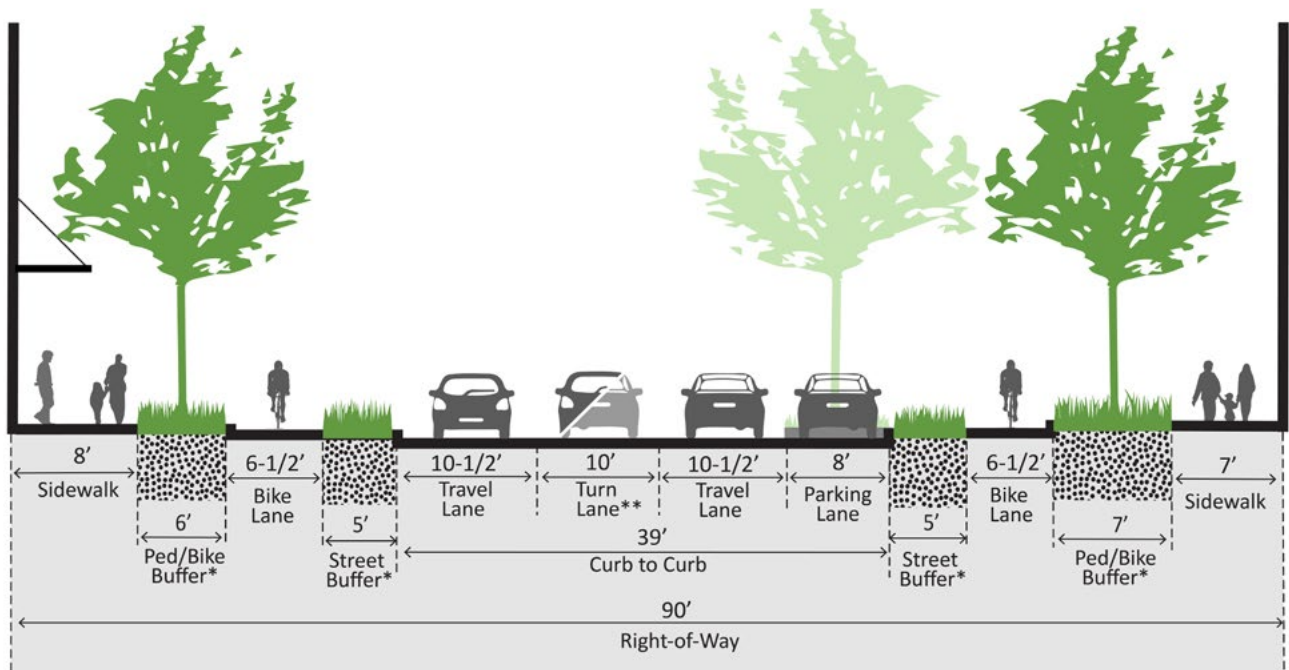


Figure 28. East-West Highway (M-20): Blair Mill Road to Blair Mill Way, Looking North (Ultimate)

* Buffer with stormwater management to employ best practices.

** Note: The center lane functions as a left-turn lane at the intersection in the corresponding direction.

Note: These sections (Figures 27-28) hold the existing curb alignment on the side of East-West Highway adjacent to the Metrorail/CSX tracks.

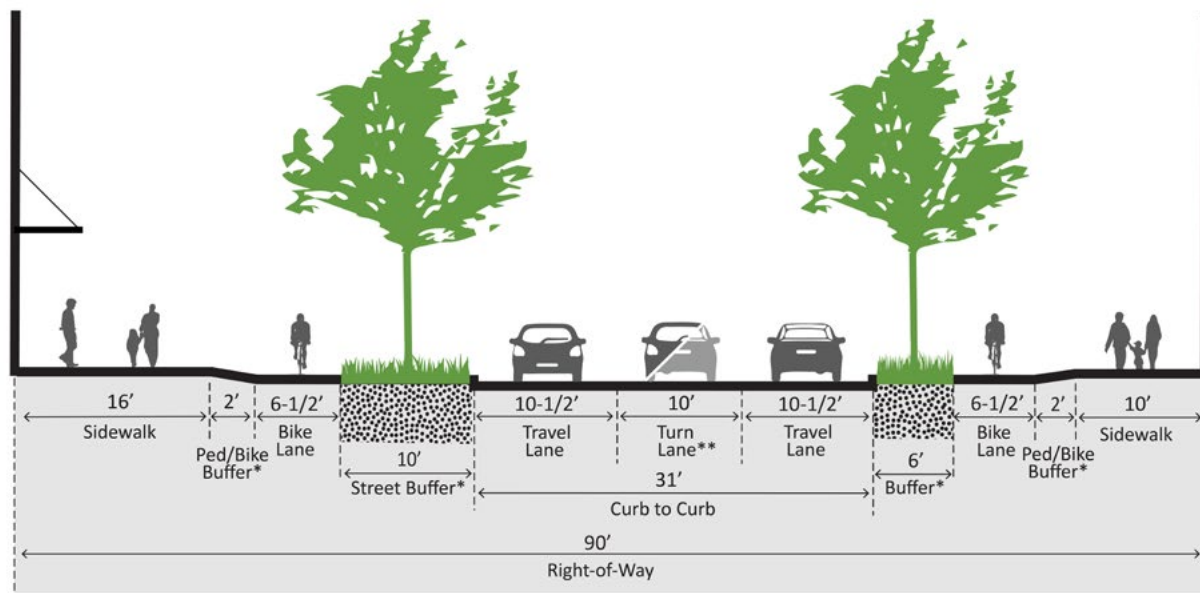


Figure 29. East-West Highway (M-20): Blair Mill Way to Georgia Avenue, Looking North (Ultimate)

* Buffer with stormwater management to employ best practices.

** Note: The center lane functions as a left-turn lane at the intersection in the corresponding direction.

Note: These sections (Figures 27-28) hold the existing curb alignment on the side of East-West Highway adjacent to the Metrorail/CSX tracks.

Colesville Road Sections

A median-running BRT alignment is not provided for the two southernmost segments of Colesville Road (Sarbanes Transit Center to East-West Highway and East-West Highway to 16th Street/Eastern Avenue). This is because the segment between the Sarbanes Transit Center and East-West Highway traverses under the CSX/WMATA/MARC elevated tracks. The “median” is therefore inflexible in placement and minimum width. It was decided that since such a relatively short segment remains further south when traveling towards the Sector Plan border with the District of Columbia, it didn’t make sense to transition the buses back to median running, south of East-West Highway.

The BRT lanes are envisioned to tie-in with the District of Columbia’s vision for dedicated BRT lanes on 16th Street, approaching Silver Spring.

The travel lanes on Colesville Road are consistently 10-feet wide in the existing condition and for that reason, both alignments envision 10-foot travel lanes to minimize the curb-to-curb widths.

Note: the sections that depict median-running BRT include the notation (MR) and the sections that depict curb-running BRT include the notation (CR).

Median-Running Bus Rapid Transit: Figures 30 - 33

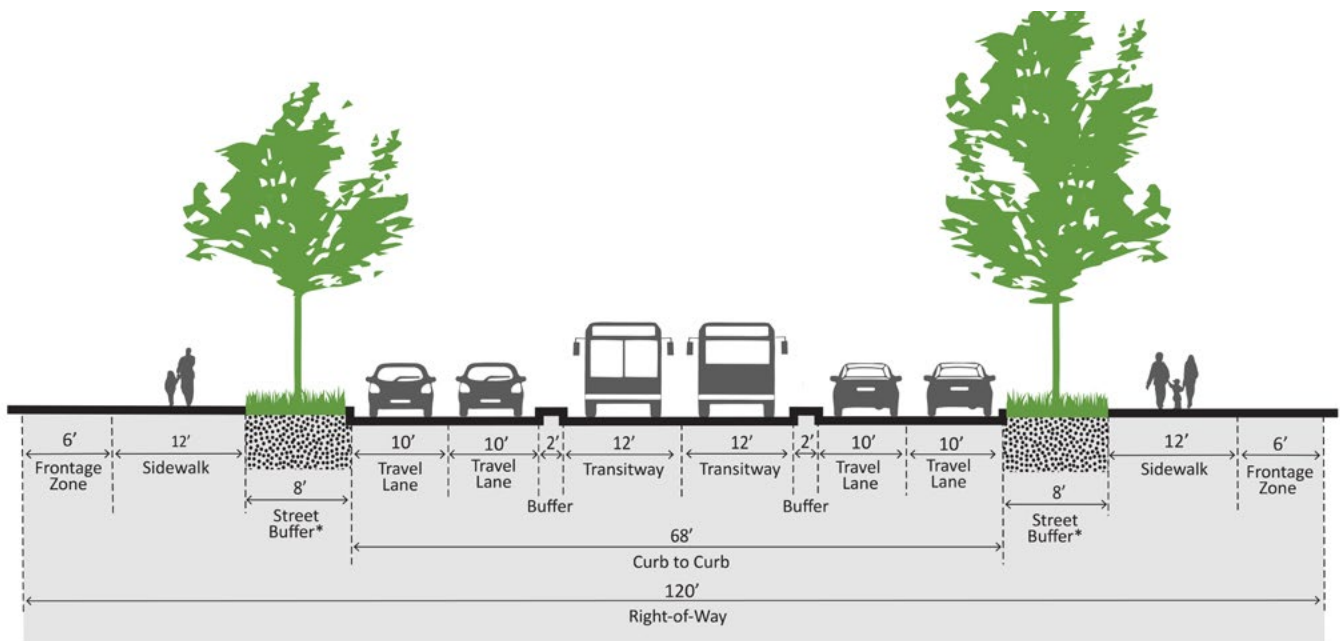


Figure 30. Colesville Road (M-10): North Noyes Drive to Spring Street, Looking North (MR)

* Buffer with stormwater management to employ best practices.

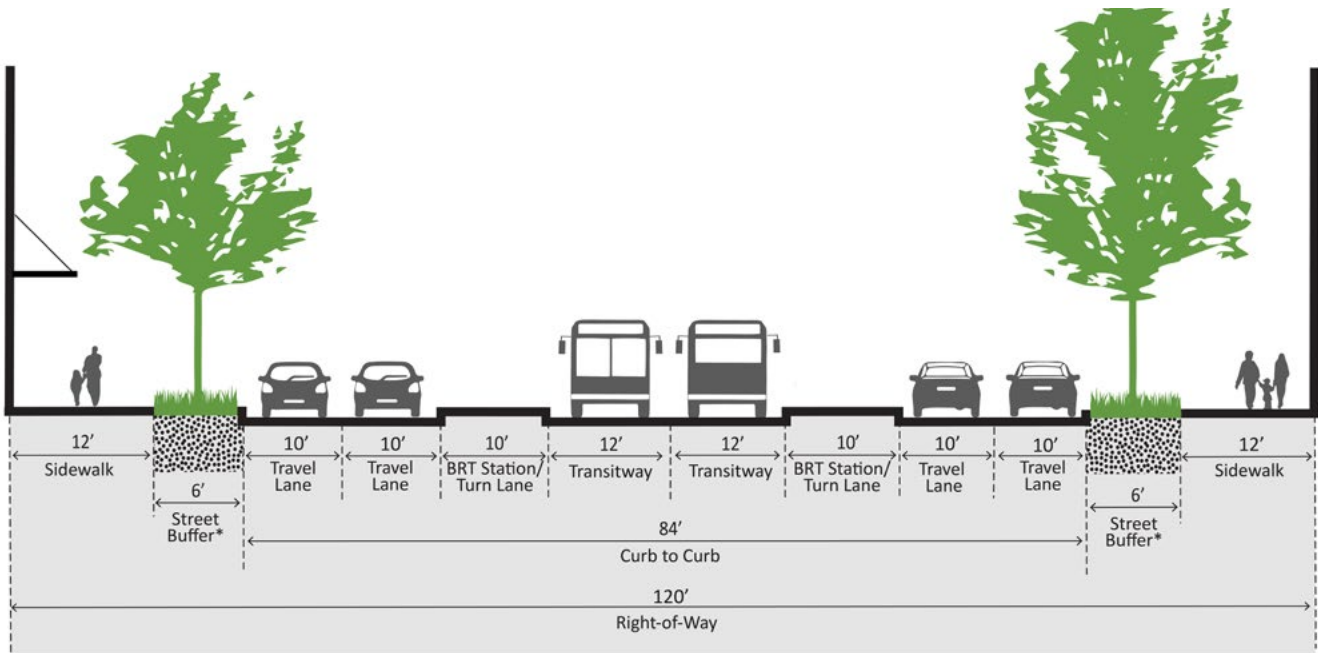


Figure 31. Colesville Road (M-10): Spring Street to Fenton Street, Looking North (MR)

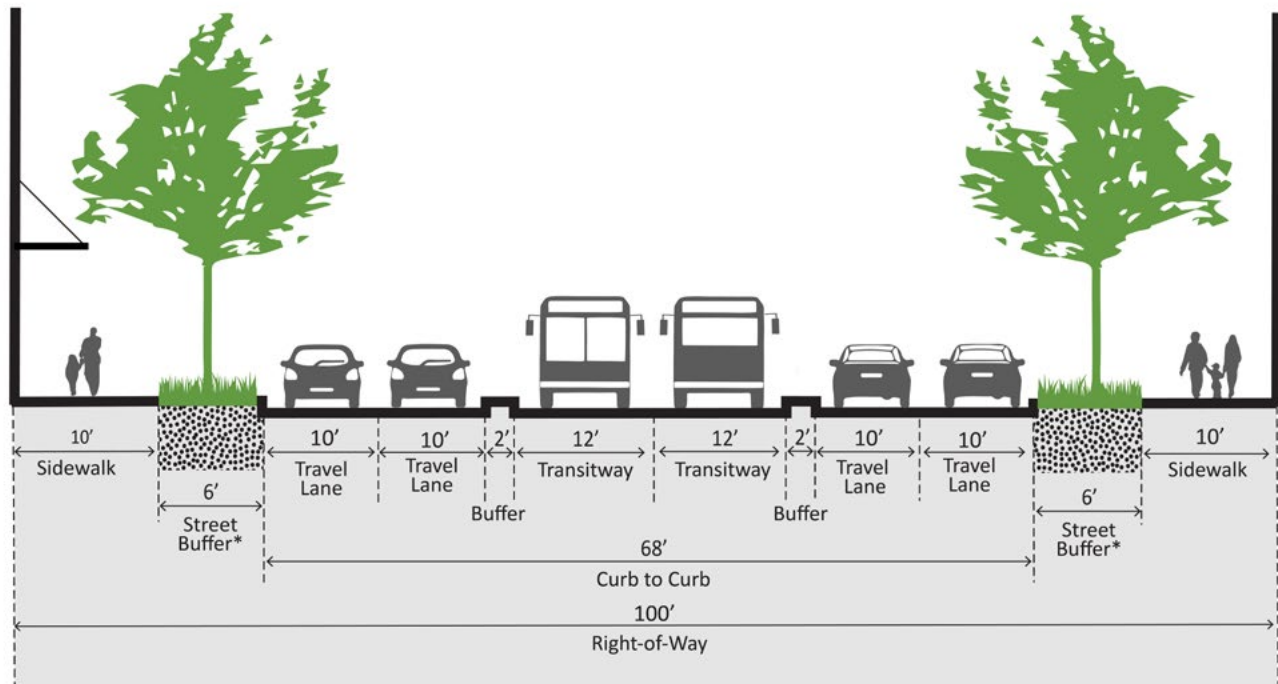


Figure 32. Colesville Road (M-10): Fenton Street to Georgia Avenue, Looking North (MR)

* Buffer with stormwater management to employ best practices.

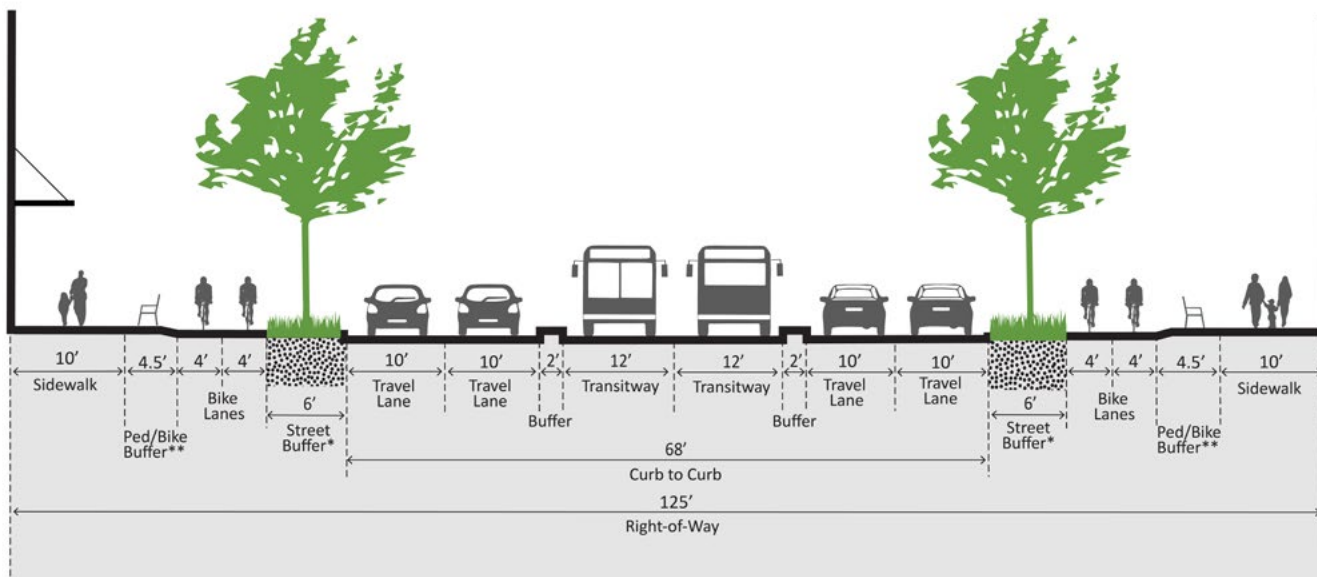


Figure 33. Colesville Road (M-10): Georgia Avenue to the Sarbanes Transit Center, Looking North (MR)

Curb-Running Bus Rapid Transit: Figures 34 -39

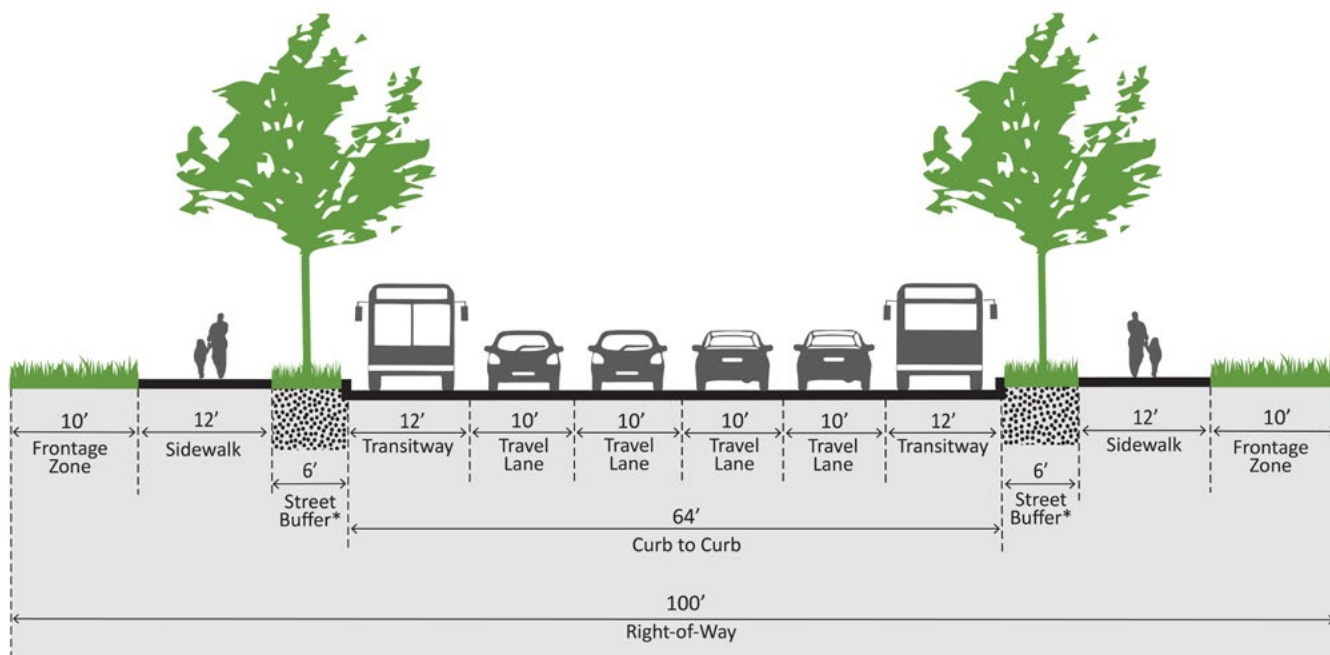


Figure 34. Colesville Road (M-10): North Noyes Drive to Spring Street, Looking North (CR)

* Buffer with stormwater management to employ best practices.

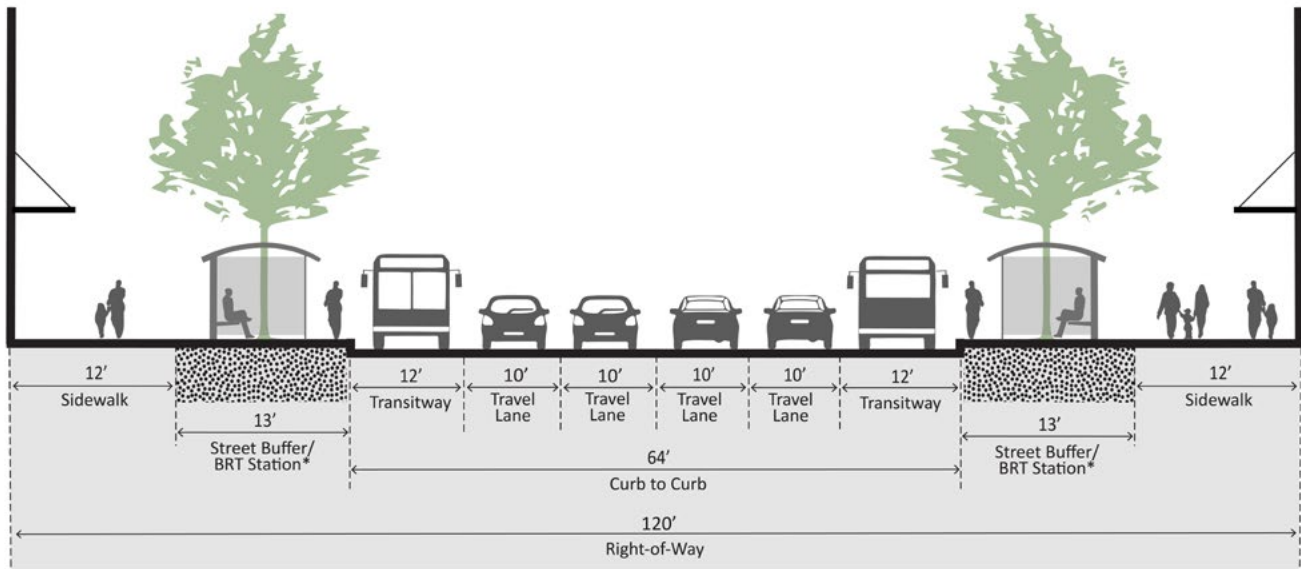


Figure 35. Colesville Road (M-10): Spring Street to Fenton Street, Looking North (CR)

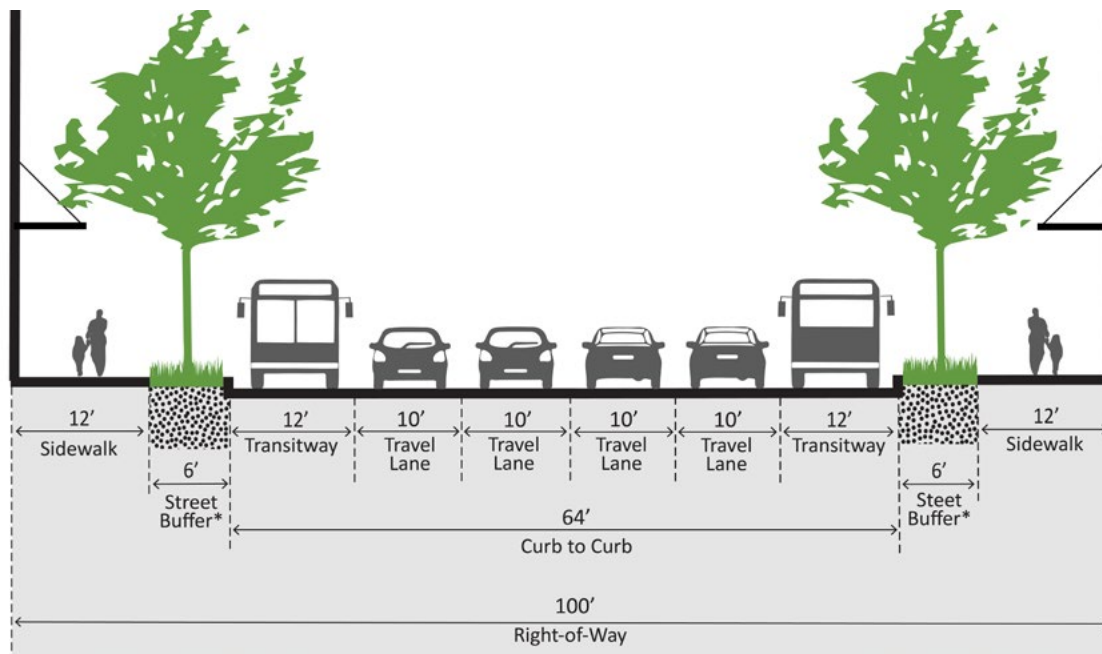


Figure 36. Colesville Road (M-10): Fenton Street to Georgia Avenue, Looking North (CR)

* Buffer with stormwater management to employ best practices.

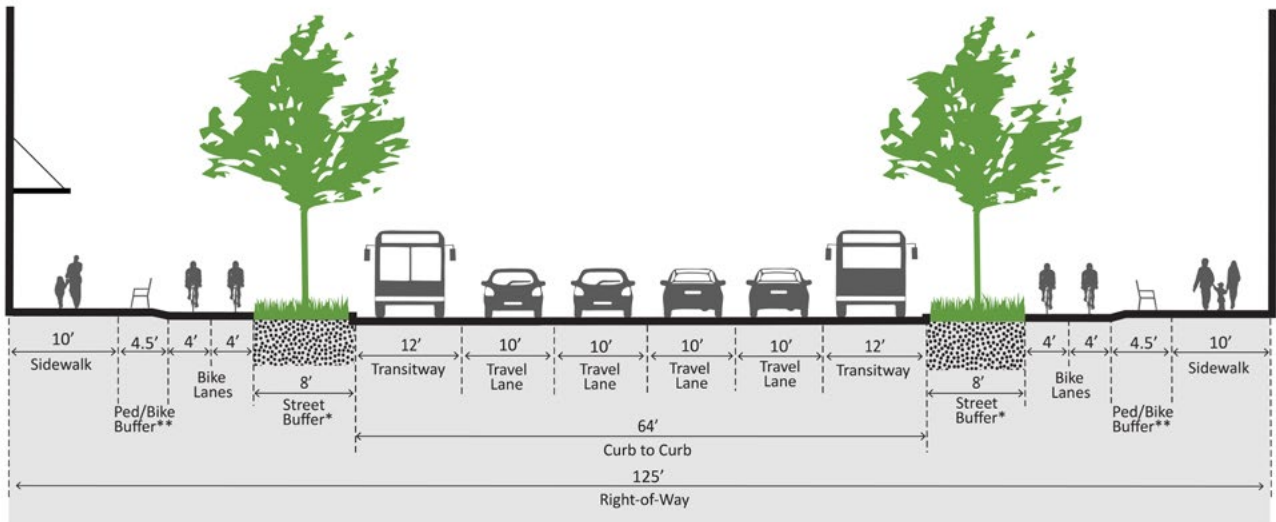


Figure 37. Colesville Road (M-10): Georgia Avenue to the Sarbanes Transit Center, Looking North (CR)

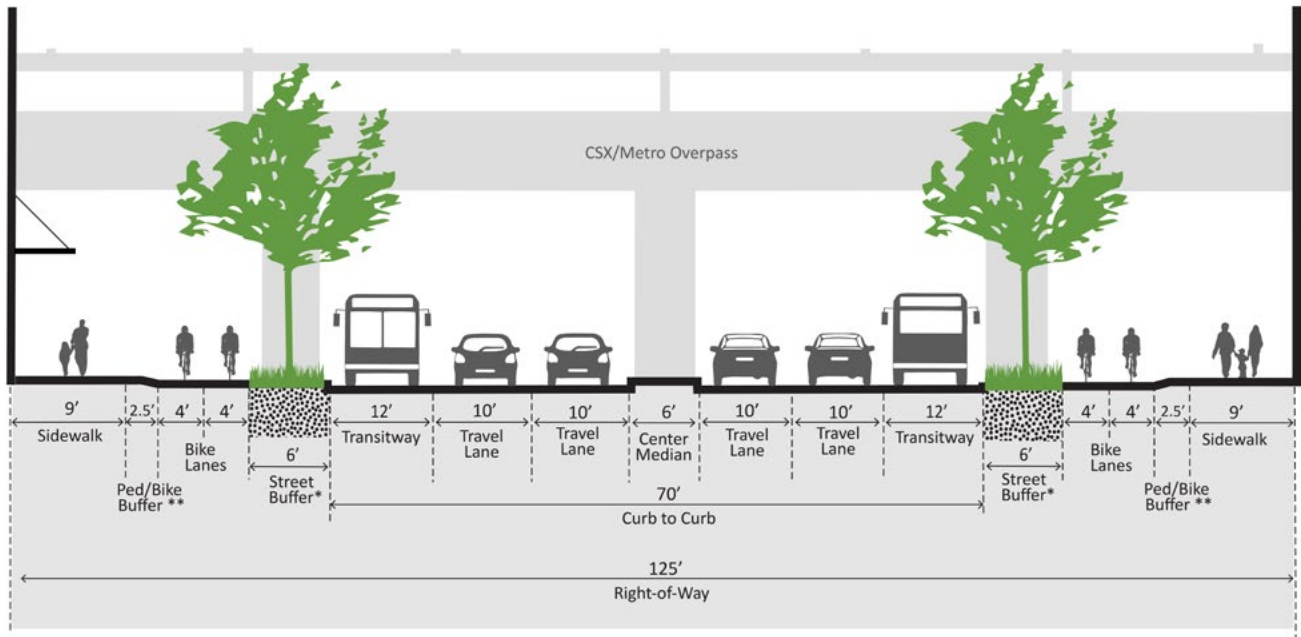


Figure 38. Colesville Road (M-10): Sarbanes Transit Center to East-West Highway, Looking North (CR)

* Buffer with stormwater management to employ best practices.

** Note: Ped/Bike Buffer to differentiate the bikeway and the sidewalk, this space includes a mountable curb (1V:4H maximum) and 1 foot paver band. The sidewalk is 3 inches above the bike lane.

Note: Public Improvement Easements can be used to further expand the Active Zone to achieve wider buffers between cyclists and pedestrians.

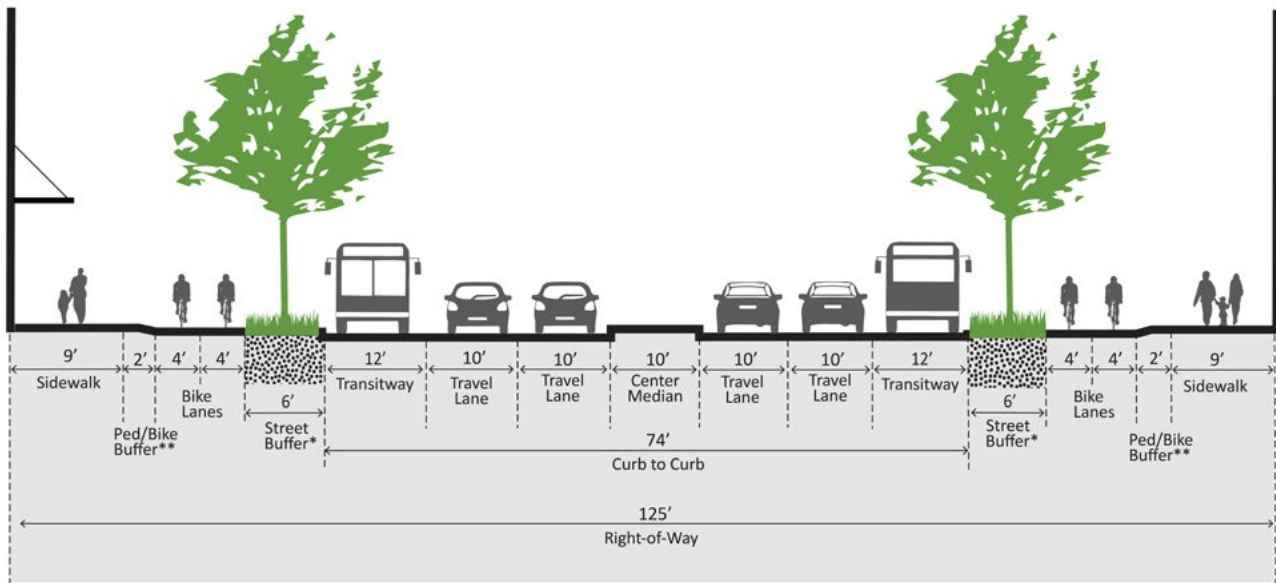


Figure 39. Colesville Road (M10): East-West Highway to 16th Street, Looking North (CR)

* Buffer with stormwater management to employ best practices.

** Note: Ped/Bike Buffer to differentiate the bikeway and the sidewalk, this space includes a mountable curb (1V:4H maximum) and 1 foot paver band. The sidewalk is 3 inches above the bike lane.

Georgia Avenue Sections

As envisioned by the 2013 *Countywide Transit Corridors Functional Master Plan*, BRT traveling south on Georgia Avenue will enter the Sarbanes Transit Center in the Southbound direction by turning right onto Colesville Road. It will exit the Transit Center in the southbound direction by turning onto Wayne Avenue and back onto Georgia Avenue. The reverse will occur in the northbound direction. For that reason, the segment of Georgia Avenue between Colesville Road and Wayne Avenue does not include dedicated transit lanes. To ensure a safe and feasible transition, this segment was included with both options for cross sections along Georgia Avenue developed for the Sector Plan.

Note: the sections that depict median-running BRT include the notation (MR) and the sections that depict curb-running BRT include the notation (CR).

Median-Running Bus Rapid Transit: Figures 40 - 43

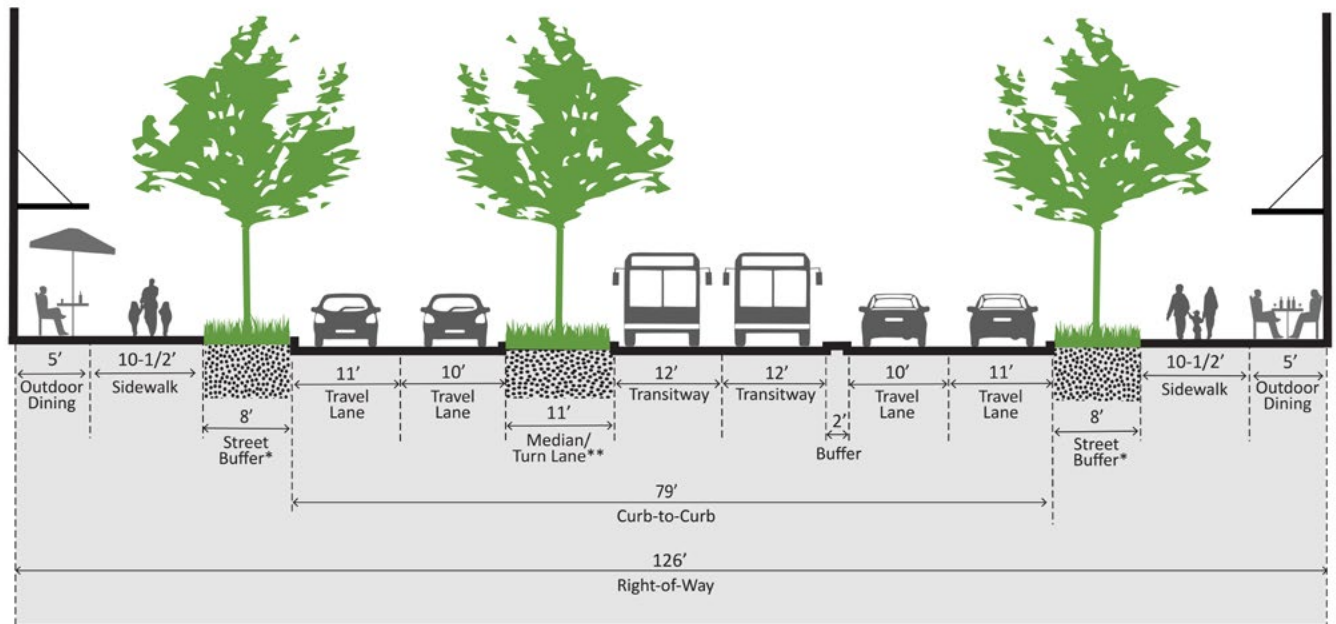


Figure 40. Georgia Avenue (M-8): Spring Street to Colesville Road, Looking North (MR)

* Buffer with stormwater management to employ best practices.

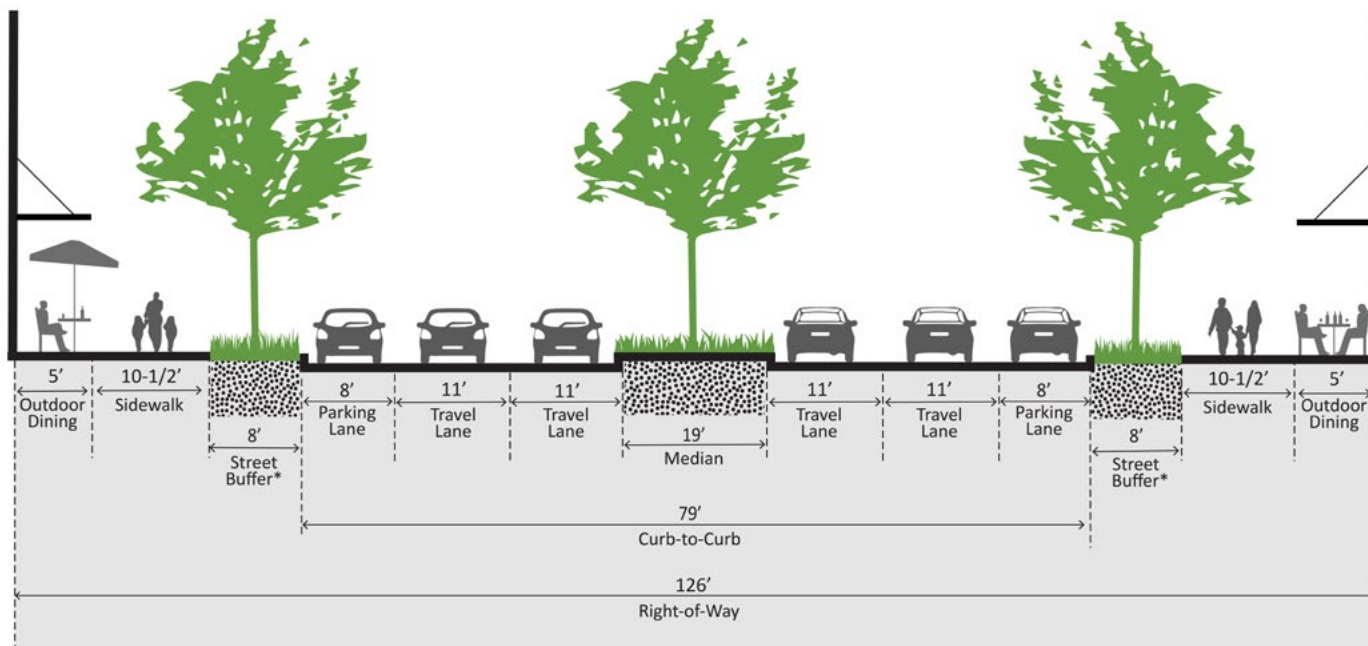


Figure 41. Georgia Avenue (M-8): Colesville Road to Wayne Avenue, Looking North

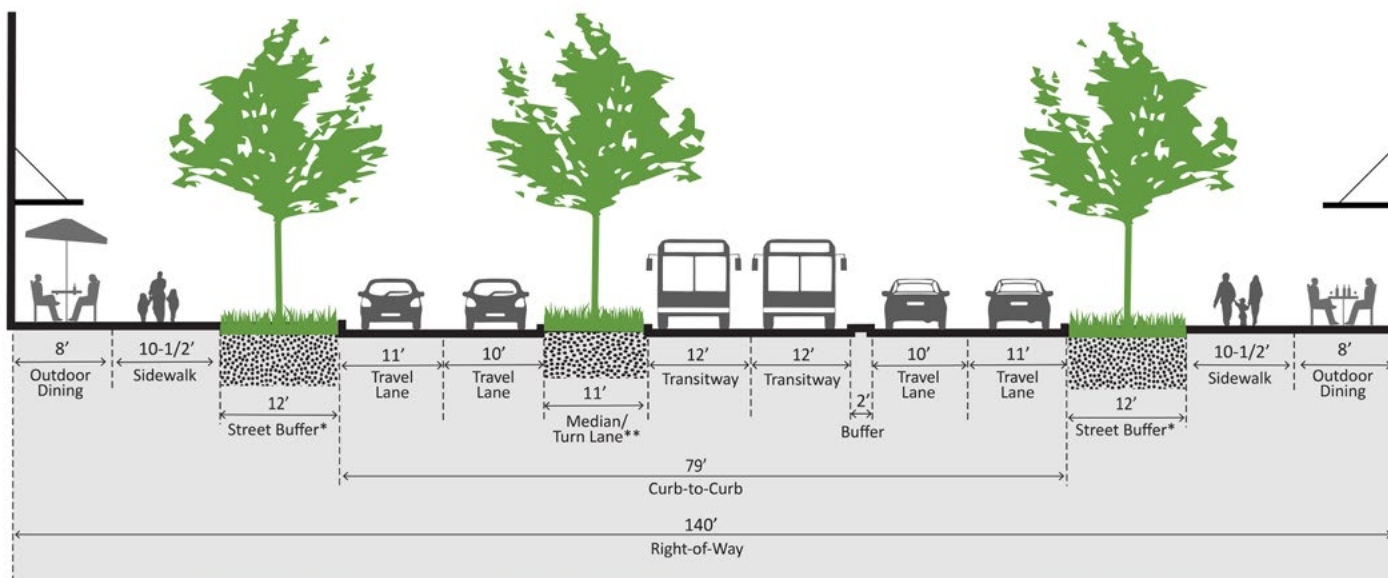


Figure 42. Georgia Avenue (M-8): Wayne Avenue to Blair Mill Road, Looking North (MR)

* Buffer with stormwater management to employ best practices.

** At turn lane condition, turn lane is 9 feet and there is a 2 foot buffer between the turn lane and the transitway.

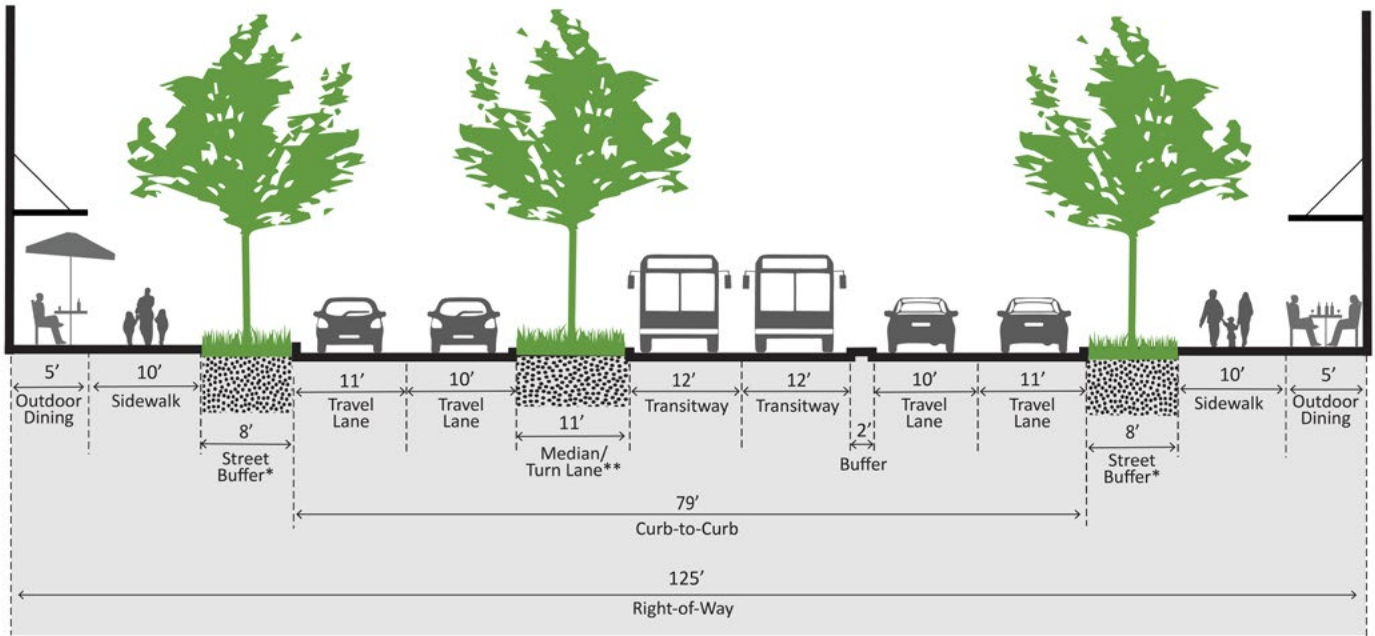


Figure 43. Georgia Avenue (M-8): Blair Mill Road to Eastern Avenue, Looking North (MR)

Curb-Running Bus Rapid Transit: Figures 44 - 47

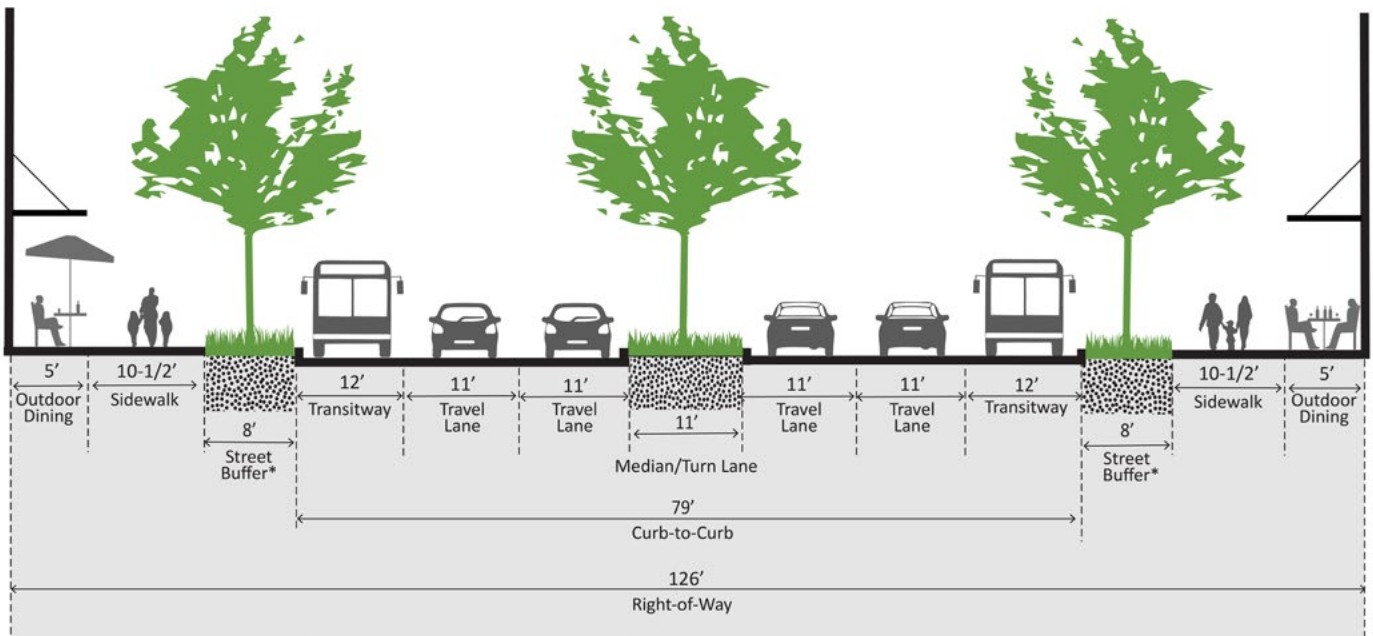


Figure 44. Georgia Avenue (M-8): Spring Street to Colesville Road, Looking North (CR)

* Buffer with stormwater management to employ best practices.

** At turn lane condition, turn lane is 9 feet and there is a 2 foot buffer between the turn lane and the transitway.

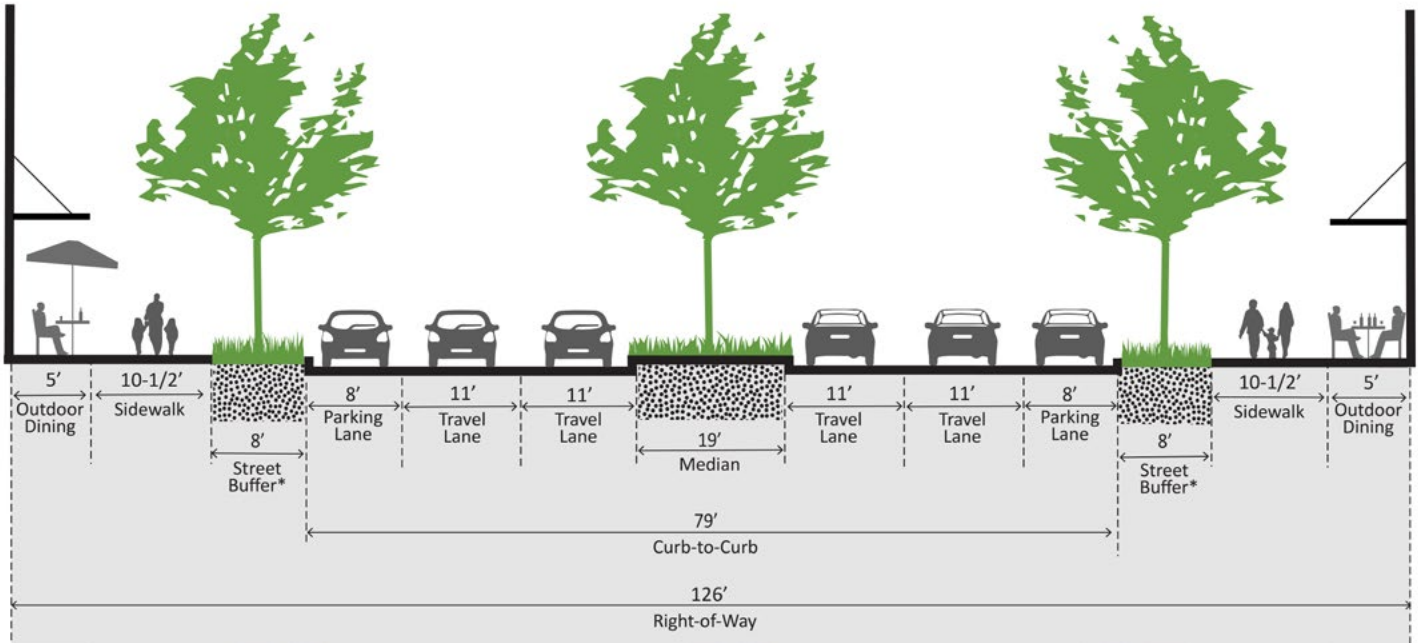


Figure 45. Georgia Avenue (M-8): Colesville Road to Wayne Avenue, Looking North

NOTE: The figure above is the same as Figure 41 in the median-running BRT option for Georgia Avenue. It is included here to maintain clarity in the sequence of sections along Georgia Avenue.

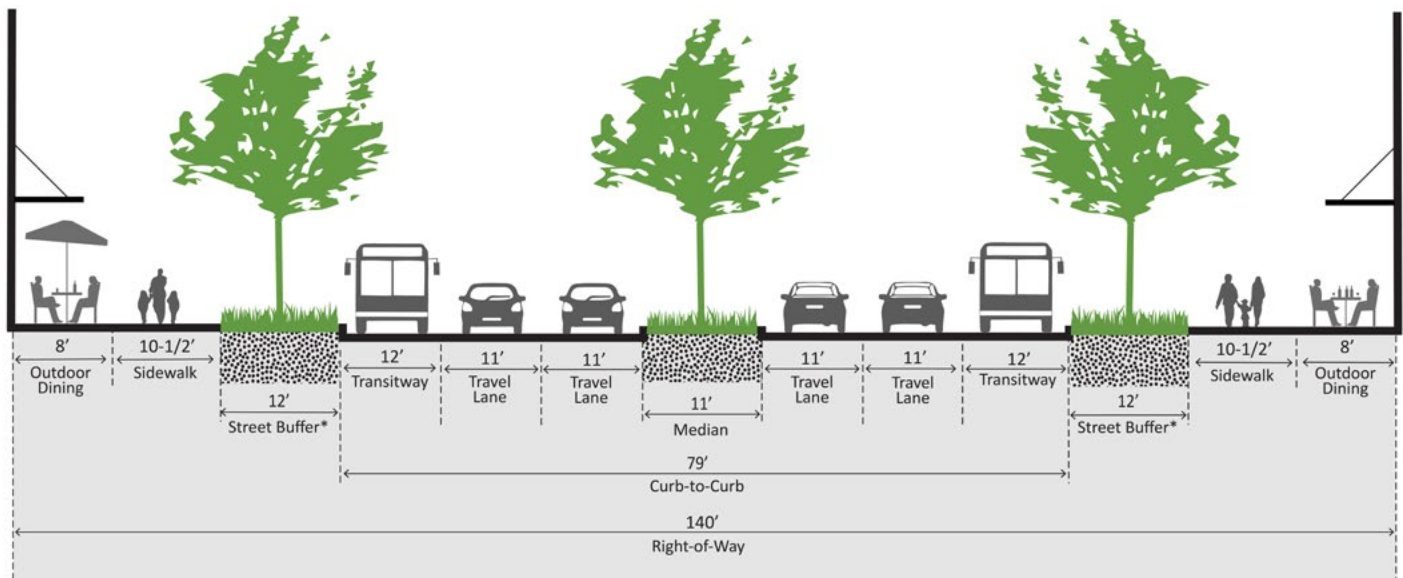


Figure 46. Georgia Avenue (M-8): Wayne Avenue to Blair Mill Road, Looking North (CR)

* Buffer with stormwater management to employ best practices.

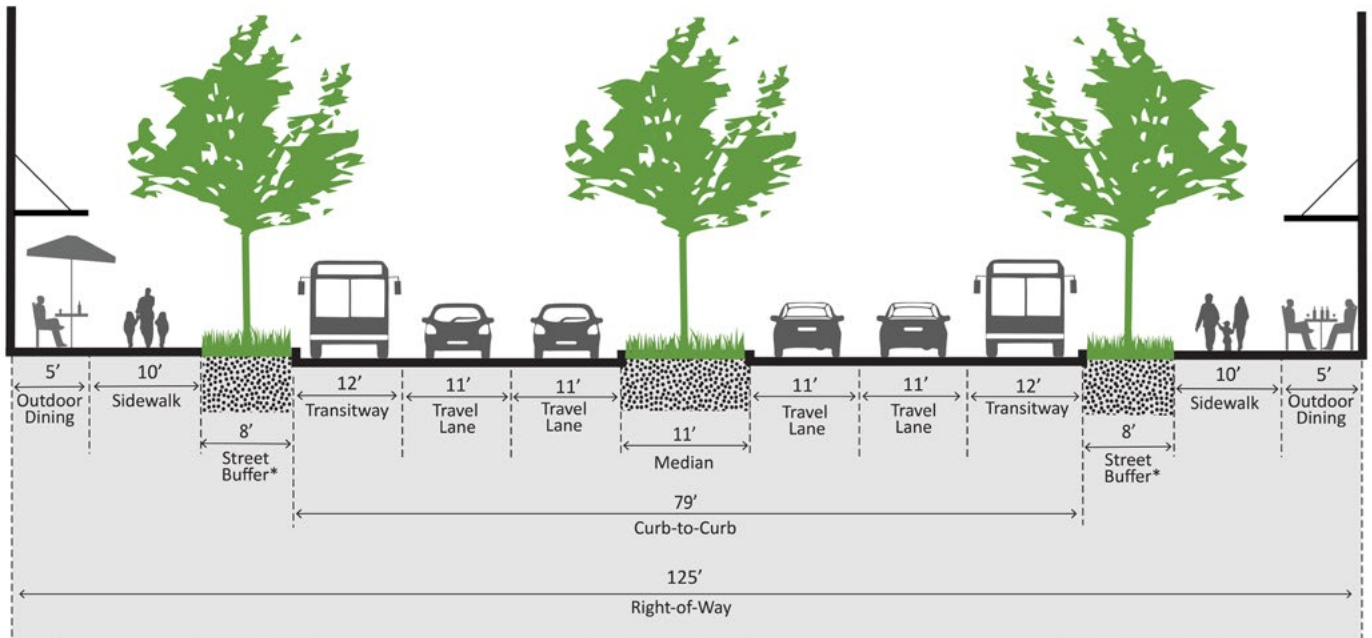


Figure 47. Georgia Avenue (M-8): Blair Mill Road to Eastern Avenue, Looking North (CR)

Additional Street Sections

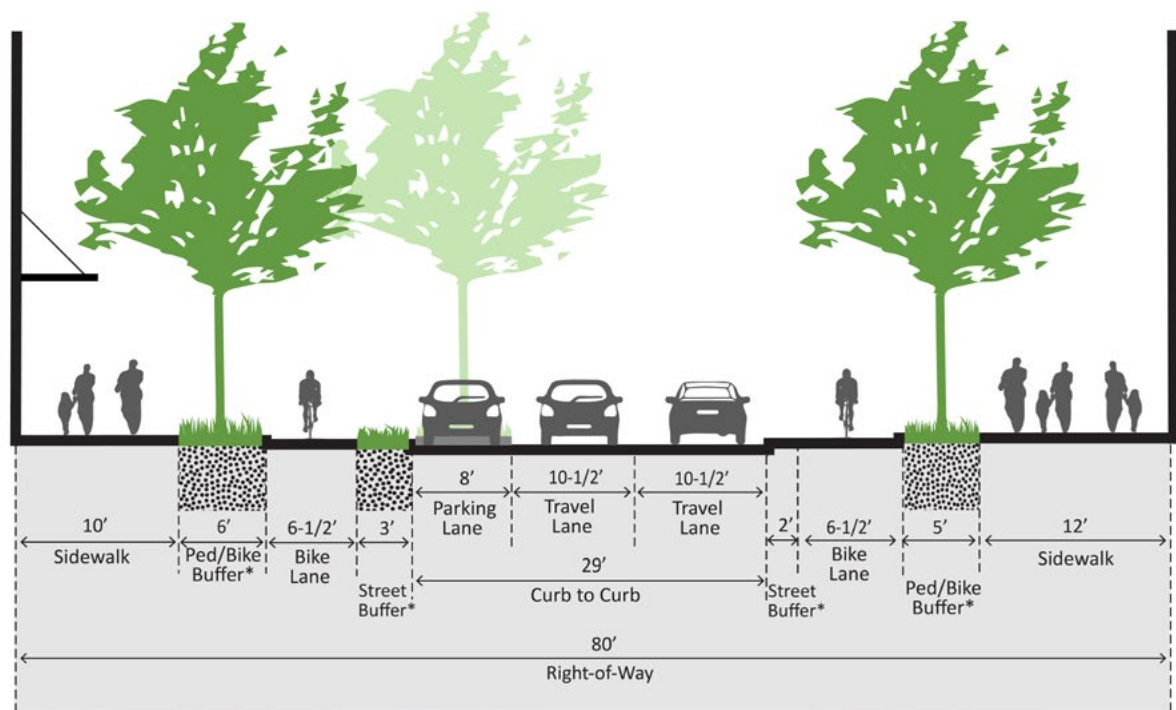


Figure 48. 13th Street (B-3): Georgia Avenue to Eastern Avenue, Looking East

* Buffer with stormwater management to employ best practices.

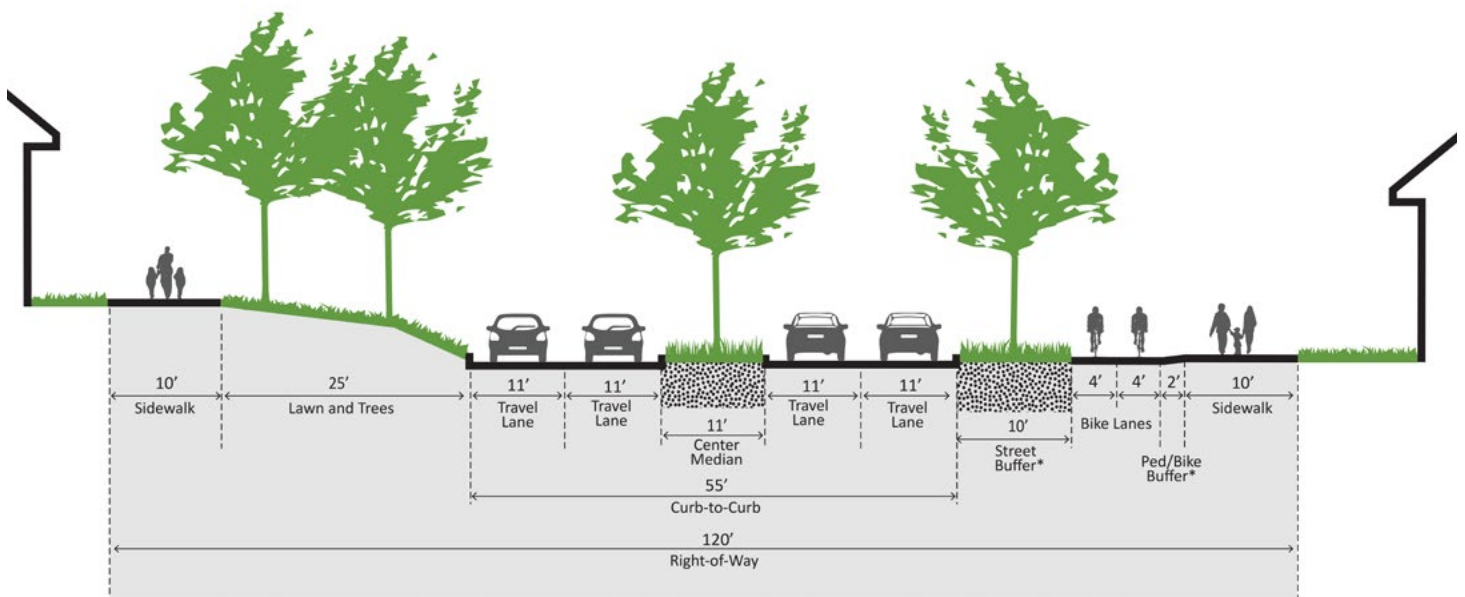


Figure 49. 16th Street (M-9): Colesville Road to East-West Highway, Looking North

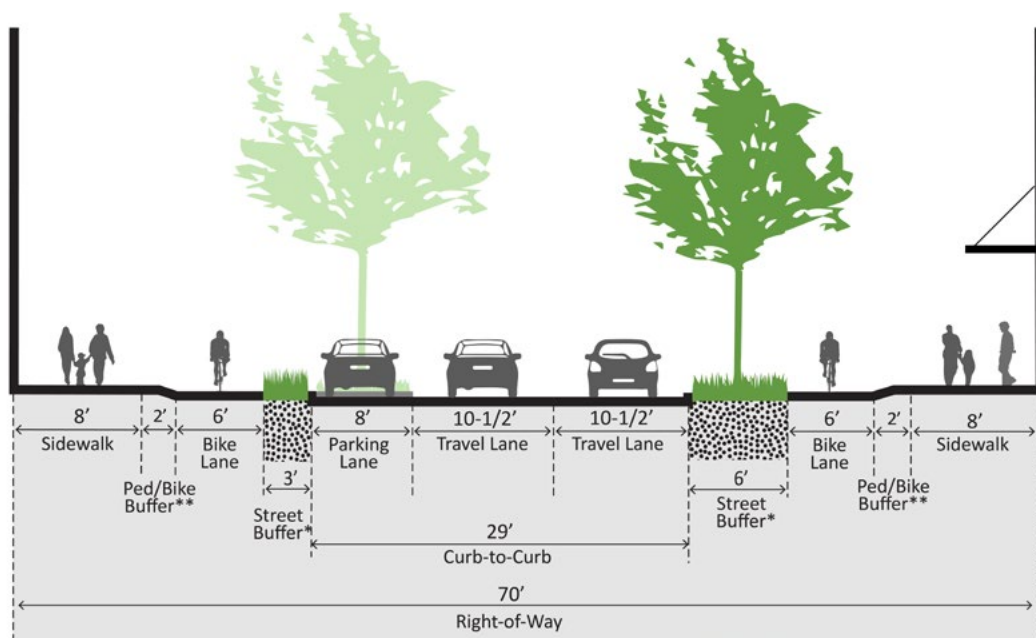


Figure 50. Silver Spring Avenue (B-25): Fenton Street to Georgia Avenue, Looking East

* Buffer with stormwater management to employ best practices.

** Note: Ped/Bike Buffer to differentiate the bikeway and the sidewalk, this space includes a mountable curb (1V:4H maximum) and 1 foot paver band. The sidewalk is 3 inches above the bike lane.

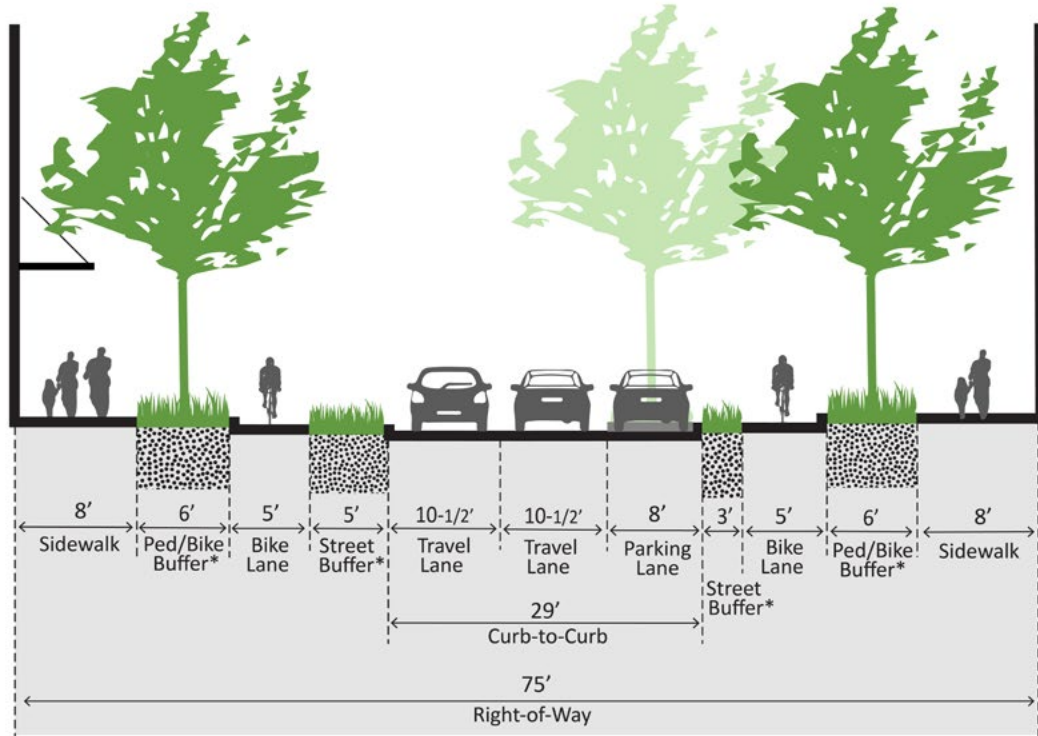


Figure 51. Cameron Street (B-28): 2nd Avenue to Georgia Avenue, Looking East

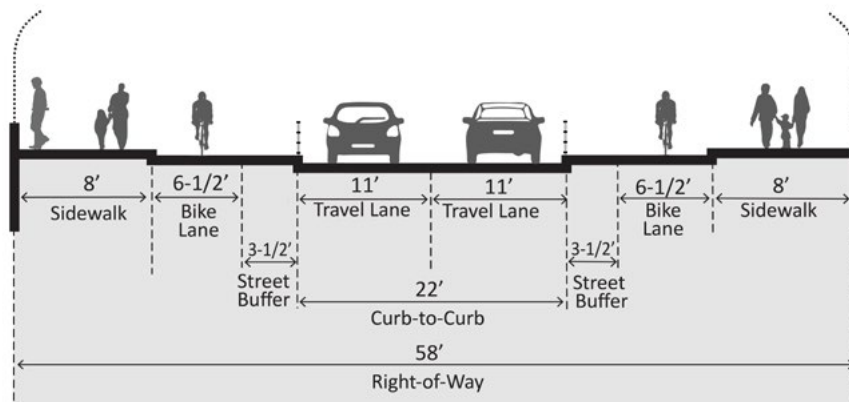


Figure 52. Burlington Avenue Bridge (M-20): Looking East

* Buffer with stormwater management to employ best practices.

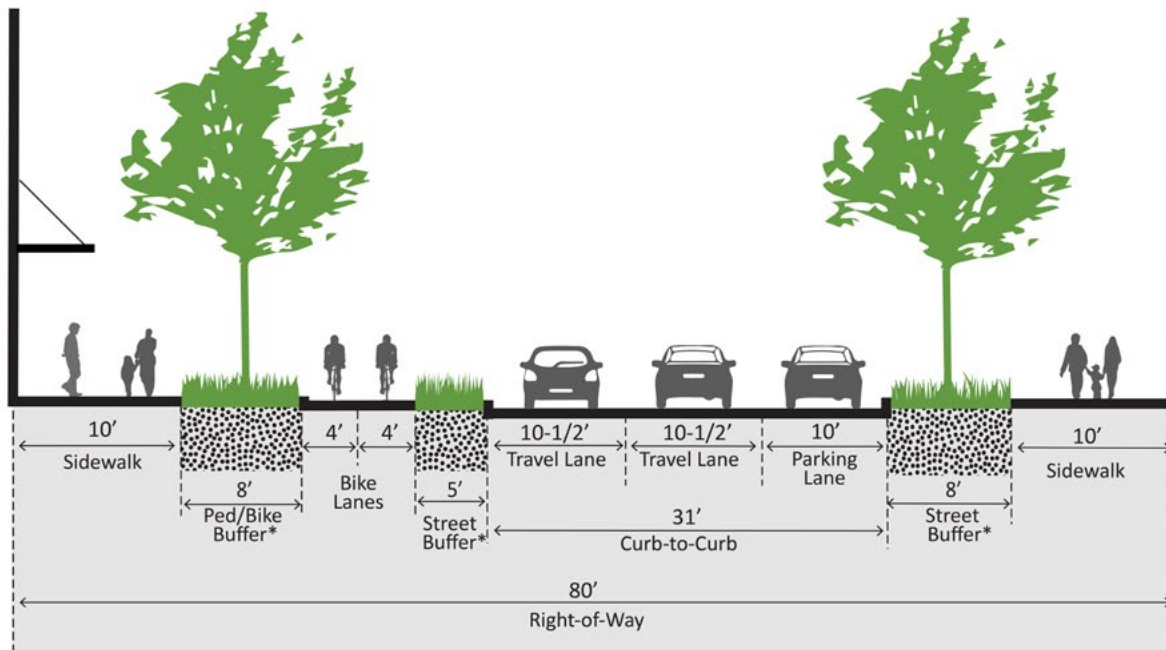


Figure 53. Fenton Street (A-264): Philadelphia Avenue to Wayne Avenue, Looking North

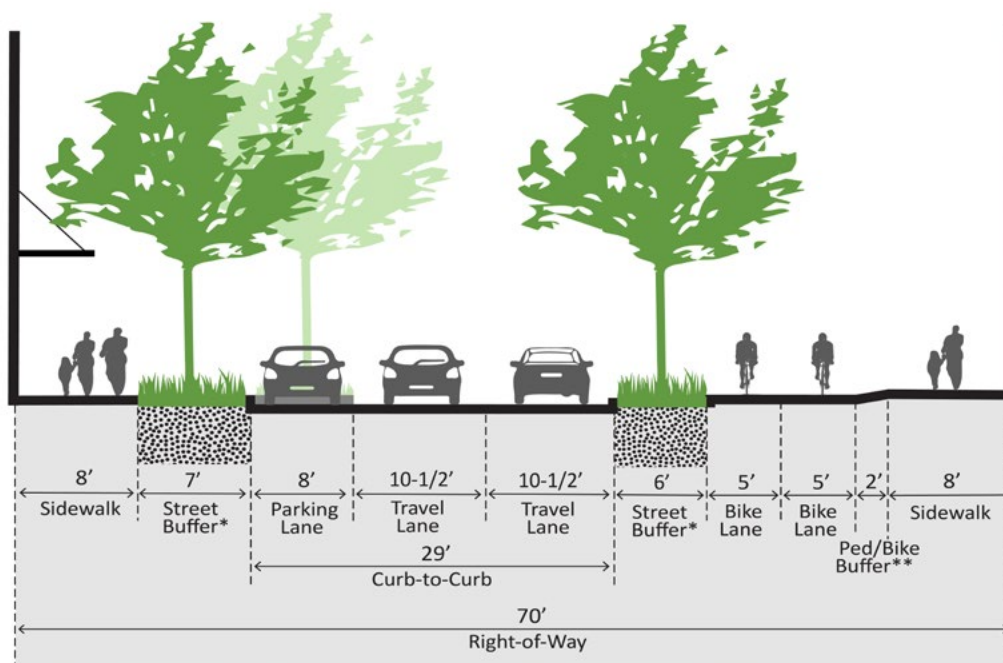


Figure 54. Blair Mill Road (B-25): Eastern Avenue to East-West Highway, Looking East

* Buffer with stormwater management to employ best practices.

** Note: Ped/Bike Buffer to differentiate the bikeway and the sidewalk, this space includes a mountable curb (1V:4H maximum) and 1 foot paver band. The sidewalk is 3 inches above the bike lane.

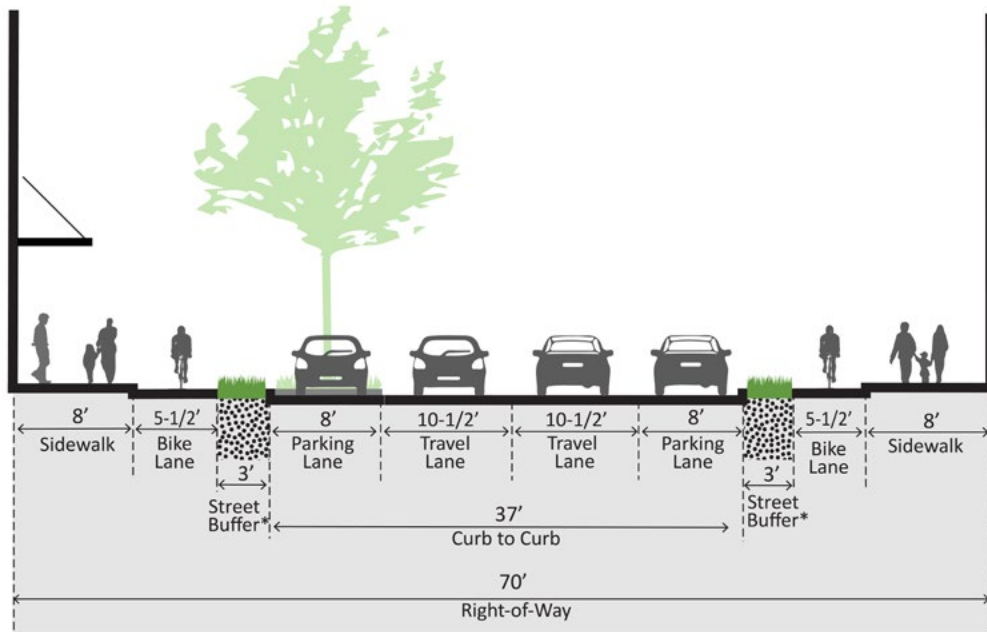


Figure 55. 1st Street (B-1): Spring Street to Fenwick Lane, Looking North

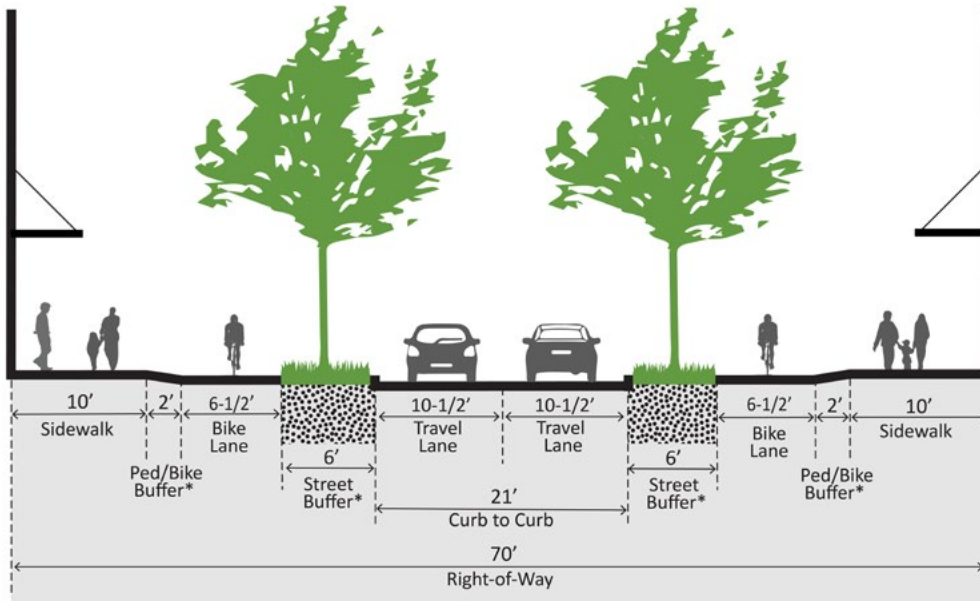


Figure 56. 1st Street (B-30): Fenwick Lane to Cameron Street, Looking East

* Buffer with stormwater management to employ best practices.

** Note: Ped/Bike Buffer to differentiate the bikeway and the sidewalk, this space includes a mountable curb (1V:4H maximum) and 1 foot paver band. The sidewalk is 3 inches above the bike lane.

3.7. RESILIENT DOWNTOWN

The Metropolitan Washington Council of Governments (MWCOG) identifies the region's most direct climate hazards as extreme temperatures, continued rising overall temperatures, increased precipitation, and drought.⁶ In response to a growing climate crisis and the reality of increased natural hazards, extreme weather and temperatures, and species loss, the objectives of the environmental recommendations are to bolster resiliency: the "overall capacity of a city to survive, adapt, and thrive no matter what kinds of chronic stresses or acute shocks experienced." While this plan cannot address all climate impacts, it aims to address those that directly intersect with urban planning. The goals and recommendations in this Plan support and reinforce the concepts put forth in both *Thrive Montgomery 2050* and the Montgomery County *Climate Action Plan*.

3.7.1. Goals:

- Reduce surface and heat island temperatures.
- Support goals of net zero carbon emissions by 2035 as outlined in the county's *Climate Action Plan*.
- Implement the Green Loop concept to create shaded, performance-based complete streets to serve the Plan area.
- Increase tree canopy cover, green cover, and native plantings throughout the Plan area.
- Improve air and water quality, and mental and physical health.
- Support the growth of urban agriculture and local food production and processing in the downtown.

3.7.2. Environmental Equity & Health

The effects of climate and environmental inequity are often most acutely felt in lower-income, elderly, and minority communities. Historically they are known to have distant or inaccessible parks or open space for rejuvenation and recreation, fewer shade trees and vegetation, poorer quality housing and insulation, lower energy efficiencies, reduced access to affordable nutritious food, and more. These conditions create chronic physical, mental, and economic stress, and illness. Environmental equity is the protection from environmental hazards as well as a guarantee of equitable access to environmental resources such as parks, open green space, trees and vegetation, community gardens, and energy-efficient buildings.

Goals:

- Provide equitable access to quality natural resources such as parks, green space, community gardens, and the Green Loop.
- Provide equitable access to cool areas with native tree and vegetation cover.
- Encourage all new construction to exceed county minimum standards for energy efficiency.

Recommendations:

- Expand vegetated parks, open space, and amenities within a five-minute walk for all residents.
- Implement the Green Loop throughout the districts.
- Design parks and open spaces with native canopy trees, shrubs, grasses, and flowers.

⁶ Summary of Potential Climate Change Impacts, Vulnerabilities, and Adaptation Strategies in the Metropolitan Washington Region: A synopsis of lessons learned from the Metropolitan Washington Council of Governments' climate adaptation planning initiatives from 2010 – 2012.

NATURE-BASED SOLUTIONS

Nature-based Solutions (NbS) are sustainable planning, design, and engineering practices that weave natural features and processes to mimic nature in the built environment. They aim to tackle climate change, food insecurity, water resources, natural disaster risks, and lost ecological performance. Several NbS are included in the Green Loop concept including cool surfaces, canopy tree and vegetation planting, shading strategies, and stormwater management. Architectural applications of NbS may include solar orientation, green roofs, gardens, water catchment, and vegetated landscapes. These elements improve human and ecological well-being, increase long-term economic prosperity, community desirability, rental retention rates, property values, and more.

THESE NATURE-BASED SOLUTIONS:



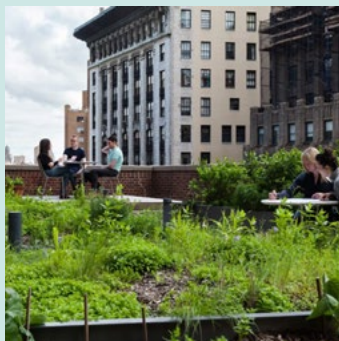
Green Roofs



Integrated Stormwater
Management



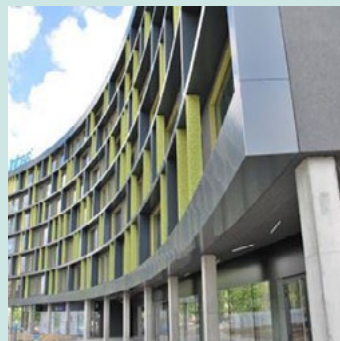
Shaded Playgrounds



Urban Agriculture



Cool Streets & Bikeways



Solar-Oriented Buildings

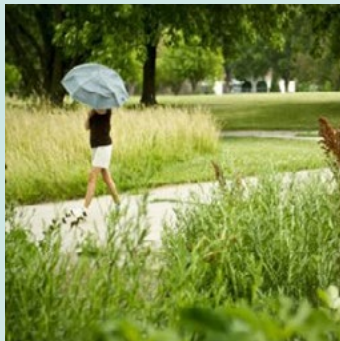


Plant Biodiversity

WILL ACHIEVE THESE OUTCOMES:



Increased Economic Success



Green Environment



Urban Wildlife

Figure 57. Nature Based Solutions

3.7.3. Urban Heat Islands and Tree Canopy

Urban heat islands occur when urban areas have substantially warmer air temperatures than the suburban surroundings due to increased impervious surface, surface color, and vehicle and building heat emissions. Silver Spring is particularly susceptible to trapped heat and elevated temperatures due a high impervious cover (77 percent) and low mature tree cover (8.6 percent). In the summer of 2020, street surface temperatures reached as high as 155 degrees Fahrenheit. Global trends indicate that temperatures will only increase in coming years.

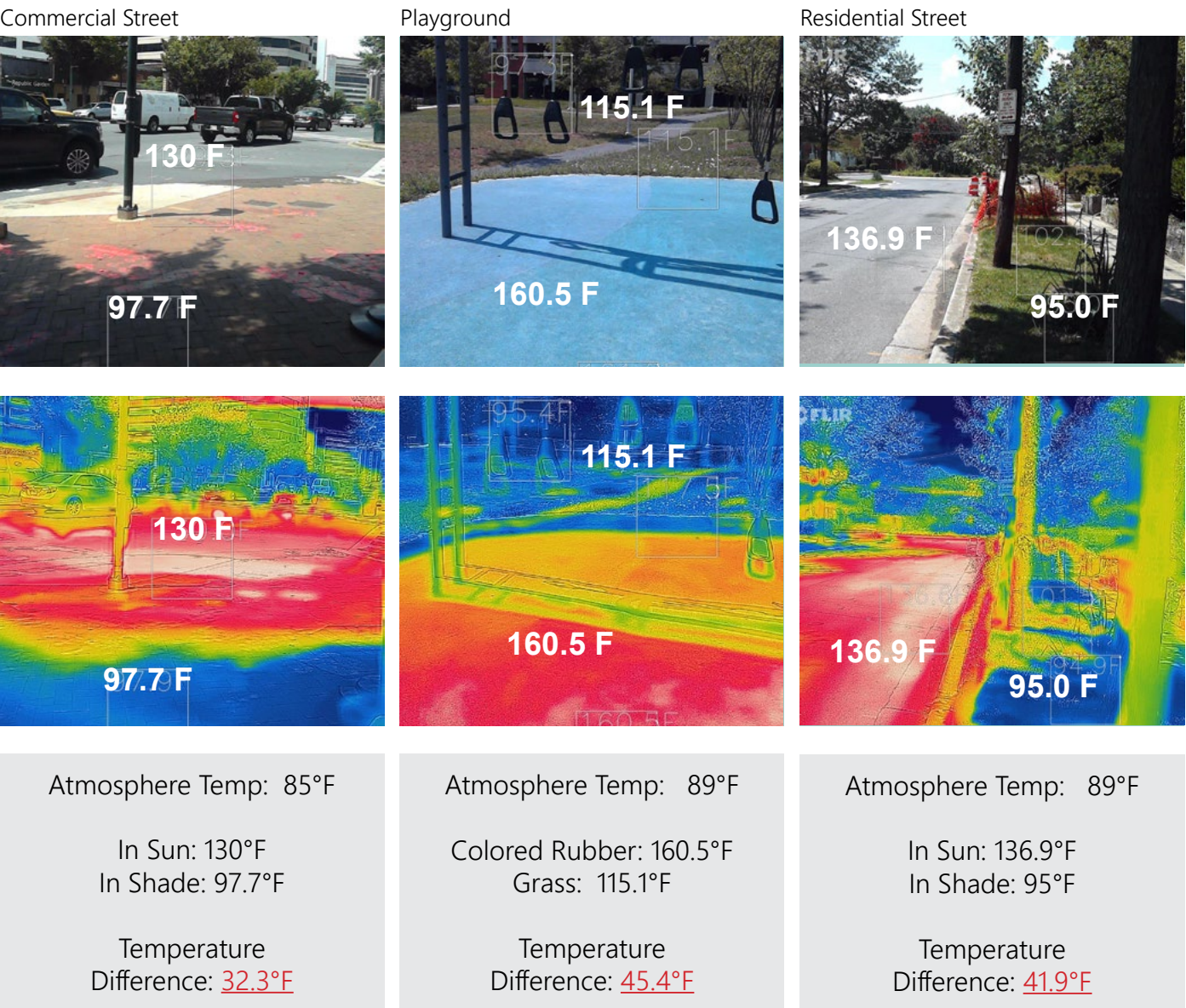


Figure 58. Rising Urban Temperatures
Heat map images showing mid-summer surface temperature conditions in several locations in the downtown. Data collected in July-August 2020.

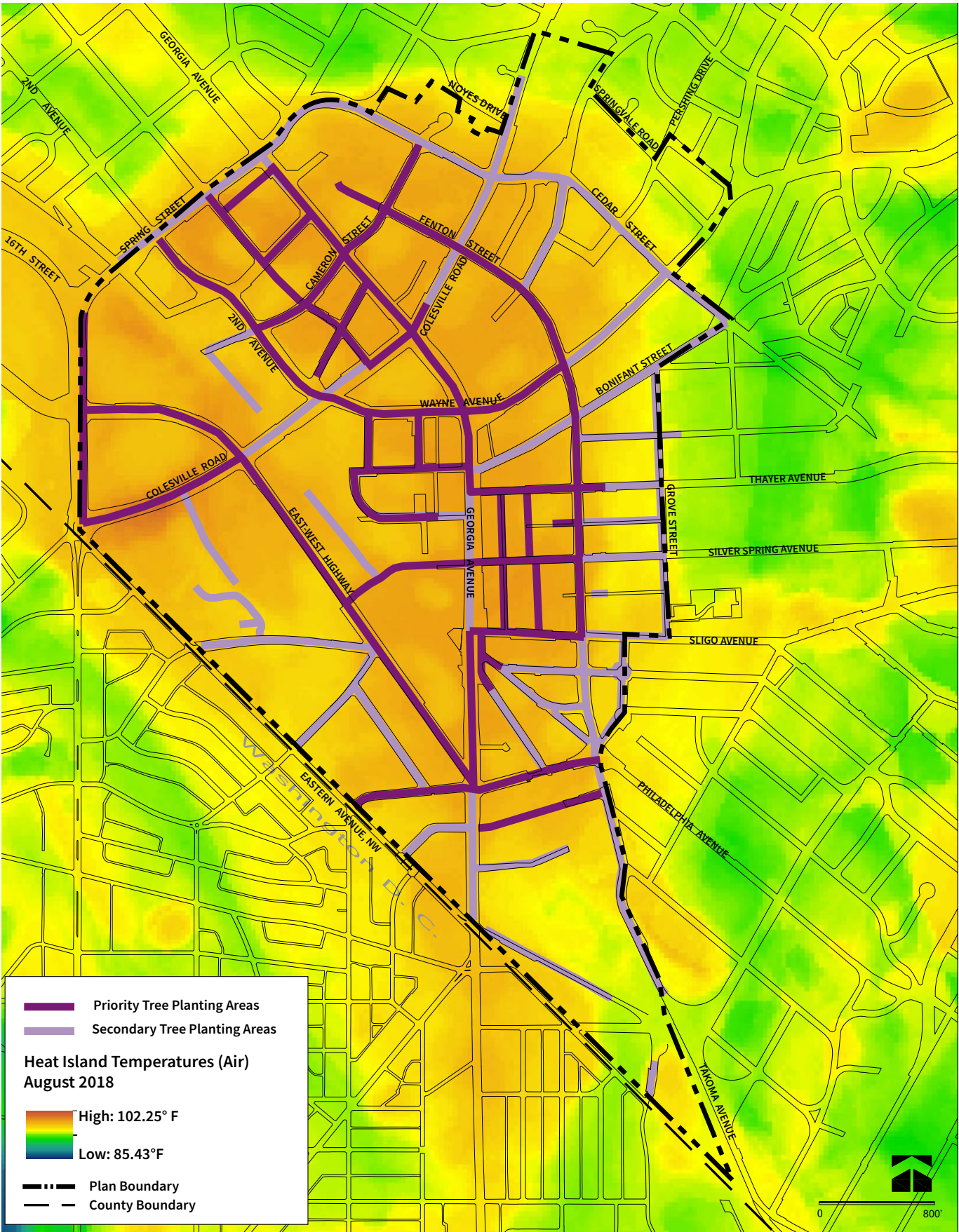
Goals:

- Reduce urban surface and air temperatures through the application of nature-based solutions throughout Silver Spring.
- Achieve 45 percent tree canopy coverage for the Plan area on both public and private property.
- Increase biodiversity and urban beautification.
- Consider a cool roof initiative that encourages existing property owners to paint roofs not covered in vegetation with reflective surfaces which can reduce the effects of heat island by as much as 33 percent.
- Encourage all property owners to take advantage of M-NCPPC's free Tree Montgomery program.
- Apply Sustainable Sites Initiatives (SITES) principals to new construction projects.

Recommendations:

- Implement the principles of the Cool Streets Recommendations that will be included in the Design Guidelines into new site development and street renovations on public and private property.
- Plant diverse, stratified, and climate- and region-appropriate native tree species to reduce vulnerabilities, diseases, and improve their ability to thrive in a changing climate.
- Prioritize urban tree canopy and green infrastructure in targeting the hottest streets and where tree canopy is deficient (see Map 27).
- Require a minimum of 35 percent green cover on Optional Method Development projects where practicable and consistent with Plan objectives. A project may provide green cover by:
 - Providing an intensive green roof (six inches or deeper) on the rooftops or other structure;
 - Providing native canopy tree cover on the landscape of the project site area at ground level; and/or
 - Providing a combination of tree canopy cover and intensive green roof for a total of 35 percent or greater green cover on the total site.
 - Site area for green cover may be reduced to accommodate on-site energy generation, other environmental site amenities, or where desirable to achieve other plan objectives.
 - New rooftops not covered in green roofs or alternative or renewable energy generation should be cool roofs or active rooftop uses.

Map 27. Right-of-Way Priority Planting Areas



3.7.4. Energy and Carbon Emissions

Burning fossil fuels, including their extraction and refinement, is the leading cause of climate change. More than 50 percent of Montgomery County's total carbon emissions comes from building inefficiencies, with another 30 percent from transportation. Reducing building energy demand, increasing on-site energy production, reducing reliance on non-electric automobiles, and sequestering carbon through NbS are essential to meeting the county's net zero carbon emissions goals by 2035.

Goals:

- Reduce greenhouse gas emissions and energy consumption.
- Increase carbon sequestration capacities on public and private property.
- Support the county's net zero carbon emissions goals.
- Reduce vehicle miles traveled per capita.
- Support and encourage maximizing energy efficiency on all sites.
- Support and encourage the use of regenerative, renewable energy.

Recommendations:

- Optimize building and site orientation to maximize passive and active solar energy.
- Support and encourage onsite alternative energy such as solar, geothermal, and/or future renewable resource energy technologies.
- Support proposed and future affordable housing development projects that incorporate emergency energy hubs. Onsite energy hubs provides equitable backup solar and battery storage during local and regional energy outages.
- Encourage exceeding the county's minimum energy standards and striving for net-zero, net positive, and/or Living Building standards.
- For public⁷ and large properties or/and consolidated land parcels with more than one building encourage the use and installation of decentralized and renewable/clean energy systems such as micro-grids and other dependable electricity sources thereby reducing reliance on distant power networks, non-renewable resources, and susceptible power lines.



OATI Microgrid Technology Center. Microgrid for campus in Bloomington, Minnesota powered by natural gas turbines, solar panels, wind turbines, its own storage of electricity and backup generators. (Mitchell Stier Photo).

⁷ Montgomery County is embracing microgrids to improve the resiliency of public facilities, reduce their environmental impact and reduce operational costs to taxpayers. Microgrids are local power systems that use clean and renewable energy sources.

3.7.5. Water Quality

Impervious surfaces are more than 77 percent of the Plan's commercial and industrial area. They prevent stormwater from infiltrating into the ground, causing runoff to transport debris, oils, and contaminants into nearby inlets and waterways. Consequently, the receiving streams of Sligo and Rock Creek have fair to poor water quality and degraded aquatic habitats. Predicted increases in precipitation and extreme storm events will further compromise the streams, water quality, and the infrastructure associated with it. As properties redevelop, creative and innovative stormwater design is essential to maximize treatment volumes and minimize untreated stormwater.

Goals:

- Reduce untreated stormwater runoff and runoff rates.
- Improve water quality.

Recommendations:

- Utilize active stormwater management strategies to minimize deviation from the standards established by Chapter 19 of the county's Erosion, Sediment Control, and Stormwater Management Code.
- Maximize green roof stormwater credit using an eight-inch soil medium or approved equivalent. This simultaneously increases building insulation, reduces urban H=heat island effect, and increases opportunity for native plant, insect and other species biodiversity.
- Consider using rainwater catchment cisterns for irrigation or other allowable uses within the zone.
- Minimize the impacts of development through the installation of green infrastructure such as bioretention areas, stormwater swales and trenches, structured cells, stormwater planters, permeable pavements, or other future green technologies.
- Design stormwater facilities separate from street tree panels for easier maintenance.
- Where surface parking is retained, integrate vegetative stormwater management systems and/or solar systems covering a minimum of 35 percent of the surface parking area.
- Integrate environmental site design (ESD) into the right-of-way of all new and proposed roads and retrofits.
- Provide decking over linear stormwater treatments to maximize treatment size while not inhibiting pedestrian use.

3.7.6. Transportation

Vehicles with combustible fuel engines account for 41 percent of carbon emissions in the county. Reducing vehicle demand and use will reduce the county's carbon and other greenhouse gases. The right-of-way contains planting zones where vegetation can perform biological services such as sequestration, stormwater management, temperature reduction, and wildlife habitat.

Goals:

- Encourage pedestrian and bicycle travel within the Plan area for local trips to reduce carbon and other greenhouse gas emissions.
- Enhance ecological performance of public rights-of-way.

Recommendations:

- Provide a healthier, cooler, more comfortable pedestrian experience along streets and open space through the planting of native canopy trees prioritizing lower income areas, the Green Loop, and the hottest streetscape areas (see Map 27).
- During street renovations and routine replacements, consider installing smart street elements such as: solar lighting, solar metering, solar crosswalks, LED lighting, electric vehicle charging portals, and/or other technologies that emerge.
- Implement cooling strategies into all streetscape plans during the site planning process, and street renovation and improvement projects. Cool Streets Recommendations will be included in the Design Guidelines.
- Encourage car-free, flexible, and car-lite streets through flexible streets, road diets, alternative modes of transportation, and bike/vehicle sharing programs.
- Encourage more vehicle charging stations than currently required on private property and within the right-of-way (ROW) where appropriate to support a growing electric vehicle demand.



3.7.7. Food Security

The Montgomery County Food Council's 2017 Montgomery County Food Security Plan envisions all people having access to safe, sufficient, and nutritious food. The Plan estimates that between six and 22 percent of families in the Plan area experience food insecurity due to a variety of factors, including access to affordable, local food. The COVID-19 pandemic disrupted the county's food supply chains, exacerbating inequities in reliable, affordable, and sufficient food.

Goals:

- Support affordable retail and food-service rental space.
- Encourage local food processing, storage, and kitchen space.
- Support food distribution and aggregation hubs.
- Support access to affordable, walkable, and healthy food retail for all residents.
- Support access to reliable long-term urban agricultural opportunities.

Recommendations:

- Allow commercial food kitchens, food processing, and rooftop farms as accessory commercial uses in the Plan area CR zones.
- Increase local capacity for the production of culturally appropriate foods, through increased opportunities for community gardening and agriculture efforts.
- Support the establishment of healthy corner stores, farmers markets, and other access points for federal benefits usage.
- Support onsite community composting and education hubs to bring us closer to meeting the County's zero waste goals.



Stormwater infrastructure integrated into the streetscape design (top, archdaily.com by John Gollings), Rooftop farm in an urban area (bottom, foodprint.org).

3.8. COMMUNITY FACILITIES

The 2000 Silver Spring CBD Sector Plan included many recommendations to improve the community facilities for the downtown area. Over the last two decades, most of these recommendations have been realized. These include:

- The construction of new community facilities to support the growing population of downtown Silver Spring, such as the Silver Spring Library, District Courthouse, Fire Station, and Civic Building.
- The restoration of cultural facilities, such as the Silver Theatre, which is now operated by the American Film Institute, and the former fire station, which is now a restaurant.
- A year-round, weekly farmer's market, organized by FRESHFARM, established in 2005.
- Facilities for seniors, such as the affordable senior housing adjacent to the Silver Spring Library, and an eldercare facility operated by Easter Seals.

The county is also currently constructing the South County Regional Recreation and Aquatic Center as part of the Elizabeth Square development in the Downtown North District, which will offer new recreation facilities such as exercise and weight room equipment, movement and dance studios, multipurpose activity rooms, public-use spaces for gathering, an Olympic-size indoor pool and a senior wellness center.

As a result of the success of these efforts over the past 20 years, few new community facilities are being recommended in this plan.

3.8.1. Safety and Security

In all urban areas, creating a street network where all people feel safe is of paramount importance. This Plan recognizes the efforts of numerous county agencies, including the county police department and the Urban District, in their coordinated efforts towards making the streets of Silver Spring safe for all.

Goal:

Maintain a downtown where streets feel safe for pedestrians throughout all districts, during the day and into the evening. Silver Spring has a nighttime economy, and it is important for the continued success of businesses that attract patrons at night for pedestrians to feel comfortable visiting those establishments after sunset.

Recommendations:

- Ensure consistent street lighting to provide a greater level of safety and comfort for pedestrians, particularly when traveling at night. The plan strongly recommends additional lighting at underpasses such as the pedestrian tunnel under the Metrorail at Georgia Avenue, and under the Metrorail at Colesville Road.

Brigadier General Charles E. McGee Library in Silver Spring



3.8.2. Homelessness and Housing Insecurity Services

Silver Spring is an important hub for services for the homeless and housing-insecure. The recently completed Progress Place, located behind the new fire station on Georgia Avenue, provides many services, while organizations like MHP, Housing Opportunities Commission of Montgomery County, and others strive to provide more deeply affordable units in the downtown.

Goal:

Continue to provide enhanced and expanded housing and support facilities and services for all residents of the Plan area.

Recommendations:

- Support ongoing partnerships between Montgomery County, service providers, and civic organizations that manage downtown Silver Spring to address the needs of the housing-insecure.
- Promote the development of additional affordable, including deeply affordable, housing units within the Plan area.
- Work with service providers to provide more places that are welcoming and supportive of the housing insecure.
- Consider installation of and improved wayfinding for restrooms at public facilities in the Plan area.

3.8.3. Montgomery College

Established on the site in 1950, the Takoma Park/Silver Spring campus of Montgomery College covers more than 100 disciplines for more than 7,800 students from more than 140 countries and is the headquarters of the college's health sciences program and the School of Art and Design.

Recommendation:

The Plan recommends working with Montgomery College to explore and develop opportunities to expand the campus program in South Silver Spring.

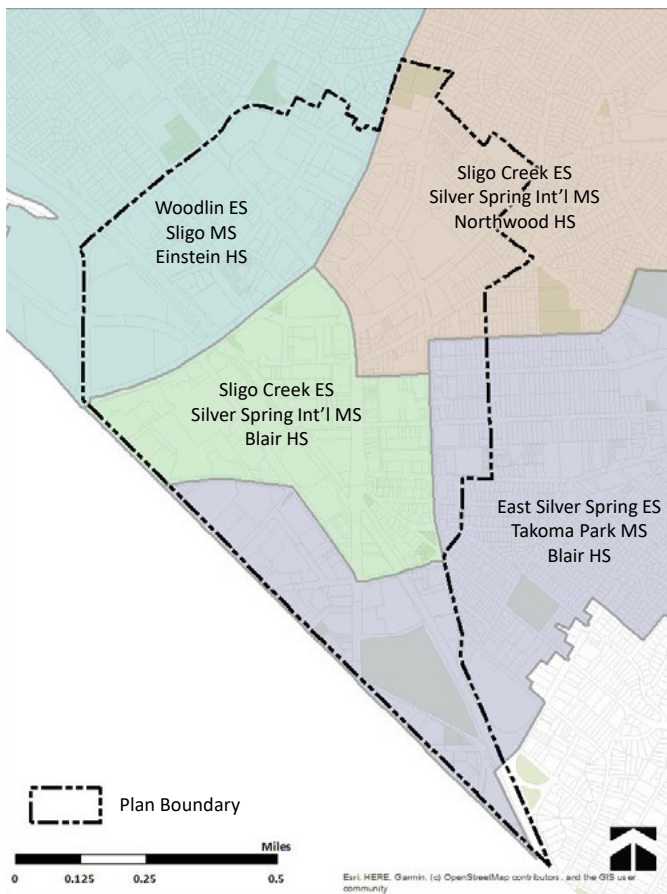
Montgomery College



3.8.4. Schools

At the time of this Plan, the Plan area is being served by Albert Einstein, Northwood, and Montgomery Blair High Schools as part of the Downcounty Consortium. At the middle school level, the area is being served by Sligo, Silver Spring International, and Takoma Park Middle Schools, and at the elementary school level by Woodlin, Sligo Creek, and East Silver Spring Elementary Schools. The service areas of the schools are subject to change in the near future when the scheduled reopening of Woodward High School is complete and students are reassigned to relieve the overutilization in the Downcounty Consortium and Walter Johnson clusters.

Map 28. School Service Area Map, 2021-2022 School Year



Enrollment Trends and Scheduled Capital Projects

Enrollment growth in Montgomery County Public Schools (MCPS) has been particularly strong since 2008. Due to a decade of large increases in elementary student enrollment, the county is now experiencing a period of growth at many secondary schools. Similar to the countywide trend, there has been significant enrollment growth in the Downcounty Consortium, including the Plan area which began at the elementary schools and has now approached the secondary school levels. To address the space needs in the area, several capital projects have been approved and are awaiting completion within the next few years.

The following are projects invested directly into relieving overutilization at schools currently serving the Plan area.⁸

- Northwood HS Addition/Facility Upgrades: 990-seat increase to the existing 1,500 seat capacity by the 2025-2026 school year.
- Charles W. Woodward HS Reopening: 2,160 seats available for the Downcounty Consortium and Walter Johnson High School by the 2025-2026 school year.
- Takoma Park MS Addition: 380-seat increase to the previous 940-seat capacity will be completed for the 2021-2022 school year.
- Silver Spring International MS Addition: 60-seat increase to the existing 1,100-seat capacity by the 2024-2025 school year
- Woodlin ES Major Capital Project: new building with 650-seat capacity will replace the existing 460-seat facility by the 2023-2024 school year.

The school system experienced lower enrollment in the 2020-2021 school year, particularly in the lower grades, due to the effect of the COVID-19 pandemic and switch to virtual learning. While it is anticipated that enrollment will return to its normal level in a few years, MCPS projects an overall slowdown in growth due to a continually declining trend in resident births, which in general leads to a smaller number of kindergarten classes, and a subsequent ripple effect into upper grades as those classes progress through the system each year.

The following graphs illustrate enrollment trends of the schools serving the Plan area for the past 15 years, and the next six years as projected by MCPS.

⁸The planned capacity and schedules reflect MCPS Superintendent's Recommended FY 2023 Capital Budget and FY 2023-2028 Capital Improvements Program, but may be subject to change by the County Executive or County Council's decisions.

The following graphs illustrate enrollment trends of the schools serving the Plan area for the past 15 years, and the next six years as projected by MCPS.



Figure 59. Plan Area Elementary Schools Enrollment Trend & Projection



Sligo Creek Elementary School (MCPS)

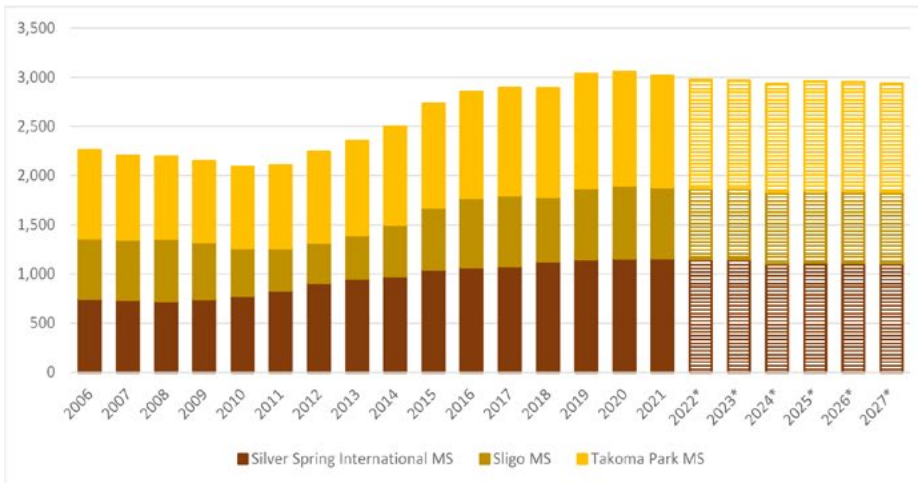
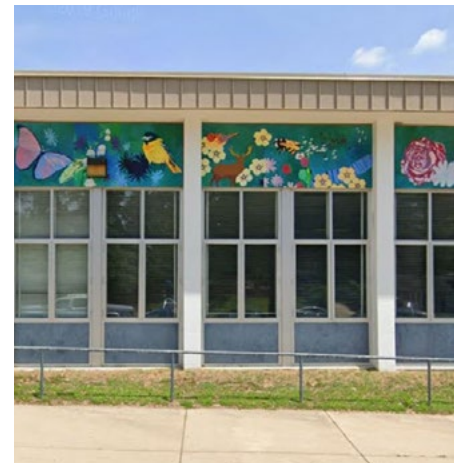


Figure 60. Plan Area Middle Schools Enrollment Trend & Projection



Mural at Sligo Middle School (MCPS)

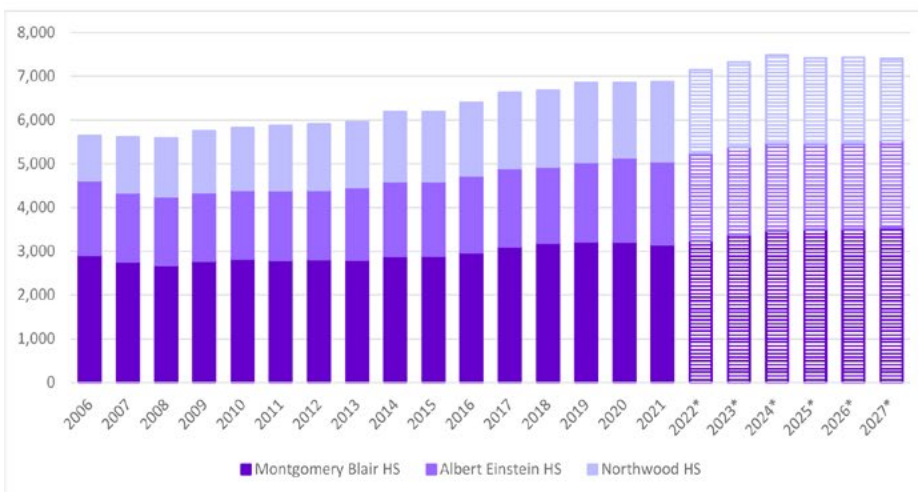
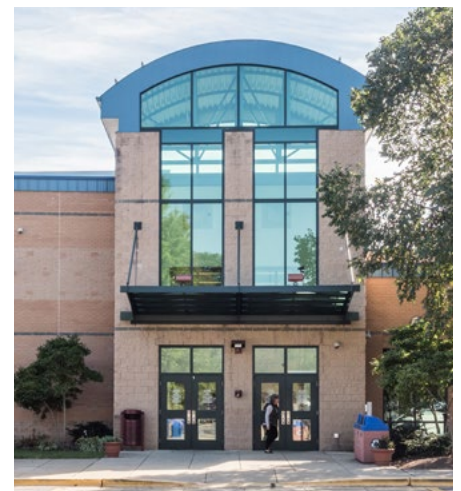


Figure 61. Plan Area High Schools Enrollment Trend & Projection



Albert Einstein High School (MCPS)

Note: Figures 40-42 depict enrollment as projected by MCPS in Superintendent's Recommended FY 2023 Capital Budget and the FY 2023-2028 Capital Improvements Program.

Plan Impact Estimate

Downtown Silver Spring and the adjacent communities are mostly classified as Infill Impact Areas by the 2020 Growth and Infrastructure Policy, except for the Woodside Park neighborhood, which is classified as a Turnover Impact Area. This indicates that the growth context of the Plan area and vicinity is predominantly multi-family housing that exhibits low impact on enrollment growth on a per unit basis.

To estimate the number of students generated by residential units proposed in a project or plan, student generation rates, which are the average number of students residing in a certain housing type within a specific area, are typically used. Following the adoption of the new Growth and Infrastructure Policy (GIP) in 2020, Montgomery Planning uses the student generation rate of residential unit types by School Impact Area uniformly for master plan and development review purposes.

Potential Impact from Multi-Family High-Rise Units

The land use vision of this Plan allows the Silver Spring Downtown area to accommodate up to an additional 11,000 new multifamily high-rise units. If the vision of this Plan were to be realized in a full residential build-out scenario, the new units may produce the following number of students at each school level approximately, based on the average student generation rates of multi-family high-rise units built in Infill Impact Areas since 1990.:

- 374 elementary school students
- 165 middle school students
- 176 high school students

A full build-out scenario of the residential visions of this Plan, however, is not a guaranteed outcome as it requires the redevelopment of numerous properties that are privately owned and driven by the market. Some properties identified by the Plan as potential sites for additional housing units may take up to 20 or 30 years to realize, and others may not even be redeveloped at all during the life of the Plan.

Potential Impact from Attainable Housing Units

Student generation rates specific to attainable housing unit types are not officially analyzed by the current County Growth and Infrastructure Policy. A preliminary analysis of existing units show a wide range of rates across the county, where neighborhood context is likely a contributing factor to the variation. Existing units located around the downtown Silver Spring vicinity were generally found to exhibit a rate similar to that of low-rise multifamily units, which tend to be approximately one quarter or one half of the rate of a single family detached unit. In addition, the number of units produced in the area on an annual basis is expected to be insignificant in terms of enrollment impact estimates. Therefore, when a single family detached structure is replaced or converted into a duplex, triplex or quadruplex structure as a result of this Plan, the net impact on enrollment is expected to be minimal.

Multifamily residential units, once fully occupied, generally maintain a consistent student generation rate throughout the life of the structure. Single family units, on the other hand, will repeat a cycle of rise and fall for a period following a sales transaction. Therefore, when an existing single-family neighborhood experiences a particularly active real estate market, a surge of enrollment growth can happen as a result of the turnover activity alone without any additional housing units being developed. Conversely, enrollment can also decline at a certain point once the turnover of existing single-family units in the area start slowing down.

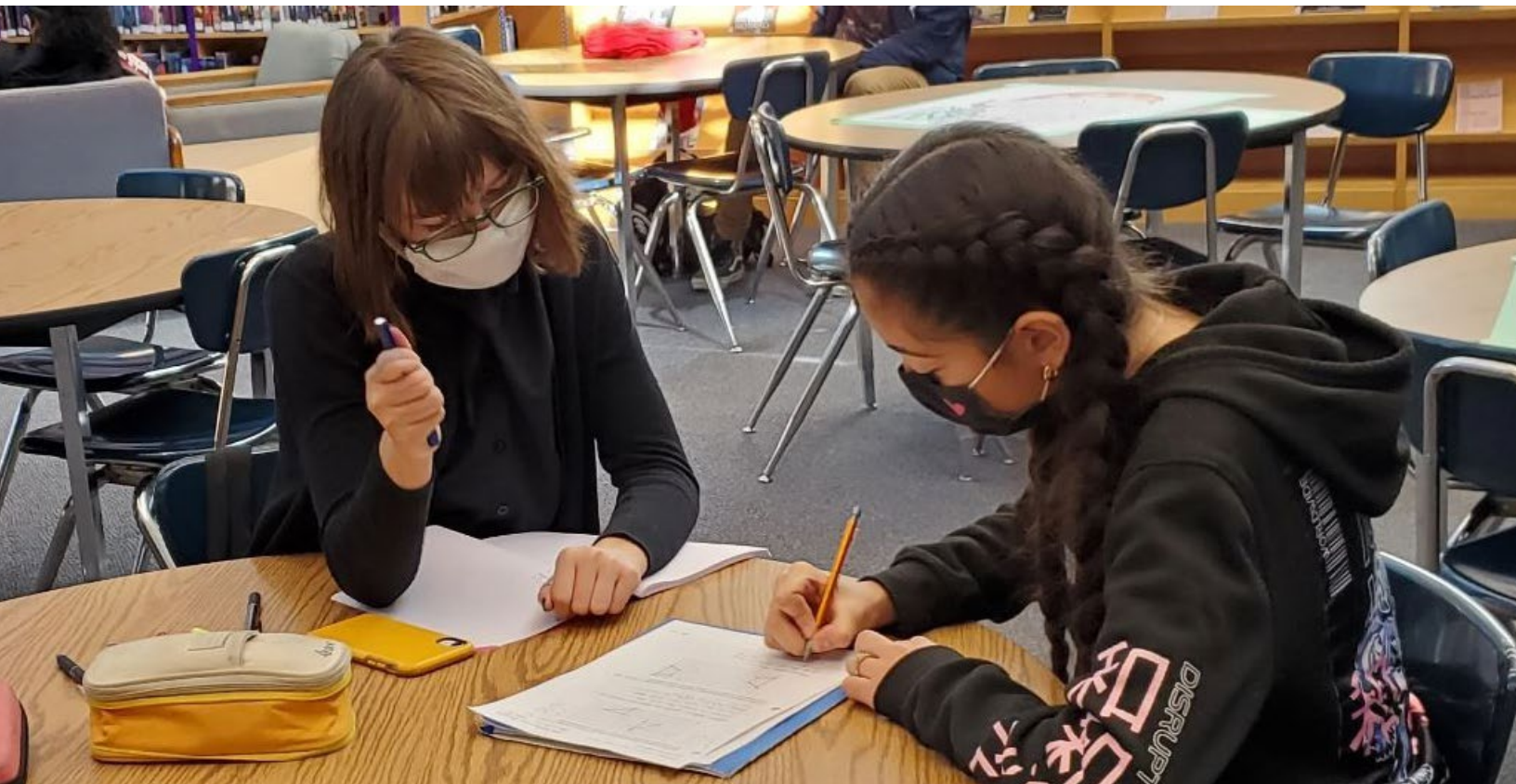
The estimated enrollment impact of this Plan therefore does not translate into an immediate demand for additional school capacity. It is possible for existing school facilities to have surplus capacity to accommodate an increased enrollment demand when the housing visions of this Plan are being realized. Therefore, evaluations on the net effect of students from new residential development on the school system occurs by each project individually at the time of approval.

School Facility Planning

MCPS Capacity Planning Strategies

The Division of Capital Planning and Real Estate at Montgomery County Public Schools (MCPS) develops strategies and long-range facility plans to meet the needs of public school capacity and programs. While this division collaborates with the Planning Department regarding a master plan's impact on public school enrollment, their forecasts and capital projects focus on the six-year time frame of a Capital Improvements Program period rather than the longer-range vision typical of a master plan. MCPS planners review the space available at schools annually by comparing the enrollment projections with program capacity in the sixth year of the CIP planning period. In the case of overutilization, an effort to judge the long-term need for permanent space is made. If deemed necessary, MCPS would then explore a range of options to address the needs appropriately. All approaches will require approval by the Board of Education before being considered for implementation. The strategies that MCPS would typically pursue are:

- Determine if space is available at adjacent or nearby schools and reassign students where adequate space is available.
- Consider classroom additions to accommodate continual enrollment increases. Additions can be considered at nearby schools, to be followed by student assignments after completion of the capital project. For a classroom addition to be considered for funding, the sixth year projection of the Capital Improvements Program period of an individual school needs to exceed the following thresholds:
 - Elementary schools – minimum 92-seat (4 classrooms) capacity deficit
 - Middle schools – minimum 150-seat (6 classrooms) capacity deficit
 - High schools – minimum 200-seat (8 classrooms) capacity deficit



Students studying at Albert Einstein High School (MCPS)

- If reassigning students or increasing capacity at existing schools is not sufficient to address the projected demand, consider the opening of a new school, or reopening of a previously closed school (see Figure 43 for former operating schools in the Plan area and vicinity). For a new school to be considered, the sixth year projection of the CIP period would have to exceed the following thresholds. An evaluation of each site will be conducted if and when a new elementary school is being considered:
 - Elementary schools – 500- to 600-seat capacity deficit cluster-wide
 - Middle schools – 800-seat capacity deficit in one or more clusters
 - High schools – 1600-seat capacity deficit in one or more clusters

Map 29. Former Operating Schools

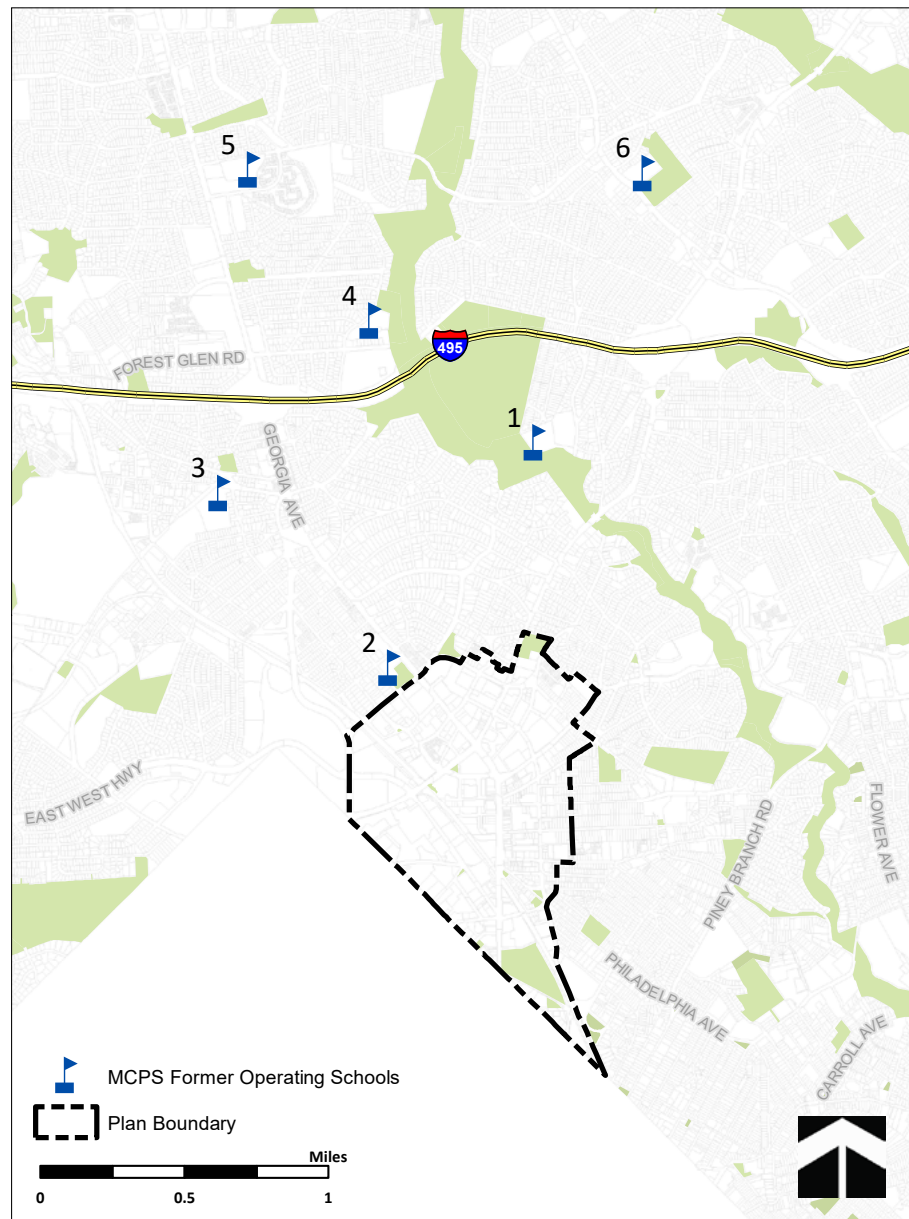


Table 11. Former Operating Schools

	Former School	Site Size (acres)	Current Use	Ownership
1	Parkside ES	6.19	TBD	BOE
2	Woodside ES	2.70	DHHS Silver Spring Center	County
3	Montgomery Hills Junior HS	8.67	Private School	County
4	Forest Grove ES	6.16	Holy Cross Hospital	County
5	Dennis Avenue ES	6.97	Dennis Avenue Health Center	County
6	Four Corners ES	5.66	HOC retirement home	County

Innovative School Facilities Initiative

Downtown Silver Spring provides an opportunity for an innovative school site that may not have been considered as an option for a school facility previously. While the options for acquiring land for traditional school sites are scarce in the immediate area, the prevalence of existing office buildings and various businesses, and the proximity to Montgomery College Takoma Park/Silver Spring campus offer a unique opportunity to initiate a non-traditional school facility.

As many school districts continue to seek additional space to serve their growing communities, some are turning to converting non-educational properties into innovative school environments. This type of adaptive reuse strategy can be a viable option in the Plan area, where there is a short supply of available land otherwise, to efficiently utilize available properties while also activating vacant office space. Through careful planning and implementation, even unconventional spaces that may not conform to MCPS's traditional facility standards can be successfully retrofitted into educational spaces and should be included when considering potential sites for school facilities. Collaborative efforts between MCPS and other appropriate agencies should also continue in order to maximize opportunities to provide off-site fields or other amenities for such facilities as needed.

In addition, some land uses such as health care, housing, or office space can be co-located with a school facility on the same site. Depending on the purpose and compatibility of the use, the spatial configurations of the site can be designed to either integrate the different uses closely, or to separate them without direct connection to each other. Such mixed-use school developments have been increasing in charter school facilities that are often less constrained legally and more open to creative solutions and can lead to successful learning environments and vibrant communities. MCPS's former operating schools in the vicinity (Map 29) are good candidate sites that be considered in this capacity, potentially by forming a partnership with other entities leasing the space to provide a special program focus.

Recommendations

Montgomery Planning produces a school utilization report that tracks the enrollment and housing trends of school service areas annually in accordance with the Growth and Infrastructure Policy. As an extension of that effort:

- Monitor the capacity needs of the schools serving the Plan area
- Impose an appropriate Utilization Premium Payment fee on residential development applications if the estimated enrollment impact is expected to burden the capacity utilization of any school(s) serving the site, as defined by the Growth and Infrastructure Policy.
- Collaborate with MCPS to ensure that potential options to provide additional capacity are available when necessary.
- Promote the viability of an innovative school facility option in the downtown Silver Spring area with the MCPS Division of Capital Planning and Real Estate.



Alexandria Public Schools purchased this office building in 2022 in order to eventually convert it into a new school. (Google Maps)

3.9. HISTORIC RESOURCES

The preservation of select historic resources is critical if the downtown is to develop in a way that reflects the authenticity and unique qualities of Silver Spring. The community's sense of place relies upon several historic buildings such as the Silver Spring Shopping Center and Theatre, the Fillmore, Hecht's Building, Canada Dry Building, Dyers and Cleaners Building, and the North Washington Shopping Center. Adaptive reuse of historic buildings provides texture and depth to the architectural character of the Plan area. The resources are thoughtfully designed, often focus on the pedestrian-level, and stand out in the urban landscape. They are manifestations of the community's collective identity. Concurrently, interpretation of these spaces will allow the community to explore, recognize, and discuss historical inequities. Silver Spring has a rich civil rights history that remains largely unrecognized in the current landscape. Recognition of sites of exclusion and resistance will permit a fuller understanding of development trends, engage new communities, and provide opportunities for important dialogues. Historic preservation is consistent with other values and goals recognized in this plan. These include the support of legacy businesses, sustainability, retention of existing market rate affordable housing, preservation of green space, and steps towards racial equity and social justice.

Protection of Montgomery County's historic resources is guided by the *Locational Atlas and Index of Historic Sites*, the *Master Plan for Historic Preservation*, and the Historic Resources Preservation Ordinance (Chapter 24A of the Montgomery County Code).

3.9.1. Goals

Silver Spring's historic buildings are critical to the community's character and collective memory; offer tangible connections to the past; provide opportunities for education and interpretation; and create a diversity of building types within the Plan area. The Silver Spring Downtown and Adjacent Communities Plan will:

- Recognize and interpret the diversity, heritage, and history of the Plan area.
- Encourage preservation and adaptive reuse of historically significant properties.
- Educate owners of historic properties on the benefits of local, state, and federal historic preservation tax credit programs.
- Document and support local, independently-owned businesses in operation for over 15 years.



The Silverton Condominiums, formerly a Canada Dry bottling facility.

3.9.2. Resources Listed in the Master Plan for Historic Preservation

The County Council adopted the *Master Plan for Historic Preservation* in 1979. The plan includes all officially designated historic sites and districts. These sites or districts have met at least one criterion for historical, cultural, or architectural significance, and merit protection under the Historic Resources Preservation Ordinance, Chapter 24A. The Historic Preservation Commission (HPC) is charged with the identification, designation, and regulation of historic sites or districts in Montgomery County. To ensure the rehabilitation of historic properties, the County Council adopted legislation to create a historic preservation tax credit program for properties listed in the *Master Plan for Historic Preservation*. Owners of properties are eligible to receive a 25 percent tax credit for qualified expenses related to maintenance, restoration, or preservation of exterior features.

Silver Spring features numerous resources listed in the *Master Plan for Historic Preservation*. These resources have architectural and historical significance, but more importantly, provide material and tangible benefits to the community. The historic sites and districts shall continue to be preserved as they contribute to the vitality of downtown Silver Spring. For example, prioritizing the continued rehabilitation of the Jesup Blair House (listed to the *Master Plan for Historic Preservation* in 1986) will allow M-NCPPC to identify a partner to activate the space and complement future park programming at Jesup Blair Park. Interpretation of historic sites and districts, however, should be reevaluated to ensure analysis of segregation, discrimination, underrepresented communities, and other histories insufficiently documented in previous surveys of Silver Spring.

In addition to local designation, historic preservation staff would assist owners with listing properties in the National Register of Historic Places. The register is the official list of the nation's historic places worthy of preservation and part of the program to identify, evaluate, and protect the county's historic and archaeological resources. Properties listed in the National Register are eligible for federal and state preservation tax credits that can be paired with the county's local preservation tax credit.

Recommendations:

- Protect and preserve resources listed in the *Master Plan for Historic Preservation*.
- Educate property owners of historic properties about the benefits of the historic preservation tax credit program.
- Promote the adaptive reuse of historic properties while retaining their character defining features.
- Assist property owners with National Register of Historic Places nominations to promote the use of state and federal tax credits.

Silver Spring Shopping Center and Theatre

The Silver Spring Downtown and Adjacent Communities Plan encourages continued adaptive reuse of the parking lot at the corner of Georgia Avenue and Colesville Road. Built in 1938, the Silver Spring Shopping Center and Theatre is historically significant as a planned regional shopping center with integrated parking. It combined a consciousness of design and merchandising techniques and recognized the importance of the automobile to the shopper. Therefore, the parking lot is part of the story of the circulation of people and goods.

In addition to its designation as a Master Plan Historic Site, the Maryland Historical Trust (MHT) holds a historic preservation easement on the property. Therefore, any proposed changes must be reviewed by the Historic Preservation Commission (HPC) and the state historic preservation office. The HPC and the MHT approved sensitively-designed modifications to this space in 2020. Alterations included decking, outdoor seating, reduction of parking spaces, landscaping changes, and public art.

The transformation and adaptive reuse of transportation infrastructure (such as parking lots) is critical for the evolution of our urban downtown and essential for our future mobility beyond the automobile. Historic preservation will contribute to such planning efforts by actively encouraging the preservation and adaptive reuse of important spaces and buildings, documenting the history of land use, and providing guidance for designated historic resources through the design review process. Further reimagination and activation of this parking lot

would be appropriate and encouraged with respect to the ongoing preservation of the site, consistent with retaining the lot's character-defining shape and features. In particular, proposals could explore the addition of a commercial one-story building that is compatible with the architecture of the shopping center and respects the relationship between the shopping center and streets. Historically, a section of the parking lot once included a gas station which provides justification for the addition of a small structure.

All future alterations would require review and approval by the Historic Preservation Commission and the Maryland Historical Trust.

Silver Spring Shopping Center



Map 30. Historic Resources Listed in the *Master Plan for Historic Preservation*



3.9.3. Resources Listed in the Locational Atlas and Index of Historic Sites

Established in 1976, the *Locational Atlas and Index of Historic Sites* identifies resources that are potentially historically significant. Resources listed on the atlas are protected from demolition or substantial alteration under Chapter 24A of the Montgomery County Code. This designation is meant to be temporary until analysis for listing in the *Master Plan for Historic Preservation* can be completed.

Recommendation

- Review properties listed on the *Locational Atlas and Index of Historic Sites* as part of any redevelopment proposal or as part of comprehensive analysis of resources listed in the atlas.

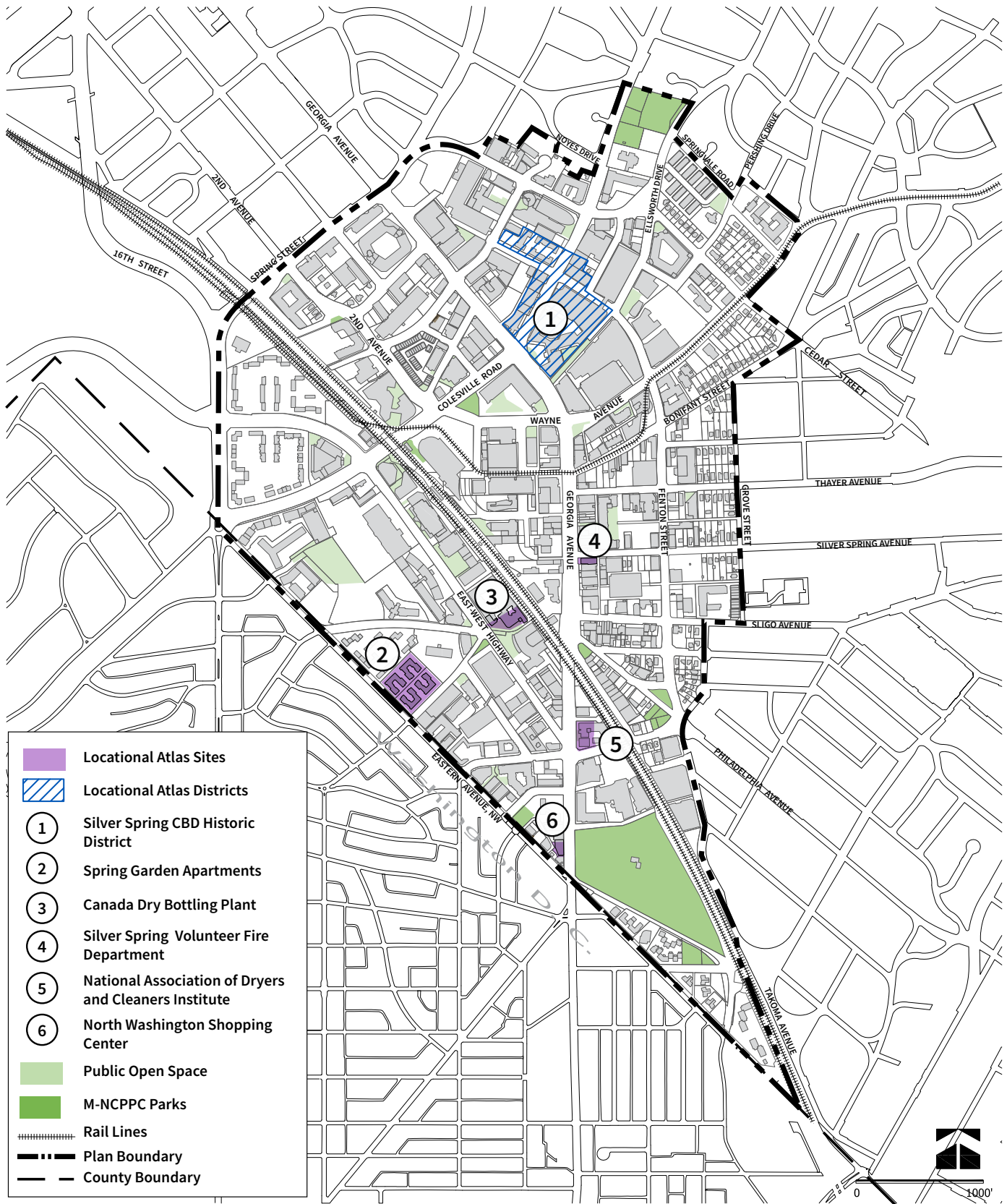
8551 Fenton Street

The building is listed in the *Locational Atlas and Index of Historic Sites* as a resource within the Silver Spring CBD Locational Atlas District. The first anchor tenant of this building (constructed in 1951) was Morton's Department Store which several sources have noted had a non-discriminatory environment at its stores. The Historic Preservation Commission and the Planning Board will evaluate the significance of this building with a redevelopment proposal or as part of the larger analysis of resources listed in the *Locational Atlas and Index of Historic Sites* as outlined in Chapter 24A, Historic Resources Preservation, of the County Code.



8551 Fenton Street

Map 31. Historic Resources Listed in the *Locational Atlas and Index of Historic Sites*



3.9.4. New Site to be Studied as Future Historic Preservation Master Plan Amendment

The Weller's Dry Cleaning Building (M:36-86) is a one-story commercial building at the intersection of Fenton Street and Thayer Avenue. Charles Weller, the original owner, constructed the building in 1961 to house his dry cleaning business. He died in 2016 at the age of 101, after owning and operating Weller's Dry Cleaners for more than 55 years. He hired prominent local architect Ted Englehardt to design the building. Englehardt founded his own firm (Englehardt & Associates) in 1949. He later served as the first president of the Potomac Valley Chapter of the American Institute of Architects (AIA).

The Weller's Dry Cleaning Building represents a rare example of a mid-twentieth century, Googie-styled commercial building in downtown Silver Spring. Googie architecture is considered a stylistic subset of the Modern Movement and often features dramatic acute

angles, boomerang and amoeba shapes, upswept cantilevered roofs and architectural elements, exposed steel, and prominent use of glazing. The large amount of glass allowed pedestrians and passing vehicles to catch a glimpse of the commercial activities within. Most Googie-styled stores were accompanied by parking lots to facilitate ease of customer and staff access. Bold commercial signage, use of bright colors, and neon attracted the attention of passing vehicles. The style continued to rise in popularity throughout the 1950s and 1960s, spreading from California across the American landscape. Owners of roadside buildings including diners, drive-ins, motels, bowling alleys, arcades, and dry cleaners embraced the style.

Recommendation:

- Study the Weller's Dry Cleaning Building for the potential future listing in the *Master Plan for Historic Preservation* and encourage the adaptive reuse of the building if the occupant and use change.



Weller's Dry Cleaning at 8237 Fenton Street

3.9.5. New Sites or Districts to be Studied for Listing on the National Register of Historic Places

The National Register of Historic Places is the official federal list of districts, sites, buildings, structures, and objects significant in American history, architecture, archeology, engineering, and culture. National Register properties have significance to the history of their community, state, or the nation. Historic Preservation Program staff will coordinate with property owners and study the designation of these sites. The honorific listing in the National Register places no restrictions on what a non-governmental owner may do with their property, up to and including demolition, unless the project receives federal assistance. The following properties are potentially eligible for the National Register of Historic Places:

- Medical Office Building (1111 Spring Street)
- Metropolitan Building (8720 Georgia Avenue)
- Montgomery Center (8630 Fenton Street)
- Operations Research, Inc., Building (1400 Spring Street)
- U.S. Industries Building (949 Bonifant Street)
- American National Bank Building (8701 Georgia Avenue)
- Garden and Mid-Rise Apartment District (see below)

Recommendations:

- Conduct outreach with property owners who own resources potentially eligible for the National Register of Historic Places.
- Educate property owners and the public on the benefits and limitations of the National Register.
- Provide technical assistance to property owners interested in pursuing designation.
- Explore future evaluation of these properties for listing in the *Master Plan for Historic Preservation*.

American National Bank Building

The American National Bank Building (M:36-107) is significant due to its architecture and association with the civil rights movement. The building, designed by local architect Edwin Weihe, is an outstanding mid-century example of the International Style. During his career, Weihe was recognized as an innovator in high-rise office and apartment building design.

Between 1962 and 1965, employees of the U.S. Department of Labor who worked in the building mounted a challenge to Montgomery County's newly enacted public accommodations ordinance that is today recognized as a watershed moment in the history of the local civil rights movement. Refused service at a Silver Spring restaurant, Roscoe Nix and his colleagues filed a formal protest with the county's Human Relations Commission. This initial action led to a series of lawsuits and protests that furthered the civil rights movement in Montgomery County.



The American National Bank Building at 8701 Georgia Avenue

Garden and Mid-Rise Apartment Historic District

The proposed Silver Spring Garden and Mid-Rise Apartment Historic District consists of seven apartment complexes constructed between 1936 and 1943 in response to the mid-to-late 20th century suburban growth of Silver Spring. The district reflects the importance and proliferation of the ideals of the Garden City movement and the nationwide response for the creation of humane housing conditions for the working and middle class. Planners, architects, and landscape architects recognized the importance of permanent building materials and fostering the relationship between interior and exterior spaces. They included landscaped courtyards with paths and trees, open spaces, and other amenities for residents. Many of these complexes continue to provide naturally occurring affordable housing in a bucolic setting of mature trees and green spaces. Some of these sites or adjacent to the proposed Green Loop and other park amenities, providing public benefits from shading and cooling the streetscape and absorption of stormwater runoff in private areas that are nevertheless highly visible from public rights-of-way.

The complexes consist of two- to six-story apartment buildings that respond to the burgeoning population growth driven by the New Deal and expansion of the federal government. The Colonial Revival design implemented at most of the apartments including The Falkland (the third FHA-insured garden apartment complex in the nation) reflects the dominance of the style. Architects' use of the Art Deco and Art Moderne styles at Montgomery Arms and Spring Gardens, however, represented a departure from this trend and correlated to the design of the commercial core.

Research efforts and future interpretation must consider and analyze the effects and legacy of racial segregation at these apartment complexes. The history of racism in the community manifested itself in both commercial and residential development practices. Property owners and managers implemented segregation at all subject apartment complexes, thereby excluding African Americans from opportunities for upward mobility. Historical records indicate that integration occurred at most properties in Silver Spring only after the passage of the Fair Housing Act (Title VIII of the Civil Rights Act) of 1968.

The apartments are not clustered in a single geographic area but are spread throughout Silver Spring's downtown. The proposed district includes:

- Falkland Gardens (1936-1938): Bound by East West Highway to the north, Draper Lane to the east, Colesville Road to the south, and single-family dwellings and townhouses to the west
- Blair Park Apartments (1937): 7719-7725 Eastern Avenue
- Eastern Avenue Apartments (ca.1937): 7603-7615 Eastern Avenue
- Blair Park Gardens (1938): 7701-7705 Eastern Avenue; 805-809 Juniper Street; 7700-7705 Blair Road
- Montgomery Arms (1941): 8700-8722 Colesville Road; 8615-8627 Fenton Street
- Spring Garden Apartments (1941): 8001-8031 Eastern Avenue
- Rock Creek Springs (1943): 8000-812 Eastern Drive; 8001-8009 Eastern Drive; 8033-8039 Eastern Avenue; 8000-8004 Blair Mill Road; 1300-1302 Blair Mill Road

Falkland Gardens and Montgomery Arms are listed individually in the Master Plan for Historic Preservation and Spring Garden Apartments is listed in the Locational Atlas and Index of Historic Sites.

Recommendation

- Study a Silver Spring Garden and Mid-Rise Apartment National Historic District and list the district on the National Register of Historic Places, which would open state and federal historic preservation tax credit opportunities to property owners.

*Opposite page -
Garden Apartments from top to
bottom: Falklands landscape,
Montgomery Arms, Blair Park
Gardens, Rock Creek Springs.*



3.9.6. Archaeological Resources

Archaeologists have conducted few investigations in downtown Silver Spring. Some artifacts related to the indigenous people of Maryland have been found in Silver Spring, but they are not associated with any known archaeological sites. At Jesup Blair Park, fieldwork conducted in a section of the property yielded a few 19th- and 20th-century artifacts (bottle glass, bricks, mortar, transfer-printed whiteware, etc.) but none were associated with the occupation of the Jesup Blair House or the Civil War. Future archaeological investigations at the park may provide additional information or unrecorded sites relating to the history of the enslaved people associated with the Blair family's tenure. The potential for archaeological sites throughout the Plan area, however, remains limited due to extensive land disturbance in the 20th century.

Recommendation:

- Advocate for archaeological investigations in areas with potential for buried resources to be present as projects are undertaken. This is often in areas with limited land development such as Jesup Blair Park.⁹

⁹ The potential to have intact archeological sites present in any given location is based on past land use and decreases in areas with more extensive ground disturbance (land disturbed through use, construction, grading, etc.). The level of ground disturbance typical in urban areas means the potential for intact resources needs to be evaluated at an individual property level. At Jesup Blair Park, Park and Planning staff archaeologists will coordinate and determine the level of archaeological investigations to be included in a Historic Area Work Permit (HAWP) application to the Historic Preservation Commission. The proposed level of investigation will be based on historical research, environmental context review, field inspections, findings of past investigations, and the location and nature of proposed improvements.

3.9.7. Cultural and Heritage Resources

The preservation and recognition of cultural heritage and history can be achieved through other means than physical preservation of buildings and sites. To nurture the Plan area's diverse histories and support local businesses, several new and expanded cultural preservation initiatives should be explored.

Recommendations:

- Establish a legacy business registry to recognize the economic, cultural, and social contributions of long-standing businesses to the fabric of Silver Spring.
- Partner with existing and new local partners to add interpretation of histories absent from our present narratives. Interpretation may include physical (historic markers and panels, public art, etc.) and digital or other ephemeral media (websites, story maps, virtual walking tours, etc.).
- Support the Streets and Parks Facilities Renaming Review Project.

Establishment and completion of the registry will allow future analysis and recommendations for potential interventions to support these places. This second phase of the project will explore established legacy business programs in other jurisdictions, the types of incentives (financial, protections, promotional, and technical assistance) provided to business and property owners, and the recommendations outlined in the 2021 *Retail in Diverse Communities Study*. The study recommended providing legacy and minority businesses easier access to capital by means of loan pools that support re-tenanting space through subsidized tenant improvement loans. Other potential tools could include tax credits for building improvements, including bonus credits for improvements undertaken within designated historic structures or buildings identified for historic preservation study. Historic Preservation and Research planning staff will coordinate with Silver Spring businesses owners to discuss areas of greatest need before recommendations are presented to the Planning Board and Council.

Legacy Business Registry

Small, local businesses make critical economic, cultural, and social contributions to downtown Silver Spring. These businesses support community identity, foster stability through their long-term occupancy, increase the tax base, and create job opportunities.

The Plan supports the creation of the Silver Spring Legacy Businesses Registry. The recommendation is supported by the 2021 *Retail in Diverse Communities Study* that listed "Legacy Business Designation" to incentivize the preservation of local businesses.

The proposed registry would identify businesses that:

- Have been open in the community for more than 15 years;
- Contribute to the streetscape, identity, and historical development of Silver Spring; and
- Are independently owned. Historic Preservation staff would refine eligibility criteria as needed (based on the recommendations established in the *Retail in Diverse Communities Study*), research and document qualifying businesses, and present the findings to the Planning Board.

LEGACY BUSINESS REGISTRY

The Legacy Business Registry will be an opportunity to support and promote businesses that have contributed to the economic, cultural and social development of Silver Spring for decades. Numerous businesses such as these have been drawing patrons to the downtown for over 20 years:



Alliance Comics

Vivanti Hair Studio

Pla-Za Artist Materials



Source: Legacy Business Program,
City of San Antonio

Precedent: San Antonio

San Antonio's Legacy Business Program (established in 2018) aims to champion the continued success of legacy businesses through promotional and educational support by means of an award ceremony, robust online mapping tool, and extensive promotion on social media of inducted businesses. The program has recognized and documented 96 businesses within the city, including Del Rio Tortilla, a neighborhood restaurant that celebrates Mexican heritage with traditional culinary dishes. U.S. war veteran Paul Perez opened the business in 1974.

The cultural, social, and developmental histories of Silver Spring provide the opportunity to explore and celebrate the accomplishments of its populace and recognize how racial discrimination and other discriminatory behavior shaped the community. Interpretation is an important means to share, celebrate, and reflect upon collective history. Downtown Silver Spring has historic markers designed, created, and installed by multiple parties including the Silver Spring Historical Society, Montgomery Parks, and private developers. As historical interpretation develops, markers should be reevaluated on a periodic basis, not to exceed 15 years, to determine if the content remains relevant and accurate, or if new markers could be installed to develop a narrative that is meaningful to residents and visitors.

The Historic Preservation Program will collaborate with all local partners and engage the broader community on future interpretation. The first goal will be to address themes and topics noted in the historic context such as, but not limited to:

- How de jure and de facto segregation shaped the development of Silver Spring;
- The civil rights movement and the actions of the Action Coordinating Committee to End Segregation in the Suburbs, NAACP, and other activist organizations;
- The life and accomplishments of Roscoe Nix;
- The first women's fire brigade at the Silver Spring Volunteer Fire Department;
- Silver Spring in World War II, American Instruments Company Building, and architect Katherine Cutler Ficken; and,
- The establishment of diverse communities in Silver Spring with a focus on the development of the Ethiopian community.

Interpretation of historical events, people, and trends should be explored in a multitude of mediums beyond historic markers. The Historic Preservation Program will explore partnerships to determine the feasibility of creating public art (temporary and permanent exhibitions) that creates connections between people and historic sites. These exhibits have the potential to explore our collective past and create distinct places within the public landscape. Social practice art—collaborative, community-drive works that engage with people on social issues—could provide the opportunity to explore historical injustices and confront current challenges.

M-NCPPC continues to review all streets and Montgomery County-owned and maintained park facilities to identify those named after Confederates or those who otherwise do not reflect Montgomery County's values. This task is a joint effort of M-NCPPC's Montgomery County Planning Department and Montgomery County Parks Department.

As part of that effort in 2020 and 2021, M-NCPPC identified three county streets and one park that had full name matches with Confederate soldiers. In June 2021, Montgomery Planning renamed J.E.B. Stuart Road and J.E.B. Stuart Court to Geneva Mason Road and Geneva Mason Court, respectively. The street previously known as Jubal Early Court was renamed to William Dove Court. New street signs were installed in July 2021.

Opposite Page:
Figure 63. Interpretation of Historic Sites

INTERPRETATION OF HISTORIC SITES



Roscoe Nix (MCPS)

Roscoe Nix (1921-2012)

World War II veteran Roscoe Nix's sit-in at a Silver Spring restaurant launched his long and lauded civil rights activism and career of public service in Montgomery County. The United States Department of Labor employed Nix at its office at the American National Bank Building (8701 Georgia Avenue). After the passage of Montgomery County's Ordinance No. 4-120, Re: Elimination of Discrimination in Places of Public Accommodation, Nix organized a sit-in at Crivella's Wayside Restaurant where the owner denied Black customers service in April 1962. Nix's actions led to widespread publicity, sit-ins, and legal actions against the business. Montgomery County residents elected Nix to serve on the Montgomery County School Board in the 1974. He later served as the President of the Montgomery County Chapter of the NAACP. Nix's contributions should be recognized and interpreted in downtown Silver Spring.



American Instrument Company Building

The American Instrument Company (AMINCO) which produced scientific, engineering, and laboratory instruments and apparatus moved their plant and operations from Washington, D.C., to 8030-8050 Georgia Avenue in 1936. AMINCO hired Katherine Cutler Ficken, the first licensed women architect in Maryland, to design the plant and later additions. During World War II, the company supported the war effort and produced a hand-operated fuse-setter. The company manufactured, assembled, and tested this precision apparatus by the thousands in Silver Spring. War-time employment at the plant increased from 80 to 345 persons and the number of women employed increased from 15 (18 percent) to 125 (36 percent). The AMINCO building represents the work of a significant architect at the local and state level, and mid-20th century manufacturing history in downtown Silver Spring.

*Description of the American Instrument Company, Inc. in 1952.
Source: Washington Post, December 3, 1952.*

Silver Spring Volunteer Fire Department's Women Fire Brigade

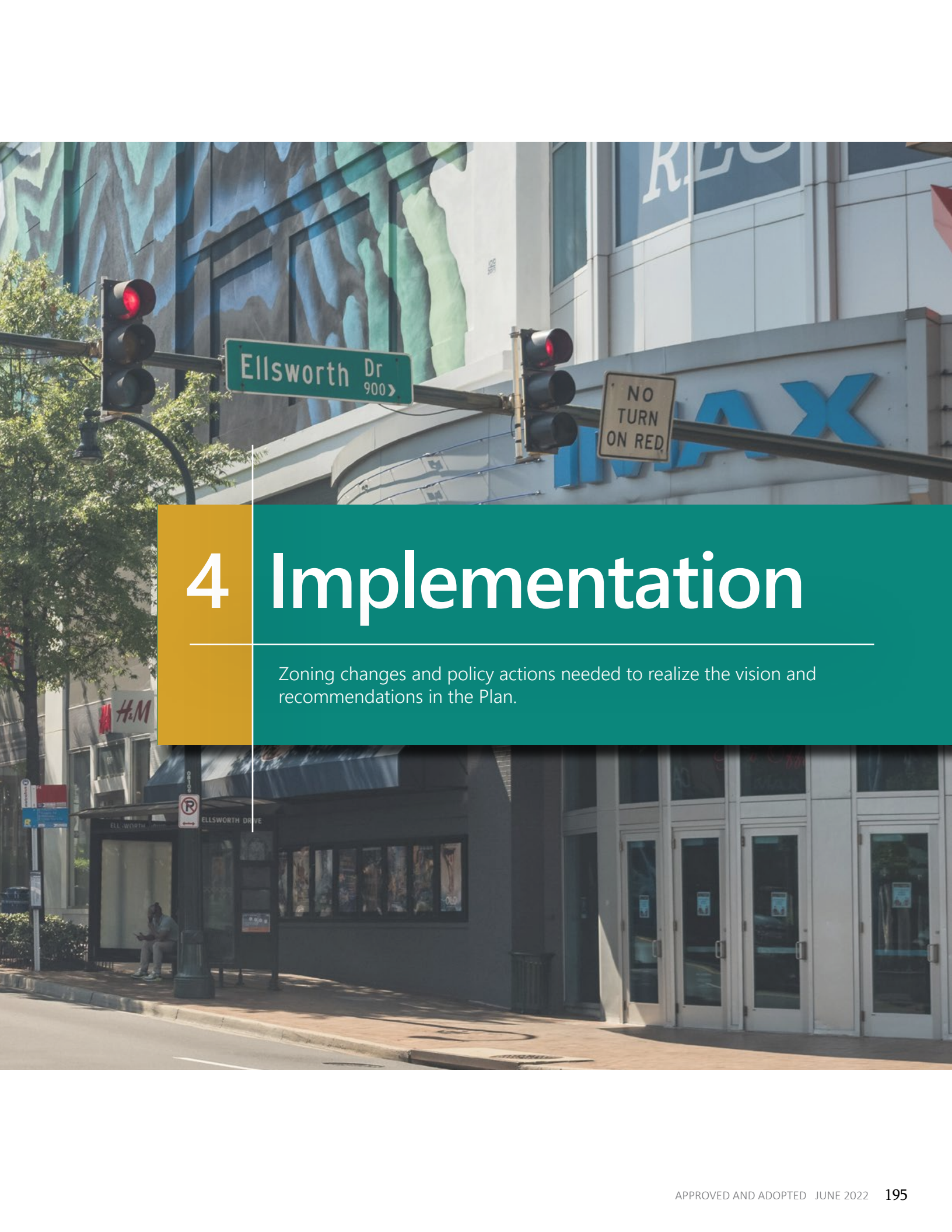


*View of the Volunteer Fire Department.
(Silver Spring Volunteer Fire Department)*

Women have a long and significant role in the development, organization, and implementation of the Silver Spring Volunteer Fire Department (SSVFD). In 1915, the Ladies' Improvement Association helped coordinate and organize the department and fire house. Local newspapers noted that "public-spirited women" raised funds to buy equipment. The SSVFD elected the Ladies' Cooperative Improvement Society as honorary members, but the women of the community looked for greater inclusion. In 1917, the SSVFD amended its constitution by striking out the word "male." As reported in the Evening Star, "It was brought out in a spirited discussion of the amendment that women have taken prominent parts in fighting fires in Silver Spring, not only in directing and aiding the efforts to extinguish the flames, but in hurrying along the chemical extinguishing apparatus which is used by the company." This aspect of the department's history is an integral part of their story and should be interpreted at 8131 Georgia Avenue. This building is currently listed in the Locational Atlas and Index of Historic Sites.



Fenton Street



4 Implementation

Zoning changes and policy actions needed to realize the vision and recommendations in the Plan.

4.1. ZONING

4.1.1. Downtown Silver Spring Overlay Zone

An overlay zone is a mapped district placed over the underlying Euclidean zone that modifies the uses or development standards of the zone. An overlay zone imposes requirements or restrictions in addition to, or in place of, those of the underlying zoning. This Plan recommends the adoption of a Downtown Silver Spring Overlay Zone for all Plan districts except the Adjacent Communities District. The overlay zone would implement the recommendations of this Plan related to density, affordable housing, public benefits, and design excellence.

4.1.2. Civic Improvement Fund (CIF)

- Any CR property in the Plan area may obtain additional density necessary to reach the mapped maximum building height by making a contribution to the Civic Improvements Fund (CIF). The contribution methodology will be determined by the zoning ordinance.
- CIF contributions will be made only for density above the total mapped maximum density for the site, density approved per density-averaging, and/or bonus density providing MPDUs above the required minimum, up to the approved maximum height.
- The CIF will be implemented by the county, and contributions will be used toward implementation of specific projects within downtown Silver Spring intended to upgrade the public realm so as to continue to attract businesses, residents, and visitors, including:
 - Enhanced Transit Center arrival experience;
 - Bridge connection over Metrorail/CSX tracks;
 - Public bicycle parking facilities;
 - Green Loop improvements beyond the frontage of a redeveloped site;
 - Or other projects identified by the Planning Board.

4.1.3. Fenton Village Overlay Zone

The plan recommends updating the Fenton Village Overlay Zone to align with the goals of the plan, while the overall goals of the zone remain the same.

- Revise the boundary to remove some parcels at the north edge of the zone and add parcels zoned CR at the eastern edge adjacent to East Silver Spring.
- Allow properties required to provide Public Open Space (POS) on-site to instead contribute financially to new open spaces recommended within the district. Refer to Section 4.1.12.
- Consistent with the 2021 *Retail in Diverse Communities Study*:
 - Limit the frontage of any storefront bay to 60 feet in façade width for large and combination retailers (with the exception of grocery stores). Allow for locating larger anchors on the basement level with a street-level entrance space.
 - Developments with ground-floor retail (except grocery stores) should provide at least one 1,200 square foot or smaller retail bay and another retail bay of 2,000 square feet or less.

4.1.4. Ripley/South Silver Spring Overlay Zone

The Plan recommends the removal of the Ripley/South Silver Spring Overlay Zone from the zoning ordinance. The Design Guidelines will address compatibility with single-family homes across Eastern Avenue.

4.1.5. CR, CRT, CRN Zoning

CR, CRT, and CRN zones are designed to encourage mixed-use development. Parcels zoned CR are given an overall FAR, and then that is broken down into maximum commercial density allowed (C) and maximum residential development allowed (R) followed by the maximum allowable height. Typically, the C and R limits are designated in such a way that the maximum allowable density cannot be achieved without a mix of commercial and residential uses. To better respond to fluctuating market conditions, on most parcels, the C and R values have been equalized to provide as much flexibility as possible for future development.

Maximum heights on properties throughout the plan have been adjusted per urban design goals of each district, with at least a 20 percent increase on most properties to eliminate the existing “T” designation. The blocks surrounding the Transit Center are recommended for heights up to 360 feet, with Planning Board approval.

The Design Guidelines that will accompany this Plan will indicate where heights should step down at specific downtown edges.

4.1.6. MPDU Minimum Percentage

All Optional Method Development projects that include residential units are subject to Chapter 25A, Moderately Priced Dwelling Unit (MPDU), and are required to provide a minimum of 15 percent of the total residential units as MPDUs. This is also considered a required Optional Method Public Benefit as described in Section 4.1.7. below.

4.1.7. Public Benefits in Commercial/ Residential (CR) Zones

- CR, CRT and CRN zones in the Plan area allow for higher density under the county’s optional method of development but require public amenities to support the additional density. Ensuring the right mix of public benefits is key to realizing the Plan’s goals of a stronger, more connected public realm that will help bring people and businesses to Silver Spring.
- The plan recommends certain public benefits as priorities for all CR parcels within the plan boundary. The plan also recommends disallowing certain public benefits to be used in optional method development for projects within the boundary of the Plan.
- The Plan requires that all Optional Method Development include an Exceptional Design Public Benefit and all projects with a residential component achieve Affordable Housing Public Benefit via the requirement for 15 percent MPDUS described in Section 4.1.6. above.
- The Plan proposes that “Transit Proximity” (under Connectivity and Mobility) be excluded from the list of potential public benefits for projects within the Plan area. All sites in the Plan area are proximate to transit by default.
- The plan prioritizes the following public benefits:
 - Major Public Facilities
 - Affordable Housing
 - Small Business Opportunity
 - Streetscape Improvement
 - Dwelling Unit Mix
 - Public Open Space
 - Tree Canopy
 - Habitat Restoration
 - Historic Preservation

4.1.8. Design Advisory Panel

The Sector Plan recommends establishment of a Design Advisory Panel (DAP), similar in structure and function to the Bethesda DAP, for all Optional Method projects in the Plan area to assess the number of Design Excellence Public Benefit Points. The DAP should be comprised of diverse individuals who represent the interests of the community.

4.1.9. Accessory Uses in the CR Zone

The Sector Plan recommends updating the Zoning Code Use Table to allow as accessory uses in the CR zone commercial food kitchens, food processing, rooftop farms and other uses not included in "Artisanal Manufacturing and Production."

4.1.10. Green Loop

The Green Loop will be primarily implemented through frontage improvements required for each development, through the Streetscape Improvement Public Benefit, or through CIP improvements. The Plan recommends that every effort be made to limit disruption to businesses in the design and implementation of any Green Loop elements.

4.1.11. Unified Mobility Program

The 2021-2024 Growth and Infrastructure Policy (GIP) recommends implementation of Unified Mobility Programs (UMPs) for all Red Policy areas in the county. UMPs are implemented to ensure there is adequate public transportation facilities within a sector plan area and they are established by a County Council Resolution after a public hearing. The Council may amend the UMP and the fee at any time, after a public hearing. In areas subject to an UMP, regulatory applicants are exempt from the Local Area Transportation Review, and instead fees are collected (typically scaled by the number of net new trips generated by the master-planned additional density) to fund essential transportation improvements such as new/improved sidewalks and other ADA accessibility infrastructure, separated bikeways, local transit stop improvements, etc.

Recognizing the unique relationship between Montgomery Hills and downtown Silver Spring, this Sector Plan confirms the recommendation included in the 2020 *Forest Glen/Montgomery Hills Sector Plan* to include a combined UMP for both areas. The CIF described above will fund civic improvements

specifically differentiated from the essential mobility projects to be funded by the UMP.

4.1.12. On-Site Public Open Space

Adequate public open space is critical to meeting this Sector Plan's goal of making Silver Spring a better place to live, work and play. While downtown Silver Spring has a reasonable amount of open space, many of these spaces are small "postage stamp" spaces that do not truly enhance the public realm. In order to ensure the right amount and quality of public open space, this Sector Plan recommends channeling resources to create new and improve existing public parks instead of creating on-site public open spaces that is too small, fail to enhance the public realm and prevent buildings from activating the street. For any Optional Method development project required to provide public open space on a site not recommended for a new public space in the Sector Plan, the Plan recommends that in lieu of on-site open space, applicants contribute to the creation of new and improvement of existing public parks recommended by the Sector Plan, preferably within the same district. These contributions will be determined during the Optional Method development review and approval process and will be based on the cost/sf of constructing an equivalent area of the recommended public space. Contributions to the Commission will be included in the Department of Parks Capital Improvement Program (CIP).



Public Art at Inventa Towers

4.2 CAPITAL IMPROVEMENTS PROGRAM

Table 12. Capital Improvements Program

Project Name	Category	Lead Agency	Coordinating Agency
Renovate/Repurpose Jesup Blair Park	Parks and Open Space	M-NCPPC*	Montgomery College, Private
Create Ripley District Park	Parks and Open Space	M-NCPPC	Private
Create Blair Park	Parks and Open Space	M-NCPPC	Private
Create Sonny's Park (Blairs Development)	Parks and Open Space	M-NCPPC	Private
Create South Silver Spring Park	Parks and Open Space	M-NCPPC*	
Create Metro Center Park	Parks and Open Space	M-NCPPC	Private
Renovate Acorn Park	Parks and Open Space	M-NCPPC*	
Renovate Ellsworth Urban Park	Parks and Open Space	M-NCPPC*	
Create Fitness Park (Blairs Development)	Parks and Open Space	M-NCPPC	Private
Develop Philadelphia Avenue Urban Park	Parks and Open Space	M-NCPPC*	
Expand/Renovate Fenton Street Park	Parks and Open Space	M-NCPPC*	
Create Fenton Village Neighborhood Green	Parks and Open Space	M-NCPPC	Private
Create Downtown North Park	Parks and Open Space	M-NCPPC	Private
Create Ellsworth District Park (WF Site)	Parks and Open Space	M-NCPPC	Private
Create Bonifant-Dixon Park	Parks and Open Space	M-NCPPC	Private
Create Rachel Carson, Blair Stomping, The Mews, and Lucy's Landing Parks	Parks and Open Space	M-NCPPC	Private
New Pedestrian Bridge Connection (South Silver Spring to Ripley District)	Transportation, Parks and Open Space	M-NCPPC, MCDOT, CSX	
Green Loop Streets	Transportation, Parks and Open Space	MCDOT, SHA M-NCPPC	Private
1st Avenue Extension	Transportation	MCDOT	Private
1st Avenue Alley	Transportation	MCDOT	Private
Thayer Avenue to Bonifant Street Extension	Transportation	MCDOT	Private
Silver Spring Avenue to Sligo Avenue Extension	Transportation	MCDOT	Private
Commercial Shared Street: Bonifant Street (Ramsey Street to Georgia Avenue)	Transportation	MCDOT	Private
Commercial Shared Street: Ellsworth Drive (Fenton Street to Veterans Plaza)	Transportation	MCDOT	Private
Commercial Shared Street: Newell Street (Kennett Street to East-West Highway)	Transportation	MCDOT	Private
Pedestrian/Bicycle Connection: Fenton Street to Mayor Lane (along with the redevelopment of Parking Lot 4 site in Fenton Village)	Transportation	MCDOT	Private
Pedestrian/Bicycle Connection: Bonifant Street to Wayne Avenue	Transportation	MCDOT	Private
East-West Highway road diet	Transportation	M-NCPPC, MDOT SHA	
Georgia Avenue road diet for dedicated bus lanes	Transportation	M-NCPPC, MDOT SHA	

Project Name	Category	Lead Agency	Coordinating Agency
Colesville Road road diet for dedicated bus lanes	Transportation	M-NCPPC, MDOT SHA	
Colesville Road: removal of dynamic lane operation	Transportation	M-NCPPC, MDOT SHA	
Upgrade all crosswalks to continental pavement marking	Transportation	M-NCPPC, MDOT SHA	Private
Protected Crossings	Transportation	M-NCPPC, MCDOT, MDOT SHA	
Install new sidewalks (close existing sidewalk gaps)	Transportation	M-NCPPC, MCDOT	Private
Traffic Calming Study on Blair Road	Transportation	M-NCPPC, MCDOT	DDOT
Upgrade bus stops with shelters	Transportation	M-NCPPC, MCDOT	Private
Long-term bicycle parking at transit stations (Sarbanes Transit Center, Silver Spring Library Purple Line Station)	Transportation	M-NCPPC, WMATA, MTA	
Short-term bicycle parking in Silver Spring CBD	Transportation	M-NCPPC	Private
13th Street (Eastern Avenue to Georgia Avenue) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
16th Street (Spring Street to District of Columbia) separated bike lanes	Transportation	M-NCPPC, MDOT SHA	Private
2nd/Wayne Avenue (Spring Street to Colesville Road) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
2nd/Wayne Avenue (Colesville Road to Cedar Street) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Capital Crescent Trail	Transportation	M-NCPPC, MCDOT	Private
Fenton Street Extended bicycle and pedestrian sidepath	Transportation	M-NCPPC, MCDOT	Private
Fenton Street (Ellsworth Drive to King Street) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Fenton Street (Cameron Street to Ellsworth Drive) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Burlington Avenue (Georgia Avenue to Fenton Street) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Cameron Street (Spring Street to 2nd Avenue) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Dixon Avenue (Wayne Avenue to Georgia Avenue) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Newell Street (District of Columbia to East-West Highway) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Silver Spring Avenue (Georgia Avenue to Fenton Street) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Spring Street/Cedar Street (16th Street to Wayne Avenue) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Blair Mill Road (Eastern Avenue and East-West Highway) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private

4.3. PARKS

4.3.1. Park Implementation: EPS Plan

Silver Spring was identified as an implementation priority area through the mapping of Experience Improvement Areas (EIAs) of the *Energized Public Spaces Functional Master Plan* (EPS Plan) with the overlay of the Equity Focus Area (EFAs). Recommendations described in this Plan will help prioritize where and how to spend limited public funds where the highest need is identified. See Section 3.5.3 Equity in Parks and Public Spaces for more information on this topic.

The first step in creating actionable results for implementation of the proposed network of parks and public spaces is to organize, evaluate, and prioritize the identified opportunities. The EPS Plan established five implementation strategies (Activate, Connect, Renovate and Repurpose, Develop, and Create) that can be used to increase the service provide by parks and public spaces.

- **Activate:** serve as an interim solution for park spaces that are planned for new construction or redevelopment in the future. In addition, activation can also help us identify barriers to public accessibility to programs and facilities.
- **Connect:** By improving walkable connections to the entire public space network, service levels can be increased at lower cost and in shorter time frames than necessary to renovate existing or build new parks.
- **Redevelop:** If countywide trends and detailed usage data for a specific park indicate that a facility is underutilized, then the facility platform may be considered for additional or alternate uses.
- **Develop:** Some opportunities for increasing park service rely upon developing new facilities on existing parkland.
- **Create:** If the previous four tools are not adequate to expand the supply of parks and public spaces to meet a community's needs, then creation of new open spaces must be considered to provide a platform for outdoor experiences.

4.3.2. Parkland Ownership

As the major parkland stakeholder in the county, Montgomery Parks is committed to increasing the number of parks and open spaces to secure public access, especially in urban areas such as Silver Spring. However, the higher cost in acquiring land, in addition to the maintenance and operation of these urban spaces with active programming, comes with a high price tag. Montgomery Parks will be strategic in determining parks and open spaces ownership. Below are some parameters that will be considered but not limited to define parkland ownership:

- If acquisition is the only way to create a cohesive public space when the parcel is less likely be part of a development proposal,
- If the recommended facilities most likely require user permits, such as athletic courts, or
- If the surrounding parcels are individually owned, therefore, it is unreasonable to expect common management of the space, unless another entity is available to manage the space.

4.4 HISTORIC PRESERVATION SUMMARY

Table 13. Historic Preservation Implementation Summary

Description	Time Frame	Category
Study and evaluate the Weller's Dry Cleaning Building (8237 Fenton Street) for future listing in the Master Plan for Historic Preservation or recordation of a Historic Preservation Easement.	Long Term	Historic Preservation
Phase One: Create a Silver Spring Legacy Business Registry that documents all retail and service-orientated businesses in the Plan area. Phase Two: Study potential incentives to preserve local, independently owned businesses.	Short Term	Historic Preservation/ DHCA/Research
Collaborate with local partners and engage the broader community to discuss potential interpretation opportunities in Silver Spring.	Short Term	Historic Preservation
<p>Study the following properties for listing in the National Register of Historic Places:</p> <p>Medical Office Building (1111 Spring Street)</p> <p>Metropolitan Building (8720 Georgia Avenue)</p> <p>Montgomery Center (8630 Fenton Street)</p> <p>Operations Research, Inc., (1400 Spring Street)</p> <p>U.S. Industries Building (949 Bonifant Avenue)</p> <p>American National Bank Building (8701 Georgia Avenue)</p> <p>Garden and Mid-Rise Garden Apartment District</p>	Long Term	Historic Preservation
Support the recommendations of the Montgomery County Street and Parks Facilities Renaming Review Project on the renaming of county streets and park facilities.	Long Term	Historic Preservation/ IRC Division

4.5. PARTNERSHIPS

The Plan supports continued cooperation between the public and private sectors to coordinate and implement initiatives and services in downtown Silver Spring.

Established by county legislation, the Silver Spring Urban District provides enhanced services within downtown Silver Spring to ensure that the downtown area is maintained, clean, safe and attractive to support the economic viability of the area. Services include security, streetscape and tree maintenance, marketing and hosting special events to bring people to downtown Silver Spring. The Silver Spring Urban District operates under the auspices of the Regional Service Center.

This Sector Plan supports the priorities of the Silver Spring Urban District, the Regional Service Center, and the Arts and Entertainment District in their common mission to provide public services and amenities to the members of the downtown Silver Spring communities.

This Plan recommends pursuing partnerships that:

- Increase the strength and resources of civic organizations championing downtown Silver Spring;
- Strengthen the resilience of Silver Spring by increasing capacity, improving the public realm, supporting independent businesses, and encouraging continued economic growth;
- Use arts and culture to celebrate the past, present, and future diverse cultures that are part of the Silver Spring community.
- Strengthen partnerships between Montgomery County, civic organizations, service providers, property owners, and businesses to address complex social challenges.

The creation of a vibrant downtown requires enhanced economic activity to generate demand for Silver Spring's physical spaces. Programs and initiatives in partnership with other agencies and stakeholders are needed to realize the plan's vision of economic growth and resiliency in Silver Spring.

Ellsworth Street



4.5.1. Partnerships for Economic Growth

- Encourage a partnership between the Montgomery County Economic Development Corporation (MCEDC) and other entities in the downtown to create a market incentive to invest in speculative office suites in office space that has been vacant for an extended period. Create a plan for the incentive that caps the amount at a reasonable cost to the county while also providing a meaningful incentive to building owners.
- Partner with county government to reinvigorate the Silver Spring Innovation Center to meet the needs of and to support women and minority entrepreneurs, in alignment with the recommendations of the Business Incubator Review and Entrepreneurial Ecosystem study prepared by Axcel Innovation LLC and presented to the Planning, Housing and Economic Development (PHED) committee on July 26, 2021.
- Encourage activation of underutilized space: Partner with MCEDC, the County Executive's Business Advancement Team, and other entities to study a vacancy tax on empty retail frontage to encourage property owners to lease and activate vacant spaces. The funds generated by this tax should be returned to the Silver Spring area. The money should be used to advance placemaking in the downtown.
- Encourage a partnership between the county and mission-oriented non-profit stakeholders to fund a diverse retail liaison position to support diverse retailers in Silver Spring. Explore creating a loan pool that could provide resources and incentives to local small business, help subsidize tenant improvements, and could support business owners in purchasing their properties. See the March 2021 *Retail in Diverse Communities Study* for additional details about these recommendations.
- Encourage property owners with underutilized and vacant street-level retail space to donate that space to mission-oriented non-profits to run retail incubators in which entrepreneurs can try new retail concepts.
- The Plan supports the strengthening of entities in the downtown that will assist with marketing, activation and maintenance. These entities could help address issues of safety and trash collection on the streets, as well as promote, highlight and support the many amenities the downtown has to offer. Downtown Silver Spring will benefit from this and will also need support from partnerships between the public sector, property owners, businesses, and social service organizations in order to full achieve the vision expressed in this Plan.

4.5.2. Arts and Entertainment District

The Silver Spring Arts and Entertainment District was designated on December 31, 2001 by the State of Maryland. It includes more than 50 arts and humanities organizations from the regionally important AFI Theater to local dance companies and art galleries. The Arts and Entertainment District offers tax incentives to both arts-related businesses and individual artists located within a district. The diverse arts organizations and public art installations in Silver Spring draw visitors from all over the county and the region.

The Plan supports the following recommendations that could enhance and grow the Arts and Entertainment District in Silver Spring and contribute significantly to the economic growth of the downtown. Many of these recommendations encourage partnership between the Arts and Entertainment District and the other entities described above.

- Create an Arts and Entertainment Plan that identifies sustained resources for an arts and entertainment management entity of the size and capacity commensurate with Silver Spring's potential. The plan should include actionable items to grow the arts and entertainment district's offerings, programming, activities, and marketing.
- Partner with property owners to capture underutilized street-level retail spaces to create an arts incubator managed by a mission-oriented non-profit. Allow developers of property to create the space when constructing new developments as part of the optional method of development point system.
- New public art in Silver Spring, whether through public process or through optional method development, should represent and support the diverse communities of Silver Spring.

4.5.3. Parking Lot District

Montgomery County Department of Transportation administers, operates, and maintains and enforces the Silver Spring Parking Lot District (PLD). The PLD is an independent taxing district with obligations to bond holders who finance PLD facilities. Today, downtown Silver Spring has 10 public parking garages and three surface lots. This Sector Plan supports the redevelopment of county parking facilities along with replacement of parking and supports the priorities of the PLD and the recommendations as outlined in Section 3.6 Transportation.

4.5.4. Partnerships with the Community

As discussed in Section 3.5 Parks, Trails and Open Spaces, this Plan supports the goal of providing sufficient open space for active recreation, social gathering, and contemplative activities. The Plan supports the following recommendation to achieve that goal:

- Increase placemaking efforts; create a fund to support placemaking throughout Silver Spring; and have the new champion for downtown Silver Spring manage the fund. Prioritize improvements that make the public realm and street network more connected and safer for nonmotorized transport, as well as more attractive as places to gather.
- Work with civic stakeholders to form a nonprofit that raises funds for, invests in, manages, and activates Jesup Blair Park. This could be a cross-jurisdictional enterprise coordinated with stakeholders and park users from adjacent Washington, D.C. neighborhoods as well.



Silver Spring Civic Center at Veterans Plaza

COLLAGE IMAGE CREDITS

Figure 2. Key Components of the Green Loop

Clockwise from top left: <https://momentummag.com>, <https://nnala.com/san-mateo-county-sustainable-green-streets-and-parking-lots-design-guidebook/>, <https://www.cnu.org/sites/default/files/philadelphia-mouzon.jpg>, <https://www.sepco-solarlighting.com/>, <https://archinect.com>, www.theurbanist.org.

Figure 18. Jesup Blair Park Experience Zones - image credits:

Contemplative Zone, clockwise from top left: www.tripadvisor.com post by user bsandrs, www.serenbeyoga.com/goat-yoga, Andrew D'Agosto www.theinfatuation.com, www.laughterlawyer.com.au, Justina Wong.

Social Zone: Facebook@grandparklosangeles, <https://www.youobserver.com/article/downtown-sarasotas-walk-in-the-park-offers-unique-live-music-experience>.

Active Zone, clockwise from top left: waiting for credit, www.climbmaxnc.com, waiting for credit, www.licjournal.com, www.jessekassel.com, Paul Patterson.

Figure 57. Nature-Based Solutions – image credits

Top Row:

Green Roofs: <https://www.hubinternational.com/-/media/HUB-International/Blog/Articles/Main-Images-A/2020/Main-Image-Vegetative-Roofs.jpg>

Integrated Stormwater Management: <https://www.pinterest.com/rgarcia0376/stormwater-management/>

Shaded Playgrounds: <https://tinybeans.com/atlanta/best-atlanta-parks-and-playgrounds/>

Middle Row:

Urban Agriculture: https://trimtab.living-future.org/blog/biophilic-design-award/attachment/ilfi_06/

Cool Streets and Bikeways: <https://www.sfmta.com/blog/plans-polk-street-bike-lane-changed-raised-road-level>

Solar Oriented Buildings: <http://gorodvo.ru/go/postpics.info/gallery/infosys-building.html>

Plant Biodiversity: Tina Schneider

Bottom Row:

Increased Economic Success: <https://www.theunderline.org/es/2014/09/03/florida-villager-vision-of-10-mile-linear-park-one-step-closer-to-reality/>

Green Environment: <https://www.pond5.com/stock-footage/37057459/rainy-park-and-woman-shorts-and-gumboots-walk-umbrella.html>

Urban Wildlife: <https://feederwatch.org/community/participant-photo/male-american-goldfinch-10/>

PLAN RESOURCES

Plans and studies referenced in the Silver Spring Downtown and Adjacent Communities Plan, as well as a link to the Montgomery Planning online glossary.

Glossary of Planning Terms

Montgomery Planning Online Glossary

<https://montgomeryplanning.org/about-planning/mission/glossary/>

Plans for Reference

Retail in Diverse Communities Study

<https://montgomeryplanning.org/tools/research/real-estate-and-land-use/retail-in-diverse-communities/>

Bicycle Master Plan

<https://montgomeryplanning.org/planning/transportation/bicycle-planning/bicycle-master-plan/>

Complete Streets Design Guide

<https://montgomeryplanning.org/planning/transportation/complete-streets/>

Countywide Transit Corridors Functional Master Plan

<https://montgomeryplanning.org/planning/transportation/transit-planning/countywide-transit-corridors-functional-master-plan/>

Montgomery Parks Plans for Reference

Energized Public Spaces Functional Master Plan

<https://montgomeryparks.org/projects/directory/energized-public-spaces-functional-master-plan/>

Park, Recreation, and Open Space Plan

<https://montgomeryparks.org/projects/directory/powered-by-parks-2022-park-recreation-and-open-space-plan/>

Previous Plans for Downtown Silver Spring

Silver Spring Central Business District Sector Plan (2000)

<https://montgomeryplanning.org/planning/communities/downcounty/silver-spring-cbd/>

Greenspace Guidelines for the Silver Spring Business District (2010)

https://www.montgomeryplanning.org/community/ssgreenspace/documents/SilverSpringGreenSpaceGuidelines_web.pdf



Cameron Street in the Downtown North District

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*Indicates staff who are no longer at Montgomery Planning.

County Council Resolution

Resolution No.: 19-1297
Introduced: May 26, 2022
Adopted: May 26, 2022

**COUNTY COUNCIL FOR MONTGOMERY COUNTY, MARYLAND
SITTING AS THE DISTRICT COUNCIL FOR THAT PORTION
OF THE MARYLAND-WASHINGTON REGIONAL DISTRICT
WITHIN MONTGOMERY COUNTY, MARYLAND**

Lead Sponsor: County Council

SUBJECT: Approval of the Winter 2022 Silver Spring Downtown and Adjacent Communities Plan

1. On January 6, 2022, the Montgomery County Planning Board transmitted to the County Executive and the County Council the Winter 2022 Planning Board Draft Silver Spring Downtown and Adjacent Communities Plan.
2. The Winter 2022 Planning Board Draft Silver Spring Downtown and Adjacent Communities Plan contains the text and supporting maps for a comprehensive amendment to the Approved and Adopted 2000 Silver Spring Central Business District Sector Plan. In addition, the Plan amends the East Silver Spring Master Plan, and the North and West Silver Spring Master Plan, both approved and adopted in 2000. It also amends The General Plan (On Wedges and Corridors) for the Physical Development of the Maryland-Washington Regional District in Montgomery and Prince George's Counties, as amended; the Master Plan of Highways and Transitways, and the Bicycle Master Plan, as amended.
3. On February 17, 2022, the County Council held a virtual public hearing on the Winter 2022 Planning Board Draft Silver Spring Downtown and Adjacent Communities Plan. The Plan was referred to the Council's Planning, Housing, and Economic Development Committee for review and recommendations.
4. On March 6, 2022, the Office of Management and Budget transmitted to the County Council the County Executive's Fiscal Impact Statement for the Winter 2022 Planning Board Draft Silver Spring Downtown and Adjacent Communities Plan.
5. On March 7, 14, 21, and 28, and on April 4, 7, 20, and 27, 2022, the Planning, Housing, and Economic Development Committee held worksessions to review the issues raised in connection with the Planning Board Draft Silver Spring Downtown and Adjacent Communities Plan.
6. On April 19, April 26, and May 3, 2022, the County Council reviewed the Planning Board Draft Silver Spring Downtown and Adjacent Communities Plan and the recommendations of the Planning, Housing, and Economic Development Committee.

Action

The County Council for Montgomery County, Maryland, sitting as the District Council for that portion of the Maryland-Washington Regional District in Montgomery County, Maryland, approves the following resolution:

The Planning Board Draft Silver Spring Downtown and Adjacent Communities Plan, dated Winter 2022, is approved with revisions. County Council revisions to the Planning Board Draft Silver Spring Downtown and Adjacent Communities Plan are identified below. Deletions to the text of the Plan are indicated by [brackets], additions by underscoring. All page references are to the Winter 2022 Planning Board Draft Silver Spring Downtown and Adjacent Communities Plan. Montgomery County Planning Department staff may make additional, non-substantive edits to correct for errors in grammar or spelling or to improve clarity prior to the Plan's adoption by the Maryland-National Capital Park & Planning Commission. Planning Department staff will also update all figures and graphics, including references and titles, consistent with the Council changes to the text.

Page 9: Revise the second paragraph under "1.1. A VISION FOR SILVER SPRING 2040" as follows:

This Plan continues to support many of the goals emphasized by the previous plan while highlighting the importance of reducing driving in the downtown, mitigating the impacts of climate change, and celebrating the cultural diversity that is a hallmark of Silver Spring. The Plan recognizes that economic growth is promoted by zoning that allows development to respond to the market, and a strong public realm including streets, parks and open spaces that serve all users. The Plan also aims to reinforce downtown Silver Spring's identity as a regional destination for arts and culture. The Arts and Entertainment District draws people of all ages and backgrounds to the downtown for theater, music, films, and art. This Sector Plan builds upon the previous plan by strengthening the existing successful components of Silver Spring, while setting forth a vision [and] for a thriving commercial core, along with recommendations for the emerging areas of the downtown that will help Silver Spring continue to be a regional destination for the next 20 years.

Page 11: Revise the second paragraph under Section 1.2. "Plan Context" as follows:

This Plan expands the boundary from the previous Sector Plan for downtown Silver Spring to include both the area formerly known as the Central Business District and the "Adjacent Communities": blocks from several residential neighborhoods primarily to the north and east of the downtown located within a 10-minute/half-mile walkshed from either the Silver Spring Transit Center or the Purple Line station at the Silver Spring Library. The Plan Area covers approximately [505] 442 acres and is generally bound by Eastern Avenue to the south, 16th Street to the west, [Ballard and] Spring Street to the north, and portions of the Seven Oaks-Evenswood and East Silver Spring neighborhoods to the east.

Page 14: Replace all text after the word "buildings" at the top of the page, as follows:

buildings, [in the downtown. The Silver Spring Metro Station opened in 1978, but its location away from the historic center of downtown limited additional development in the surrounding blocks.

The closing of major and mid-sized white-owned retailers in the 1980s and 1990s created commercial vacancies that began to be filled by the rich, ethnically diverse, local-business community that

continues today. While there are many immigrant communities in the plan area, the Ethiopian community has had particular success in Silver Spring and regards this urban area as a center for Ethiopian life and culture in the region.

Since 2000, revitalization efforts envisioned by the Sector Plan have seen the redevelopment of the downtown Silver Spring retail core, the promotion of an arts and entertainment district and the reopening of the historic Silver Theatre, and the opening of the Discovery corporate headquarters. With this, the former light industrial areas west of Georgia Avenue have been redeveloped with multi-family housing and the Paul S. Sarbanes Transit Center has been completed at the Silver Spring Metro station. Planning and initial construction for the Purple Line light-rail system connecting Bethesda to New Carrollton in Prince George's County has created new opportunities.] Office development of Silver Spring waned in the 1970s. Many pinned their hopes for the revitalization of Silver Spring on the opening of the Red Line of the Metro operated by the Washington Metropolitan Area Transit Authority (WMATA). Early plans for the line first sited the Silver Spring station on Georgia Avenue. Developers and the business community, however, successfully lobbied the County Council and WMATA to approve the station near its present-day location to service a proposed, but never realized mixed-use, high-rise redevelopment on part of the Falkland Chase Apartments site. The station served as the terminus of the line when it opened in 1978, but it failed to spur revitalization. In part, the station's isolation from the Georgia Avenue and Colesville Road intersection dampened any immediate effects on the existing commercial and business districts.

The presence of the Metro station did eventually lead to the construction of high-rise office buildings. Regional developer Lloyd Moore built 1100 Wayne Avenue, the first consequential office tower in nearly a decade, in 1983. Developers then built a series of offices within the triangular area bound by Colesville Road, Georgia Avenue, and Second and Wayne Avenues. This included the Metro Plaza complex completed in 1987.

The burgeoning office environment did not coincide with a revival of the local retail market. The closing of many of the mid-sized retailers followed by the Silver Spring Theatre (1984), Hecht's Department Store (1987), and J.C. Penney's (1989) left the area with a definite void. The commercial vacancies, however, were filled by the establishment of a rich, ethnically diverse, local business community that continues today.

Montgomery County and local developers started to plan for the revitalization of Silver Spring in the mid-1980s. The first two proposals, the Silver Triangle and the American Dream, failed to be realized due to community opposition, extensive litigation, and inability to secure tenants or financing. Regional developer Lloyd Moore introduced the Silver Triangle proposal that included an enclosed mall with two department stores, a hotel, and four office buildings in 1988. The Silver Spring-Takoma Traffic Coalition, led by Pat Singer and historic preservationists, objected to the scale, traffic congestion, and demolition of historic resources including the Silver Spring Theatre and Shopping Center required by the proposal. After years of public hearings and approvals, Montgomery County and Lloyd Moore severed an agreement to build the mall in 1995. That same year, Triple Five proposed the American Dream, an urban entertainment mall with a wave pool, ice skating rink, indoor roller coaster, IMAX, hotel, restaurants, and retailers. Montgomery County entered a memorandum of understanding with the developers, but as with the Silver Triangle proposal, the plan faced community opposition and failed to secure necessary financing.

In 1997, Foulger-Pratt and the Peterson Company proposed the successful “Town Center” plan, later renamed “Downtown Silver Spring.” The project included a retail core with street-level entrances, public plaza, civic building, office building, and parking garage. The redevelopment plan included the restoration of the Silver Spring Shopping Center and Theatre.

While there are many immigrant communities in the plan area, the Ethiopian community has had particular success in Silver Spring and the downtown is a center for Ethiopian life and culture in the region. In the late 1990s, Ethiopians moved from Washington, D.C., to Silver Spring and Alexandria, Virginia. Ethiopian food became a defining element of downtown with at least a dozen current businesses operating in Fenton Village. Other organizations such as the Ethiopian Community Center of Maryland opened an office in Silver Spring to assist immigrants and further foster a sense of community. Since 2000, revitalization efforts envisioned by the Silver Spring Central Business District Sector Plan have seen the redevelopment of the Downtown Silver Spring retail core, the establishment of the Arts and Entertainment District including the reopening of the historic Silver Theatre, and the opening of the Discovery corporate headquarters. With this, the former light industrial areas west of Georgia Avenue have been redeveloped with multi-family housing and the Paul Sarbanes Transit Center has been completed at the Silver Spring Metro station. The 2000 Sector Plan also recommended several civic and community facilities that draw people to downtown Silver Spring today including the Civic Building and Veteran’s Plaza and the Silver Spring Library. In the coming years, both this Plan and the Purple Line light-rail system connecting Silver Spring to Bethesda and New Carrollton will create new opportunities for growth and development in the downtown.

Page 15: Delete section “1.4. THRIVE MONTGOMERY 2050” as follows:

[1.4. THRIVE MONTGOMERY 2050

As one of the county’s most urban areas, Silver Spring is uniquely positioned to embody *Thrive Montgomery 2050*’s organizing principle of urbanism, which emphasizes “compact forms of development, diverse uses and building types, and transportation networks that take advantage and complement these two land use strategies at all densities and scales.” While Silver Spring already offers compact development with a robust transportation network, which will be further enhanced by the completion of the Purple Line, the plan’s recommendations also support *Thrive Montgomery 2050*’s aim for Complete Communities that offer a diverse mix of uses and form along with a strong open space network. The plan advances *Thrive Montgomery 2050*’s goal to integrate parks and public spaces along with economic development strategies and land use planning in order to “attract employers and workers, build social connections, encourage healthy lifestyles, and create vibrant places.”]

Page 17: Revise the first bullet under “Resiliency” as follows:

- Support sustainable and lasting economic growth and resiliency in and around the commercial core through flexible zoning, development incentives, and public realm improvements.

Page 18: Revise the first two bullets under “1.6. SUMMARY OF RECOMMENDATIONS” as follows:

- Establish the [Connectivity and Infrastructure] Civic Improvements Fund (CIF) to allow all Commercial/Residential properties to obtain additional density if needed to meet maximum building heights. The fund would support a world-class arrival experience at the transit center, a

new bridge connection across the rail tracks, and strategic [utility] civic and streetscape [infrastructure] improvements.

- [Create a Silver Spring Building Height Incentive Zone (BHIZ) to allow properties in the commercial core of] Increase heights throughout the downtown to [obtain additional height up to 150 percent of the mapped maximum height for flexibility] encourage redevelopment.

Page 22: Revise the first and second paragraph under “Adjacent Communities” as follows:

These blocks include portions from [four distinct] the established neighborhoods [: Woodside,] of Woodside Park, Seven Oaks-Evanswood and East Silver Spring.

Consistent with [the recommendations of the Attainable Housing Strategies Initiative] the Plan goal of housing diversity, the Adjacent Communities can include a greater variety of housing types, fully integrated into the existing neighborhood fabric, to allow a wider range of residents to enjoy the valued proximity to the downtown and nearby transit options.

Page 26: Insert the following bullet before the first bullet under “Recommendations”:

- Ensure accessibility and mobility for pedestrians (of all abilities) and bicyclists should any bridge require reconstruction or substantial repair in the future. Reconstruction and replacement present a new opportunity to improve access, comfort, and safety.

Page 27: Revise the bullet under “Pedestrian bridge at Montgomery College/Jesup Blair Park” as follows:

- [The] Should the Montgomery College bridge [should be widened or otherwise improved] ever be reconstructed, it should be designed and built to allow the use of bicycles for adequate access to the Metropolitan Branch Trail and upcoming Fenton Street separated bike lanes.

Page 29: Insert the following bullet before the first bullet under “Recommendations”:

- All new rail connections (over or under) should be designed with best practices for access, comfort, safety, and mobility to address the needs of pedestrians who walk and roll, and bicyclists.

Page 29: Revise the first bullet and change the second bullet to a sub-bullet under “Recommendations” as follows:

- Provide a new pedestrian and bicycle connection across the rail from East-West Highway to the western terminus of Silver Spring Avenue in the Ripley District.
 - The Plan proposes this new connection be located between 1215 East-West Highway (The Bennington) and 1201 East-West Highway (The Silverton), crossing the rail and connecting on the east side of the rail at the intersection of Dixon Street and Silver Spring Avenue in the Ripley District. The Plan envisions a landscaped bridge connection with adequate shade and views to the north and south. The bridge would land at an open space in the Ripley District that connects to Silver Spring Avenue.

This connection would provide easy and safe access from South Silver Spring to the Metro Center area or Fenton Village. This bridge would also provide a connection to the Metropolitan Branch Trail, which provides a bicycle connection to Union Station in Washington DC to the south and will connect with the Capital Crescent Trail at the Transit Center to the north of this connection.

Page 29: Add a new bullet at the end under “Recommendations”:

- At the time of redevelopment, property adjacent to any proposed new connection shall coordinate with the Montgomery County Department of Transportation (MCDOT) to define the right-of-way or easement requirements to achieve the connection in the event of a publicly built connection, such as ensuring unfettered public access and limiting physical, visual, and noise encroachment. Should the connection be built outside of the Capital Improvement Program, conditions of approval may stipulate hours of access, ease of navigation and convenience, ADA accessibility, visibility and “obviousness: of the route”, wayfinding, etc. In the event the applicant is not constructing the connection, the applicant must work with MCDOT to define what preparations need to be made to the site such that the connection can be constructed in the future. This may include but is not limited to defined stubs that can be later expanded into a future tunnel or bridge.

Page 32: Revise the first bullet under “Urban Design:” as follows:

- New tall buildings along Georgia Avenue and Colesville Road [and Wayne Avenue] should [include a tower setback above the second floor to continue the pedestrian experience along the sidewalk of a low- to mid-rise building form] have a clearly differentiated base that relates to the pedestrian scale, with substantial variation in building massing, façade, and materials. Towers should step above the base along streets, open spaces, and through-block connections in a way that distinctly differentiates the tower from building base. Refer to the Design Guidelines that accompany this Plan.

Page 32: Revise the second bullet under “Urban Design:” as follows:

- Establish Ellsworth Drive between Fenton Street and the exit from the Towne Square Garage as a [“flexible street”] commercial shared street that can be closed for a farmer’s market and/or other festivals and outdoor events as an expansion of Veterans Plaza. Vehicle traffic would be limited to loading and emergency vehicles, and street parking could be removed or limited.

Page 33: Revise the fourth bullet under “Opportunity Sites:” as follows:

- 8551 [Colesville] Fenton Street: [Opportunity site at the corner of Fenton Street and Colesville Road] Refer to Section 3.9.3 in the Historic Resources section for additional information about this property.

Page 33: Revise the fifth bullet under “Opportunity Sites” as follows:

- Parking Lot at [Ellsworth Plaza] Silver Spring Shopping Center: Refer to Section 3.9.[1]2 in the Historic Resources section on appropriate approach for improving this parking lot.

Page 33: Delete the third bullet under “Zoning” as follows:

- [Parcels in Building Height Incentive Zone are able to achieve heights above the maximum mapped zoning per the proposed recommendations in Section 4.1.]

Page 35: Delete “Note: Overlay Zones are not shown on district zoning maps for clarity; see Map 16 Proposed Zoning.”

Page 36: Revise Table 1. Proposed Ellsworth District Zoning as follows:

Table 1. [Proposed] Ellsworth District Zoning

Map Number	Existing Zoning	[Proposed]Plan Recommended Zoning	Justification
1A	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-[240]300	Increase flexibility for future mixed-use development.
1B	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-[240]300	Increase flexibility for future mixed-use development.
1C	CR-3.0 C-2.0 R-2.75 H-145 T	CR-3.0 C-3.0 R-3.0 H-[175]260	Increase flexibility for future mixed-use development.
1D	CR-3.0 C-2.0 R-2.75 H-145 T	CR-6.0 C-6.0 R-6.0 H-[145]260	Proposed zoning brings parcel into conformance.
2A	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development.
2B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-6.5 C-6.5 R-6.5 H-[175]260	Proposed zoning brings parcel into conformance.
2C	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development.
3A	EOF 3.0 H-100	CR-3.0 C-3.0 R-3.0 H-[100]150	Update to CR zone from pre-2014 EOF zone and increased height for flexibility.
3B	EOF 3.0 H-100	CR-3.0 C-3.0 R-3.0 H-100	Update to CR zone from pre-2014 EOF zone.
4A	CR-5.0 C-4.0 R-4.75 H-75 T	CR-5.0 C-5.0 R-5.0 H-[110]165	Increase flexibility for future mixed-use development.
4B	CR-3.0 C-2.0 R-2.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development.
4C	CR-5.0 C-4.0 R-4.75 H-75 T	CR-5.0 C-5.0 R-5.0 H-[90]135	Increase flexibility for future mixed-use development.
5	CR-1.5 C-1.0 R-1.5 H-60 T	CR-1.5 C-1.5 R-1.5 H-70	Proposed zoning brings parcel into conformance.
6A	CR-1.5 C-1.0 R-1.5 H-90 T	CR-1.5 C-1.5 R-1.5 H-110	Increase flexibility for future redevelopment.
6B	CR-3.0 C-2.0 R-2.75 H-145 T	CR-3.0 C-3.0 R-3.0 H-175	Increase flexibility for future mixed-use development.
6C	CR-1.5 C-1.0 R-1.5 H-60 T	CR-1.5 C-1.5 R-1.5 H-70	Increase flexibility for future mixed-use development.

Page 37: Revise the fourth bullet under “Goals” as follows:

- Enhance Fenton Street as the main street for the district by preserving and improving opportunities for active retail, and arts and entertainment venues.

Page 39: Revise the two bullets under “Parks and Public Spaces” as follows

- Expand the existing Fenton Street Urban Park into a cohesive neighborhood gateway park that is directly connected to the Green Loop, should property in this area become available. Additional facilities should encourage active recreation and social gathering.
- Provide a [½-acre] consolidated green [public space] community-focused space of approximately ½-acre but no less than 1/3 acre along Fenton Street [with] as part of any future redevelopment of Public Parking Garage 4 and adjacent sites.

Page 39: Revise the first bullet under “Opportunity Sites” as follows:

- Public Garage 4: Encourage the redevelopment of Parking Garage 4 and surrounding properties through a public-private partnership with the Parking Lot District. If the redevelopment of this site includes the removal of the existing parking structure, [The] the Plan recommends that this large block be divided via a new north-south connection [that aligns with the north-south connection at the block to the north] that accommodates both pedestrians and vehicles. This connection could provide loading and service connections for the new development. In addition, the Plan recommends an east-west through-block pedestrian connection [as part of any redevelopment of the garage parcel] along with redevelopment. A consolidated [½-acre] green community-focused open space of approximately ½ acre but no less than 1/3 acre should be located at this site, fronting on Fenton Street. This open space could be an opportunity to celebrate local artists and the diversity of Fenton Village. This site is also large enough that it may provide a unique opportunity to consider urban agriculture facilities, either at the ground or as part of a green roof concept.

Page 41: Delete “Note: Overlay Zones are not shown on district zoning maps for clarity; see Map 16 Proposed Zoning.”

Page 42: Revise Table 2. Proposed Fenton Village Zoning as follows:

Table 2. [Proposed] Fenton Village Zoning			
Map Number	Existing Zoning	[Proposed] Plan Recommended Zoning	Justification
7	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175] ²⁶⁰	Increase flexibility for future mixed-use development.
8A	CR-5.0 C-4.0 R-4.75 H-145 T	CR-7.0 C-7.0 R-7.0 H-175	Proposed zoning brings parcel into conformance.
8B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-175	Increase flexibility for future mixed-use development.
8C	CR-5.0 C-4.0 R-4.75 H-145 T	CR-7.0 C-7.0 R-7.0 H-175	Proposed zoning brings parcel into conformance.
8D	CR-3.0 C-2.0 R-2.75 H-110 T	CR-3.0 C-3.0 R-3.0 H 130	Increase flexibility for future mixed-use development.
8E	CR-3.0 C-2.0 R-2.75 H-[110] ⁷⁵ T	CR-3.0 C-3.0 R-3.0 H-[130] ⁹⁰	Increase flexibility for future mixed-use development.

9A	CR-3.0 C-2.0 R-2.75 H-75 T	CR-3.0 C-3.0 R-3.0 H-90	Increase flexibility for future mixed-use development.
9B	CR-1.5 C-1.0 R-1.5 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
9C	<u>CR-3.0 C-2.0 R-2.75 H-60 T</u>	<u>CR-3.0 C-3.0 R-3.0 H-90</u>	<u>Consolidate split zoned properties under one zone.</u>
9D	<u>R-60</u>	<u>CR-3.0 C-3.0 R-3.0 H-70</u>	<u>Rezone to CR for future mixed-use development.</u>
10A	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development.
10B	CR-3.0 C-2.0 R-2.75 H-110 T	CR-3.0 C-3.0 R-3.0 H-130	Increase flexibility for future mixed-use development.
11A	CR-1.5 C-1.0 R-1.5 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase allowable density proximate to high-capacity transit.
11B	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
12A	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
12B	CR-1.5 C-1.0 R-1.5 H-60 T	CR-1.5 C-1.5 R-1.5 H-70	Increase zoning flexibility. Silver Spring Tower remains non-conforming.
13A	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development.
13B	CR-3.0 C-2.0 R-2.75 H-110 T	CR-3.0 C-3.0 R-3.0 H-130	Increase flexibility for future mixed-use development.
14	CR-3.0 C-2.0 R-2.75 H-110 T	CR-[3.0]5.0 C-[3.0]5.0 R-[3.0]5.0 H-130	Increase flexibility for future mixed-use development.
15A	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
15B	CR-1.5 C-1.0 R-1.5 H-60 T	CR-1.5 C-1.5 R-1.5 H-70	Increase flexibility for future mixed-use development.
16A	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
16B	CR-1.5 C-1.0 R-1.5 H-60 T	CR-1.5 C-1.5 R-1.5 H-70	Increase flexibility for future mixed-use development.
17	CR-3.0 C-2.0 R-2.75 H-110 T	CR-3.0 C-3.0 R-3.0 H-130	Increase flexibility for future mixed-use development.
18	CR-3.0 C-2.0 R-2.75 H-110 T	CR-3.0 C-3.0 R-3.0 H-130	Increase flexibility for future mixed-use development.
19A	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
19B	CR-1.5 C-1.0 R-1.5 H-60 T	CR-1.5 C-1.5 R-1.5 H-70	Increase flexibility for future mixed-use development.
20A	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
20B	CR-1.5 C-1.0 R-1.5 H-60 T	CR-1.5 C-1.5 R-1.5 H-70	Increase flexibility for future mixed-use development.
21	CR-3.0 C-2.0 R-2.75 H-110 T	CR-3.0 C-3.0 R-3.0 H-130	Increase flexibility for future mixed-use development.
22	CR-3.0 C-2.0 R-2.75 H-60 T	CR-3.0 C-3.0 R-3.0 H-70	Increase flexibility for future mixed-use development.
23	CR-3.0 C-2.0 R-2.75 H-110 T	CR-3.0 C-3.0 R-3.0 H-130	Increase flexibility for future mixed-use development.
24	IL-1.0 H-50	IL-1.0 H-50	Confirm existing zoning.

Page 44: Insert a new recommendation as the first bullet under “Urban design” as follows:

- New tall buildings along Colesville Road should have a clearly differentiated base that relates to the pedestrian scale, with substantial variation in the building massing, facade and materials. Towers should step back above the base along streets, open spaces and through-block connections in a way that distinctly differentiates the tower from the building base. Refer to the Design Guidelines that accompany this Plan.

Page 44: Revise the bullet under “Parks and Public Spaces” as follows:

- Expand and reinforce the open space connection between the arrival experience at the transit center, the recently redeveloped spaces along Wayne Avenue – including Gene Lynch Park – to Ellsworth Drive, and beyond to the Civic Building.

Page 44/45: Revise the first and second bullets under “Opportunity Sites” as follows:

- Transit Center Development Site: Design a signature building or buildings at the intersection of Colesville Road and Wayne Avenue and consider integrating the development with the Transit Center. The new development should activate the corner at Colesville Road and Wayne Avenue with a ground-floor use appropriate to the center of an urban area. This building should be architecturally significant and a landmark for Silver Spring. As this site is constrained, consider providing no parking or developing this site along with part of the Bonifant/Dixon garage site. The Plan recommends that the maximum allowed building height on this site be [permitted to exceed 300 feet,] 360 feet, with Planning Board approval. [consistent with the provisions of the Building Height Incentive Zone (BHIZ).]
- County Parking Garages 5/55 (Bonifant/Dixon Garage): In alignment with the Parking Lot District’s goals for this site, redevelop this garage that is located in both the Metro Center district and the Ripley District. The Plan recommends that the upper-level connections over Bonifant Street and Dixon Avenue be removed, which will create two large redevelopment sites and a small site that may be appropriate for an open space (see Ripley District recommendations). The redevelopment of the parcel located in the Metro Center district (north side of Bonifant Street) could maintain a large parking garage that could provide both public and private parking or could provide additional support for bus operations at the Transit Center as needed. This site is a prominent, visible site from Colesville Road and the Transit Center and would be appropriate for a low base of several stories with an articulated tower set back above the base. The ground floor experience on this site should respond appropriately to the Purple Line tracks that will occupy all of Bonifant Street in this location. The Plan recommends that the maximum allowed building height on this site be [permitted to exceed 300 feet,] 360 feet, with Planning Board approval. [consistent with the provisions of the Building Height Incentive Zone (BHIZ).]

Page 45: Revise the fourth bullet under “Opportunity Sites” as follows:

- Colesville Road and East-West Highway Northwest corner: This site is located immediately adjacent to the northern Metro Station entrance but is significantly constrained by the WMATA substation central to the site. The Plan recommends continuing to pursue relocation of the substation to promote a signature development at this location. If the substation is not able to be

relocated, it may be incorporated into the redevelopment of the site. The Plan recommends incorporating or connecting this site to the arrival experience at the transit station.

Page 45: Delete the second bullet under “Zoning” as follows:

- [Parcels in Building Height Incentive Zone are able to achieve heights above the maximum mapped zoning per the proposed recommendations in Section 4.1.]

Page 47: Delete “Note: Overlay Zones are not shown on district zoning maps for clarity; see Map 16 Proposed Zoning.”

Page 48: Revise Table 3. Proposed Metro Center Zoning as follows:

Table 3. [Proposed] Metro Center Zoning			
Map Number	Existing Zoning	[Proposed] Plan Recommended Zoning	Justification
25	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-[240]300	Increase flexibility for future mixed-use development.
26	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-[240]300	Increase flexibility for future mixed-use development.
27A	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development.
27B	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-[240]300	Increase flexibility for future mixed-use development.
28	CR-5.0 C-4.0 R-4.75 H-145 T	CR-[5.0]8.0 C-[5.0]8.0 R-[5.0]8.0 H-[200]300	Increase flexibility for future mixed-use development.
29	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-[240]300	Increase flexibility for future mixed-use development.
30	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-[240]300	Increase flexibility for future mixed-use development.
31A	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-[240]300	Increase flexibility for future mixed-use development.
31B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development.
32	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-[240]300	Increase flexibility for future mixed-use development.
33	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-[240]360	Increase flexibility for future mixed-use development, particularly around the Transit Center.
34	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-[240]300	Increase flexibility for future mixed-use development.
35	CR-5.0 C-4.0 R-4.75 H-145 T	CR-8.0 C-8.0 R-8.0 H-[240]360	Increase flexibility for future mixed-use development, particularly around the Transit Center. [Increase allowable height due to significant site constraints.]
36	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-[240]300	Increase flexibility for future mixed-use development.

Page 50: Revise the first bullet under “Urban Design” as follows:

- [At redevelopment sites along Georgia Avenue, design buildings with a low-rise two-story base that relates to the scale of the commercial development across the street. Setback upper floors by a minimum of 15 feet so that there is a clear articulation between base and tower.] New tall buildings along Georgia Avenue should have a clearly differentiated base that relates to the pedestrian scale, with substantial variation in the building massing, facade and materials. Towers should step back above the base along streets, open spaces and through-block connections in a way that distinctly differentiates the tower from the building base. Refer to the Design Guidelines that accompany this Plan.

Page 50/51: Revise the first and second bullets under “Parks and Public Spaces” as follows:

- Create a Civic Green at the center of the Ripley District per the recommendations of the 2010 Green Space Plan Guidelines for Silver Spring and the 2018 Energized Public Spaces Functional Master Plan, that will be an important focal point and social gathering space for this neighborhood. It will connect to the proposed pedestrian bridge over the Metrorail/CSX tracks between the South Silver Spring District and the Ripley District.
- Create an urban recreational park at the parcel currently occupied by the stair tower of Parking Garage 5/55 on the south side of Bonifant Street to the east of Dixon Avenue, adjacent to an existing alley. This park can have small scale active recreation activities to serve the residents of the Ripley District.

Page 51: Revise the first and second bullets under “Opportunity Sites” as follows:

- Parking Garages 5/55 (Bonifant/Dixon Garage): In alignment with the Parking Lot District’s goals for this site, redevelop this garage that is located in both the Metro Center district and the Ripley District (refer to Metro Center District recommendations above). Provide a linear green space with a continuous pedestrian connection from the Purple Line station at the Transit Center to Georgia Avenue and the center of the downtown. This green space will also serve as a buffer from the rail tracks that will occupy all of Bonifant Street between the station and Dixon Avenue. Currently the garage bridges both over Bonifant Street and an alley to the east of Bonifant Street. The Plan recommends that the redevelopment of this garage eliminate these bridges. The development site in the Ripley District should respond to both the need for a safe, green, inviting pedestrian connection along Bonifant Street to Georgia Avenue for commuters and visitors, and the opportunity for a tall, architecturally distinctive tower in such a prominent location proximate to the transit center. The Plan recommends that the maximum allowed building height on this site be ~~[permitted to exceed 300]~~360 feet, with Planning Board approval. [consistent with the provisions of the Building Height Incentive Zone (BHIZ).]
- Block of Ripley Street, Dixon Avenue, and Georgia Avenue: This block consists of several [is a large] potential development ~~[site]~~sites along Georgia Avenue. ~~[The]~~If there is an opportunity for these parcels to redevelop together, the Plan recommends redevelopment of this site to include a central open space, potentially connected across Ripley Street to the 8230 Georgia Avenue site.

Page 51: Revise the fourth bullet under “Opportunity Sites” as follows:

- 8230 Georgia Avenue (Sherwin-Williams site): This site is significantly constrained by the Master-Planned right-of-way for Ripley Street between Dixon Avenue and Georgia Avenue, the

dedication of which would significantly reduce the developable area. Streetscape improvements and other public use facilities should not further significantly reduce the developable area. [Previous efforts to incorporate this site into adjacent developments were unsuccessful. Staff recommends coordinating redevelopment of this site with the property across Ripley Street.]

Page 52: Delete the second and third bullets under “Zoning” as follows:

- [Parcels in Building Height Incentive Zone are able to achieve heights above the maximum mapped zoning per the proposed recommendations in Section 4.1.]
- [Revise zoning for Site 39B to reflect height previously approved by the Planning Board through the Ripley-South Silver Spring (RSS) Overlay zone. This Plan recommends deletion of the RSS overlay zone.]

Page 53: Delete “Note: Overlay Zones are not shown on district zoning maps for clarity; see Map 16 Proposed Zoning.”

Page 54: Revise Table 4. Proposed Ripley District Zoning as follows:

Map Number	Existing Zoning	[Proposed] Plan Recommended Zoning	Justification
37A	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-[240]360	Increase flexibility for future mixed-use development, <u>particularly near the Transit Center.</u>
37B	<u>CR-5.0 C-4.0 R-5.0 H-200 T</u>	<u>CR-5.0 C-5.0 R-5.0 H-300</u>	<u>Increase flexibility for future mixed-use development.</u>
37C	<u>CR-5.0 C-4.0 R-5.0 H-200 T</u>	<u>CR-5.0 C-5.0 R-5.0 H-300</u>	<u>Increase flexibility for future mixed-use development.</u>
38A	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-[240]300	Increase flexibility for future mixed-use development.
38B	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-[240]300	Increase flexibility for future mixed-use development.
39A	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-[240]300	Increase flexibility for future mixed-use development.
39B	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-[270]300	Increase flexibility[; bring height into conformance] <u>for future mixed-use development.</u>
39C	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-[240]300	Increase flexibility for future mixed-use development.
39D	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-240	Increase flexibility for future mixed-use development.

Page 55: Revise the third bullet under “Goals” as follows:

- [Preserve existing market-rate affordable housing.] Strive for no net loss of affordable housing in the event of redevelopment of naturally occurring affordable housing.

Page 55: Revise the first bullet under “Urban Design” as follows:

- [At redevelopment sites along Georgia Avenue, design buildings with a low-rise two-story base that relates to the scale of the existing retail development along Georgia Avenue south of East West Highway. Setback upper floors by a minimum of 15 feet so that there is a clear articulation between base and tower.] New tall buildings along Georgia Avenue should have a clearly differentiated base that relates to the pedestrian scale, with substantial variation in the building massing, facade and materials. Towers should step back above the base along streets, open spaces and through-block connections in a way that distinctly differentiates the tower from the building base. Refer to the Design Guidelines that accompany this Plan.

Page 56: Revise the two bullets under “Parks and Public Spaces” as follows:

- Provide a new urban recreational park in South Silver Spring per the recommendations of the 2010 Green Space Plan Guidelines for Silver Spring and 2018 *Energized Public Spaces Functional Master Plan*. The Plan recommends this park be located between Kennett Street and East-West Highway and will serve as both a park and a through-block connection between those streets. The vision for this park is an active recreation space that can serve as a complement to the historic, contemplative setting of Acorn Park [just] up the street that will also be renovated. This park will also provide a green space [that can] to support the retail and food services establishments along East-West Highway.
- Renovate Jesup Blair Park to create a unique open place that includes social, active, and contemplative experiences throughout its 14 acres. The new Jesup Blair Park will be a gateway and a regional destination that promotes an active lifestyle and offers the unique historical and cultural setting of a special park designated on the Master Plan for Historic Preservation. Renovate the historic Jesup Blair House for appropriate community uses that complement the park programming. [Increasing transit connections to this park should be studied further. For a full vision of the program of this proposed renovation, see] Refer to Parks and Public Spaces Recommendations in Section 4.2.

Page 56: Insert the following bullets after the second bullet under “Parks and Public Spaces” as follows:

- Support the existing community garden use at 7980 Georgia Avenue until the parcel is redeveloped.
- Support the approved project plan for a mixed-use infill redevelopment, found in the Blairs Master Plan, that includes several new public open spaces in the northern part of South Silver Spring west of East-West Highway. This includes a large civic space and smaller complementary spaces with active recreation opportunities.

Page 57: Revise the first and second bullets under “Opportunity Sites” as follows:

- 7980 Georgia Avenue: The Plan recommends [considering the] redevelopment of this site as a mixed-use development [on the portion of the parcel that fronts on] along Georgia Avenue., while] Consider [preserving the existing] incorporating a community garden [at the western end of the parcel] use as part of the open space on this parcel. Improve connections in this area of

South Silver Spring by creating a through-block connection. [at this site from King Street to Georgia Avenue that aligns with the mid-block connection adjacent to the Galaxy Apartments. The Plan recommends retaining the community garden as part of any redevelopment.]

- 8040 13th Street/Days Inn: This is a key opportunity site in South Silver Spring and a strong site for mixed-use development with active ground floor uses along 13th street, including retail. This site should include a public open space and a pedestrian [through-block] connection [providing pedestrian access through the site and connecting] through the site to the proposed [Urban Recreational] South Silver Spring Park along Kennett Street. Heights should step down towards Eastern Avenue as identified [in the existing zoning] in the Design Guidelines. This Plan [further] recommends coordination with the Parking Lot District regarding the potential use of the existing surface lot and parking garage as a parking resource for any redevelopment.

Page 57: Add a seventh bullet after the last bullet under “Opportunity Sites” as follows:

- Blairs Master Plan sites: Support the approved project plan for a mixed-use infill redevelopment that includes multiple parcels in South Silver Spring west of East-West Highway, including the redevelopment of the existing Blair Park Shopping Center.

Page 57: Add a section titled “Garden Apartments”, before “Zoning”, as follows:

- Garden Apartments
 - South Silver Spring is home to several garden style apartment complexes built predominantly in the 1930s and 1940s that provide a significant source of naturally occurring affordable housing for the Silver Spring community. Like many aging multi-family properties discussed in previous master plans, these garden facilities may require significant renovation, rehabilitation, or redevelopment in the coming decades. In the event of redevelopment, these properties should strive for no net loss of affordable housing. No net loss is where naturally occurring affordable housing units are replaced with an equivalent number of new income-restricted affordable housing units through redevelopment.
 - 7719-7725 Eastern Avenue (Blair Park Apartments): This Plan recommends rezoning to allow flexibility and redevelopment that would support future affordable housing development.
 - 7701-7705 Eastern Avenue; 805-809 Juniper Street; 7700-7705 Blair Road (Blair Park Gardens): Support a future application for a CRT Floating Zone with residential density of up to 4.0 FAR, that strives for no net loss of affordable housing.
 - 7603-7615 Eastern Avenue (Eastern Avenue Apartments): Support a future application for a CRT Floating Zone with residential density of up to 4.0 FAR, that strives for no net loss of affordable housing.
 - 8000-8012 Eastern Drive; 8001-8009 Eastern Drive; 8033- 8039 Eastern Avenue; 8000-8004 Blair Mill Road; 1300-1302 Blair Mill Road (Rock Creek Springs): Support a future application for a CRT Floating Zone with residential density of up to 4.0 FAR, that strives for no net loss of affordable housing.
 - 8001-8031 Eastern Avenue (Spring Garden Apartments): Support a future application for a CRT Floating Zone with residential density of up to 4.0 FAR, that strives for no net loss of affordable housing.

Page 57: Delete the third bullet under “Zoning” as follows:

- [Parcels in Building Height Incentive Zone are able to achieve heights above the maximum mapped zoning per the proposed recommendations in Section 4.1.]

Page 59: Delete “Note: Overlay Zones are not shown on district zoning maps for clarity; see Map 16 Proposed Zoning.”

Page 60: Revise Table 5. Proposed South Silver Spring Zoning as follows:

Table 5. [Proposed] South Silver Spring Zoning			
Map Number	Existing Zoning	[Proposed] Plan Recommended Zoning	Justification
40	CR-5.0 C-0.5 R-5.0 H-200 T	CR-5.0 C-5.0 R-5.0 H-240	Increase flexibility for future mixed-use development.
41A	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-[200]300	Increase flexibility for future mixed-use development.
41B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development.
41C	CR-5.0 C-4.0 R-4.75 H-200 T	CR-5.0 C-5.0 R-5.0 H-240	Increase flexibility for future mixed-use development.
42A	R-10	R-10	Confirm current zoning for existing affordable housing.
42B	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
42C	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
43A	CR-3.0 C-2.0 R-2.75 H-125 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
43B	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
44	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
45	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
46	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
47A	CR-3.0 C-2.0 R-2.75 H-125 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development; one property remains non-compliant.
47B	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
48A	CR-3.0 C-2.0 R-2.75 H-[90]125 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
48B	CR-3.0 C-2.0 R-2.75 H-125 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
48C	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
48D	CR-3.0 C-2.0 R-2.75 H-125 T	CR-3.0 C-3.0 R-3.0 H-125	Increase flexibility for future mixed-use development.
49A	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.

49B	IM-2.5 H-50	CR-3.0 C-3.0 R-3.0 H 110	Rezone to CR from IM to allow future mixed-use development.
50	R-60	R-60	Confirm existing zoning; existing park.
51A	CRT-2.25 C-1.5 R-0.75 H-75	CRT-2.25 C-2.25 R-2.25 H-75	Increase flexibility for future mixed-use development.
51B	R-10	CRT-3.0 C-0 R-3.0 H 75	Rezoned to CR to provide flexibility for future development.
51C	CRT-2.25 C-1.5 R-0.75 H-75	CRT-2.25 C-2.25 R-2.25 H-75	Increase flexibility for future mixed-use development.
51D	R-10	R-10	Confirm current zoning for existing affordable housing.
52	R-10	R-10	Confirm current zoning for existing affordable housing.
53	R-10	R-10	Confirm current zoning for existing affordable housing.
54A	IL-1.0 H-50	IL-1.0 H-50	Confirm existing zoning.
54B	IM-2.5 H-50	CR 2.5 C-2.5 R-2.5 H-55	Rezone to CR to provide flexibility for future development.
55A	IL-1.0 H-50	CR-2.5 C-2.5 R-2.5 H 55	Rezone to CR to provide flexibility for future development.
55B	IM-2.5 H-50	CR-2.5 C-2.5 R-2.5 H 55	Rezone to CR to provide flexibility for future development.
55C	R-60	R-60	Confirm existing zoning.

Page 62: Insert a new recommendation as the first bullet under ‘Urban Design’ as follows:

- New tall buildings along Georgia Avenue should have a clearly differentiated base that relates to the pedestrian scale, with substantial variation in the building massing, façade, and materials. Towers should step back above the base along streets, open spaces and through-block connections in a way that distinctly differentiates the tower from the building base. Refer to the Design Guidelines that accompany this Plan.

Page 63: Delete the third bullet under “Zoning” as follows:

- [Parcels in Building Height Incentive Zone are able to achieve heights above the maximum mapped zoning per the proposed recommendations in Section 4.1.]

Page 65: Delete “Note: Overlay Zones are not shown on district zoning maps for clarity; see Map 16 Proposed Zoning.”

Page 66: Revise Table 6. Proposed Downtown North Zoning as follows:

Table 6. [Proposed] Downtown North Zoning			
Map Number	Existing Zoning	[Proposed] Plan Recommended Zoning	Justification
56	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
57A	CR-3.0 C-1.25 R-3.0 H-145 T	CR-3.0 C-3.0 R-3.0 H-175	Increase flexibility for future mixed-use development.

57B	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
57C	CR-3.0 C-2.0 R-2.75 H-90 T	CR-5.0 C-5.0 R-5.0 H-175	Increase flexibility for future mixed-use development.
58A	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-175	Increase flexibility for future mixed-use development.
58B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-6.75 C-6.75 R-6.75 H-175	Adjust zoning to bring parcel into conformance.
59	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development.
60	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development.
61	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development.
62	CR-5.0 C-1.0 R-5.0 H-200 T	CR-5.0 C-5.0 R-5.0 H-[240]300	Increase flexibility for future mixed-use development.
63A	CR-5.0 C-1.0 R-5.0 H-200 T	CR-5.0 C-5.0 R-5.0 H-[240]300	Increase flexibility for future mixed-use development.
63B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development.
64	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-[240]300	Increase flexibility for future mixed-use development.
65A	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-110	Increase flexibility for future mixed-use development.
65B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development; resolve split zoning.
65C	CR-5.0 C-4.0 R-4.75 H-145 T	CR-6.0 C-6.0 R-6.0 H-[175]260	Adjust zoning to bring parcel into conformance.
65D	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development.
65E	CR-3.0 C-2.0 R-2.75 H-90 T	CR-3.0 C-3.0 R-3.0 H-[110]165	Increase flexibility for future mixed-use development.
66A	EOF-3.0 H-100	CR-3.0 C-3.0 R-3.0 H-100	Update to CR zone from pre-2014 EOF zone.
66B	R-60	CR-3.0 C-3.0 R-3.0 H-100	Consolidate split zone properties under one zone.
66C	EOF-3.0 H-100	CR-3.0 C-3.0 R-3.0 H-100	Update to CR zone from pre-2014 EOF zone.
67	CR-3.0 C-0.75 R-3.0 H-145 T	CR-3.0 C-3.0 R-3.0 H-[175]260	Increase flexibility for future mixed-use development.
68A	CR-5.0 C-4.0 R-4.75 H-145 T	CR-5.0 C-5.0 R-5.0 H-[175]260	Increase flexibility for future mixed-use development.
68B	CR-5.0 C-4.0 R-4.75 H-145 T	CR-6.0 C-6.0 R-6.0 H-[175]260	Adjust zoning to bring parcel into conformance.
68C	CR-8.0 C-6.0 R-7.5 H-200 T	CR-8.0 C-8.0 R-8.0 H-[240]300	Increase flexibility for future mixed-use development.

Page 67: Revise the first bullet point under “Goals” as follows:

- Preserve the historically significant Falklands South parcel just south of East-West Highway as a sylvan retreat of [market-rate] naturally occurring affordable housing in the middle of a dense urban environment.

Page 67: Delete the second and third bullets under “Urban Design” as follows:

- [Connect the renovated stream valley in the southeast quadrant to a consolidated public use space on the north parcel that is green and clearly public in nature.]
- [Implement the recommendation from the 2010 Greenspace Guidelines for a renovated stream valley in the southern portion of the Falklands parcel between East-West Highway and Colesville Road.]

Page 67: Add a “Parks and Public Spaces” section after the “Urban Design” section under “Recommendations” as follows:

- Parks and Public Spaces

- Implement the recommendation from the 2010 Greenspace Guidelines for a renovated stream valley in the southern portion of the Falklands parcel between East-West Highway and Colesville Road.
- Connect the renovated stream valley in the southeast quadrant to a consolidated public use space on the north parcel that is green and clearly public in nature.

Page 67: Add an “Opportunity Sites” section after the new “Parks and Public Spaces” section under “Recommendations” as follows:

- Opportunity Sites

- Northern Parcel of Falklands Apartments: This parcel is bounded by 16th Street to the west, East-West Highway to the south and the Metrorail/CSX tracks to the east. This property has an approved site plan for a mixed-use project and the Plan supports the redevelopment of this parcel.

Page 69: Delete “Note: Overlay Zones are not shown on district zoning maps for clarity; see Map 16 Proposed Zoning.”

Page 70: Revise the text under “Vision” as follows:

Consistent with [the recommendations of the Attainable Housing Strategies Initiative,] the Plan goal of housing diversity, the Adjacent Communities can include a greater variety of housing types, fully integrated into the existing neighborhood fabric, to allow a wider range of residents to enjoy the valued proximity to the downtown and nearby transit options.

Page 70: Revise the second and third bullets under “Goals” as follows:

- Encourage a greater diversity of housing types [as recommended by the proposed Attainable Housing Strategies Initiative].
- Preserve existing [market-rate] naturally occurring affordable housing in this District.

Page 70: Delete the first bullet under “Urban Design” as follows:

- [Refer to Attainable Housing Strategies Initiative on building form and relationship to street for new permitted housing types.]

Page 70: Delete the first sub-bullet to the fourth bullet under “Urban Design” as follows:

Implement as many Green Loop elements as possible along the following streets that will be Green Loop Connectors into the downtown:

- [2nd Avenue]
- Ellsworth Drive

- Bonifant Street

Page 71: Revise the first bullet under “Zoning” as follows:

- Confirm all existing zoning with the exception of the mapped areas shown and described in the table below.

Page 71: Delete the second bullet under “Zoning” as follows:

- [Convert parcels zoned EOF to CR as shown in the map and the table.]

Page 71: Add a zoning recommendation as the last bullet under “Zoning” as follows:

- 8808 Colesville Road: This property is currently zoned R-60 but is being used as a commercial property. The Plan recommends this property as a candidate for a CRN floating zone of CRNF-0.75 C-0.75 R-0.75, H-40.

Page 74: Revise Table 8. Proposed Adjacent Communities Zoning as follows:

Table 8. [Proposed] Adjacent Communities Zoning			
Map Number	Existing Zoning	Plan Recommended Zoning	Justification
[71]	[R-60]	[R-60]	[Confirm existing zoning.]
[72]	[R-60]	[R-60]	[Confirm existing zoning.]
73	R-60	R-60	Confirm existing zoning.
[74]	[R-60]	[R-60]	[Confirm existing zoning.]
75A	R-60	R-60	Confirm existing zoning.
75B	RT-12.5	THD	Update to THD from pre-2014 zone.
76A	R-60	R-60	Confirm existing zoning.
76B	RT-12.5	THD	Update to THD from pre-2014 zone.
[77]	[R-60]	[R-60]	[Confirm existing zoning.]
78	R-60	R-60	Confirm existing zoning.
79	R-60	CRT-1.5 C-0 R-1.5 H-65	Rezone to allow increased flexibility for multifamily development.
[80]	[R-60]	[R-60]	[Confirm existing zoning.]
[81]	[R-60]	[R-60]	[Confirm existing zoning.]
[82]	[R-60]	[R-60]	[Confirm existing zoning.]
[83]	[R-60]	[R-60]	[Confirm existing zoning.]
[84]	[R-60]	[R-60]	[Confirm existing zoning.]
[85]	[R-60]	[R-60]	[Confirm existing zoning.]
[86]	[R-60]	[R-60]	[Confirm existing zoning.]
[87]	[R-60]	[R-60]	[Confirm existing zoning.]
88A	R-60	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
88B	R-60	CR-3.0 C-3.0 R-3.0 H-70	Rezone to CR for flexibility for future mixed-use development.

[89]	[R-60]	[R-60]	[Confirm existing zoning.]
90A	R-30	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
90B	R-60	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
90C	CRN-0.5 C-0.5 R-0.25 H-35	CRN-0.75 C-0.5 R-0.75 H-40	Rezone to match adjacent overall FAR while maintaining commercial density that allows the existing use.
[91]	[R-60]	[R-60]	[Confirm existing zoning.]
92A	R-60	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
92B	R-20	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
[93]	[R-60]	[R-60]	[Confirm existing zoning.]
94A	R-20	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
94B	R-60	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
95	R-60	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
96	R-60	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
97	CRNF-1.25 C-0.25 R-1.0 H-65	CRN-1.25 C-0.25 R-1.0 H-65	Confirm existing zoning.
98	R-60	CRN-0.75 C-0 R-0.75 H-40	Rezone to CRN to allow increased residential density.
99	R-60	R-60	Confirm existing zoning.

Page 77: Revise the seventh bullet under “Goals” as follows:

- Promote the development of diverse housing types in the Adjacent Communities district as recommended by this Plan [(and the ongoing Attainable Housing Strategies Initiative)].

Page 77: Delete the last bullet under “Goals” as follows:

- [Support upgrades to the current utilities and infrastructure that support the Plan, including water and sewer.]

Page 78: Revise the first bullet under “Recommendations” as follows:

- [Connectivity and Infrastructure] Civic Improvement Fund
 - Any CR property in the Plan Area may obtain additional density necessary to reach the mapped maximum building height[, or additional height approved for a property in the BHIZ,] by making a contribution to the [Connectivity and Infrastructure] Civic Improvement Fund (CIF). CIF contributions will be made only for density above the total mapped maximum density for the site not achieved through [, density approved per] density-averaging[, and/] or [bonus density] for providing MPDUs above the required minimum, up to the approved maximum height. Refer to Section 4.1 Implementation.

Page 78: Delete and replace the second bullet under “Recommendations” as follows:

- 695 • [Building Height Incentive Zone
- 696 ○ Establish a Silver Spring Downtown Building Height Incentive Zone (BHIZ), as shown in
- 697 Map 32, to allow CR-zoned properties pursuing Optional Method Development to increase
- 698 building heights by up to 150 percent of the mapped height to a maximum of 300 feet.
- 699 Approved height will be subject to the Design Review process through the Design Advisory
- 700 Panel. Refer to Section 4.1 Implementation for further detail.
- 701 ○ The Planning Board may approve certain properties identified in the Metro Center District
- 702 and the Ripley District to realize an increased building height up to 360 feet, consistent with
- 703 the recommendations of the Sector Plan and Design Guidelines, subject to the Design Review
- 704 process through the Design Advisory Panel.]
- 705 • Increase heights on CR properties in the downtown to encourage redevelopment. The blocks
- 706 surrounding the Transit Center should permit heights up to 360 feet, with Planning Board
- 707 approval. Refer to the Design Guidelines that accompany this Plan.
- 708

709 Page 84: Revise the fifth paragraph under “Economic Growth” as follows:

710
711 This Sector Plan envisions a future in which downtown Silver Spring is among the region’s
712 premier office markets, has continued to grow its profile as a unique retail destination, and has a
713 diverse base of high-quality jobs in numerous industries, making it resilient to evolving economic
714 conditions and an attractive place to work and do business. The Sector Plan recommendations to
715 improve and enhance the public realm, including the creation of the Green Loop, along with
716 increased heights and flexible zoning are designed to achieve this vision.
717

718 Page 85: Revise the first bullet under “Recommendations” as follows:

719
720 Encourage the conversion of [obsolete] vacant office space to other uses (short-term or long-

721 term), including residential (condominiums or rentals), hotels/hospitality centers, healthcare

722 facilities, laboratory/research facilities, education facilities, and non-profit service centers.

723

724 Page 85: Insert the following between the second and third bullets under “Recommendations”:

- 725
- 726 ▪ Encourage ground-floor spaces in new development that are divided into smaller bays
- 727 appropriately sized for small businesses where feasible and when consistent with other Plan
- 728 recommendations.
- 729

730 Page 86: Delete the second and third paragraphs and insert a new paragraph under “3.3 HOUSING” as

731 follows:

732

733 [This Plan represents the first opportunity to implement the vision for housing laid out in Thrive

734 Montgomery 2050, the 2021 update to the county’s General Plan that lays out a long-range policy

735 framework that will guide future land use and growth. Thrive Montgomery 2050’s housing

736 recommendations, found in the “Housing for All” chapter, have wide ranging policies to help

737 make housing more attainable, including increasing housing production and preserving existing

738 affordable and attainable housing. The vision laid out in Thrive Montgomery 2050 is consistent

739 with the Housing Element of 2011 and builds upon a foundation of progressive housing policy

740 laid out in several previous master plans, including the Bethesda Downtown Plan, the Veirs Mill

741 Corridor Master Plan, and the Forest Glen Montgomery Hills Sector Plan.

742

In implementing the recommendations of Thrive Montgomery 2050 in the Silver Spring Downtown and Adjacent Communities Plan, this Plan becomes the first Sector Plan to acknowledge and begin to address the deep disparities in wealth and homeownership that were shaped by a legacy of discriminatory lending practices, restrictive covenants, and single-family zoning and its secondary impacts on neighborhoods that is still being felt today.]

The vision for housing laid out in the Silver Spring Downtown and Adjacent Communities Plan is consistent with the Housing Element of 2011 and builds upon a foundation of progressive housing policy laid out in several previous master plans, including the Bethesda Downtown Plan, the Veirs Mill Corridor Master Plan, and the Forest Glen/Montgomery Hills Sector Plan that strive to increase housing production, promote housing diversity, and preserve existing naturally occurring affordable housing. This Plan acknowledges the deep disparities in wealth and homeownership that were shaped by a legacy of discriminatory lending practices, restrictive covenants, and single-family zoning. The impacts of these policies are still being felt today and the Plan strives to address some of these inequities in the recommendations below.

Page 87: Delete the first paragraph and revise the second, third, and fourth paragraphs as follows:

[This Plan supports strategic alignment with the recommendations in the Attainable Housing Strategies to begin to address decades of inequities to create more equitable, mixed-income neighborhoods and ensure that exclusively single-family zoning is not a barrier to development in an area like Silver Spring, with its many amenities and transit and employment accessibility. This alignment with Attainable Housing Strategies also helps to provide a transition from the downtown commercial uses to the surrounding residential neighborhoods and introduces the potential for new housing typologies in the adjacent communities in the plan area. These parcels would be appropriate for housing types like duplexes and triplexes.]

This Plan [also] recommends prioritizing providing a range of unit types for a diversity of households, including families, seniors, and persons with disabilities, to allow more diverse households to take advantage of Silver Spring's amenities. The existing rental units in the downtown are predominately smaller bedroom units.

Within the downtown area are more than thirty-five multi-unit residential buildings containing more than 12,000 units. Approximately 43 percent of all units in multi-unit residential buildings are currently [market-rate] naturally occurring affordable housing, meaning they are affordable to households earning incomes below 80 percent of the Washington, D.C. metropolitan region's area median income (AMI). At 80 percent AMI in 2020, these units would be affordable to households of three earning around \$90,720 a year for a household of three.

This Plan aims to balance the preservation of existing [market-rate] naturally occurring affordable housing with the production of new housing, which will result in the production of new MPDUs. [This preservation and production strategy] The Plan seeks to [retain the existing zoning on many existing market-rate properties, and to rezone select properties to maximize density near transit] achieve this through rezoning of select properties or by supporting future applications for a Floating Zone that prioritizes the replacement of existing naturally occurring affordable housing with new income-restricted affordable housing.

Page 88: Revise the first paragraph under "3.3.1 Goals" as follows:

The housing goals for the plan are guided by the following policies and practices found in previous [and ongoing] plans including [Thrive Montgomery 2050,] the Housing Element of 2011, and [previous] recent masterplans. The intent of this Plan is to ensure that this Plan is consistent and in sync with current best practice in planning and housing policy. The goals, as aligned with the Housing Element of 2011, include:

Page 88: Delete the first and second bullets under “3.3.1 Goals” as follows:

- [Expand opportunities to increase residential density, especially along major corridors and in locations where additional housing can assist in the development of Complete Communities.]
- [Facilitate the development of a variety of housing types in every part of the county but especially in areas near transit, employment, and educational opportunities.]

Page 88: Insert the following bullet as the first bullet under “3.3.1 Goals”:

- Build the majority of new housing in transit-oriented locations and near jobs and employment centers.

Page 88: Revise the third bullet under “3.3.1 Goals”: as follows:

- Encourage and support neighborhood diversity with a range of unit sizes, types, and occupancy (including rental and ownership options) to serve individuals and families of all ages, incomes, and backgrounds. The range of housing types to be encouraged and supported may include [Support creative housing options including single-room occupancy units (SROs)] Personal Living Quarters (PLQs) and/or micro-units; “missing middle” housing types such as tiny houses, cottages, duplexes, multiplexes, and small apartment buildings; shared housing, co-housing, and accessory dwelling units (ADUs), social housing, and cooperative housing [to help meet housing needs and diversify housing options].

Page 88: Delete the fourth, fifth, and sixth bullets under “3.3.1 Goals” as follows:

- [Increase the number of income-restricted affordable housing units, especially for low-income households.]
- [Prioritize use of public land for co-location of housing and other uses, particularly where government agencies design new facilities or dispose of real property.]
- [Calibrate the applicability of the Moderately Priced Dwelling Unit (MPDU) program and other affordable housing programs to provide price-regulated units appropriate for income levels ranging from deeply affordable to workforce.]

Page 88: Add the following four bullets at the end the section “3.3.1. Goals” as follows:

- Create mixed-use neighborhoods with local small retail businesses and basic services within walking distance of housing.
- Encourage housing cooperatives, faith-based organizations, neighborhood housing groups, and employers to use their existing property or to purchase land and buildings for the production and preservation of housing affordable to households with low and moderate incomes.

- 839 ▪ Make housing affordable to low-, moderate-, and middle-income households a priority.
- 840 ▪ Provide underused and strategically located surplus public properties for housing, including units
- 841 affordable to low- and moderate-income households, at a higher percentage than required in the
- 842 MPDU program and using best design practices. Property that is designated as parkland is not be
- 843 considered surplus.
- 844

845 Page 88: Revise the second and third bullets under “3.3.2. Recommendations” as follows:

- 846
- 847 ▪ Preserve existing, [market-rate] naturally occurring affordable housing where possible, striving
- 848 for no net loss of affordable housing in the event of redevelopment.
- 849 ▪ Publicly owned properties should be encouraged to provide [up to] a minimum of 30 percent
- 850 MPDUs, with 15 percent affordable to households earning at the standard MPDU level of 65-70
- 851 percent or less of Area Median Income (AMI) and 15 percent affordable to households earning
- 852 less than 50 percent of AMI.
- 853

854 Page 89: Delete the eighth and ninth bullets under “Recommendations” as follows:

- 855
- 856 ▪ [The Council should take action on Attainable Housing Strategies and adopt a countywide
- 857 Zoning Text Amendment to allow duplexes, triplexes, and quadplexes by-right with conformance
- 858 with a pattern book in the Adjacent Communities.]
- 859 ▪ [If there is no action on Attainable Housing Strategies, the relevant recommendations to allow
- 860 duplexes, triplexes, and quadplexes by-right with conformance with a pattern book in the R-60
- 861 zone should be adopted in the Adjacent Communities through a Zoning Text Amendment.]
- 862

863 Page 90: Revise the second bullet under “Recommendations” as follows:

- 864
- 865 ▪ Create a Design Advisory Panel (DAP) that allows community participation and is representative
- 866 of the Plan area. Design Review will be required for all Optional Method Development Projects
- 867 in the Plan Area. [Additional height obtained through the Building Height Incentive Zone will
- 868 be subject to the Design Review process through the Design Advisory Panel.]
- 869

870 Page 90: Insert the following new bullet prior to the last bullet under “Recommendations”:

- 871
- 872 ▪ Implement noise mitigation strategies for new development to comply with the County’s noise
- 873 ordinance.
- 874

875 Page 91: Move “Map 19. Recommended Maximum Heights Map” to the Plan-wide “Land Use and

876 Zoning” section.

877

878 Page 92: Revise the fourth bullet under “Goals” as follows:

- 879
- 880 • Activate [under-utilized] vacant spaces (both publicly and privately owned) through temporary
- 881 placemaking strategies. These tactical moves can help spur long-term public realm
- 882 transformation and improve public life.
- 883

884 Page 94: Revise the first three paragraphs under “3.5 PARKS, TRAILS AND PUBLIC SPACES” as

885 follows:

The [plan] Plan envisions a community where [it is easy and irresistible for] all residents, visitors, and workers [to] will enjoy a network of welcoming, multi-use parks, trails, and public spaces. Parks and public spaces, [especially those] in urban areas [like Silver Spring] are a [platform] center for community experiences and interaction, [. Urban parks support] and improve the quality of life [of residents] for community members of all ages, races, cultures, income levels, and physical abilities.

[As participants noted during the public engagement process, Silver Spring needs more parks and public spaces, especially spaces for active recreation.]

The goals and recommendations for this plan were developed in concert with policy guidance from previous [and ongoing] plans including the 2010 Silver Spring CBD Green Space Guidelines, the 2017 PROS Plan, and the 2018 Energized Public Spaces Functional Master Plan, [Thrive Montgomery 2050, and 2022 Parks, Recreation and Open Space (PROS) Plan (on-going),] in addition to input from the community [input received during the planning process] who voiced the importance of parks and public spaces [in] to their quality of life.

The goals and recommendations below support the plan's overall themes of diversity, connectivity, resiliency, and community health.

Page 96: Revise the text under "Equity" as follows:

Equity:

- [Everybody's backyard – promote] Promote a fair and equitable distribution of attractive, safe, and fun parks and public spaces as [common] shared civic spaces with facilities and programming open to people of all ages, race, culture, income, and abilities.
- [More Active Recreation -] Add [additional] park facilities and programs that promote physical activity and social interaction. [, including in small spaces, with introduction of basketball hoops, ping-pong tables, interactive art, areas for hopscotch games, tai chi, and other activities that keep people moving and meeting each other. The results of the EPS Plan analysis (see Appendix D) show that the plan area particularly lacks active recreation experiences. The need is especially great in the center of downtown where the shortage of active experiences is higher.]
- ["Right park in the right places" - Parks will be designed to accommodate major functions, but all should provide social, active and nature-based experiences to the extent possible. Ensure a fair distribution of the] Equitably distribute park experiences across the various districts of the [is sector p] Plan. Parks should be located near transit, accessible [by] via walking and biking, and surrounded by active building frontages. See Section 4.5.4 for proposed park locations.
- [Multifunctional Spaces - Facilitate the creation of new and the renovation of] Design parks to be multifunctional. Create new and/or renovate existing parks and public spaces [that] to accommodate multiple needs, including recreation, education, community-building, and environment stewardship within the urban context. Consider co-location, adaptive reuse, joint programming, and shared use of land, buildings and facilities whenever possible to optimize limited available land.
 - [For example, community gardens in Silver Spring promote social engagement and physical activity in high-density areas to a diverse population in addition to providing access to fresh and healthy foods. Fenton Street Urban Park will be consolidated with the existing community garden which will be expanded and integrated with additional land

dedication and acquisition. Proposed and existing community gardens are identified in Section 4.5.4.

- Consider co-location, adaptive reuse, joint programming, and shared use of land, buildings, and facilities whenever possible to optimize limited available land. This Plan recommends the renovation of the largest park inside the sector plan – Jesup Blair Park – to bring new and renovate existing amenities, add an intense programming, and welcome future and existing park users to gather as a community in this regional destination.]

- [Celebrate Silver Spring’s Culture and History -] Promote facilities that celebrate cultural and historic aspects of the community, and that encourage social connection and a sense of pride of ownership of shared spaces. [serve the distinct social connection needs of seniors, teenagers, young adults and people with disabilities and contribute to the sense of pride and ownership of parks. The proposed Cultural Art Loop in Jesup Blair park will be a venue to celebrate the more inclusive Silver Spring history proposed in this Plan. See Section 4.9.6 Cultural and Heritage Resources.]

Page 97: Revise the last two bullets under “Equity” as follows:

- [Access to nature and future stewards –] Expand the urban tree canopy coverage and pervious surfaces in both publicly and privately owned parks and public spaces[, both publicly and privately owned]. Promote watershed connectivity through education and best stormwater management practices. [See more info on this recommendation at section] Refer to Section 3.7 - Resilient Downtown.
- Promote the creation of [“Friends of”] local organizations to support [in] major parks and public spaces. Engage [the community] stakeholders early in the planning, programming and design of parks and public spaces to collaborate on creative solutions and to ensure [the resulting place is meaningful and reflective of] that ultimate park designs reflect community needs and identity. [Engage stakeholders such as property owners, developers, non-profit groups, community members, artists, and public agencies to collaborate in delivering creative solutions and development strategies.] Work with the community and schools to develop early advocacy programs and activities to encourage nature appreciation, education, and stewardship.

Page 97: Revise the text under “Green Loop Connectivity” as follows:

Green Loop Connectivity

- Implement the proposed Green Loop to connect existing and proposed parks and public spaces with other land uses inside the Plan and the surrounding region. [promoting]
- Promote walking and biking to these places [with] along comfortable, sustainable, safe and shaded roads, sidewalks and trails. The Plan recommends improving signage and wayfinding of parks, public spaces and trails within Silver Spring.
- [All parks and public spaces - Ensure access to all parks and public spaces – including POPS – that are designed to support casual, impromptu use, and connection with nature and other land uses.
- Wayfinding – Improve signage and wayfinding of parks, public spaces, and trails; consider partnering with future commercial/businesses organizations to create a public space map and signage for the Green Loop and the open space network in Silver Spring.

- Integrate amenities with walking loops – Promote physical activity, people watching, social connections and integration of amenities and parks and public spaces with internal walking loops inside public spaces and through connections to the Green Loop, respectively.]

Page 97: Revise the text under “Creative Implementation” as follows:

Creative Implementation

- [Temporary/Interim Pilot Spaces -] Consider short-term/temporary solutions and “pop-up” programming that reflect community identity within temporary/interim parks. Consider empty lots, surface parking areas or other opportunity sites adjacent to the Green Loop as potential pilot sites.

Page 98: Revise the first paragraph under “3.5.3 Equity in Parks and Public Spaces Implementation” as follows:

Silver Spring was identified as an implementation priority area through the mapping of Experience Improvement Areas (EIAs) of the Energized Public Spaces (EPS) Plan with the overlay of the Equity Focus Areas (EFAs) from Planning’s Equity Focus Areas Analysis. [Thrive Montgomery 2050.] Recommendations described in this Plan will help prioritize where and how to spend limited public funds where the highest need is identified.

Page 98: Revise the text under “Goal” as follows:

[Address park resources distribution in the County’s urbanizing areas. Give a voice to] Prioritize underserved communities in the distribution of park resources and contribute to the efforts initiated by the County Council on racial equity and social justice.

Page 98: Delete the first bullet under “Recommendations” as follows:

[Equity: Everybody’s backyard – promote a fair distribution of attractive, safe and fun parks and public spaces as common civic spaces with facilities and programming open to all ages, race, culture, income, and abilities.]

Page 99: Revise the text under “3.5.4 Proposed Park Locations and Park Recommendations” as follows:

The 2017 and 2022 PROS Plans and the 2018 EPS Plan state that each area master plan should recommend an interconnected system of parks that achieve multiple objectives[. The parks and facilities] whose size, primary and supporting functions should be directly proportional to the projected density and land use patterns of the community.

[Map 21 illustrates the proposed parks and public space’s location within an interconnected network through the Green Loop system. The designation of active recreation destinations, Civic Green, Plaza, Neighborhood Greens, etc., is to highlight the major function emphasis but not to limit other experiences. All parks should provide social, active, and nature-based experiences to the extent possible across the various districts of this Plan.] The ongoing 2022 PROS Plan will be revising the Park Classification system categories associated with the Proposed Park Locations shown in Figure 21. The categories of the proposed park recommendations are based on the 2017 PROS Plan Parks Classification categories: Civic Green, Plaza, Urban Recreational Park, etc.

[The following proposed park locations approach should be applied to Silver Spring:] All urban parks should be strategically located to allow access by walking and biking to and from public transit and other complementary land uses to support the goals of encouraging physical activity, facilitating social connection, accessing nature, and promoting economic prosperity.

For the Sector Plan Area:

- Opportunities for active, contemplative, and social gathering experiences within parks
- Central civic spaces emphasizing social gatherings
- Interconnected system of sidewalks and trails to connect parks and open spaces through the proposed Green Loop

For each Urban District: Recreational amenities that can be accessed by walking or biking.

Map 21 identifies proposed locations for parks and public spaces in the Plan area. The map illustrates how the proposed green space network is connected to the Green Loop. The designation of active recreation destinations, Civic Green, Plaza, Neighborhood Greens, etc., highlights the major function of each open space but does not limit other experiences. All parks should provide social, active, and nature-based experiences to the extent possible.

In addition, the park recommendations also incorporate community input, previous studies, legacy infrastructure, localized needs, location of Experience Improvement Areas (EPS Plan methodology), Equity Focus Areas [(Thrive 2050)] and other factors to deliver the “right parks in the right places.”

[The ongoing 2022 PROS Plan will be revising the Park Classification system categories associated with the Proposed Park Locations approach described above. The categories of the proposed park recommendations are based on the 2017 PROS Plan Parks Classification categories: Civic Green, Plaza, Urban Recreational Park, etc. All urban parks should be located strategically to allow access by walking and biking to and from public transit and other complementary land uses to support the goals of encouraging physical activity, facilitating social connection, accessing nature, and promoting economic prosperity.]

The current naming of the proposed parks and public spaces is subject to change and will be defined during the implementation phase of the project.

Page 101: Revise text under “A – Civic Green” as follows:

Civic Greens are [Parks and Public] spaces that emphasize social gathering. They are flexible[, programmable] gathering spaces for informal or large special events. [Depending on size, they] Civic Greens may support open air markets, concerts, festivals, and/or special events. A large central lawn is often the focus, [sometimes with a complementary plaza space] often accompanied by a plaza with adjacent spaces providing complementary uses such as gardens, water features and/or shade structures. Supporting functions might include opportunities for physical activity and nature enjoyment.

A1 - Create: Ripley District Civic Green

Location: Ripley District

Likely ownership: POPS

Vision: This civic green space will be an important focal point and social gathering space for the Ripley District [and the rest of the Plan]. Its central[ized] location within the Plan[’s] area and along the [proposed] Central Green Loop will serve the highest density commercial and residential areas. [It will connect Georgia Avenue and East-West Highway with improved pedestrian routes and bike trails.] [With] Along with the proposed [Silver Spring Bridge Connector] bridge over the rails, it will [work] serve as a gateway between East-West Highway in [the] South Silver Spring and Georgia Avenue in the Ripley District. [districts connecting opposite sides of the proposed Central Green Loop: East-West Highway to Fenton Street through Silver Spring Avenue. The main programming of this] This space may include a central lawn and will be [flexible] flexibly designed to accommodate a variety of experiences such as community events, unstructured recreation, [in a central lawn, activities that encourage] physical activity, contemplation, and interaction with nature. [and places for nature interaction and contemplation.] [Provide] This space should include a balance of sunny and shaded areas. [Explore] [i]Interactive installations with educational components and/or visitor amenities could be explored.

Recommended Size: 0.5 acre minimum; 1.5 acres ideal

Page 102: Revise the text under “A2- Create: Gene Lynch Civic Green (currently under construction)” as follows:

Vision: This civic green [space] will be an important focal point and social gathering space for the Metro Center District. Its location across from the Transit Center [metro and purple line stations] will function as a gateway to the [central core of] commercial core of downtown Silver Spring [along Colesville Road]. The main programming of this space will be flexible to accommodate community events and unstructured recreation with a central lawn area enclosed by lush plants and trees. [An informal amphitheater space with a large programmable digital display and playful bench swings will serve as a focal point within the park.] A variety of seating options will be provided in both sunny and shaded areas, along with an amphitheater space and bench swings.

Size: 0.25 acres

Page 103: Revise the text under “B - Plaza” as follows:

[These spaces] Plazas often [align with and] complement [the] Civic Greens, [urban parks subcategory. T] as these spaces also emphasize social gathering. [The main differences are (1)] Plazas typically have a central hardscape area rather than a central lawn, and are often located [surrounded by supporting facilities that should encourage physical activity and enjoying nature instead of a central lawn, and (2) the location generally] near transit stops and/or commercial and high[er] density residential areas. [They] Plazas should face streets and building frontages to maximize pedestrian use and exposure and have access to sunlight. [Plazas] They may support activities including open air markets, concerts, festivals, and special events.

Page 103: Revise text under “C- Countywide Urban Recreational Park” as follows:

[Oriented to] A Countywide Urban Recreational Park responds to the recreational needs of surrounding neighborhoods and districts, and [this type of park] provides spaces for [many] a variety of activities. Athletic fields, playing courts, picnic areas, dog parks, sitting areas and flexible grassy open spaces, [may all be common in this park subcategory] all may be found in this type of park. Programming might include farmer’s markets, outdoor exercise classes, and/or community yard sales, etc. There should be [space for] a safe area for vehicular drop-offs and integrated accessible

parking for those who cannot walk to the park. Physical activity is the main emphasis of this park, but social and contemplative opportunities should also be considered.

Page 105: Revise the text under “Concept Plan” as follows:

In response to previous redevelopment recommendations from the 2018 *Energized Public Spaces Functional Master Plan*, along with community input, the Parks Department is developing a concept plan for this park concurrent with the sector plan process. [in order to coordinate its analysis and design to capitalize on the synergies that the planning process can bring to the future redesign and implementation of improvements to this site.] The concept plan will establish a framework for the redevelopment of the park into an active, regional destination with amenities that appeal to a broad range of interests and will be further refined and developed through the Facility Planning process.

[Improve physical and visual access to the park by:] Recommendations:

The following recommendations improve physical and visual access to the park:

- Implement connections to the Green Loop [Connections –] at the southern tip of the proposed [outer loop] Outer Loop as shown on Map 4. [within the green loop system and in the border of] At the border with Washington DC, this park will be an important gateway welcoming people to Maryland. [anchor to bring people from MD and DC together and can be a great model for partnership in edge locations. The outer loop connects to the central loop through Fenton Street and Georgia Avenue.]
- Expand connectivity/visibility and presence of the park along Georgia [Ave - Pursue future partnership with] Avenue by pursuing a partnership with the DC Office of Planning to discuss opportunities to develop the corner sites along Blair Road and Georgia Avenue with active building frontages along the park and open viewsheds to the park along Georgia Avenue.
- [Wayfinding: Signage and Public Art along Fenton Street – Pursue collaboration] Improve wayfinding and signage in and around the park by collaborating with Montgomery College to improve visibility of the [only] entrance to the park along Fenton Street at the pedestrian bridge. Install [signages] signs along the Metropolitan Branch Trail and below the pedestrian bridge with distances to the park to direct users to this entrance. Consider partnering with the owner of the adjacent storage facility [property owner] to install a public art mural to provide more visibility to the entrance ramp area and [instigate] encourage people to cross the bridge to the park [explore what is across the rails. Consider a partnership with MCDOT and Montgomery College to expand the use of the pedestrian bridge to connect bike users to and from the Metropolitan Branch Trail along Fenton Street]. Consider partnering with organizations to create additional art installations across the rail track to attract attention to the location of the park from Fenton Street.
- [Pedestrian Network Improvements:] Consider implementing traffic calming features along Blair Road to make it more safe, accessible, and comfortable. Work with SHA/MCDOT to add new and renovate existing pedestrian crossings: (1) at both ends of the park along Georgia Avenue to improve the pedestrian access and experience across this busy road and connect to the small businesses along that stretch of Georgia Avenue; (2) Blair Road at the edge of the park boundary to allow pedestrians safe crossing to the park entrances along Blair Road [– see proposed location on the illustrative concept and in Section 3.6 – Transportation.] If development patterns along and surrounding Blair Road change in the long term, and traffic calming measurements are already in place, consider working with MCDOT and SHA to study the feasibility of a closure of Blair Road along the perimeter of Jesup Blair Park on

1173 weekends and/or weekdays. It would allow expansion of the proposed active zone at the
 1174 southern portion of the park and would allow direct connection to Juniper Blair
 1175 Neighborhood Park across the street expanding the zone for recreation and social interactions.
 1176

1177 Page 106: Revise the fifth and sixth bullets under “Concept Plan” as follows:
 1178

- 1179 • [Art Crossings:] Work with SHA/MCDOT to consider art treatment of the two pedestrian
 1180 crossings along Georgia Avenue as well as improvements on links to the [POPS] privately owned
 1181 public spaces along Jesup Blair Drive to mark the park entrances and transition to other
 1182 pedestrian destinations.
- 1183 • [Perimeter Fence:] Consider removal and/or reduction of the perimeter fence around the park to
 1184 make the park feel more welcoming. [fenced area to add a welcoming feeling to the perimeter of
 1185 the park].
 1186

1187 Page 106: Delete the seventh bullet under “Concept Plan” as follows:
 1188

- 1189 • [Connection to a potential future transit stop – if development patterns change in the long term
 1190 and the Plan gets updated zoning that would allow higher density near this location increasing
 1191 potential ridership and justifying a return of investment of that scale, consider locating a public
 1192 transit stop near the park.]
 1193

1194 Page 106: Revise the text below the seventh bullet under “Concept Plan” as follows:
 1195

1196 [Promote] The following recommendations promote social connection, diversity, community health,
 1197 identity and sense of ownership of the park:
 1198

- 1199 • [Strong Programming – promote] Promote [a] robust programming for the park with activities
 1200 and events that can appeal to a diverse audience and [invite] will encourage people [back] to
 1201 return to explore the park on their own.
 1202
- 1203 • [Multifunctional Spaces – encourage] Encourage multi-use spaces such as lawn, play areas and
 1204 courts instead of specialized uses. The existing central soccer field would continue to work as a
 1205 central lawn space [(that still hold a good size for informal soccer games)] and multi-age play
 1206 area. The transformation of this local park into a countywide destination park will allow its
 1207 facilities and spaces to serve a much larger and diverse audience during large events. The park
 1208 infrastructure should also consider the daily or weekly use of local residents. Consider including
 1209 space for community-scale events such as performances and farmer’s markets. Allow space for
 1210 kiosks and community bulletin boards, cafes, or street vendors.
 1211

1212 Page 107: Revise the third bullet under “Promote social connection, diversity, community health,
 1213 identity, and sense of ownership” as follows:
 1214

- 1215 • Create a variety of experience zones inside this large park emphasizing active, social and
 1216 contemplative areas, making the park a destination place with many things to do for different age
 1217 groups, interests, and abilities.
 - 1218 ○ Balance the amount of active, contemplative, and social experiences offered. [by the
 1219 existing and new park facilities].

- Inside the contemplative zone consider going beyond the traditional passive uses by introducing active programs such as yoga, tai-chi, and other activities that [can benefit of] would be enhanced by the natural setting[s] of this zone including [its beautiful] a restored [historic building] Jesup Blair House.
- The social zone can accommodate a variety of uses such as café, lawn area, stage/amphitheater, dog park, etc. The active zone may accommodate multi-age play areas, multi-use courts, zip line, skate park, and other activities [that will keep the community physically active].

Page 107: Delete the fourth bullet under “Promote social connection, diversity, community health, identity, and sense of ownership” as follows:

- [Diverse scale of events – consider space for programming and events at different scales to attract diverse users.]

Page 107: Revise the fifth and sixth bullets under “Promote social connection, diversity, community health, identity, and sense of ownership” as follows:

- [Introduce opportunities for public art in the park – promote] Promote partnerships that celebrate cultural aspects of the community through public art. Explore temporary installations (especially along Georgia Ave) and permanent pieces of art in the park. A great example to consider is the synergy between a café in the park and a sculpture area.

[Historic setting – improve connectivity of the historic house and surrounding setting to the rest of park through the implementation of the cultural art loop, and other improvements to this portion of the park to activate the frontage along Georgia Avenue, Blair Road and Jesup Blair Drive to invite people to get inside the park and enjoy its many experiences. Especially in the contemplative zone, promote human and nature synergies – consider interactive design solutions to enhance the relationship between people and elements of the natural environment. Identify future partner who will help activate the contemplative zone potential to attract users to the park and gain community support to steward its unique historic setting and enjoy the many activities inside the park.]

- [Cultural Art Loop – create] Create a signature internal [active, recreational, and cultural art trail loop] active/recreational Cultural Art Loop Trail with mileage [marks] markers, historic signage, art, benches, a mix of shaded and sunny areas and fitness equipment [along its path inside Jesup Blair park] to integrate the existing and future proposed recreational facilities and encourage physical exercise.

Page 107: Insert the following two bullets after the sixth bullet under “Promote social connection, diversity, community health, identity, and sense of ownership” as follows:

- Renovate the historic Jesup Blair House for appropriate community uses that complement the park programming. Identify a future partner who will help activate the contemplative zone to attract users to the park, gain community support to steward the unique historic setting, and enjoy the many activities in the park.

- Improve connectivity of the historic house and surrounding setting to the rest of park through the implementation of the cultural art loop, and other improvements that will activate the frontages along Georgia Avenue, Blair Road and Jesup Blair Drive to invite people into the park.

Page 107: Revise the seventh, eighth, and ninth bullets under “Promote social connection, diversity, community health, identity, and sense of ownership” as follows:

- [Diversify natural features – consider] Consider interactive design solutions to enhance the relationship between people and elements of the natural environment. [the use of] Use a diverse palette of plant and tree species, including native plants, that provide different colors, form, and textures throughout all the seasons. Apply a diverse range of plants to minimize potential expansive destruction from pests or harsh climate conditions.
- [Friends of Jesup Blair Park – engage] Engage the community to [activate this park] support this park through local organizations, such as the Friends of Jesup Blair Park. Create a framework for an inclusive and collaborative approach to promote recreational opportunities to all.
- [Park operating hours –] Consider extending the operating hours of this park beyond sunset and introduce lighting to safely extend the use of the park [Lighting at this park will be a very important item to keep the extended use of the park safe].

Page 109: Revise text under “D - Community Use Urban Recreational Park” as follows:

[These parks] Community Use Urban Recreational Parks serve the immediate neighborhood. This type of park is primarily programmed for physical activity, with supporting programming for social and contemplative opportunities. [Physical activity is the main emphasis of this park, but social and contemplative opportunities should also be considered.] Typical facilities include smaller sport courts, skating spots, [and may include] lawns, playgrounds, or similar neighborhood recreation facilities. Sizes vary considerably depending on the land available and community size served.

D1 – Create: South Silver Spring Park

Ownership: M-NCPPC (acquired in 2021)

[Vision: Key active recreation destination in the South Silver Spring District. Different from Jesup Blair park, this urban recreational] This park will serve as a through-block connection between East-West Highway and Kennett Street at the current location of the National Tire and Battery site. This park will be a key local active recreation destination in South Silver Spring that will serve the surrounding blocks. [Its] The direct connection to the [central green loop] Central Green Loop will link this location [promote complementary active facilities] to other recreational parks [being] proposed in this Plan. Montgomery Parks [is under contract on] acquired one of the two [property] parcels for this park in 2021. [and expects to own the property by the end of 2021]. Its proximity to Acorn Urban Park [can] will promote a synergy of park user experiences[: from] between the historic and contemplative setting of Acorn Park to the more active and social-driven space on the new proposed urban recreational park. Consider working with SHA and MCDOT to [implement a potential outer loop] create a connection between these two parks [using] via the existing sidewalks [between these two parks to allow park users to enjoy this location with different options where to meet as a community to enjoy each other and also] that would benefit the park users and the local businesses [surrounding] proximate to these two parks. [Public façades along Kenneth Street and

East-West Highway – due to location of commercial businesses along East-West Highway and a public parking lot right across the potential new parkland, the park will likely be used as a through-block connection. Plan for tables and seating areas on this park visible from an internal and external loop systems to capture the pedestrian flow generated by other surrounding land uses.] Consider including a seating area with tables that could support the businesses along East West Highway. Recreational amenities that could be considered for this park may include multi-use courts, small open lawn, picnic area, multi-age play area, dog run, exercise stations, etc. Considering potential [partnering] partnerships with adjacent [property owner to create a community art mural on adjacent Caldor/Discovery Channel building blind façade facing the proposed site] properties to align park facilities with neighborhood needs, including opportunities for public art, such as murals, on existing building walls facing the site.

Size: ~1.62 acre

D2 – Create: Downtown North Park

Location: Downtown North District

Likely Ownership: POPS

[Vision: Key] This park will be a key active outdoor recreation destination in the Downtown North [Silver Spring] District, [Create a park during] and will be created along with the redevelopment of the Cameron Garage site as proposed in the Plan. [with focus on active recreation. Its direct connection to] It is located along the Central Green Loop and will connect to [promote] complementary active facilities [to other recreational parks being proposed in this Plan and complement] elsewhere in the Plan as well as to the indoor active recreational experiences offered by the South County Regional Recreation and Aquatic Center [at the opposite corner] across the street (currently under construction).

Recommended Size: minimum 0.5 acre

Page 111: Revise text under “D3 - Create: Metro Center Park” as follows:

[Vision: Key] This park will be a key active recreation destination in the Metro Center District. Its [direct connection to] location along the Central Green Loop will [promote] connect it to complementary [active] facilities [to] at other recreational parks [being] proposed in this Plan. This park will be created along with the redevelopment of the Bonifant-Dixon garage site and may be located on a lower rooftop. [Create park space on proposed roof or create park during redevelopment of site. Amenities, including] The park’s facilities may include multi-use courts and play areas[, are encouraged].

Recommended Size: minimum 0.5 acre

Page 111: Revise text under “D4 - Create: Bonifant Park” as follows:

Location: [Metro Center] Ripley District

Likely Ownership: POPS

[Vision: Create park space on entrance of residential development. Focus] This park will be adjacent to new development in the Ripley District. It will focus on alternative forms of active recreation [in small] at a smaller scale such as: game tables and basketball hoops[, and others surrounding with landscape features to add nature]. [Local] It will serve as a local destination for active experiences in the center of the downtown. [This park can be a great model of introducing active play in smaller spaces.]

Recommended Size: minimum 1/10 acre

Page 111: Revise text under “D5 - Create: Fitness Park (currently approved under The Blairs Master Plan)” as follows:

[Vision: Active] This park will be an active place [inside] as part of the full build-out of the Blairs Master Plan [Development full build-out]. Located off of Portal Drive and alongside an existing surface parking lot, this park will have outdoor fitness equipment for adults and children as well as a sitting area for rest.

Proposed Size: ~ 0.22 acre

Page 112: Revise text under “D6 – Renovate/Repurpose: Ellsworth Park” as follows:

[Vision: Key] This park is an important active recreation destination in the Adjacent Communities District. [Renovate] Further renovation of reclaimed space from brick house [to] will expand awareness of the presence of the park along Colesville Road. [Activate existing public park as part of comprehensive.] Consider programming that is year-round [park activation program]. Currently, the presence of a dog park in this location makes it a regional destination. As more dog parks/runs are developed, this park will more likely serve as a local destination. Improve pedestrian crossing across Colesville Road at the north edge of the park.

Size: 3.6 acres

Page 112: Revise text under “E- Neighborhood Green” as follows:

A Neighborhood Green is [This park is very] a flexible space that [and] supports social connections, physical activities and access to nature. [The] This type of park provides spaces for informal gatherings, lunchtime relaxation, or small special event gatherings. A lawn, play area, shaded seating, pathways, and wooded areas are typical features.

Page 112: Revise text under “E1 – Renovate/Expand: Fenton Street Park” as follows:

[Vision: Gateway] This park will be a gateway to the Fenton Village District. [The expansion of this park to consolidate parkland] As adjacent properties become available, consolidate and expand this park into a cohesive neighborhood park that will promote and [preserve] support the diverse character of this district [with its diverse local shops and smaller scale retailers and attract new park users to this location]. Encourage programming that supports active recreation and social gathering. [Direct] The direct connection to the [central green loop] Central Green Loop will enhance the synergy of this park with other locations. [Keep] Consider keeping and/or expanding the community garden function already on site. [and expand its size in alignment with the recommendations found in Section 3.7.8. Add active and social gathering experiences that will complement each other and serve this community better].

Size: ~ 1.75 acres

Page 112: Revise text under “E2 – Create Fenton Village Park” as follows:

[Vision: Hanging-out space for the community along Fenton Street. This park in conjunction with the Fenton Street Park will activate and serve the district commercial corridors and residents.] This park is envisioned as a community gathering space for Fenton Village. The community will have a

place to [gather] come together and celebrate their diverse [identity and character] identities and cultures of the neighborhood, as well as a place to sit and enjoy food from the many local eateries in Fenton Village. This site [is also] may be large enough [that it may provide a unique opportunity] to consider urban agriculture facilities, either at the ground or as part of a green roof concept in alignment with the recommendations found in Section 3.7.8.
Recommended Size: between 0.3 and 0.5 acre minimum

Page 113: Revise the text under “E3 – Create: Ellsworth District Park” as follows:

[Vision: Green space to] This park will complement and connect to Veteran’s plaza on the site of the expand [Veterans Plaza activities at] existing Whole Foods parking lot. It will be created as part of the redevelopment of the eastern side of the parking lot [Create open space or park during future redevelopment of existing surface parking lot] to meet or exceed open space requirements. [Provide] It will provide green space and/or recreational amenities to complement Veterans Plaza. [Provide] and will provide a mix of shaded and sunny areas with a row of tree canopies facing the main entries and facades of proposed development.
Recommended Size: minimum 0.5 acre

Page 113: Revise the first sentence of the text under “E4 – Create: Rachel Carson, Blair Stomping, The Mews, and Lucy’s Landing Park inside the Blairs Development (currently approved under The Blairs Master Plan)” as follows:

[Vision: Connected] These parks will be a connected system of green open spaces focused on contemplative and social experiences inside the full future buildout of the proposed Blairs Master Plan. [development full build-out.]

Page 113: Revise “E4 - Create: Falkland Park & Connect: Falkland Stream Restoration to north parcel” and related text as follows:

[E4] E5 – Create: Falkland Park & Connect: Falkland Stream Restoration to north parcel

Location: Falklands District

Ownership: POPS

[Vision: Create] This Plan envisions a visual and physical connection between the stream valley renovation in the southeast quadrant of the Falklands to a consolidated public use spaces on the north parcel that is green and clearly public in nature. Consider adding wayfinding signage to address the entries to these publicly accessible spaces. This would be realized along with the redevelopment of the Falkland north parcel.

Size: minimum 0.5 acre of consolidated open space

Page 113: Insert “E6 – Create/Develop: King Street Park” after “E4 - Create: Falkland Park & Connect: Falkland Stream Restoration to north parcel” as follows:

E6 – Create/Develop: King Street Park

Location: South Silver Spring

Ownership: POPS, currently leased to M-NCPPC for the use of the community garden on site. This park will be a neighborhood social gathering place. Consider retaining the existing community

1459 garden at the western end of the site at 7980 Georgia Avenue until the parcel is redeveloped. Upon
 1460 redevelopment consider including a community garden program as part of the open space. Connect
 1461 this park to the proposed through-block connection on this site and to the Outer Green Loop.
 1462 Size: 0.2 – 0.4 acre
 1463

1464 Page 114: Revise the text under “F – Pocket Greens” as follows:
 1465

1466 [This smaller park will] Pocket Greens allow [for] “pauses” with a landscaped setting along the
 1467 streets between larger parks within the parks and public spaces network. [Research has suggested
 1468 that smaller breaks during the work schedule increases productivity and health, especially mental
 1469 health.] Pocket Greens are particularly important in high-density commercial areas to provide spaces
 1470 where workers can take a restorative break during the day. Due to its small scale, the space should
 1471 be highly visible from the street and include protection from wind, traffic, and noise. Pocket Greens
 1472 serve residents, workers and visitors and are appropriate for seating, areas to eat or read, and small
 1473 play or game areas.
 1474

1475 [Pocket Greens are particularly important in areas where commercial activities are taking place. Due
 1476 to its small scale, the space should be highly visible from the street and include protection from the
 1477 wind, traffic and noise allowing workers to take a lunch or coffee break while enjoying a
 1478 contemplative environment with pedestrians strolling by. They can provide seating, areas to eat or
 1479 read, and small play areas or game areas. This park will serve nearby residents, workers, and visitors.]
 1480

1481 Page 114: Revise text under “F1 – Renovate: Acorn Park” as follows:
 1482

1483 [Vision: Contemplative] The existing Acorn Park is a contemplative historic pocket green. Its
 1484 proximity to the proposed South Silver Spring Park [can] will promote a synergy of park [users]
 1485 experiences: from the historic and contemplative setting of Acorn Park to the more active and social-
 1486 driven space on the new proposed recreational park. [Activate public park] Acorn Park could sustain
 1487 more park activation than the existing condition, possibly in partnership with a nearby privately
 1488 owned public space (POPS).
 1489

1490 In Summer 2020, as part of [the Shared] its Open Streets program, MCDOT closed Newell St
 1491 between Kennett Street and East-West Highway to expand recreational opportunities to residents and
 1492 businesses. The street closure allows users of Acorn Park to enjoy a larger play area increasing park
 1493 usage and stewardship of its historic asset. [Newell Street can function as a temporary park until the
 1494 proposed South Silver Spring Park project gets implemented.] In Section 3.6.5 of this Plan, this street
 1495 segment is recommended to be a shared street in the future.
 1496

1497 Page 114: Delete “F3 – Retain King Street Park” as follows:
 1498

1499 [F3 – Retain: King Street Park
 1500 Location: South Silver Spring
 1501 Ownership: POPS, leased to M-NCPPC
 1502 Vision: Retain existing community garden at the western end of the parcel on the 7980 Georgia
 1503 Avenue site as part of any redevelopment opportunity. Connect community garden to proposed
 1504 through-block connection.
 1505 Size: ~0.38 acre]
 1506

Page 115: Revise text under “Temporary/Interim Park” as follows:

A temporary park is a type of park created for a certain period of time in a location that is not currently planned as future public space. There is generally a beginning and an end time established. *An interim park* is implemented in a short time frame on a site that is actively being designed for a future permanent public space. Both temporary and interim parks can provide opportunities to partner with local businesses and non-profits to engage with the community in creative ways to deliver places for social gathering, active or contemplative experiences while building a sense of community and belonging at the location. When feasible, new parks should consider implementation of temporary or interim parks. For parks implementation strategies and park ownership recommendations, see Section 4.3. [a type of park created to bridge the time gap between design, funding, and construction of a permanent public space and can accommodate temporary uses until resources for permanent uses become available. Owners and/or county staff can use these temporary facilities to collect information on programming and functions that will serve a future permanent public space. Temporary parks can also provide opportunities to partner with local businesses and non-profits to engage with the community in creative ways to deliver places for social gathering, active or contemplative recreation while building a sense of community and belonging.]

Page 115: Delete “3.5.5. Parkland Ownership” as follows:

[3.5.5. Parkland Ownership

As the major parkland stakeholder in the county, Montgomery Parks is committed to increasing the number of parks and open spaces to secure public access, especially in urban areas such as Silver Spring. However, the higher cost in acquiring land, in addition to the maintenance and operation of these urban spaces with active programming, comes with a high price tag. Montgomery Parks will be strategic in determining parks and open spaces ownership. Below are some parameters that will be considered but not limited to define parkland ownership:

- If acquisition is the only way to create a cohesive public space when the parcel is less likely be part of a development proposal,
- If the recommended facilities most likely require user permits, such as athletic courts, or
- If the surrounding parcels are individually owned, therefore, it is unreasonable to expect common management of the space, unless another entity is available to manage the space.]

Page 119: Revise the fourth bullet under “Recommendations” as follows:

- [The] If the Montgomery College bridge should ever need to be reconstructed, its replacement should be [widened or otherwise improved] constructed with additional width to allow the use of bicycles for adequate access to the Metropolitan Branch Trail and upcoming Fenton Street separated bike lanes.
- Bicyclists should [also] be permitted to use existing trails like the outer loop of Jesup Blair Park for transportation and recreational cycling.

Page 120: Revise Table 9. Bikeway Network Recommendations, as follows:

Table 9. Bikeway Network Recommendations

Street	From	To	Facility Type	Bikeway Type
Burtonsville to Silver Spring Breezeway				
Ellsworth Drive	Cedar Street	[Fenton Street] Veteran's Plaza	Shared Road	Shared Street
<u>Ellsworth Drive</u>	<u>Veteran's Plaza</u>	<u>Fenton Street</u>	<u>Separated Bikeway</u>	<u>Separated Bike Lanes</u>
Capital Crescent Trail Breezeway				
Capital Crescent Trail	Spring Street	Silver Spring Transit Center	Trail	Off-Street Trail
Glenmont to Silver Spring Breezeway				
Fenton St Extended	Spring St	Cameron St	Trail	Off-Street Trail
Fenton St	Cameron St	Ellsworth Dr	Separated Bikeway	Separated Bike Lanes (Two-Way, West Side)
Metropolitan Branch Trail Breezeway				
Metropolitan Branch Trail	Silver Spring Transit Center	Silver Spring/Takoma Park (East) Policy Area	Trail	Off-Street Trail
Additional Recommendations				
13th St	District of Columbia	Georgia Ave (MD 97)	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
16th St (MD 390)	Spring St	District of Columbia	Separated Bikeway	Separated Bike Lanes (Two-Way, East Side)
2nd/Wayne Ave	Spring St	Colesville Road (MD 384)	Separated Bikeway	Separated Bike Lanes (Two-Way, East Side)
	Colesville Road (MD 384)	Cedar Street	Separated Bikeway	Separated Bike Lanes (Two-Way, North Side)
Burlington Ave (MD 410)	Georgia Ave (MD 97)	Fenton Street	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
Cameron Street	Spring Street	2nd Ave	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
Colesville Rd (MD 384)	16th St (MD 390)	Draper Lane	Separated Bikeway	Separated Bike Lanes (Two-Way, North Side) and Sidepath (South Side)
Colesville Rd (MD 384)	Draper Lane	East West Hwy (MD 410)	Separated Bikeway	Separated Bike Lanes (Two-Way, Both Sides)
Colesville Rd (MD 384)	East West Hwy (MD 410)	Sarbanes Transit Center Entrance	Separated Bikeway	Separated Bike Lanes (Two-Way, Both Sides)
Colesville Rd (MD 384)	Sarbanes Transit Center Entrance	Georgia Ave (MD 97)	Separated Bikeway	Separated Bike Lanes (Two-Way, Both Sides)
Dixon Ave	Wayne Ave	Georgia Ave (MD 97)	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
East-West Hwy (MD 410)	16th St (MD 390)	Colesville Road (MD 384)	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
	Colesville Road (MD 384)	Georgia Ave (US 29)	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
Fenton Street	Ellsworth Drive	King Street	Separated Bikeway	Separated Bike Lanes (Two-Way, West Side)
Metropolitan Branch Trail/ King St (Interim)	Railroad Tracks	Fenton Street	Separated Bikeway	Sidepath (South Side)
Metropolitan Branch Trail/ Fenton St (Interim)	King St	New York Ave	Separated Bikeway	Sidepath (West Side)

Newell Street	District of Columbia	East West Hwy (MD 410)	Striped Bikeway	Conventional Bike Lanes
Philadelphia Ave/Gist Ave	Selim Rd	Silver Spring/Takoma Park (East) Policy Area	Shared Road	
Selim Rd	Philadelphia Ave	Metropolitan Branch Trail	Separated Bikeway	Sidepath (West Side)
Silver Spring Ave	Fenton Street	Silver Spring/Takoma Park (East) Policy Area	Shared Road	Priority Shared Lane Markings
Silver Spring Avenue	Georgia Avenue	Fenton Street	Separated Bikeway or Striped Bikeway	Separated Bike Lanes (One-Way, Both Sides)
Spring St/Cedar St	16th St (MD 390)	Wayne Ave (MD 594-A)	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
Blair Mill Road	Eastern Avenue	East-West Highway (MD 410)	Separated Bikeway	Separated Bike Lanes (two-way South Side)
<u>1st Street</u>	<u>Spring Street</u>	<u>Fenwick Lane</u>	<u>Separated Bikeway</u>	<u>Separated Bike Lanes (One-Way, Both Sides)</u>
1st Ave Extension (B-30)	[1st Avenue] <u>Fenwick Lane</u>	Ramsey Avenue	Separated Bikeway	Separated Bike Lanes (One-Way, Both Sides)
New street name TBD (B-31)	Bonifant Street	Thayer Avenue	[Separated Bikeway] <u>Shared Road</u>	[Separated Bike Lanes (One-Way, Both Sides)]
New street name TBD (B-32)	Silver Spring	Sligo Avenue	[Separated Bikeway] <u>Shared Road</u>	[Separated Bike Lanes (One-Way, Both Sides)]

Page 126: Insert the following after the seventh bullet under “Recommendations”:

- Create the following new pedestrian connections:
 - Fenton Street to Mayor Lane (along with the redevelopment of Parking Lot 4 site in Fenton Village), and
 - Bonifant Street to Wayne Avenue.

Page 129: Revise the first and seventh bullets under “Recommendations” as well as the text at the bottom of the page as follows:

- This Plan confirms BRT stations and routes for the Georgia Avenue South Bus Rapid Transit Route (Corridor 2) in the 2013 Countywide Transit Corridor Functional Master Plan. [Dedicated bus lanes should be provided along Georgia Avenue and space for dedicated bus lanes should come from repurposing existing general-purpose traffic lanes.]
- Evaluate the feasibility of increasing rapid transit service along Georgia Avenue to and from Jesup Blair Park [a new Metrorail station at Jesup Blair Park in even that future redevelopment of Jesup Blair Park spurs land use and development changes in the blocks surrounding the park].

[Note: The cross section proposes one potential alignment that should be further evaluated for implementation. The intent of the cross section is to demonstrate that there is support to reallocate travel lanes for exclusive transit use and that such alignment can fit within the master-planned right-of-way.]

Page 130: Revise text under “3.6.5 Roadways” as follows:

The Plan area has a well-connected road network of high and low volume streets that provide access to and within this vibrant community. This Plan focuses on ensuring safety for all users, especially pedestrians, and further improving connectivity and circulation throughout the road network. At the time of this Plan's approval, the 2021 Complete Streets Design Guide (CSDG), a countywide policy developed jointly by the Planning Department and the Montgomery County Department of Transportation (MCDOT) has been approved, but the CSDG recommendations to updating County Code Chapter 49 ("Road Code") have not been codified. For this reason, the Plan includes roadway classifications per the 2018 Master Plan of Highways and Transitways and the County's Rode Code as well as the CSDG in anticipation of updates to the County Road Code.

Page 130: Revise the second and third sub-bullets to the first bullet under "Recommendations" as follows:

- Create a new street connecting Bonifant Street to Thayer Avenue. This connection will improve connectivity within the corridor from Bonifant Avenue to Sligo Avenue. This street could be private or dedicated to public right-of-way. The design of the street should match that of Private Street A that was built as part of the Studio Plaza project, which connects Thayer Avenue and Silver Spring Avenue.
- As part of the redevelopment of the Parking Lot 4 site and adjacent parcels (if the existing parking structure is removed), create a new street through the existing block from Sligo Avenue to Silver Spring Avenue [that aligns with the north-south mid-block connection to the north]. This street, designed to accommodate both pedestrians and vehicles, could be private or dedicated to public right-of-way.

Page 130: Insert the following text between the second and third bullets under "Recommendations" as follows:

Shared streets are designed to create an environment that encourages low vehicle speeds and prioritizes pedestrians. Shared Streets are often curbsless, providing pedestrians with freedom of movement and creating optimal spaces for special events. They can support a variety of land uses, including commercial, entertainment, dining, and residences. Shared Streets should include strategically defined edges and zones, and unique paving materials where feasible. Designs should allow for flexibility, so that streets can be easily closed to automobile traffic for events and reconfigured to support a wide range of social and cultural functions. Streetscape elements must facilitate navigation by pedestrians with vision disabilities, as shared streets allow free-form movement through all spaces for pedestrians and bicyclists. Tactile surfaces should indicate pedestrian-only zones and safe crossings.

Page 130: Delete the third bullet under "Recommendations" as follows:

- [Create the following new pedestrian connections:
 - Fenton Street to Mayor Lane (along with the redevelopment of Parking Lot 4 site in Fenton Village), and
 - Bonifant Street to Wayne Avenue.]

Page 134: Revise Table 10. Street Classifications and ROW Recommendations as follows:

Table 10. Street Classifications and ROW Recommendations^{1,2,3}

Roadway	From	To	Functional Classification	Complete Streets Design Guidelines Street Type	Master Planned ROW (Minimum)	Existing Lanes	Planned Lanes	BRT Type
Major Highway								
Georgia Ave (M-8)	Spring Street	Colesville Road	Major Highway	Downtown Boulevard	126'	6D	[6D] <u>4D + 2T</u>	Dedicated
Georgia Ave (M-8)	Colesville Road	Wayne Avenue	Major Highway	Downtown Boulevard	120'	6-6D	6-6D	None
Georgia Ave (M-8)	Wayne Avenue	[Blair Mill Road] Sligo Avenue	Major Highway	Downtown Boulevard	125'-140'	6-6D	[6-6D + 0T] <u>4-4D + 2T</u>	Dedicated
<u>Georgia Ave (M-8)</u>	<u>Sligo Avenue</u>	<u>Blair Mill Road</u>	<u>Major Highway</u>	<u>Downtown Boulevard</u>	<u>125'-140'</u>	<u>6-6D</u>	<u>4-4D + 2T</u>	<u>Dedicated</u>
Georgia Ave (M-8)	Blair Mill Road	Eastern Avenue	Major Highway	Downtown Boulevard	125'	6-6D	[6-6D + 0T] <u>4-4D + 2T</u>	Dedicated
16th Street (M-9)	Colesville Road	East-West Highway (MD 410)	Major Highway	Downtown Boulevard	120'	6D	[6D] <u>4D</u>	None
16th Street (M-9)	East-West Highway	630' South of Lyttonsville Road	Major Highway	Town Center Boulevard	120'	6D	<u>4D</u>	None
Colesville Road (M-10)	SS Transit Station Entrance	Eastern Avenue/16th Street	Major Highway	Downtown Boulevard	125'	6	[6 + 0T] <u>4 + 2T</u>	Dedicated
Colesville Road (M-10)	Georgia Avenue (MD 97)	SS Transit Station Entrance	Major Highway	Downtown Boulevard	125'	6	[6 + 0T] <u>4 + 2T</u>	Dedicated
Colesville Road (M-10)	Fenton Street	Georgia Avenue	Major Highway	Downtown Boulevard	100'	5-6	[5-6 + 0T] <u>3-4 + 2T</u>	Dedicated
Colesville Road (M-10)	[Sligo Creek Parkway] <u>North Noyes Drive</u>	Fenton Street	Major Highway	Downtown Boulevard	120'	5-6	[5-6 + 0T] <u>3-4 + 2T</u>	Dedicated
East-West Highway (M-20)	16th Street	300' East of 16th Street	Major Highway	Downtown Boulevard	118'	4	4	None
East-West Highway (M-20)	300' East of 16th Street	Blair Mill Road	Major Highway	Downtown Boulevard	110'	4	3	None
East-West Highway (M-20)	Blair Mill Road	Blair Mill Way	Major Highway	Downtown Boulevard	90'	2-4	3	None
East-West Highway (M-20)	Blair Mill Way	Georgia Avenue	Major Highway	Downtown Boulevard	84'	2-4	3	None
Burlington Ave (M-20)	Georgia Ave	Fenton St	Major Highway	Downtown Street	80'	3	3	None
Arterial								
Spring St (A-263)	16th St (MD 390)	630' west of Second Ave	Arterial	Downtown Street	100'	4D	2D	None
Spring St (A-263)	630' west of Second Ave	First Ave	Arterial	Downtown Street	100'	2D	2D	None

Spring St (A-263)	First Ave	Georgia Ave	Arterial	Downtown Street	100'	2D	2D	None
Spring St (A-263)	Georgia Ave	Fairview Rd	Arterial	Downtown Street	100'	2D	2D	None
Spring St (A-263)	Fairview Rd	Cameron St	Arterial	Downtown Street	80'	2	2	
Spring St (A-263)	Cameron St	Colesville Rd	Arterial	Downtown Street	100'	2	2	None
Spring St (A-263)	Colesville Rd	Ellsworth Dr	Arterial	Downtown Street	80'	4-4D	2	None
Spring St (A-263)	Ellsworth Dr	Wayne Ave (MD 594-A)	Arterial	Downtown Street	80'	2	2	None
Fenton St (A-264)	Cameron St	Takoma Avenue	Arterial	Downtown Street	80'	2	2	None
Wayne Ave (A-76)	Colesville Rd (MD 384)	Georgia Ave (MD 97)	Arterial	Downtown Street	120'	4D	4	Mixed Traffic
Wayne Ave (A-76)	Georgia Ave (US 29)	Cedar Street	Arterial	Downtown Street	80'	4-4D	4	None
[Wayne Ave (A-76)]	[Fenton St]	[Cedar St]	[Arterial with planned light rail]	[Downtown Street]	[70-100']	[4]	[2 + 2T]	[None]
Wayne Ave (A-76)	Cedar St	Sligo Creek Pkwy	Arterial with planned light rail	Neighborhood Connector	[70 – 100'] <u>80'</u>	4	2 + 2T	None
Minor Arterial								
Dale Dr (MA-16)	Wayne Ave	Piney Branch Rd	Minor Arterial	Neighborhood Connector	70'	2	2	None
Sligo Ave (MA-35)	Approx. 149' east of Fenton St	Piney Branch Rd	Minor Arterial	Neighborhood Connector	50'	2	2	None
Business								
1st Ave (B-1)	Spring St	Fenwick Ln	Business	Downtown Street	70'	2	2	None
1st Ave (B-1)	Fenwick Ln	Cameron St	Business	Downtown Street	70'	0	2	None
2nd Ave (B-2)	Spring St	Cameron St	Business	Downtown Street	80'	2	2	None
2nd Ave (B-2)	Cameron St	Colesville Rd (MD 384)	Business	Downtown Street	105'	4	4	None
13th St (B-3)	Georgia Ave	Eastern Ave (D.C. Line)	Business	Downtown Street	80'	2	2	None
Apple Ave (B-4)	CSX Railroad	Second Ave	Business	Downtown Street	60'	2	2	None
Blair Mill Rd (B-5)	Blair Mill Way	Georgia Ave	Business	Downtown Street	60'	2	2	None
Blair Rd (B-6)	Georgia Ave (MD 97)	CSX Railroad	Business	Downtown Street	60'	2	2	None
Bonifant St (B-7)	CSX Railroad	[Purple Line] Ramsey Avenue	Business with planned light rail	Downtown Street	70'	2	2	None
[Bonifant St (B-7)]	[Purple Line]	[Fenton St]	[Business with planned light rail]	[Downtown Street]	[40']	[2]	[2 + 2T]	[None]
Bonifant St (B-7)	[Purple Line]	Fenton St	Business with	Downtown Street	[40 – 70'] <u>70'</u>	2	2 + 2T	None

	Georgia Avenue		planned light rail					
Bonifant St (B-7)	Fenton St	Approx. 488' east of Fenton St	Business	Downtown Street	80'	2	2	None
[Colonial Ln (B-9)]	[Ripley St]	[Georgia Ave]	[Business]	[Downtown Street]	[80']	[N/A]	[4]	[None]
Dixon Ave Extended (B-10)	Ripley St	Silver Spring Ave	Business (Planned)	Downtown Street	80'	N/A	4	None
[Draper Ln (Proposed) (B-11)]	[Colesville Rd]	[Blair Mill Rd]	[Business (Planned)]	[Downtown Street]	[60']	[N/A]	[2]	[None]
Ellsworth Dr (B-12)	[Cedar St] <u>Fenton Street</u>	450' west of Fenton St	Business	Downtown Street	70'	2	2	None
<u>Ellsworth Dr (B-12)</u>	<u>Cedar Street</u>	<u>Veteran's Plaza</u>	<u>Business</u>	<u>Downtown Street</u>	<u>70'</u>	<u>2</u>	<u>2</u>	<u>None</u>
Fenwick Ln (B-13)	Georgia Ave	Second Ave	Business	Downtown Street	80'	2	2	None
Fidler Ln (B-14)	Second Ave	Georgia Ave	Business	Downtown Street	Varies	2	2	None
Gist Ave (B-15)	Philadelphia Ave	Fenton St	Business	Downtown Street	70'	2	2	None
Kennett St (B-16)	Newell St	13th St	Business	Downtown Street	60'	2	2	None
King St (B-17)	Eastern Ave (D.C. Line)	50' east of Georgia Ave and From B & O Railroad to Albany Av	Business	Downtown Street	60'	2	2	None
Newell St (B-18)	Eastern Ave (D.C. Line)	[East-West Highway] <u>Kennett Street</u>	Business	Downtown Street	70'	2	2	None
Planning Pl (B-19)	Georgia Ave	Silver Spring Parking Lot # 2	Business	Downtown Street	60'	2	2	None
Philadelphia Ave (B-20)	Selim Rd	Fenton St	Business	Downtown Street	70'	2	2	None
Ramsey Ave (B-21)	Cameron St	Colesville Rd (MD 384)	Business	Downtown Street	54'	2	2	None
Ramsey Ave (B-21)	Wayne Ave	Bonifant St	Business	Downtown Street	70'	2	2	None
Ripley St (B-22)	Georgia Ave	Bonifant Street	Business	Downtown Street	70'	2	2	None
[Ripley St (B-22)]	[End of existing Street]	[Bonifant Street]	[Business]	[Downtown Street]	[70']	[2]	[2]	[None]
Roeder Rd (B-23)	Fenton St	Cedar St	Business	Downtown Street	60'	2	2	None
Selim Rd (B-24)	Sligo Ave	Philadelphia Ave	Business	Downtown Street	70'	2	2	None
Blair Mill Rd (B-25)	Eastern Ave (D.C. Line)	East-West Hwy (MD 410)	Business	Downtown Street	70'	2	2	None
Silver Spring Ave (B-25)	Georgia Ave	Approx. 280' east of Fenton St	Business	Downtown Street	70'	2	2	None
Sligo Ave (B-26)	Fenton St	Approx. 149' east of Fenton St	Business	Downtown Street	80'	2	2	None

Sligo Ave (B-26)	Georgia Ave	Fenton St	Business	Downtown Street	70'	2	2	None
Thayer Ave (B-27)	Fenton St	Approx. 288' east of Fenton St	Business	Downtown Street	70'	2	2	None
Thayer Ave (B-27)	Georgia Ave	Fenton St	Business	Downtown Street	60'	2	2	None
Cameron St (B-28)	Georgia Ave	Spring St	Business	Downtown Street	75'	2	2	None
Cameron St (B-28)	Second Ave	Georgia Ave	Business	Downtown Street	74'	2	2	None
Dixon Ave (B-29)	Wayne Ave	Ripley St	Business	Downtown Street	80'	2	2	None
[1st Ave Extension (B-30)]	[1st Avenue]	[Ramsey Avenue]	[Business]	[Downtown Street]	[70']	[2]	[2]	[None]
New street name TBD [(B-31)] B-30	Bonifant Street	Thayer Avenue	Business	Downtown Street	[70'] TBD	[2] 0	2	None
New street name TBD [(B-32)] B-31	Silver Spring Avenue	Sligo Avenue	Business	Downtown Street	[70'] TBD	[2] 0	2	None
Shared Streets								
<u>Bonifant St (B-7)</u>	<u>Ramsey Avenue</u>	<u>Georgia Avenue</u>	<u>Business with planned light rail</u>	<u>Shared Street</u>	<u>40-70'</u>	<u>2</u>	<u>2</u>	<u>None</u>
<u>Ellsworth Dr (B-12)</u>	<u>Veteran's Plaza</u>	<u>Fenton Street</u>	<u>Business</u>	<u>Shared Street</u>	<u>70'</u>	<u>2</u>	<u>2</u>	<u>None</u>
<u>Newell St (B-18)</u>	<u>Kennett Street</u>	<u>East-West Hwy (MD 410)</u>	<u>Business</u>	<u>Shared Street</u>	<u>70'</u>	<u>2</u>	<u>2</u>	<u>None</u>

Page 137: Add a third Table Note under "Table Notes" as follows:

3. Minimum rights-of-way do not include lanes for turning, parking, acceleration, deceleration, or other purposes auxiliary to through travel. Additional rights-of-way may also be needed to accommodate master planned bicycle and transit facilities, including Protected Intersections, the envelopes of transit stations, and pedestrian crossing refuges.

Pages 138-140: Delete section titled "Street Cross Sections".

Page 143: Revise the second bullet at the top of the page under "Recommendations" for "3.6.7. Transportation Demand Management (TDM)" as follows:

- Expand the NADMS goal to apply to [both] all commuters, both employees commuting into TMD for jobs, and residents of the TMD commuting from their homes to jobs. Increase the NADMS goal to a Blended Average [and residents and increase to a combined average] of [60] 65 percent for both groups. This goal may be re-evaluated and amended in the future as part of the Growth and Infrastructure Policy.

Page 143: Insert a new section titled "3.6.9. Street Cross Sections" as follows:

3.6.9. Street Cross Sections

Cross sections were developed for street segments within the Sector Plan boundary that are recommended for dedicated transit lanes or designated bikeways that would necessitate road diets or other right-of-way reallocations. A cross section was also developed for new street B-30 (1st Avenue Extended).

These cross sections incorporate policy and design guidance from the Complete Streets Design Guide. The intent of providing these cross sections is to offer conceptual direction for future Capital Improvement Projects (CIP) and development regulatory applications, demonstrating that there is sufficient master-planned right-of-way to meet the objectives of the Complete Streets Design Guide¹. Further study of traffic operations will be necessary, and therefore the ultimate cross section may differ from what is recommended in the Sector Plan. For example, dedicated Bus Rapid Transit (BRT) lanes are recommended on Colesville Road and Georgia Avenue. Two sets of cross sections for each corridor from north to south were developed that envision either curb running or median running alignments of the bus rapid transit lanes. Future studies of traffic operations will determine which alignment is preferred (or if a new hybrid would be more feasible) and will refine the geometric design of the right-of-way.

The street sections are organized in the following manner:

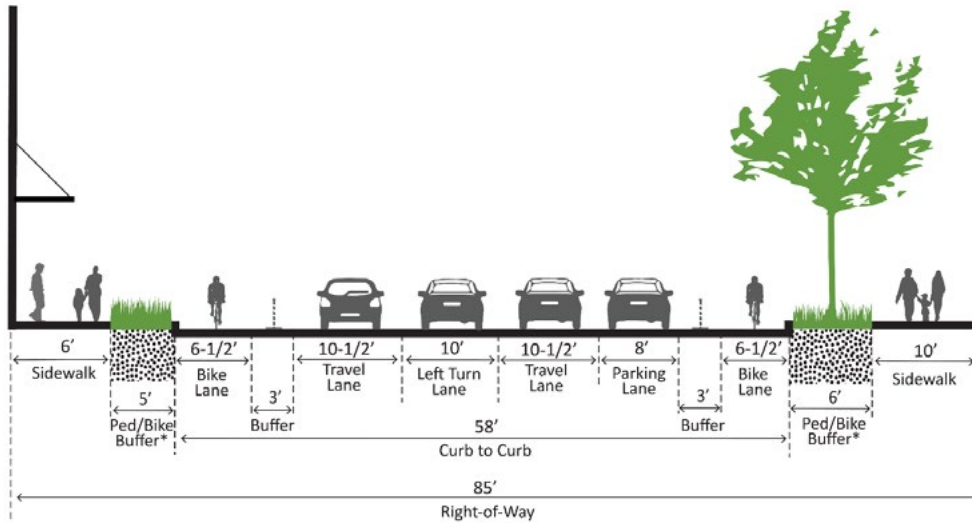
- East-West Highway sections
 - Interim Conditions
 - Ultimate Conditions
- Colesville Road Sections
 - Median-Running BRT
 - Curb-Running BRT
- Georgia Avenue Sections
 - Curb-Running BRT
 - Median-Running BRT
- Additional Street Sections

East-West Highway Sections

Designated bicycle lanes are recommended on East-West Highway. The Sector Plan recommends fitting the bicycle facilities into the right-of-way with a road diet, which reallocates a travel lane to the bikeway and street buffer. This road diet may be achieved with a CIP project or redevelopment. If the CIP project is implemented first, an interim condition is envisioned that fits the separated bikeway within the existing curb widths (Interim). As redevelopment occurs, the curbs can be relocated inward, and the bike lane can be relocated to the streetscape (Ultimate).

¹ The Sector Plan confirms all minimum master-planned rights-of-way widths in the 2018 *Master Plan of Highways and Transitways*. The Sector Plan is not making any recommendations to widen what is currently master planned.

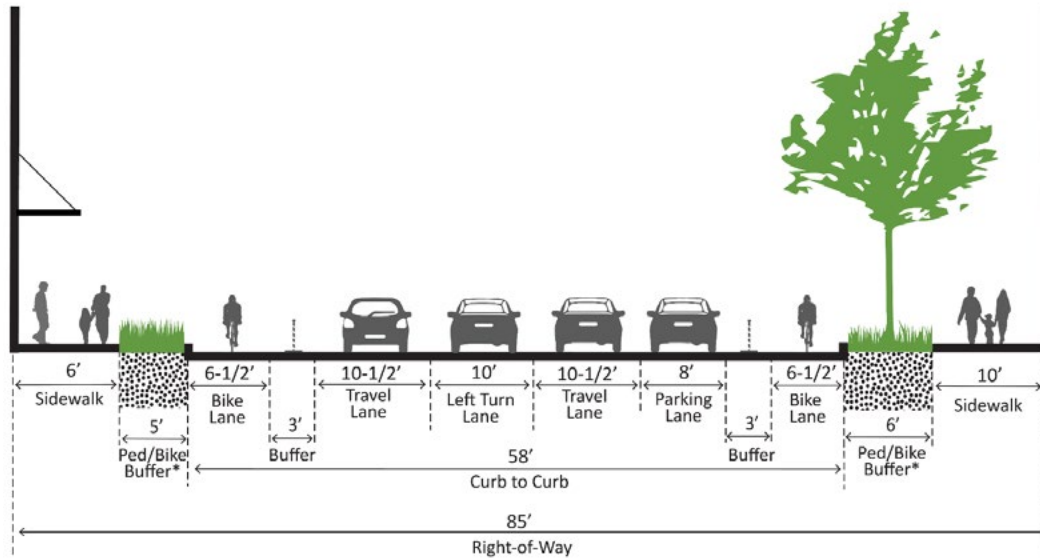
1685 *Interim Condition: Figures 1, 2, and 3.*



* Buffers with SWM to employ Best Management Practices

1686

1687 Figure 1. East-West Highway (M-20): 16th Street to Blair Mill Way, Looking North



* Buffers with SWM to employ Best Management Practices

1688

1689 Figure 2. East-West Highway (M-20): Blair Mill Road to Blair Mill Way, Looking North

1690

1691

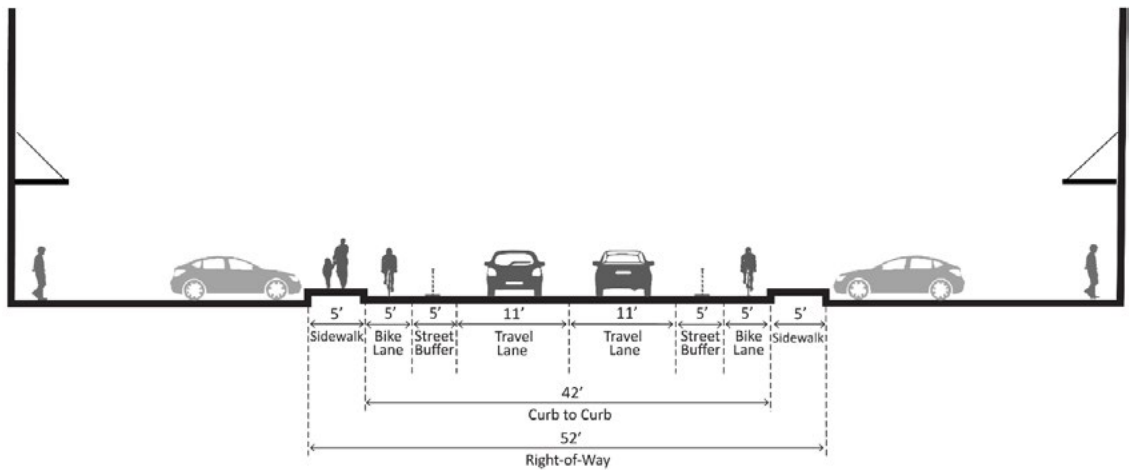
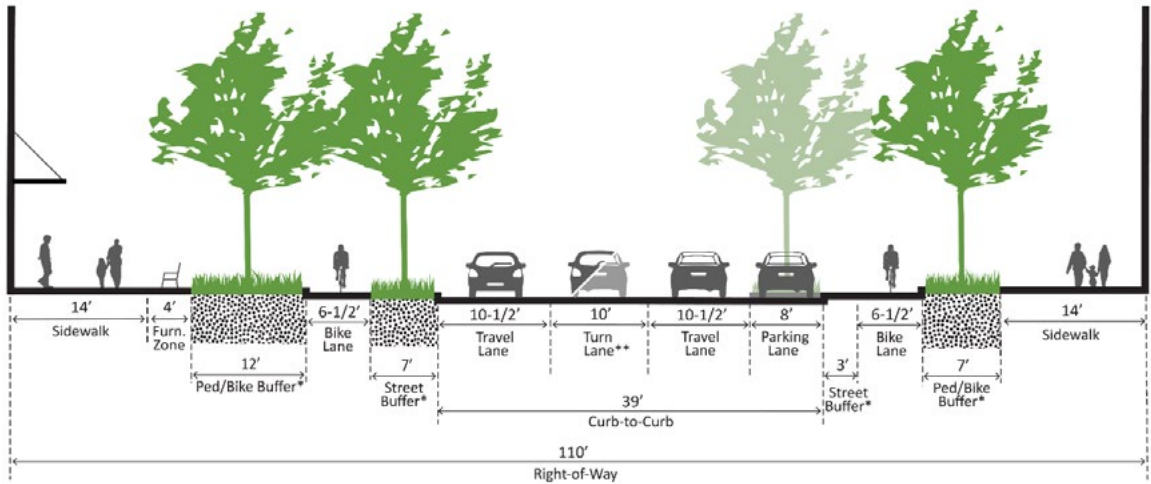


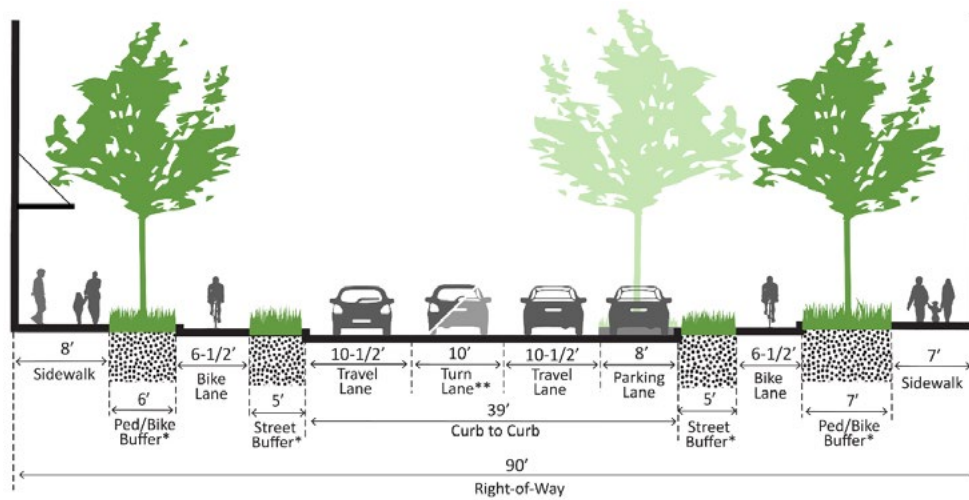
Figure 3. East-West Highway (M-20): Blair Mill Way to Georgia Avenue, Looking North

Ultimate Condition: Figures 4, 5, and 6



* Buffer with SWM to employ Best Management Practices
** Note: The center lane functions as a left turn lane at the intersection in the corresponding direction.
Note: This section holds the existing curb alignment on the side of East-West Highway adjacent to the Metrorail/CSX tracks.

Figure 4. East-West Highway (M-20): 16th Street to Blair Mill Road, Looking North



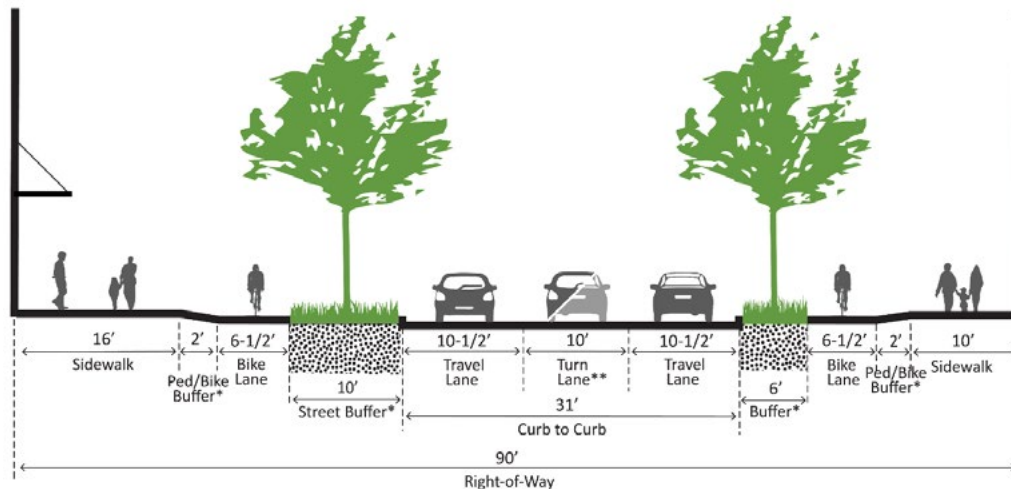
* Buffer with SWM to employ Best Management Practices

** Note: The center lane functions as a left turn lane at the intersection in the corresponding direction.

Note: This section holds the existing curb alignment on the side of East-West Highway adjacent to the Metrorail/CSX tracks.

1699

1700 Figure 5. East-West Highway (M-20): Blair Mill Road to Blair Mill Way, Looking North



* Buffer with SWM to employ Best Management Practices

** Note: The center lane functions as a left turn lane at the intersection in the corresponding direction.

Note: This section holds the existing curb alignment on the side of East-West Highway adjacent to the Metrorail/CSX tracks.

1701

1702 Figure 6. East-West Highway (M-20): Blair Mill Way to Georgia Avenue, Looking North

1703

1704 Colesville Road Sections

1705 A median-running BRT alignment is not provided for the two southernmost segments of Colesville
 1706 Road (Sarbanes Transit Center to East-West Highway and East-West Highway to 16th Street/Eastern
 1707 Avenue). This is because the segment between the Sarbanes Transit Center and East-West Highway

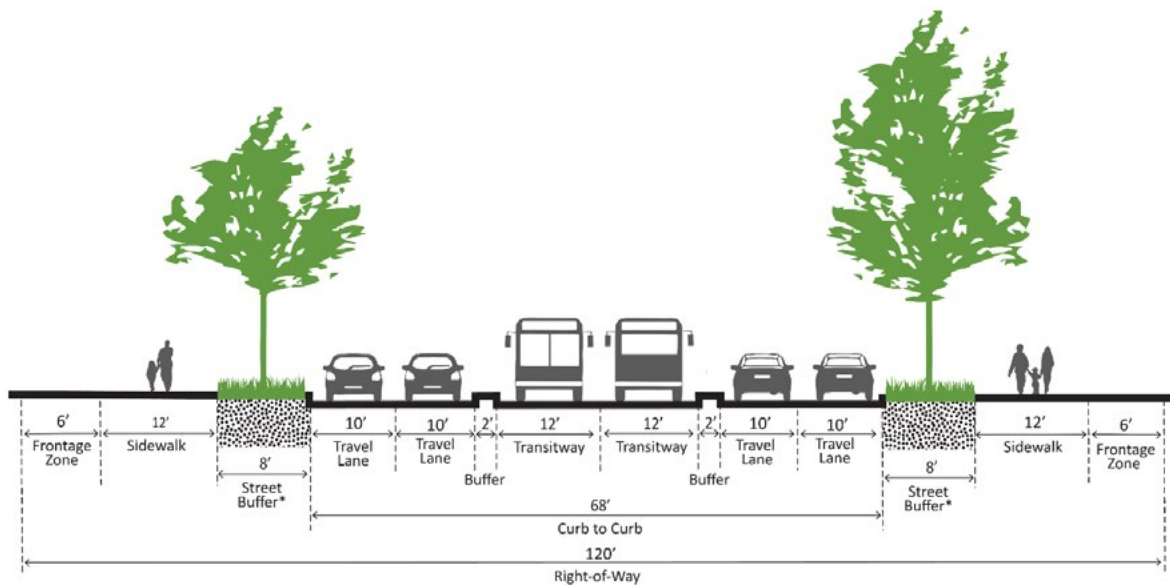
traverses under the CSX/WMATA/MARC elevated lanes. The “median” is therefore inflexible in placement and minimum width. It was decided that since such a relatively short segment remains further south when traveling towards the Sector Plan border with the District of Columbia, it didn’t make sense to transition the buses back to median running, south of East-West Highway.

This is because the segment between the Sarbanes Transit Center and East-West Highway traverses under the CSX/WMATA/MARC elevated lanes. The median is therefore inflexible in placement and minimum width. It was decided that since it is a relatively short segment between the Transit Center and the Sector Plan border at the District of Columbia, it didn’t make sense to transition the buses back to median running south of East-West Highway.

The BRT lanes are envisioned to tie-in with the District of Columbia’s vision for dedicated BRT lanes on 16th Street, approaching Silver Spring.

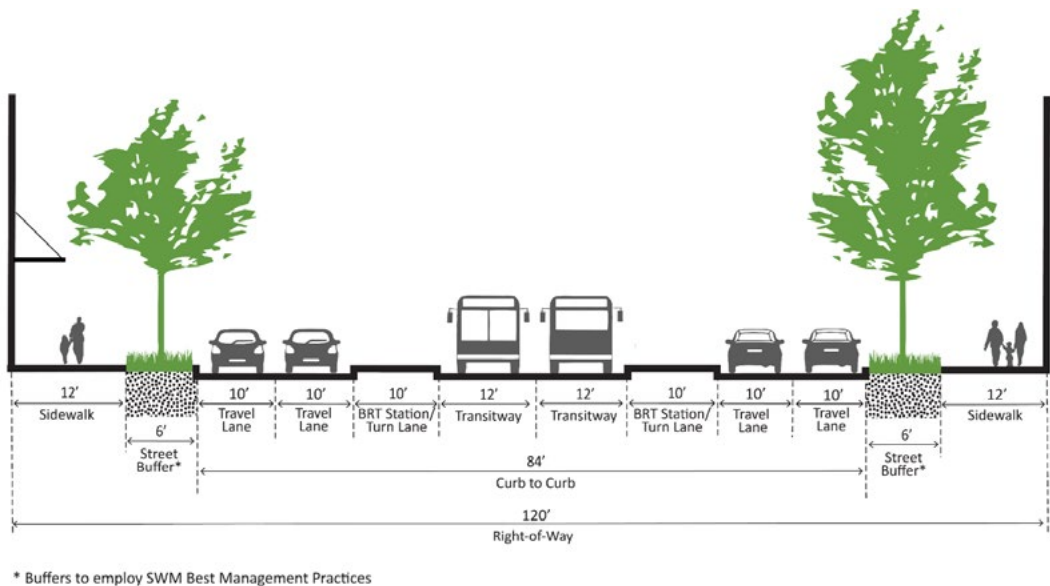
The travel lanes on Colesville Road are consistently 10-feet wide in the existing condition and for that reason, both alignments envision 10-foot travel lanes to minimize the curb-to-curb widths.

Median-Running Bus Rapid Transit: Figures 7 - 10



* Buffers with SWM to employ Best Management Practices

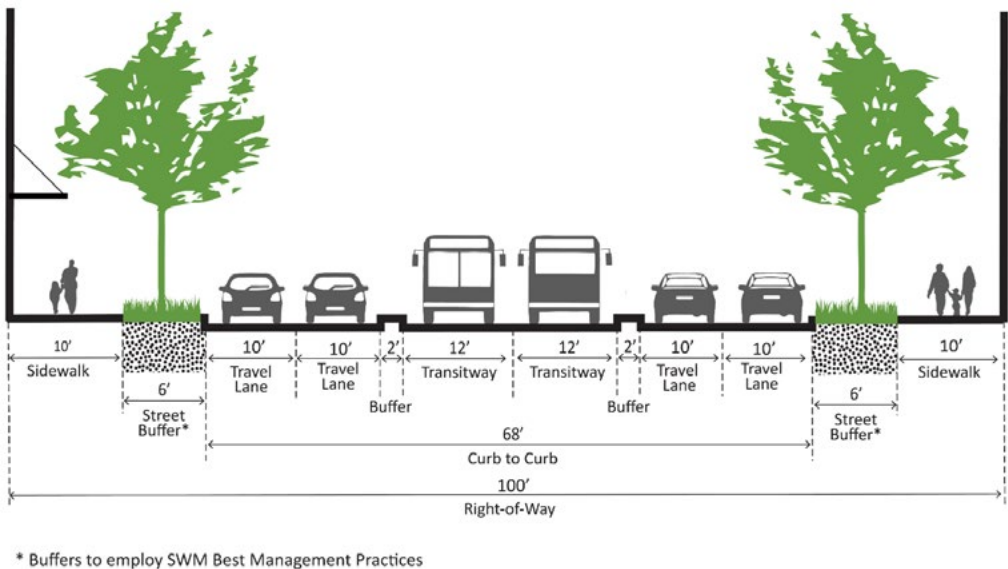
Figure 7. Colesville Road (M-10): North Noyes Drive to Spring Street, Looking North



1731

1732 Figure 8. Colesville Road (M-10): Spring Street to Fenton Street, Looking North

1733

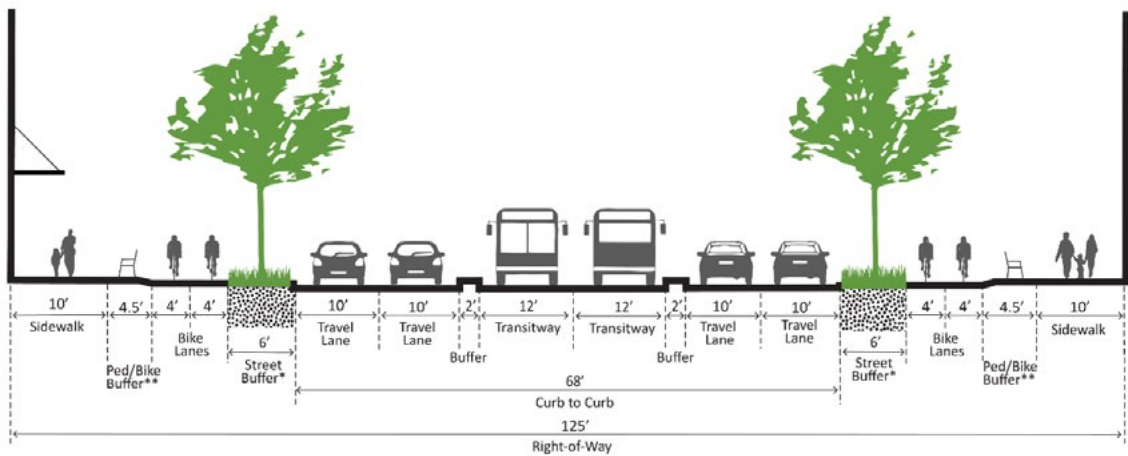


1734

1735 Figure 9. Colesville Road (M-10): Fenton Street to Georgia Avenue, Looking North

1736

1737

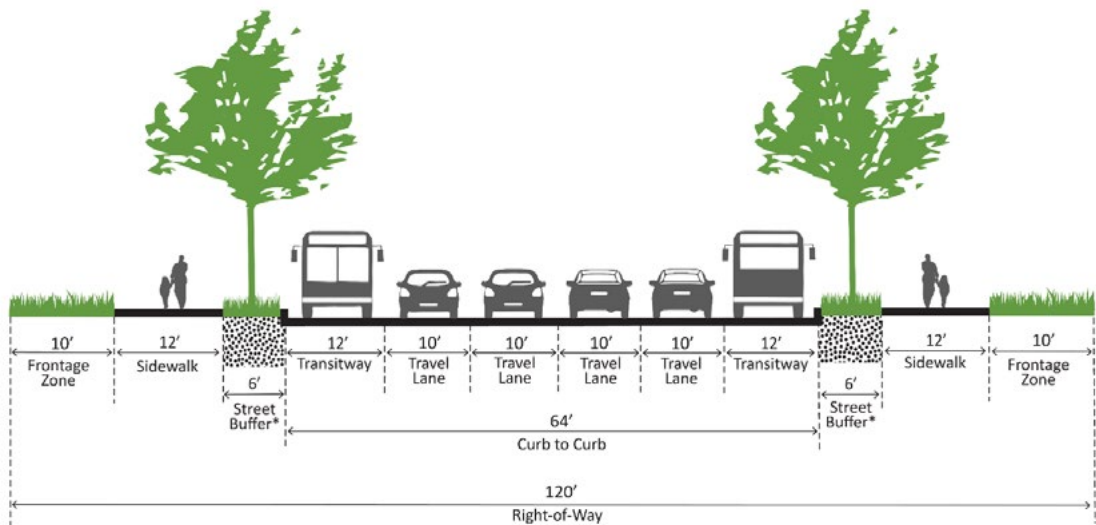


* Buffers with SWM to employ Best Management Practices

** Note: Ped/Bike Buffer: to differentiate the bikeway and the sidewalk, this space includes a mountable curb (1V:4H maximum) and 1 foot paver band. The sidewalk is 3 inches above the bike lane. 4.5' can also accommodate street furniture or similar.

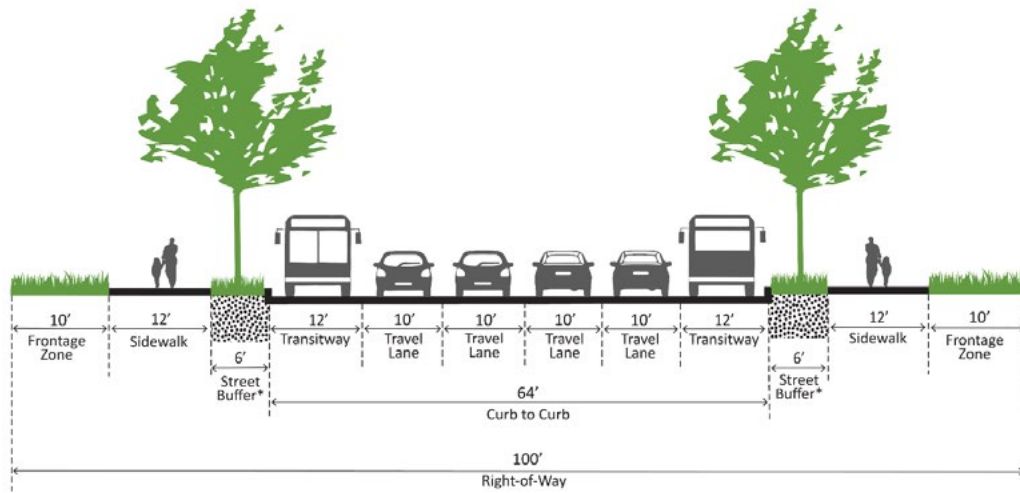
Figure 10. Colesville Road (M-10): Between Georgia Avenue and the Sarbanes Transit Center, Looking North

Curb-Running Bus Rapid Transit: Figures 11 -16



* Buffers with SWM to employ Best Management Practices

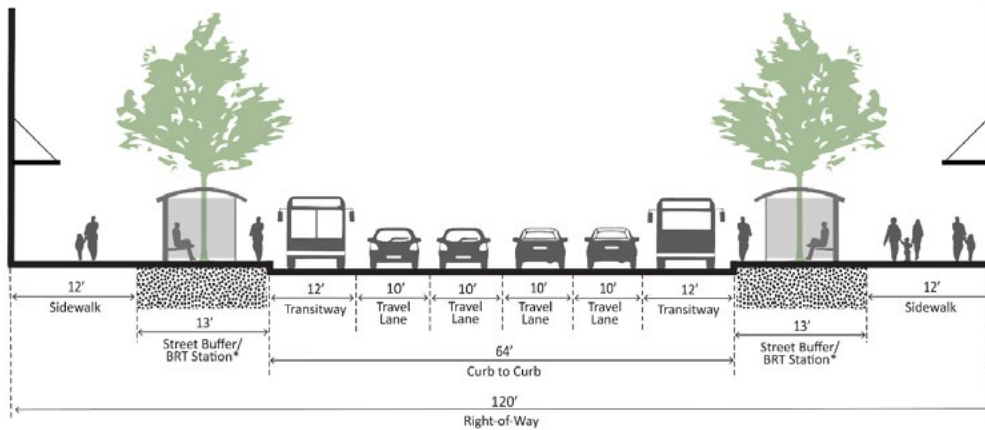
Figure 11. Colesville Road (M-20): Sligo Creek Parkway to Noyes Drive, Looking North



1746 * Buffers with SWM to employ Best Management Practices

1747 Figure 12. Colesville Road (M-20): Noyes Drive to Spring Street, Looking North

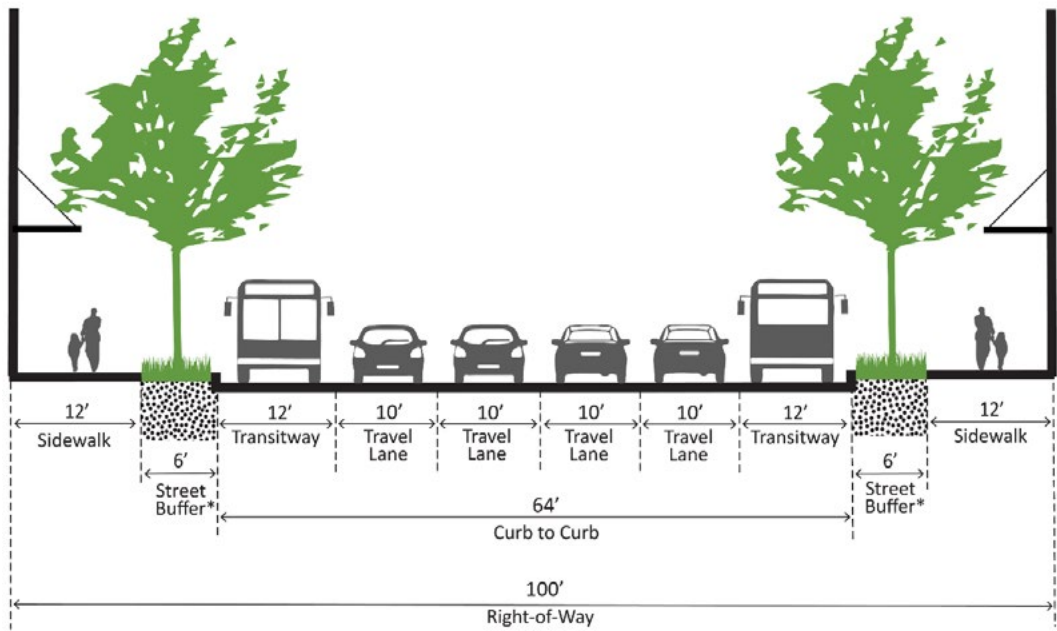
1748



1749 * Buffers with SWM to employ Best Management Practices

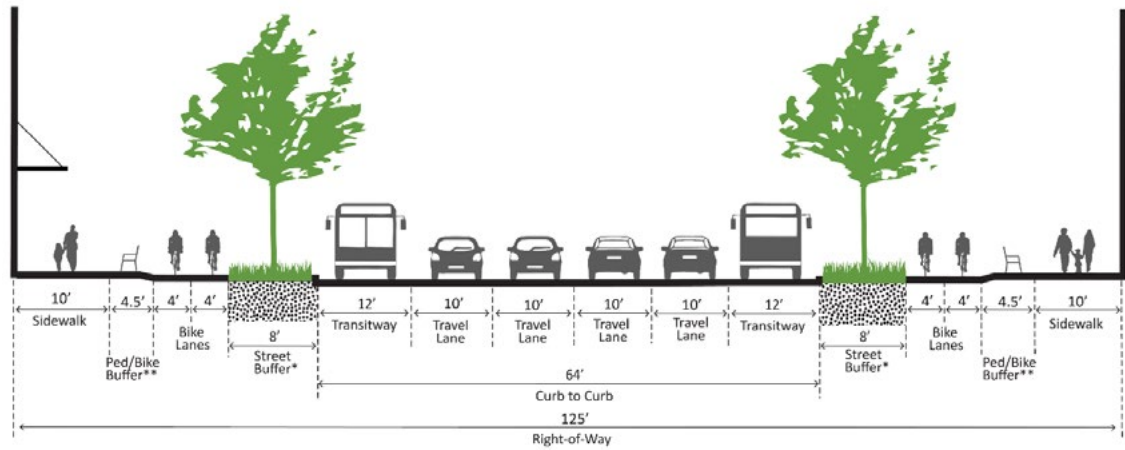
1750 Figure 13: Colesville Road (M-10): Spring Street to Fenton Street, Looking North

1751



* Buffers with SWM to employ Best Management Practices

Figure 14: Colesville Road (M-20): Spring Street to Georgia Avenue, Looking North



* Street Buffer and SWM to employ Best Management Practices

** Note: Ped/bike Buffer: to differentiate the bikeway and the sidewalk, this space includes a mountable curb (1V:4H maximum) and 1 foot paver band. The sidewalk is 3 inches above the bike lane

Figure 15. Colesville Road (M-10): Georgia Avenue to the Sarbanes Transit Center, Looking North

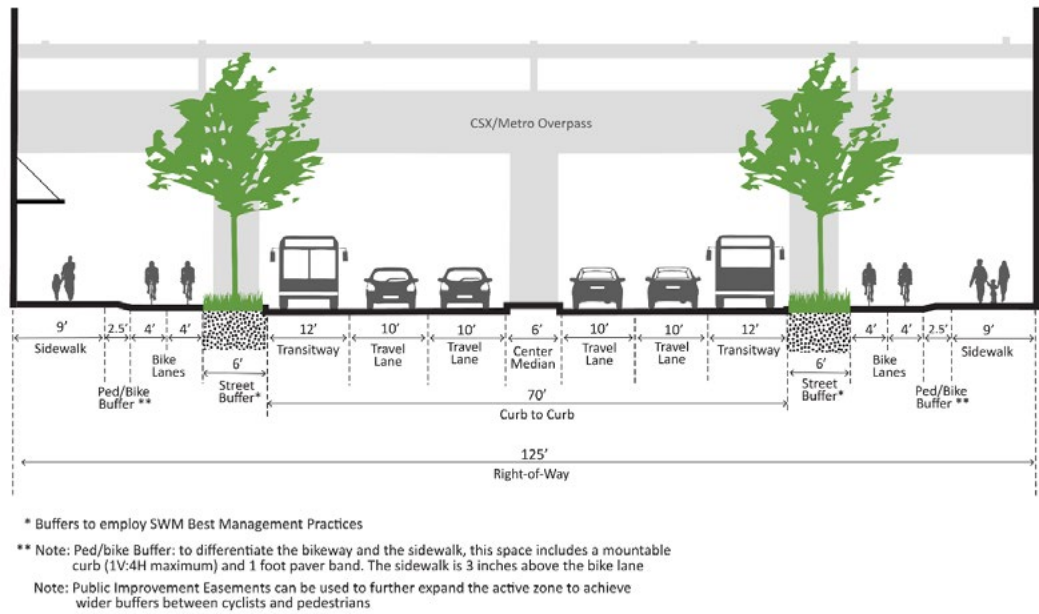


Figure 16: Colesville Road (M-20), Sarbanes Transit Center to East-West Highway, Looking North

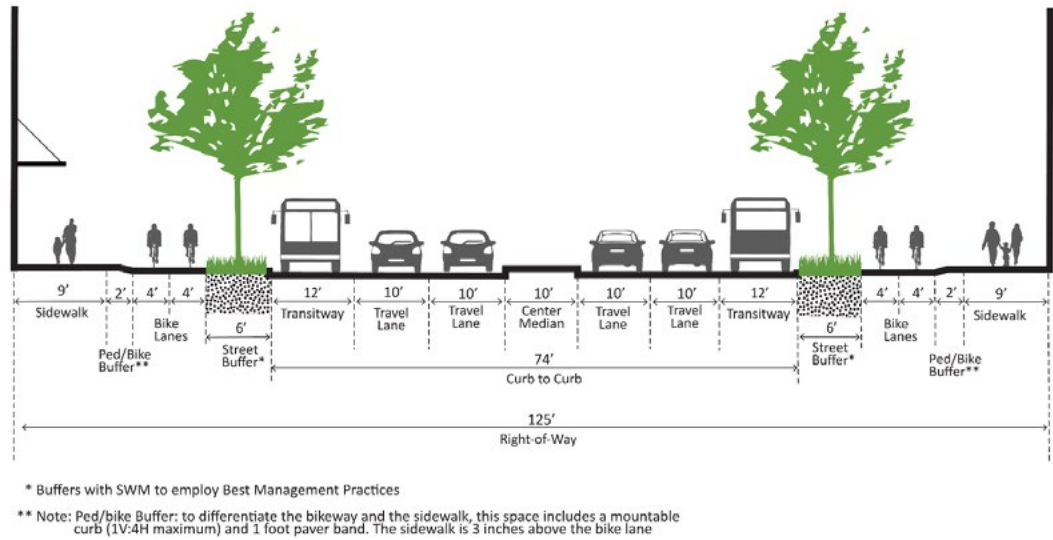


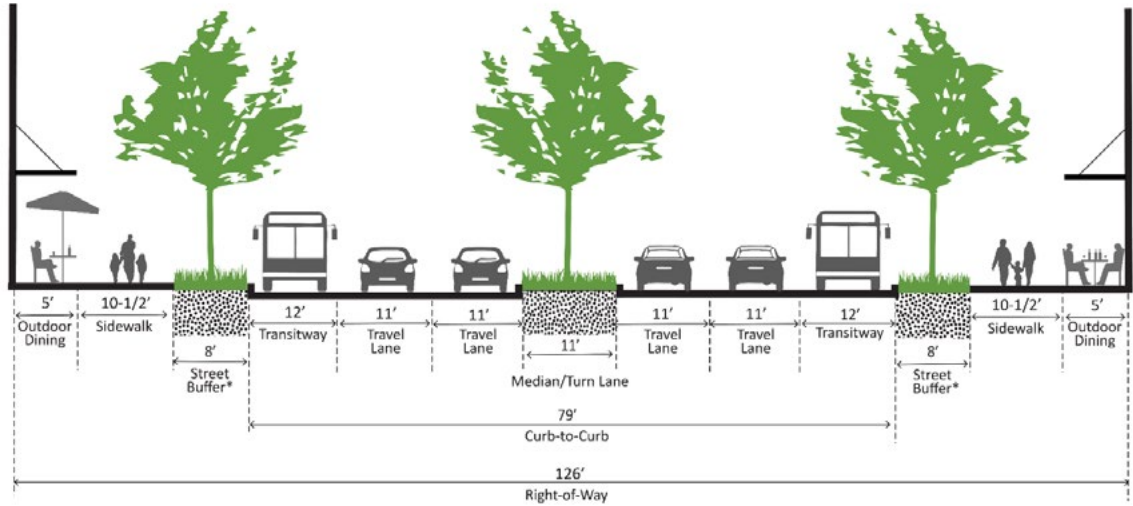
Figure 17. Colesville Road (M-20): East-West Highway to 16th Street, Looking North

Georgia Avenue Sections

As envisioned by the 2013 Countywide Transit Corridor Functional Master Plan, BRT traveling south on Georgia Avenue will enter the Sarbanes Transit Center in the Southbound direction by turning right onto Colesville Road. It will exit the Transit Center in the southbound direction by turning onto Wayne

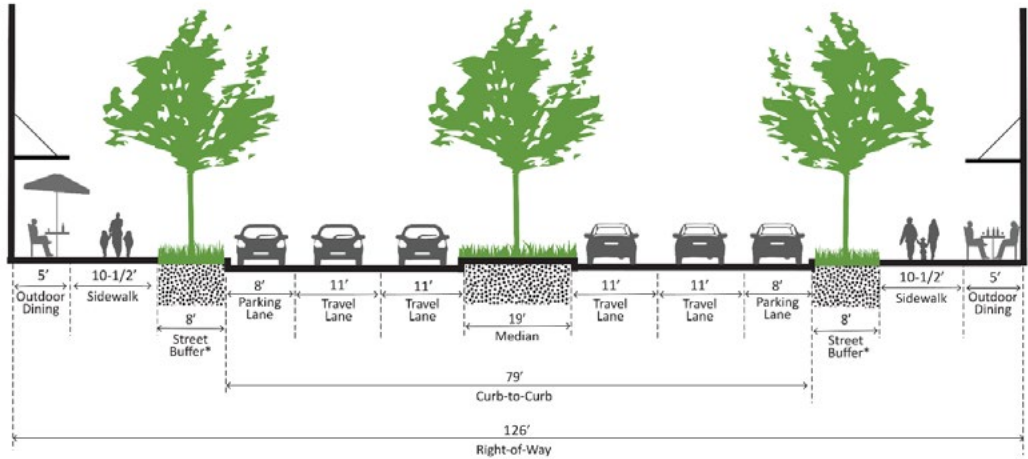
Avenue and back onto Georgia Avenue. The reverse will occur in the northbound direction. For that reason, the segment of Georgia Avenue between Colesville Road and Wayne does not include dedicated transit lanes. To ensure a safe and feasible transition, this segment was included with both options for cross sections along Georgia Avenue developed for the Sector Plan.

Curb-Running Bus Rapid Transit: Figures 18 – 21



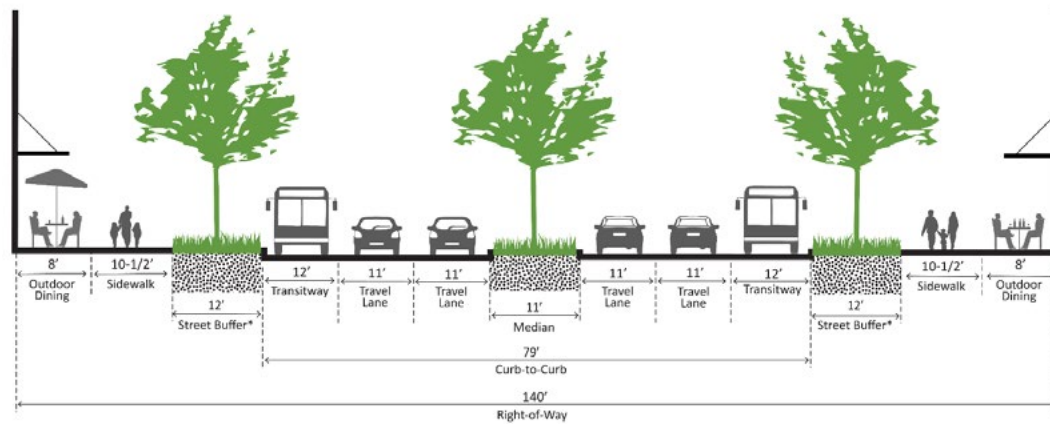
* Buffers to employ SWM Best Management Practices

Figure 18. Georgia Avenue (M-8): Spring Street to Colesville Road, Looking North



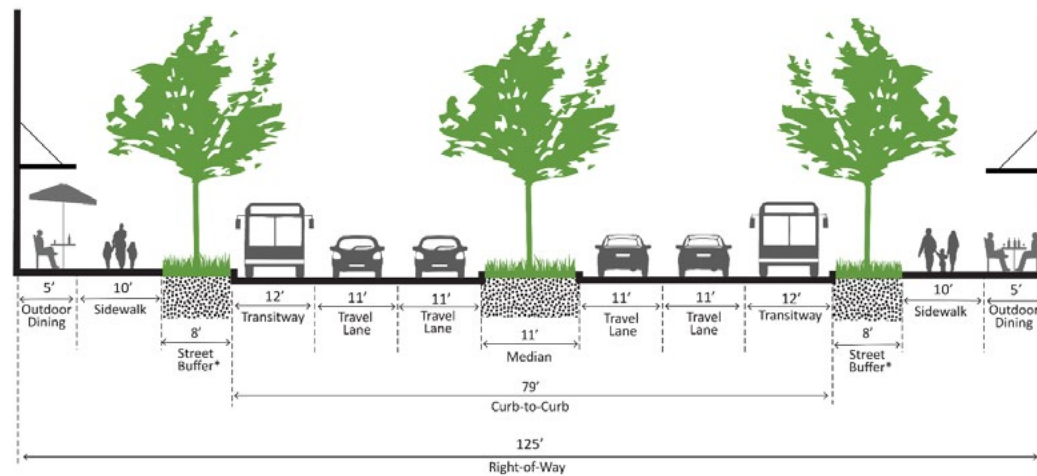
* Buffers to employ SWM Best Management Practices

Figure 19. Georgia Avenue (M-8): Colesville Road to Wayne Avenue, Looking North



* Buffers with SWM to employ Best Management Practices

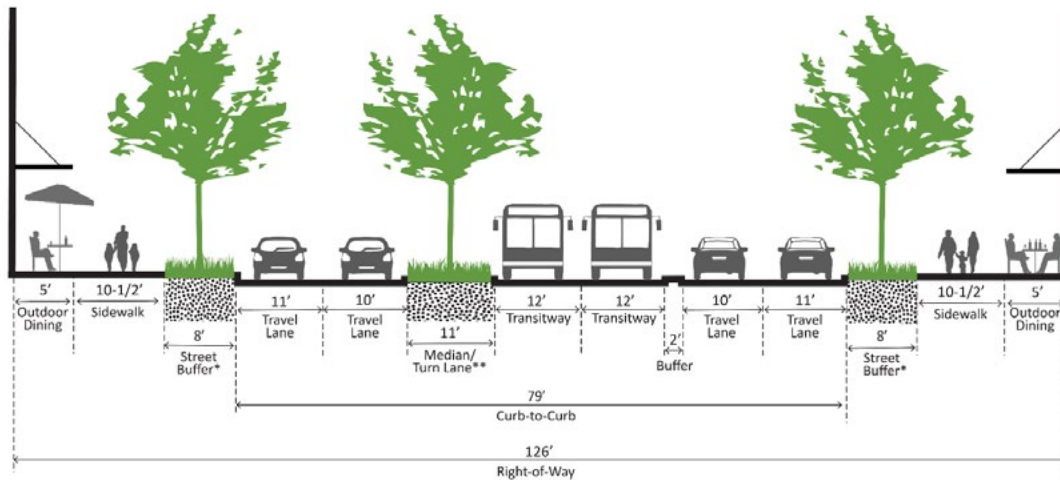
Figure 20. Georgia Avenue (M-8): Wayne Avenue to Blair Mill Road, Looking North



* Buffers with SWM to employ Best Management Practices

Figure 21. Georgia Avenue (M-8): Blair Mill Road to Eastern Avenue, Looking North

Median-Running Bus Rapid Transit: Figures 23 - 26

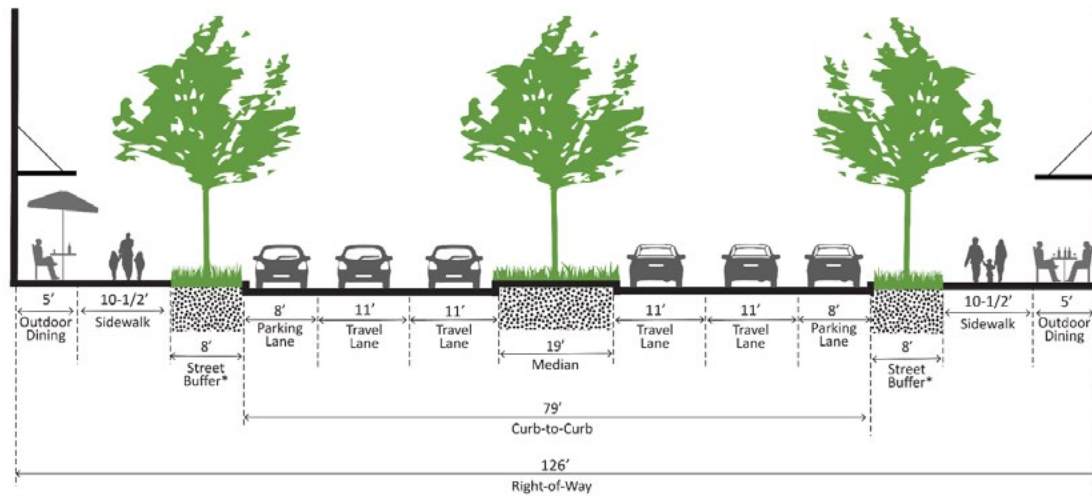


* Buffers to employ SWM Best Management Practices

** At turn lane condition, turn lane is 9' and there is a 2' buffer between the turn lane and the transitway.

1787

1788 Figure 22. Georgia Avenue (M-8): Spring Street to Colesville Road, Looking North



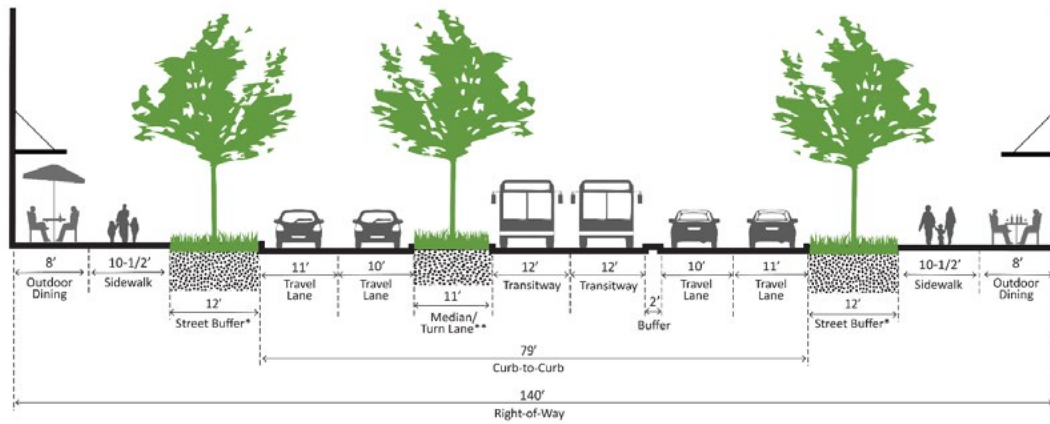
* Buffers to employ SWM Best Management Practices

1789

1790 Figure 23. Georgia Avenue (M-8): Colesville Road to Wayne Avenue, Looking North

1791 Note: The figure above is the same as Figure 20 in the curb-running BRT option for Georgia Avenue.

1792



* Buffers to employ SWM Best Management Practices

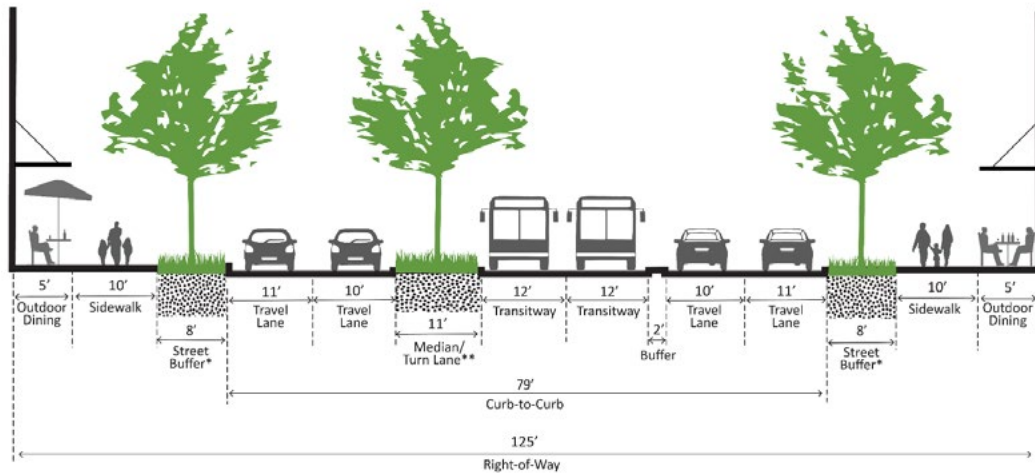
** At turn lane condition, turn lane is 9' and there is a 2' buffer between the turn lane and the transitway.

1793

1794 Figure 24. Georgia Avenue (M-8): Wayne Avenue to Blair Mill Road, Looking North

1795

1796



* Buffers to employ SWM Best Management Practices

** At turn lane condition, turn lane is 9' and there is a 2' buffer between the turn lane and the transitway.

1797

1798 Figure 25. Georgia Avenue (M-8): Blair Mill Road to Eastern Avenue, Looking North

1799

1800 Additional Street Sections

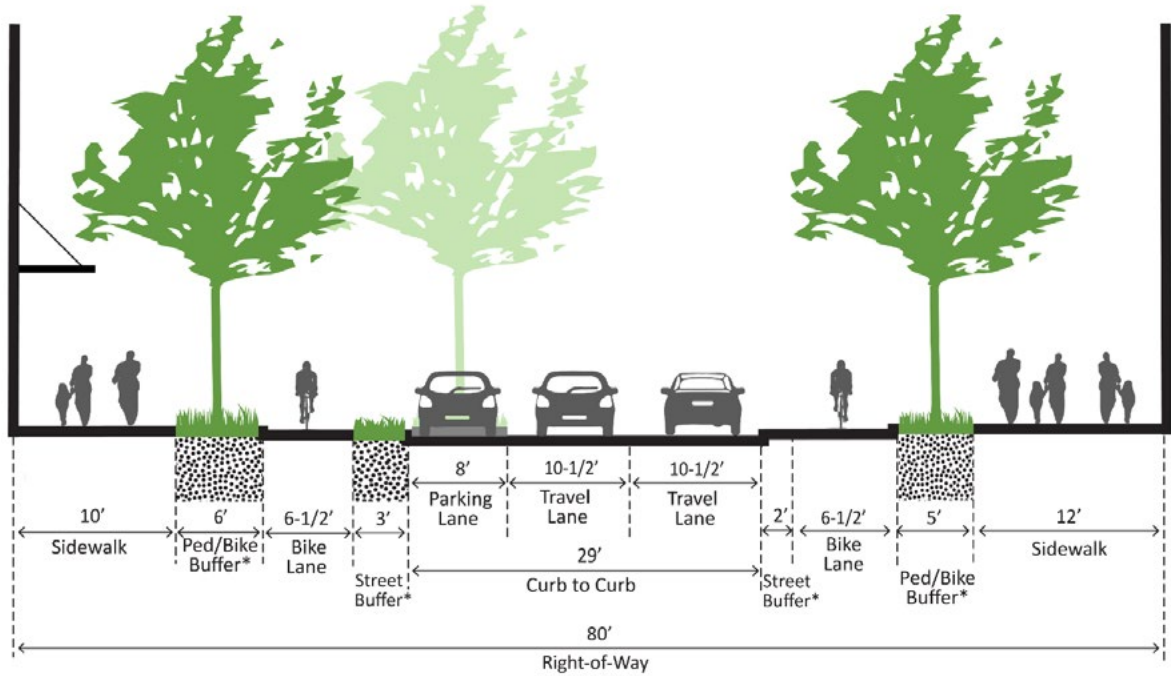


Figure 26. 13th Street (B-3): Georgia Avenue to Eastern Avenue, Looking East

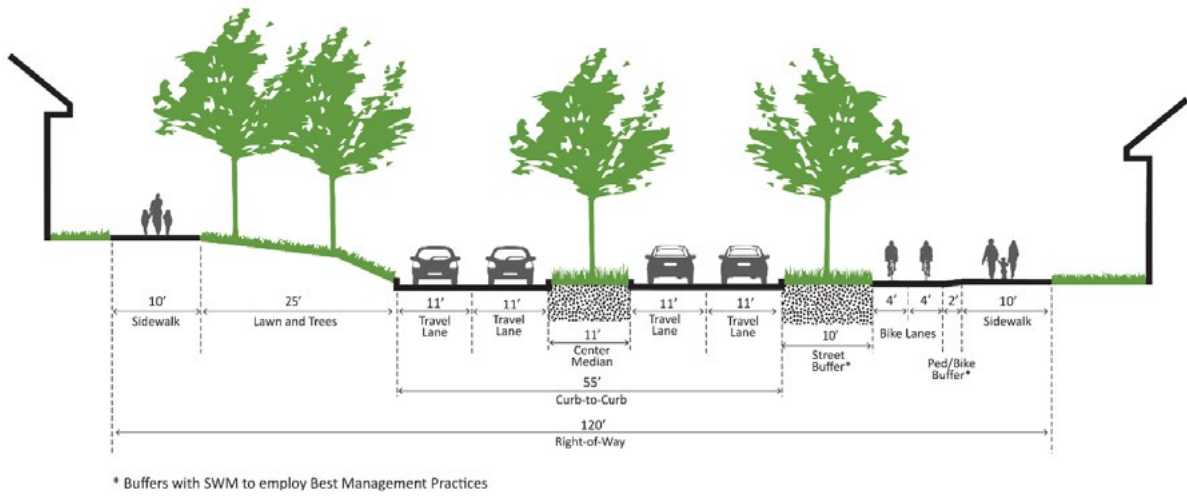


Figure 27. 16th Street (M-9): Colesville Road to East-West Highway, Looking North

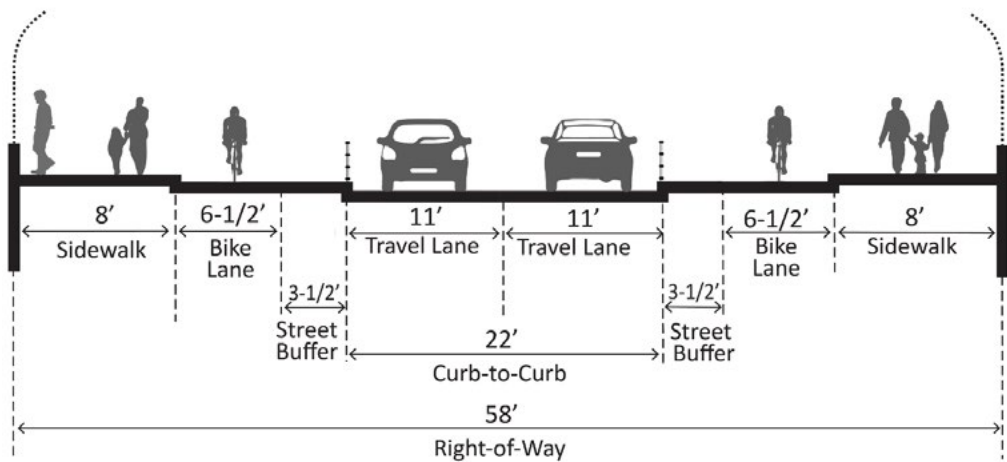
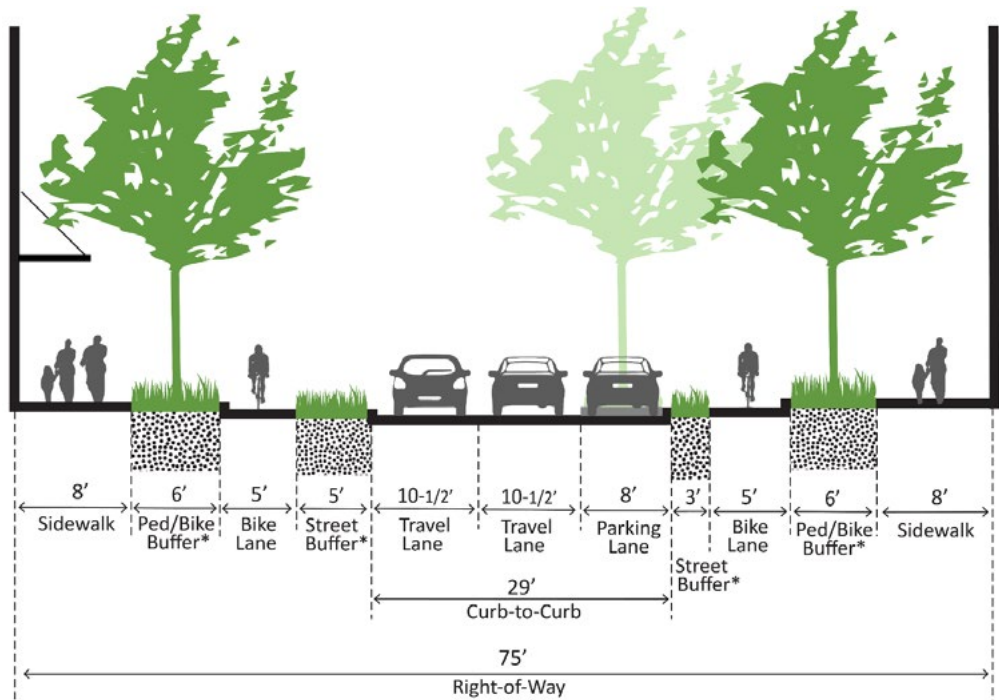
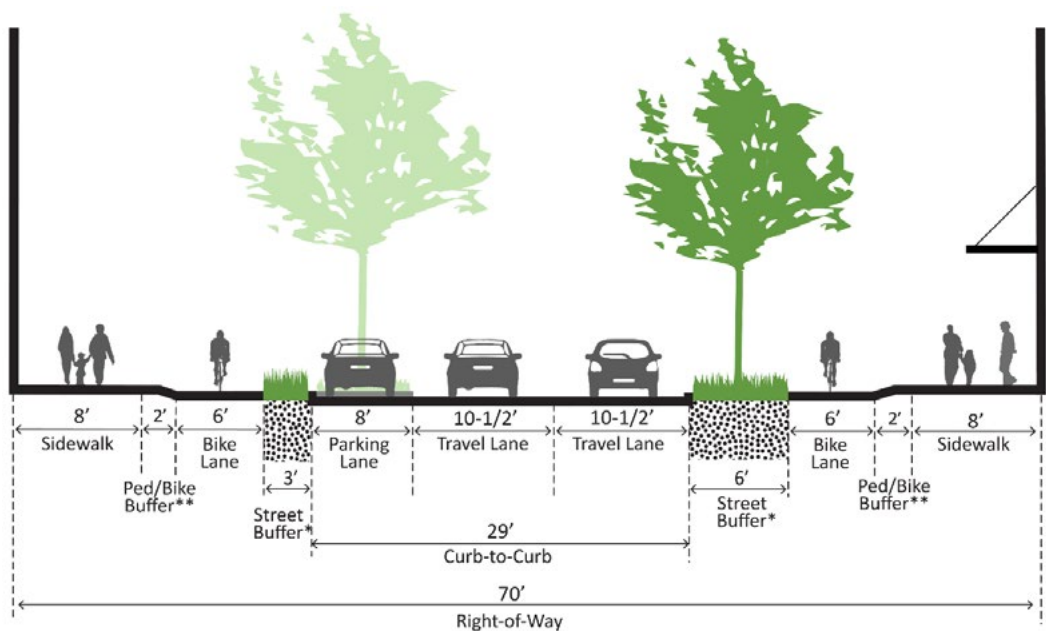


Figure 28. Burlington Avenue Bridge (M-20): Looking East



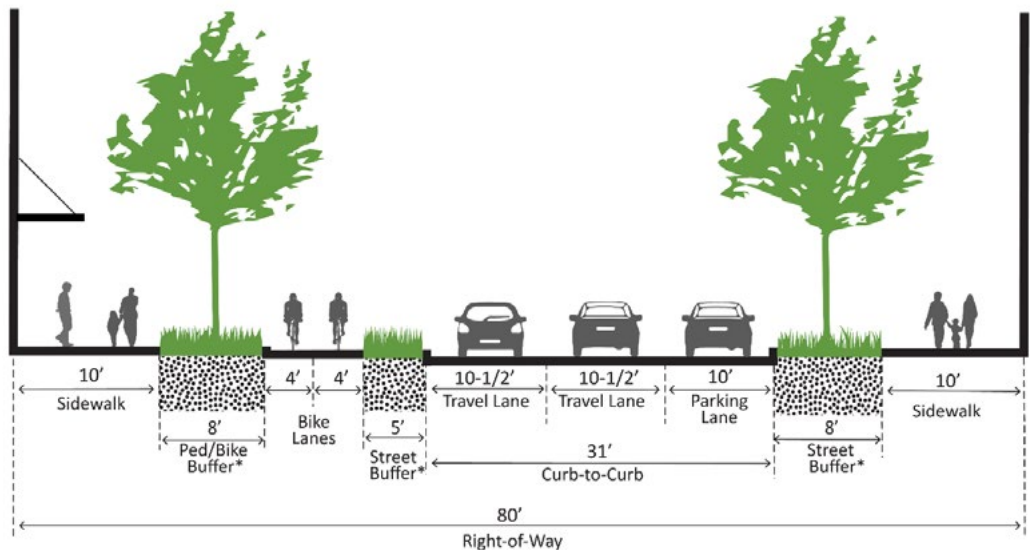
* Buffers with SWM to employ Best Management Practices

Figure 29. Cameron Street (B-28): 2nd Avenue to Georgia Avenue, Looking East



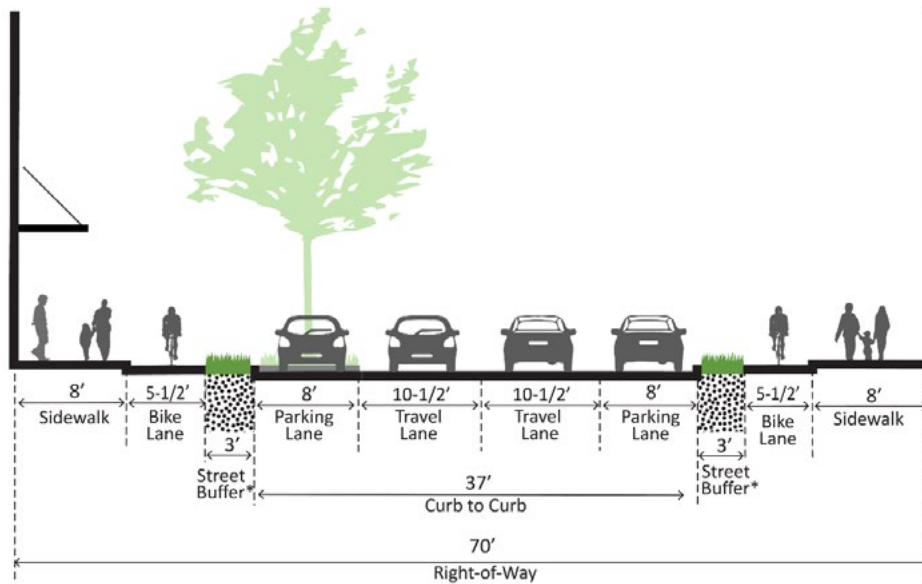
* Buffers with SWM to employ Best Management Practices
** Note: Ped/Bike Buffer: to differentiate the bikeway and the sidewalk, this space includes a mountable curb (1V:4H maximum) and 1 foot paver band. The sidewalk is 3 inches above the bike lane.

Figure 30. Silver Spring Avenue (B-25): Fenton Street to Georgia Avenue, Looking East



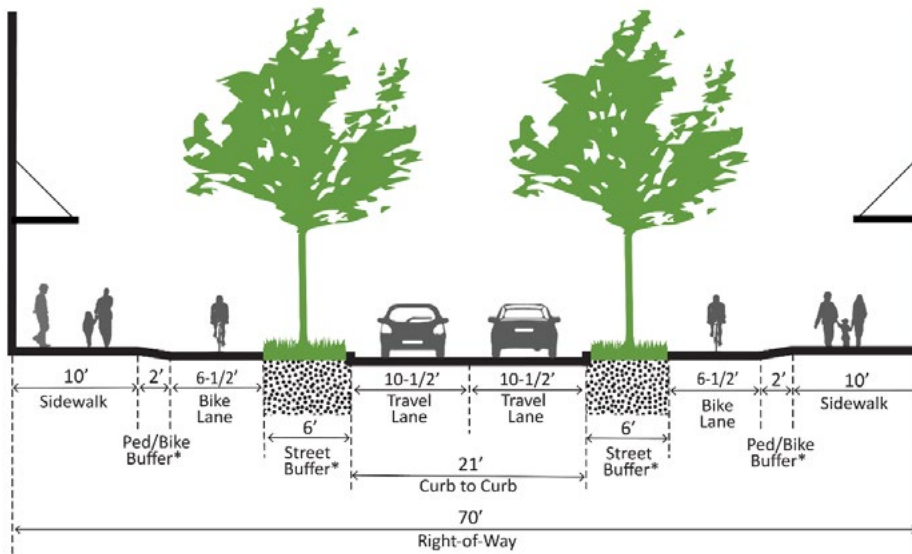
* Buffers with SWM to employ Best Management Practices

Figure 31. Fenton Street (A-264): Philadelphia Avenue to Wayne Avenue, Looking North



* Buffers with SWM to employ Best Management Practices.

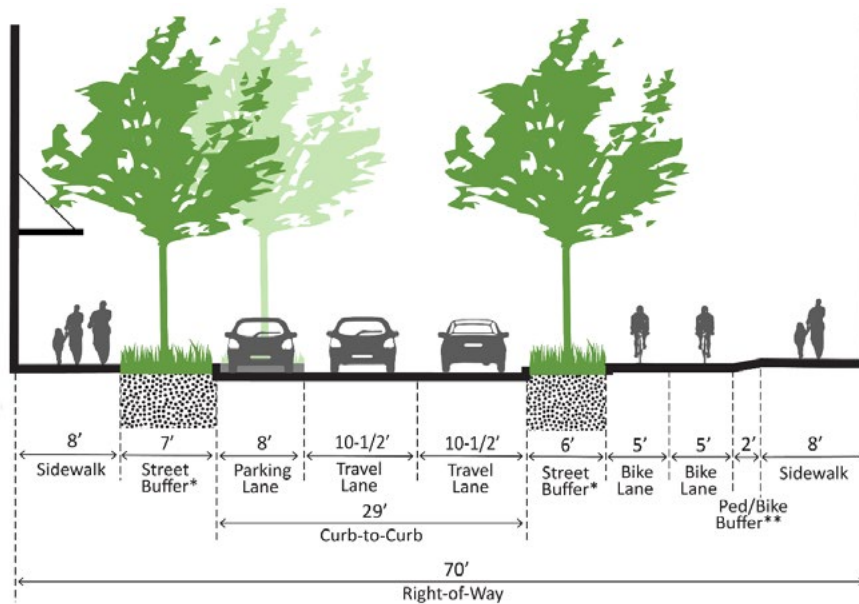
Figure 32. 1st Street (B-1): Spring Street to Fenwick Lane, Looking North



* Buffers with SWM to employ Best Management Practices

** Note: Ped/Bike Buffer: to differentiate the bikeway and the sidewalk, this space includes a mountable curb (1V:4H maximum) and 1 foot paver band. The sidewalk is 3 inches above the bike lane

Figure 33. 1st Street (B-30) Extended, Fenwick Lane to Cameron Street, Looking North



* Buffers with SWM to employ Best Management Practices

** Note: Ped/Bike Buffer: to differentiate the bikeway and the sidewalk, this space includes a mountable curb (1V:4H maximum) and 1 foot paver band. The sidewalk is 3 inches above the bike lane

Figure 34. Blair Mill Road (B-25): Eastern Avenue to East-West Highway, Looking East

Page 145: Add text after the last bullet under “Goals” as follows:

Nature-based Solutions (NbS) are sustainable planning, design, and engineering practices that weave natural features and processes to mimic nature in the built environment. They aim to tackle climate change, food insecurity, water resources, natural disaster risks, and lost ecological performance. Several NbS are included in the Green Loop concept including cool surfaces, canopy tree and vegetation planting, shading strategies, and stormwater management. Architectural applications of NbS may include solar orientation, green roofs, gardens, water catchment, and vegetated landscapes. These elements improve human and ecological well-being, increase long-term economic prosperity, community desirability, rental retention rates, property values, and more. The Plan recommends implementing numerous Nature-based Solutions to achieve the goals outlined above.

Page 146: Delete section “3.7.3. Nature-Based Solutions for Urban Environmental Needs” as follows:

[Nature-based Solutions (NbS) are sustainable planning, design, and engineering practices that weave natural features and processes to mimic nature in the built environment. They aim to tackle climate change, food insecurity, water resources, natural disaster risks, and lost ecological performance. Several NbS are included in the Green Loop concept including cool surfaces, canopy tree and vegetation planting, shading strategies, and stormwater management. Architectural applications of NbS may include solar orientation, green roofs, gardens, water catchment, and vegetated landscapes. These elements improve human and ecological well-being, increase long-term economic prosperity, community desirability, rental retention rates, property values, and more. The Plan recommends implementing numerous Nature-based Solutions to achieve the goals outlined above.]

1849

1850 Page 146: Insert the following bullet between the first and second bullets under “Goals” for “3.7.4. Urban
1851 Heat Islands and Tree Canopy” as follows

1852

- 1853 • Achieve 45% tree canopy coverage for the Plan area on both public and private property.

1854

1855 Page 147: Revise the fourth bullet under “Recommendations” as follows:

1856

- 1857 • [~~Encourage~~Require a minimum of 35 percent green cover on Optional Method Development
1858 projects where practicable and consistent with Plan objectives. A project may [achieve the 35
1859 percent green cover requirement] provide green cover by:

- 1860 ○ Providing an intensive green roof (six inches or deeper) on the [rooftop of the buildings]
1861 rooftop or other structure;

- 1862 ○ [~~Proving~~Providing native canopy tree cover on the landscape of the project site area at
1863 ground level; and/or

- 1864 ○ Providing a combination of tree canopy cover and intensive green roof for a total of 35
1865 percent or greater green cover on the total site.

- 1866 ○ Site area for green cover [May] may be reduced [for] to accommodate on-site energy
1867 generation, other environmental site amenities, or where desirable to achieve other plan
1868 objectives.

- 1869 ○ [All new] New rooftops not covered in green roofs or alternative or renewable energy
1870 generation should be cool roofs or active rooftop uses.

1871

1872 Page 148: Revise the fifth bullet under “Recommendations” as follows:

1873

- 1874 • Consider a Cool Roof Initiative that encourages existing property owners to paint [their] roofs
1875 not covered in vegetation with reflective surfaces which can reduce the effects of heat island by
1876 as much as 33 percent.

1877

1878 Page 156: Revise the first bullet under “Recommendations” for “3.8.1. Safety and Security” as follows:

1879

- 1880 • Ensure consistent street lighting to provide a greater level of safety and comfort for pedestrians,
1881 particularly when [walking] traveling at night. The plan strongly recommends additional lighting
1882 at underpasses such as the pedestrian tunnel under the Metrorail at Georgia Avenue, and under
1883 the Metrorail at Colesville Road.

1884

1885 Page 157: Revise third bullet under “Recommendations” for “3.8.2. Homeless and Housing Insecurity
1886 Services” as follows:

1887

- 1888 • Work with service providers to provide [sufficient] more places that are welcoming and
1889 supportive of the housing insecure.

1890

1891 Page 166: Delete the following heading after the second paragraph under “3.9 Historic Resources” as
1892 follows:

1893

1894 [Building Preservation Opportunities in Downtown Silver Spring]

1895

Page 166: Revise the text under “Building Preservation Opportunities in Downtown Silver Spring” as follows:

3.9.1 Goals

Silver Spring’s historic buildings are critical to the community’s character and collective memory; offer tangible connections to the past; provide opportunities for education and interpretation; and create a diversity of building types within the Plan area. The Silver Spring Downtown and Adjacent Communities Plan will [encourage preservation and adaptive reuse of designated historic properties]:

- Recognize and interpret the diversity, heritage, and history of the Plan area.
- Encourage preservation and adaptive reuse of historically significant properties.
- Educate owners of historic properties on the benefits of local, state, and federal historic preservation tax credit programs.
- Document and support local, independently-owned businesses in operation for over 15 years.

Page 166: Revise the following heading under “Building Preservation Opportunities in Downtown Silver Spring” as follows:

[3.9.1] 3.9.2 Resources Listed in the Master Plan for Historic Preservation

Page 166: Revise the heading after “3.9.1 Master Plan for Historic Preservation” as follows:

[Goals] Recommendations:

Page 167: Revise the second paragraph as follows:

Silver Spring features numerous resources listed in the *Master Plan for Historic Preservation*. These resources have architectural and historical significance, but more importantly, provide material and tangible benefits to the community. The historic sites and districts shall continue to be preserved as they contribute to the vitality of downtown Silver Spring. For example, prioritizing the continued rehabilitation of the Jesup Blair House (listed to the Master Plan for Historic Preservation in 1986) will allow M-NCPPC to identify a partner to activate the space and complement future park programming at Jesup Blair Park. Interpretation of [these places] historic sites and districts, however, should be re-evaluated to ensure analysis of segregation, discrimination, underrepresented communities, and other histories insufficiently documented in previous surveys of Silver Spring.

Page 167: Revise the last paragraph as follows:

The transformation and adaptive reuse of transportation infrastructure (such as parking lots) is critical for the evolution of our urban downtown and essential for our future mobility beyond the automobile. Historic preservation will contribute to such planning efforts by actively encouraging the preservation and adaptive reuse of important spaces and buildings, documenting the history of land use, and providing guidance for designated historic resources through the design review process. Further reimagining and activation of this parking lot would be appropriate and encouraged with respect to the ongoing preservation of the site, consistent with retaining the lot’s character-defining shape and features. In [addition] particular, proposals could explore the addition of a commercial one-story building that is compatible with the architecture of the shopping center and respects the

1943 relationship between the shopping center and streets. Historically, a section of the parking lot once
 1944 included a gas station which provides justification for the addition of a small structure.

1945

1946 Page 168: Delete section “3.9.2. Locational Atlas and Index of Historic Sites” as follows:

1947

1948 [3.9.2. Locational Atlas and Index of Historic Sites

1949 Established in 1976, the Locational Atlas and Index of Historic Sites identifies resources that are
 1950 potentially historically significant. Resources listed on the atlas are protected from demolition or
 1951 substantial alteration under Chapter 24A of the Montgomery County Code. This designation is meant
 1952 to be temporary until analysis for listing in the Master Plan for Historic Preservation can be
 1953 completed.]

1954

1955 Page 168: Insert a new section regarding the Locational Atlas and Index of Historic Places as follows:

1956

1957 3.9.3. Resources Listed in the Locational Atlas and Index of Historic Sites

1958

1959 Recommendation:

- 1960 • Review properties listed on the Locational Atlas and Index of Historic Sites as part of any
 1961 redevelopment proposal or as part of comprehensive analysis of resources listed in the atlas.

1962

1963 Established in 1976, the Locational Atlas and Index of Historic Sites identifies resources that are
 1964 potentially historically significant. Resources listed on the atlas are protected from demolition or
 1965 substantial alteration under Chapter 24A of the Montgomery County Code. This designation is meant
 1966 to be temporary until analysis for listing in the Master Plan for Historic Preservation can be
 1967 completed.

1968

1969 8551 Fenton Street

1970 The building is listed in the Locational Atlas and Index of Historic Sites as a resource within the
 1971 Silver Spring CBD Locational Atlas District. The first anchor tenant of this building (constructed in
 1972 1951) was Morton’s Department Store which several sources have noted had a non-discriminatory
 1973 environment at its stores. The Historic Preservation Commission and the Planning Board evaluate
 1974 the significance of buildings proposed for redevelopment or as part of a larger analysis of resources
 1975 listed in the Locational Atlas and Index of Historic Sites as outlined in Chapter 24A, Historic
 1976 Resources Preservation, of the County Code.

1977

1978 Page 171: Revise headings at the top of the page as follows:

1979

1980 [3.9.3] 3.9.4 New Sites [or Districts] to be Studied as Future Historic Preservation Master Plan
 1981 Amendment(s)

1982

1983 Recommendations:

1984

- 1985 • Study the Weller’s Dry-Cleaning Building for the potential future listing in the *Master Plan for*
 1986 *Historic Preservation* and encourage the adaptive reuse of the building if the occupant and use
 1987 change.

1988

1989 Page 171: After the first two paragraphs, delete “Recommendation” and associated text as follows:

1990

- 1991 [Recommendation:
 1992 Study the Weller's Dry-Cleaning Building for the potential future listing in the Master Plan for
 1993 Historic Preservation and encourage the adaptive reuse of the building if the occupant and use
 1994 change.]
 1995
- 1996 Page 172: Revise headings at the top of the page as follows:
 1997
- 1998 [3.9.4] 3.9.5 New Sites or Districts to be Studied for listing on the National Register of Historic
 1999 Places
 2000
- 2001 [Goals] Recommendations:
 2002
- 2003 Page 172: Add the following heading after the fourth bullet under "Goals":
 2004
- 2005 Properties Potentially Eligible for the National Register of Historic Places
 2006
- 2007 Page 172: Delete the "Perpetual Bank Building" from the list of potentially eligible properties as
 2008 follows:
 2009
- 2010 • Operations Research, Inc., Building (1400 Spring Street)
 - 2011 • [Perpetual Bank Building (8700 Georgia Avenue)]
 - 2012 • U.S. Industries Building (949 Bonifant Street)
 - 2013
- 2014 Page 173: Modify the heading regarding Garden Apartments as follows:
 2015
- 2016 [Proposed Silver Spring] Garden and Mid-Rise Apartment [Complex] Historic District
 2017
- 2018 Page 173: Add the following bullet under the revised Garden and Mid-Rise Apartment heading as
 2019 follows:
 2020
- 2021 • Study a Silver Spring Garden and Mid-Rise Apartment National Historic District and list the
 2022 district on the National Register of Historic Places, which would open state and federal
 2023 historic preservation tax credit opportunities to property owners.
 2024
- 2025 Page 174: Delete the last sentence on the page as follows:
 2026
- 2027 [Staff recommends listing the district to the National Register of Historic Places, which would open
 2028 state and federal historic preservation tax credit opportunities to property owners.]
 2029
- 2030 Page 175: Revise the headings and first paragraph at the top of the page as follows:
 2031
- 2032 [3.9.5.] 3.9.6. Archeological Resources
 2033
- 2034 [Goals] Recommendation:
 2035

- 2036 • Advocate for archaeological investigations in areas with potential for buried resources to be
 2037 present, as projects are undertaken. This is often in areas with limited land development such
 2038 as [minimal land disturbance (primarily located at] Jesup Blair Park[) when projects are
 2039 undertaken].

2040

2041 Page 175: Add the following information as a footnote to the recommendation regarding burial
 2042 resources:

2043

2044 The potential to have intact archeological sites present in any given location is based on past land
 2045 use and decreases in areas with more extensive ground disturbance (land disturbed through use,
 2046 construction, grading, etc.). The level of ground disturbance typical in urban areas means the
 2047 potential for intact resources needs to be evaluated at an individual property level. At Jesup Blair
 2048 Park, Park and Planning staff archaeologists will coordinate and determine the level of archaeological
 2049 investigations to be included in a Historic Area Work Permit (HAWP) application to the Historic
 2050 Preservation Commission. The proposed level of investigation will be based on historical research,
 2051 environmental context review, field inspections, findings of past investigations, and the location and
 2052 nature of proposed improvements.

2053

2054 Page 175: Revise the heading and bullets following the first paragraph under “3.9.6. Cultural and
 2055 Heritage Resources” as follows:

2056

2057 [Goals] Recommendations:

2058

- 2059 • Establish a legacy business registry to recognize the economic, cultural, and social
 2060 contributions of long-standing businesses to the fabric of Silver Spring.
 2061 • [Study potential incentives to preserve local, independently owned businesses.]
 2062 • Partner with existing and new local partners to add interpretation of histories absent from our
 2063 present narratives. Interpretation may include physical (historic markers and panels, public
 2064 art, etc.) and digital or other ephemeral media (websites, story maps, virtual walking.
 2065 • [Celebrate the diversity, heritage, and history of the Plan area.]
 2066 • Support [any future recommendations of] the Streets and Parks Facilities Renaming Review
 2067 Project.

2068

2069 Page 178: Revise the heading and insert text under “4.1 ZONING” as follows:

2070

2071 4.1.1 [Connectivity and Infrastructure Fund (CIF)] Downtown Silver Spring Overlay Zone

2072

2073 An overlay zone is a mapped district placed over the underlying Euclidian zone that modifies the
 2074 uses or development standards of the zone. An overlay zone imposes requirements, restrictions, or
 2075 allowances in addition to, or in place of, those of the underlying zoning. This Plan recommends the
 2076 adoption of a Downtown Silver Spring Overlay Zone that will cover all Plan districts except the
 2077 Adjacent Communities District. The overlay zone would implement the recommendations of this
 2078 Plan related to density, affordable housing, public benefits, design excellence and the Civic
 2079 Improvement Fund.

2080

2081 Page 178: Insert the following heading after section 4.1.1. as follows:

2082

2083 4.1.2. Civic Improvement Fund (CIF)

Page 178: Revise the first bullet under “4.1.1. Connectivity and Infrastructure Fund (CIP)” as follows:

- Any CR property in the Plan area may obtain additional density necessary to reach the mapped maximum building height [, or additional height approved for a property in the BHIZ,] by making a contribution to the [Connectivity and Infrastructure] Civic Improvement Fund (CIF). The Contribution methodology will be determined by the Zoning Ordinance.

Page 178: Revise the third bullet under “4.1.1. Connectivity and Infrastructure Fund (CIP)” as follows:

- The CIF will be implemented by the [Planning Department under the direction of the Planning Board] County and contributions will be used toward implementation of specific projects within downtown Silver Spring intended to upgrade the public realm so as to continue to attract businesses, residents, and visitors, including:
 - Enhanced Transit Center Arrival Experience;
 - Bridge connection over Metrorail/CSX tracks;
 - Public bicycle parking facilities;
 - Green Loop improvements beyond the frontage of a redeveloped site;
 - [Select utility improvements;]
 - Or other projects identified by the County or the Planning Board.

Page 178: Delete section “4.1.2 Silver Spring Downtown Building Height Incentive Zone (BHIZ)” as follows:

[4.1.2. Silver Spring Downtown Building Height Incentive Zone (BHIZ)]

- This plan recommends the establishment of a Silver Spring Downtown Building Height Incentive Zone (BHIZ) as shown in Map 32 to allow CR-zoned properties pursuing Optional Method Development to increase building heights by up to 150 percent of the mapped height to a maximum of 300 feet. Approved height will be subject to the Design Review process through the Design Advisory Panel.
- The Planning Board may approve certain properties identified in the Metro Center District A to realize an increased building height in excess of 300 feet, consistent with the recommendations of the Sector Plan and Design Guidelines, subject to the Design Review process through the Design Advisory Panel.
- To qualify for the additional height under the BHIZ, projects must provide a combination of greater than the minimum 15 percent MPDUs on-site, a contribution to the Montgomery County Housing Initiative Fund (HIF), and/or include an activating ground-floor Neighborhood Service including, but not limited to, small business, art gallery/venue, community meeting space, educational or medical use, historic preservation, or non-ground floor area dedicated to Design for Life residences.]

Page 179: Revise the second bullet under “4.1.3. Fenton Village Overlay Zone (FVOZ)” as follows:

- Allow properties required to provide Public Open Space (POS) on-site to instead contribute financially to new open spaces recommended within the district. [Coordinate with Montgomery Parks to determine a new off-site open space contribution formula to better realize the goals of the Plan.]

2132 Page 180: Delete Map 31.

2133

2134 Page 181: Revise the second and third paragraphs under “4.1.5. CR, CRT, CRN Zoning” as follows:

2135

2136 [Properties inside the BHIZ are confirmed at the existing height maximums.] Maximum heights on
 2137 properties throughout the plan have been adjusted per urban design goals of each district, with at
 2138 least a 20 percent increase on most properties to eliminate the existing “T” designation. Select
 2139 blocks surrounding the Transit Center are recommended for heights up to 360 feet, with Planning
 2140 Board approval.

2141

2142 The Design Guidelines that will accompany this Plan will indicate where heights should step down
 2143 at specific downtown edges [the edge of the BHIZ].

2144

2145 Page 181: Revise the fourth bullet under “4.1.12. Public Benefits in the CR Zones” as follows:

2146

- 2147 • The Plan proposes that “Transit Proximity” [and “Structured Parking”] (under Connectivity and
 2148 Mobility) be excluded from the list of potential public benefits for projects within the Plan area.

2149

2150 Page 182: Revise the text under “4.1.13. Design Advisory Panel” as follows:

2151

2152 The Sector Plan recommends establishment of a Design Advisory Panel (DAP), similar in structure
 2153 and function to the Bethesda DAP, for all Optional Method projects in the Plan area to assess the
 2154 number of Design Excellence Public Benefit Points [and for maximum height within the BHIZ]. The
 2155 Design Advisory Panel should be comprised of a diverse group of individuals who represent the
 2156 interests of the community.

2157

2158 Page 182: Insert the following section after section “4.1.15. Green Loop” as follows:

2159

2160 4.1.11. Unified Mobility Program

2161

2162 The 2021-2024 Growth and Infrastructure Policy (GIP) recommends implementation of Unified
 2163 Mobility Programs (UMPs) for all Red Policy areas in the County. Unified Mobility Programs are
 2164 implemented to ensure there is adequate public transportation facilities within a sector plan area and
 2165 they are established by Council Resolution after a public hearing. The Council may amend the UMP
 2166 and the fee at any time, after a public hearing. In areas subject to an UMP, regulatory applicants are
 2167 exempt from the Local Area Transportation Review, and instead fees are collected (typically scaled
 2168 by the number of net new trips generated by the master-planned additional density) to fund essential
 2169 transportation improvements such as new/improved sidewalks and other ADA accessibility
 2170 infrastructure, separated bikeways, local transit stop improvements, etc..

2171

2172 Recognizing the unique relationship between Montgomery Hills and downtown Silver Spring, this
 2173 Sector Plan confirms the recommendation included in the 2020 Forest Glen and Montgomery Hills
 2174 Plan to include a combined UMP for both areas. The CIF described above will fund civic
 2175 improvements specifically differentiated from the essential mobility projects to be funded by the
 2176 UMP.

2177

2178 Page 184: Revise Table 12. Capital Improvements Program as follows:

2179

Table 12. Capital Improvements Program

Project Name	Category	Lead Agency	Coordinating Agencies
Renovate/Repurpose Jesup Blair Park	Parks and Open Space	M-NCPPC*	Montgomery College, Private
Create Ripley District Park	Parks and Open Space	M-NCPPC	Private
Create Blair Park	Parks and Open Space	M-NCPPC	Private
Create Sonny's Park (Blairs Development)	Parks and Open Space	M-NCPPC	Private
Create South Silver Spring Park	Parks and Open Space	M-NCPPC*	
Create Metro Center Park	Parks and Open Space	M-NCPPC	Private
Renovate Acorn Park	Parks and Open Space	M-NCPPC*	
Renovate Ellsworth Urban Park	Parks and Open Space	M-NCPPC*	
Create Fitness Park (Blairs Development)	Parks and Open Space	M-NCPPC	Private
Develop Philadelphia Avenue Urban Park	Parks and Open Space	M-NCPPC*	
Expand/Renovate Fenton Street Park	Parks and Open Space	M-NCPPC*	
Create Fenton Village Neighborhood Green	Parks and Open Space	M-NCPPC	Private
Create Downtown North Park	Parks and Open Space	M-NCPPC	Private
Create Ellsworth District Park (WF Site)	Parks and Open Space	M-NCPPC	Private
Create Bonifant-Dixon Park	Parks and Open Space	M-NCPPC	Private
Create Rachel Carson, Blair Stomping, The Mews, and Lucy's Landing Parks	Parks and Open Space	M-NCPPC	Private
New Pedestrian Bridge Connection (South Silver Spring to Ripley District)	Transportation, Parks and Open Space	M-NCPPC, MCDOT, CSX	
Green Loop Streets	Transportation, Parks and Open Space	MCDOT, SHA M-NCPPC	Private
1st Avenue Extension	Transportation	MCDOT	Private
1st Avenue Alley	Transportation	MCDOT	Private
Thayer Avenue to Bonifant Street Extension	Transportation	MCDOT	Private
Silver Spring Avenue to Sligo Avenue Extension	Transportation	MCDOT	Private
[Mayor Lane to Fenton Street Extension]	[Transportation]	[MCDOT]	[Private]
<u>Commercial</u> Shared Street: Bonifant Street (Ramsey Street to Georgia Avenue)	Transportation	MCDOT	Private
<u>Commercial</u> Shared Street: Ellsworth Drive (Fenton Street to Veterans Plaza)	Transportation	MCDOT	Private
<u>Commercial</u> Shared Street: Newell Street (Kennett Street to East-West Highway)	Transportation	MCDOT	Private
Pedestrian/Bicycle Connection: Fenton Street to Mayor Lane (along with the redevelopment of Parking Lot 4 site in Fenton Village)	Transportation	MCDOT	Private
Pedestrian/Bicycle Connection: Bonifant Street to Wayne Avenue	Transportation	MCDOT	Private
East-West Highway road diet	Transportation	M-NCPPC, MDOT SHA	
Georgia Avenue road diet <u>for dedicated bus lanes</u>	Transportation	M-NCPPC, MDOT SHA	
Colesville Road road diet <u>for dedicated bus lanes</u>	Transportation	M-NCPPC, MDOT SHA	
Colesville Road: removal of dynamic lane operation	Transportation	M-NCPPC, MDOT SHA	
<u>Upgrade all crosswalks to continental pavement marking</u>	<u>Transportation</u>	<u>M-NCPPC, MDOT SHA</u>	<u>Private</u>
Protected Crossings	Transportation	M-NCPPC, MCDOT, MDOT SHA	

Install new sidewalks (close existing sidewalk gaps)	Transportation	M-NCPPC, MCDOT	Private
Traffic Calming Study on Blair Road	Transportation	M-NCPPC, MCDOT	DDOT
Upgrade bus stops with shelters	Transportation	M-NCPPC, MCDOT	Private
Long-term bicycle parking at transit stations (Sarbanes Transit Center, Silver Spring Library Purple Line Station)	Transportation	M-NCPPC, WMATA, MTA	
Short-term bicycle parking in Silver Spring CBD	Transportation	M-NCPPC	Private
13th Street (Eastern Avenue to Georgia Avenue) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
16th Street (Spring Street to District of Columbia) separated bike lanes	Transportation	M-NCPPC, MDOT SHA	Private
2nd/Wayne Avenue (Spring Street to Colesville Road) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
2nd/Wayne Avenue (Colesville Road to Cedar Street) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Capital Crescent Trail	Transportation	M-NCPPC, MCDOT	Private
Fenton Street Extended bicycle and pedestrian [shared use] side path	Transportation	M-NCPPC, MCDOT	Private
Fenton Street (Ellsworth Drive to King Street) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Fenton Street (Cameron Street to Ellsworth Drive) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Burlington Avenue (Georgia Avenue to Fenton Street) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Cameron Street (Spring Street to 2nd Avenue) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Dixon Avenue (Wayne Avenue to Georgia Avenue) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Newell Street (District of Columbia to East-West Highway) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Silver Spring Avenue (Georgia Avenue to Fenton Street) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Spring Street/Cedar Street (16th Street to Wayne Avenue) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private
Blair Mill Road (Eastern Avenue and East-West Highway) separated bike lanes	Transportation	M-NCPPC, MCDOT	Private

Page 186: Revise Table 13. Historic Preservation Implementation Summary as follows:

Table 7. Historic Preservation Implementation Summary

Description	Time Frame	Category
Study and evaluate the Weller's Dry-Cleaning Building (8237 Fenton Street) for future listing in the <i>Master Plan for Historic Preservation</i> or recordation of a Historic Preservation Easement.	Long Term	Historic Preservation

Phase One: Create a Silver Spring Legacy Business Registry that documents all retail and service-orientated businesses in the Plan area. Phase Two: Study potential incentives to preserve local, independently owned businesses.	Short Term	Historic Preservation/DHCA/ Research
Collaborate with local partners and engage the broader community to discuss potential interpretation opportunities in Silver Spring.	Short Term	Historic Preservation
Study the following properties for listing in the National Register of Historic Places: <ul style="list-style-type: none"> o Medical Office Building (1111 Spring Street) o Metropolitan Building (8720 Georgia Avenue) o Montgomery Center (8630 Fenton Street) o Operations Research, Inc., (1400 Spring Street) o [Perpetual Bank Building (8700 Georgia Avenue)] o U.S. Industries Building (949 Bonifant Avenue) o American National Bank Building (8701 Georgia Avenue) o Garden and Mid-Rise Garden Apartment District 	Long Term	Historic Preservation
Support the recommendations of the Montgomery County Street and Parks Facilities Renaming Review Project on the renaming of county streets and park facilities.	Long Term	Historic Preservation/ IRC Division

Page 187: Revise the following bullets under “4.5.1 Partnerships for Economic Growth” as follows:

- [Partner with] Encourage a partnership between the Montgomery County Economic Development [Council] Corporation (MCEDC) and other entities in the downtown [the champion for downtown] to create a market incentive to invest in speculative office suites in office space that has been vacant for an extended period. [Have MCEDC fund the program and the champion for the downtown market and help property owners secure funds.] Create a plan for the incentive that caps the amount at a reasonable cost to the county while also providing a meaningful incentive to building owners.
- [Analyze the feasibility of a fund operated by MCEDC to invest in office-using start-ups that locate in Silver Spring. Have the champion for downtown market the fund and use the fund to attract additional business to Silver Spring. Create a plan for the fund that establishes accepted levels of risk that is consistent with purchasing equity positions in start-up firms.]
- Partner with [the County Executive’s Business Advancement Team] County government to reinvigorate the Silver Spring Innovation Center to meet the needs of and to support women and minority entrepreneurs, in alignment with the recommendations of the Business Incubator Review and Entrepreneurial Ecosystem study prepared by Axcel Innovation LLC and presented to the PHED committee on July 26, 2021. [Create a business plan to define how the incubator

- 2202 can add value to start-up firms to guide operations and to enable evaluation of whether the
 2203 incubator is achieving its objectives.]
- 2204 • Encourage activation of underutilized space: Partner with MCEDC, the County Executive’s
 2205 Business Advancement Team, and [the champion for downtown] other entities to study a vacancy
 2206 tax on empty retail frontage to encourage property owners to lease and activate vacant spaces.
 2207 The funds generated by this tax should be returned to the Silver Spring area. The money should
 2208 be [provided to the new champion for downtown Silver Spring and go into the funds this Plan
 2209 recommends creating to advance placemaking] used to advance placemaking in the downtown.
 - 2210 • [Create capacity to support small retailers: Form] Encourage a partnership between the
 2211 [Montgomery County Office of the County Executive Small Business Navigator] County and
 2212 mission-oriented non-profit stakeholders to fund a diverse retail liaison position to support
 2213 diverse retailers in Silver Spring. Explore creating a loan pool that could provide resources and
 2214 incentives to local small business, help subsidize tenant improvements, and could support
 2215 business owners in purchasing their properties. See the March 2021 Retail in Diverse
 2216 Communities Report [published in March 2021] for additional details about these
 2217 recommendations.
 - 2218 • Encourage property owners with underutilized and vacant street-level retail space to donate that
 2219 space to mission-oriented non-profits to run retail incubators in which entrepreneurs can try new
 2220 retail concepts.
 - 2221 • The Plan supports the [establishment of a “champion” entity] strengthening of entities in the
 2222 downtown that will assist with marketing, activation, and maintenance. [Such an entity] These
 2223 entities could help address issues of safety and trash collection on the streets, as well as promote,
 2224 highlight, and support the many amenities the downtown has to offer. Downtown Silver Spring
 2225 will [benefit from a strong champion for downtown and will] need support from partnerships
 2226 between the public sector, property owners, businesses, and social service organizations in order
 2227 to full achieve the vision expressed in this Plan.

2228
 2229 Page 188: Revise the second paragraph under “4.5.2. Arts and Entertainment District” as follows:

2230
 2231 The Plan supports the following recommendations that could enhance and grow the Arts and
 2232 Entertainment District in Silver Spring and contribute significantly to the economic growth of
 2233 the downtown. Many of these recommendations encourage partnership between the Arts and
 2234 Entertainment District and the [“champion for downtown”] other entities described above.

2235
 2236 Page 189: Revise the third bullet under “4.5.2. Arts and Entertainment District” as follows:

2237
 2238 New public art in Silver Spring, whether through public process or through optional method
 2239 development, should represent and support the diverse communities of Silver Spring.

2240
 2241 Page 189: Revise the first paragraph under “4.5.4 Partnerships with the Community” as follows:

2242
 2243 As discussed in Section [2.6]3.5 Parks and Public Spaces, this Plan supports the goal of providing
 2244 sufficient open space for active recreation, social gathering, and contemplative activities. The
 2245 Plan supports the following recommendation to achieve that goal:

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 2247
 2248
 2249 **General**

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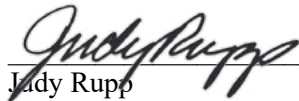
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All illustrations and tables included in the Plan will be revised to reflect the District Council changes to the Planning Board Draft Silver Spring Downtown and Adjacent Communities Plan (Winter 2022). The text and graphics will be revised as necessary to achieve and improve clarity and consistency, to update factual information, and to convey the actions of the District Council. Graphics and tables will be revised and re-numbered, where necessary, to be consistent with the text and titles.

This is a correct copy of Council action.



Judy Rupp

Clerk of the Council

Elected and Appointed Officials

County Council

Gabe Albornoz, President

Evan Glass, Vice President

Andrew Friedson

Tom Hucker

Will Jawando

Sidney Katz

Nancy Navarro

Criag Rice

Hans Riemer

County Executive

Marc Elrich

The Maryland-National Park and Planning Commission

Casey Anderson, Chair

Peter A. Shapiro, Vice Chair

Montgomery County Planning Board:

Casey Anderson, Chair

Partap Verma, Vice Chair

Gerald Cichy

Tina Patterson

Carol Rubin

Prince George's County Planning Board:

Peter A. Shapiro, Chair

Dorothy Bailey, Vice Chair

William Doerner

Manuel Geraldo

A. Shuanise Washington



SILVER SPRING

DOWNTOWN AND ADJACENT COMMUNITIES PLAN

APPROVED AND ADOPTED JUNE 2022

M-NCPPC
Montgomery County Planning Department
2425 Reedie Drive, Floor 14
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 **Montgomery Planning**

[MontgomeryPlanning.org](https://montgomeryplanning.org)