Address:	1 High Street, Brookeville	Meeting Date:	8/17/2022
Resource:	Primary Resource (Brookeville Historic District)	<b>Report Date:</b>	8/10/2022
Applicant:	Sajid Niazi (Jeffrey Lees, Architect)	Public Notice:	8/3/2022
Review:	HAWP	Tax Credit:	N/A
Case Number:	Pending	Staff:	Michael Kyne
PROPOSAL:	New construction, hardscape alterations, and new fence		

# MONTGOMERY COUNTY HISTORIC PRESERVATION COMMISSION STAFF REPORT

# **STAFF RECOMMENDATION:**

Staff recommends that the HPC approve with conditions the HAWP application.

- 1. The exposed foundation of the addition along Market Street will be simplified, and the entire foundation will be either parged CMU or brick, with final review and approval delegated to staff.
- 2. The proposed PVC trim and frieze board will be fiber cement.
- 3. The proposed vinyl fencing will be wood, with final review and approval delegated to staff.
- 4. The proposed 6' high replacement fencing along the east (rear) property line will not be extended closer to Market Street. Any additional fencing in this location will be constructed from wood, no higher than 4', and have an open picket design, with final review and approval delegated to staff.

# **ARCHITECTURAL DESCRIPTION:**

SIGNIFICANCE:Primary Resource within the Brookeville Historic District<br/>Brookeville Post OfficeDATE:1922



Fig. 1: Subject property at the southeastern corner of High Street (Georgia Avenue) and Market Street.

# **BACKGROUND:**

The applicant previously appeared before the Commission for a preliminary consultation at the April 20, 2022 HPC meeting.<sup>1</sup>

# **PROPOSAL:**

The applicant proposes new construction, hardscape alterations, and a new fence at the subject property.

# **APPLICABLE GUIDELINES:**

When reviewing alterations and new construction within the Brookeville Historic District several documents are to be utilized as guidelines to assist the Commission in developing their decision. These documents include the *Brookeville Historic District Master Plan Amendment (Amendment), Montgomery County Code Chapter 24A-8 (Chapter 24A-8)*, and the *Secretary of the Interior's Standards for Rehabilitation (Standards).* The pertinent information in these documents is outlined below.

# Montgomery County Code; Chapter 24A-8

- (a) The commission shall instruct the director to issue a permit, or issue a permit subject to such conditions as are found to be necessary to ensure conformity with the purposes and requirements of this chapter, if it finds that:
  - (1) The proposal will not substantially alter the exterior features of an historic site or historic resource within an historic district; or

<sup>&</sup>lt;sup>1</sup> Link to April 20, 2022 preliminary consultation staff report: <u>https://montgomeryplanning.org/wp-content/uploads/2022/04/II.A-1-High-Street-Brookeville-Preliminary-Consultation.pdf</u> Link to April 20, 2022 HPC meeting audio/video transcript:

http://mncppc.granicus.com/MediaPlayer.php?publish\_id=55685b4d-c1a8-11ec-a5da-0050569183fa

- (2) The proposal is compatible in character and nature with the historical, archeological, architectural or cultural features of the historic site or the historic district in which an historic resource is located and would not be detrimental thereto or to the achievement of the purposes of this chapter; or
- (3) The proposal would enhance or aid in the protection, preservation and public or private utilization of the historic site or historic resource located within an historic district in a manner compatible with the historical, archeological, architectural or cultural value of the historic site or historic district in which an historic resource is located; or
- (4) The proposal is necessary in order that unsafe conditions or health hazards be remedied; or
- (5) The proposal is necessary in order that the owner of the subject property not be deprived of reasonable use of the property or suffer undue hardship; or
- (6) In balancing the interests of the public in preserving the historic site or historic resource located within an historic district, with the interests of the public from the use and benefit of the alternative proposal, the general public welfare is better served by granting the permit.
- (b) It is not the intent of this chapter to limit new construction, alteration or repairs to any 1 period or architectural style.
- (d) In the case of an application for work on an historic resource located within an historic district, the commission shall be lenient in its judgment of plans for structures of little historical or design significance or for plans involving new construction, unless such plans would seriously impair the historic or architectural value of surrounding historic resources or would impair the character of the historic district. (*Ord. No. 9-4, § 1; Ord. No. 11-59.*)

# Secretary of Interior's Standards for Rehabilitation

The Secretary of the Interior defines rehabilitation as "the act or process of making possible a compatible use for a property through repair, alterations, and additions while preserving those portions or features, which convey its historical, cultural, or architectural values." The applicable *Standards* are as follows:

- 2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.
- 9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.
- 10. New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

## **STAFF DISCUSSION:**

The subject property is a 1,502 SF one-story building located at the southeast intersection of High Street (Georgia Avenue) and Market Street in the Brookeville Historic District. The historic building was constructed in 1922 and formerly served as the Brookeville Post Office. It has a traditional ell form and fronts on High Street to the west. The rear ell is at the northeast (rear/left, as viewed from the public right-of-way of High Street) side of the building. There is an existing one-story addition in the southeast (rear/right) corner of the historic building and a parking lot to the south (right).

The applicant previously appeared before the Commission for a preliminary consultation at the April 20, 2022 HPC meeting. At the preliminary consultation, the applicant proposed to remove the existing onestory addition and construct a new 2,582 SF (1,426 SF on each level) two-story addition at the east (rear) and southeast (rear/right) side of the historic building. The proposed new addition was to be connected to the historic building by a hyphen with stairwell and elevator, and the proposed elevator was to be expressed at the south (right) side of the addition. The proposed materials for the addition included fiber cement siding, PVC trim, asphalt shingle roofing, aluminum-clad ganged windows, and an aluminum-framed storefront entrance at the south (right) side. The applicant also proposed to reduce the number of parking spaces in the existing parking lot to the south (right) side of the historic building and enlarge the existing one-car parking area/driveway at the north (left) side off Market Street, creating a two-car parking pad.

The Commission commented that the historic building is a prominent and significant building within the Brookeville Historic District. They found that the previous proposal was incompatible with the historic district, and that it detracted from the character of the historic building.

Specific comments included:

- The style of the proposed addition is too modern and suburban in character, and it should be more reflective of the historic building and the district's rural village character. Specifically:
  - The design and details of the proposed addition should be simplified, so that the addition is clearly secondary and deferential to the historic building.
  - The proposed windows should be single, not ganged.
  - The roofing should be metal, taking cues from the historic building, not asphalt shingles.
  - If an elevator is required, the expressed tower should be removed, with the elevator moved entirely to the interior, and the mechanics moved to the basement to eliminate the tower/dormer above the roofline.
  - The majority supported the use of alternative materials (i.e., aluminum-clad wood windows and fiber cement siding).
  - There was some support for the proposed hipped roof form, as it takes design cues from the historic building.
- The scale and massing of the proposed addition is incompatible with the historic building; it is too tall and should be scaled down.
  - The majority found that the proposed addition should be one story (with programming needs reduced in size and/or moved to the basement level, if necessary), and that it should not exceed the height of the historic building.
  - The use of space needs to be carefully considered, and the proposed addition and programming should be scaled down to fit what is approvable within the historic district.
  - Some suggested expanding the footprint of the proposed addition, keeping it to one story while allowing for needed interior space.

- Others suggested lowering the floor lines and the ridge of the proposed two-story addition, or, alternatively, building the first floor of the proposed two-story addition into the grade, so that it is perceived as one story and does not exceed the height of the historic building.
  - Where this was suggested, the Commissioners found that the hyphen/connector between the historic building and proposed addition should be smaller in scale and held to one story, providing separation and differentiation.
- The parking requirements need to be considered carefully, as there is no on street parking in the immediate vicinity, and the proposed number of offices suggests a need for a larger number of parking spaces than can be accommodated.
  - Some commented that, if the building size and programming are reduced, the amount of required parking would also be reduced.
- The Commission recommended that the applicant work with staff and the Brookeville LAP as they revise their proposal.

The applicant has returned with a revised proposal in response to the Commission's previous comments and recommendations. Specifically: the proposed addition has been reduced to one-story, eliminating the need for an elevator; the footprint of the proposed addition has been reduced, going from 1,426 SF to 1,050 SF; the height of the proposed addition has been reduced to be lower than the historic building; the fenestration has been revised, with single windows in place of the previously proposed ganged windows; and the proposed parking has been increased from five to seven spaces.

Regarding materials, the applicant proposes fiber cement lap siding, PVC trim and frieze board, one-overone aluminum clad wood windows, aluminum storefront framing for differentiation where the addition connects to the historic building, and asphalt shingle roofing. The applicant still proposes to enlarge the existing one-car parking area/driveway at the north (left) side of the building off Market Street, creating a two-car parking pad. New concrete walkways and steps from Market Street are also proposed.

One new aspect of the proposal is the replacement of the existing 6' high wood privacy fence at the east (rear) side of the property with a new 6' high vinyl privacy fence. The existing fence does not run the entire length of the east (rear) property line, beginning approximately in the middle of the parking lot, and terminating where the parking lot drops off to the one-car parking area/driveway off Market Street. However, the proposed replacement fence will run almost the entire length of the east (rear) property line, beginning at the south (right) end of the property and extending closer to Market Street. Additional fencing of the same height and material is proposed along the south (right) property line.

Staff is generally supportive of the applicant's revised proposal, finding that it responds appropriately to the Commission's previous comments and recommendations. The revised proposal is generally compatible with the subject property and surrounding streetscape, in terms of location, scale and massing, materials, and design. The subject property has been a commercial and/or institutional building since its construction in 1922, making the proposed use appropriate. Additionally, the proposal will reactivate this historically significant building, which has been vacant and unused for some time, at this highly prominent intersection in the historic district.

However, to ensure compatibility, staff recommends several conditions of approval. Specifically:

• As proposed, the exposed foundation of the addition along Market Street is designed with implied brick columns at the corners and parged CMU between. Staff recommends that this design be simplified, and that the entire foundation be either parged CMU or brick.

- Regarding the proposed trim and frieze board, staff finds that PVC is an incompatible material, which does not accurately reflect traditional materials, and recommends that fiber cement be used instead.
- Staff finds that the proposed vinyl fencing is incompatible with the Brookeville Historic District and that the proposed fencing should be wood.
- Finally, to preserve the openness of the streetscape, staff finds that the proposed 6' high fencing along the east (rear) property line should not be extended closer to Market Street. Any additional fencing in this location should be constructed from wood, no higher than 4', and have an open picket design.

Staff finds that the applicant's proposal, as modified by the recommended conditions, will not remove or alter materials, features, or spaces that characterize the property, per *Standard* #2. Additionally, the proposed new construction will be differentiated from and compatible with the massing, size, scale, and architectural features of the property and its environment, per *Standard* #9. In accordance with *Standard* #10, the proposed work will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

After full and fair consideration of the applicant's submission, staff finds the proposal, as modified by the recommended conditions, consistent with the Criteria for Issuance in Chapter 24A-(b) 1 and 2, having found it consistent with the *Secretary of the Interior's Standards for Rehabilitation #2, #9*, and *#10*, as outlined above.

# **STAFF RECOMMENDATION**

Staff recommends that the Commission **approve with the conditions outlined on Page 1** the HAWP application under the Criteria for Issuance in Chapter 24A-8(b) (1), (2), and (d), having found that the proposal, as modified by the conditions, will not substantially alter the exterior features of the historic resource and is compatible in character with the purposes of Chapter 24A;

and with the Secretary of the Interior's Standards for Rehabilitation #2, #9, and #10;

and with the general condition that the applicant shall present an electronic set of drawings, if applicable, to Historic Preservation Commission (HPC) staff for review and stamping prior to submission for the Montgomery County Department of Permitting Services (DPS) building permits;

and with the general condition that final project design details, not specifically delineated by the Commission, shall be approved by HPC staff or brought back to the Commission as a revised HAWP application at staff's discretion;

and with the general condition that the applicant shall notify the Historic Preservation Staff if they propose to make **any alterations** to the approved plans. Once the work is completed the applicant will <u>contact the staff person</u> assigned to this application at 301-563-3400 or <u>michael.kyne@montgomeryplanning.org</u> to schedule a follow-up site visit.

HISTORIC AF	CATION F REA WORI SERVATION COM 01.563.3400	OR K PEF	DATE ASS	F ONLY:
APPLICANT:				
Name: HRA Holdings LLC	E-ma	il:		
Address: 2624 Dapple Grey Co	ourt City:	Olney		Zip: 20832
Daytime Phone:	Tax A	Account No	o.:	
AGENT/CONTACT (if applicable):				
Name:	E-ma	il:		
Address:	City:			Zip:
Daytime Phone:	Cont	ractor Reg	gistration No	D.:
LOCATION OF BUILDING/PREMISE: MIHE	P # of Historic Prop	erty		
Is the Property Located within an Historic D Is there an Historic Preservation/Land Trus map of the easement, and documentation Are other Planning and/or Hearing Examine (Conditional Use, Variance, Record Plat, etc supplemental information.	No/Ind t/Environmental E from the Easemen er Approvals /Revi c.?) If YES, include	ividual Sit asement nt Holder s ews Requ informatio	te Name on the Prope supporting th ired as part	erty? If YES, include a his application. of this Application?
Building Number: s	Street: High St	treet		
Town/City: Brookeville	Nearest Cross Stre	et:		
Lot: Block: S	Subdivision:	_ Parcel:		
Addition Fence	this application. oply: /Porch cape/Landscape make the foregoir Il comply with plan	Incomple	ete Applicat Shed/Garag Solar Tree remova Window/Doo Other: tion, that th d and appro	tions will not ge/Accessory Structure al/planting or e application is correct oved by all necessary

# HAWP APPLICATION: MAILING ADDRESSES FOR NOTIFING

[Owner, Owner's Agent, Adjacent and Confronting Property Owners]

Owner's mailing address	Owner's Agent's mailing address
	nfronting Property Owners mailing addresses
212 Market Street Brookeville, MD 20833	3 High Street Brookeville, MD 20833
211 Market Street Brookeville, MD 20833	2 High Street Brookeville, MD 20833

# Jeffrey A. Lees, AIA

MD. Registered Architect #5919

07 August, 2022

Montgomery County Historic Preservation Commission 2425 Reedie Drive Wheaton, MD 20902

# Re: #1 High Street Brookeville Redevelopment (revised)

Dear Commission Members;

I am writing this to introduce you to the proposed development of the above contributing structure within the Brookeville Historic District.

It is the owner's intent to create two (2) professional Office Suites via a combination of rehabilitating the existing one story (former) Brookeville Post Office building as one office suite, and then adding a second suite within a one story addition, for an overall total of 2787 square feet. The existing later shed roofed addition on the east side will be removed.

The occupancy will include minimum personnel, with guests/visitors accommodated strictly through scheduled appointments. The use requires limited parking and eliminates the potential for additional traffic or congestion.

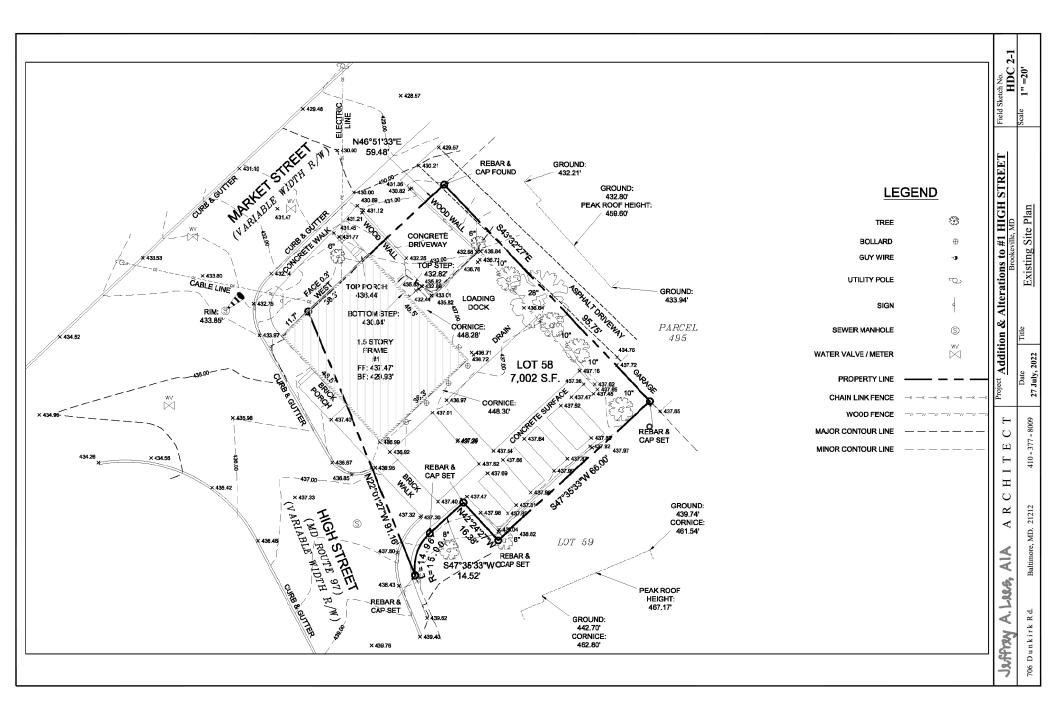
The exterior of the existing building currently represents changes wrought during a 1999 renovation for an accounting office, with some contemporary windows and patio doors introduced into the rear shed roofed addition. There is little to no historic fabric remaining on the interior of the structure, which was basically eviscerated during the 1999 renovation.

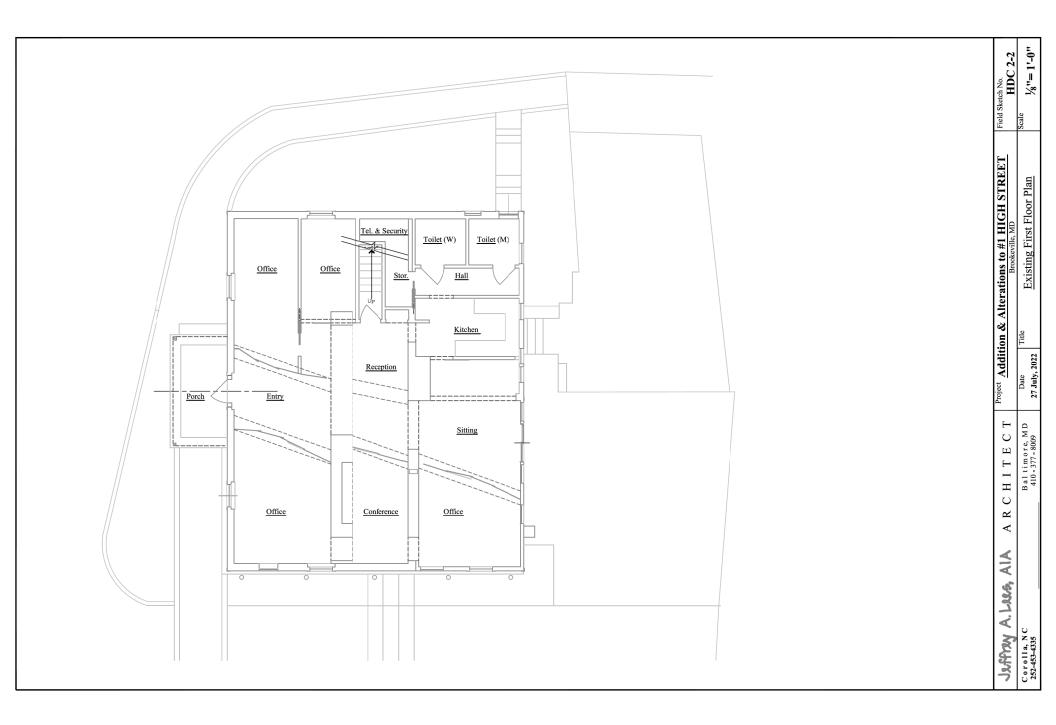
Materials for the one story addition are intended to be compatible with the existing structure, and include fiber cement lap type siding, aluminum clad wood windows, aluminum storefront framing (to separate and distinguish new and old) and asphalt roof shingles, similar in color, but again distinct from the existing metal roofing.

Our goal is to create a balance between honoring the historic presence of the property, while enhancing the community's appeal and usability of the property. Many residents within the community have expressed concern about the aging and deterioration of the property due to its remaining vacant for a considerable amount of time, lacking maintenance or attention. This often leaves residents frustrated and ultimately results in a continued decline in the community, as opposed to enhancing the properties functionality and street appeal, while maintaining the charm and significance of the property and the community.

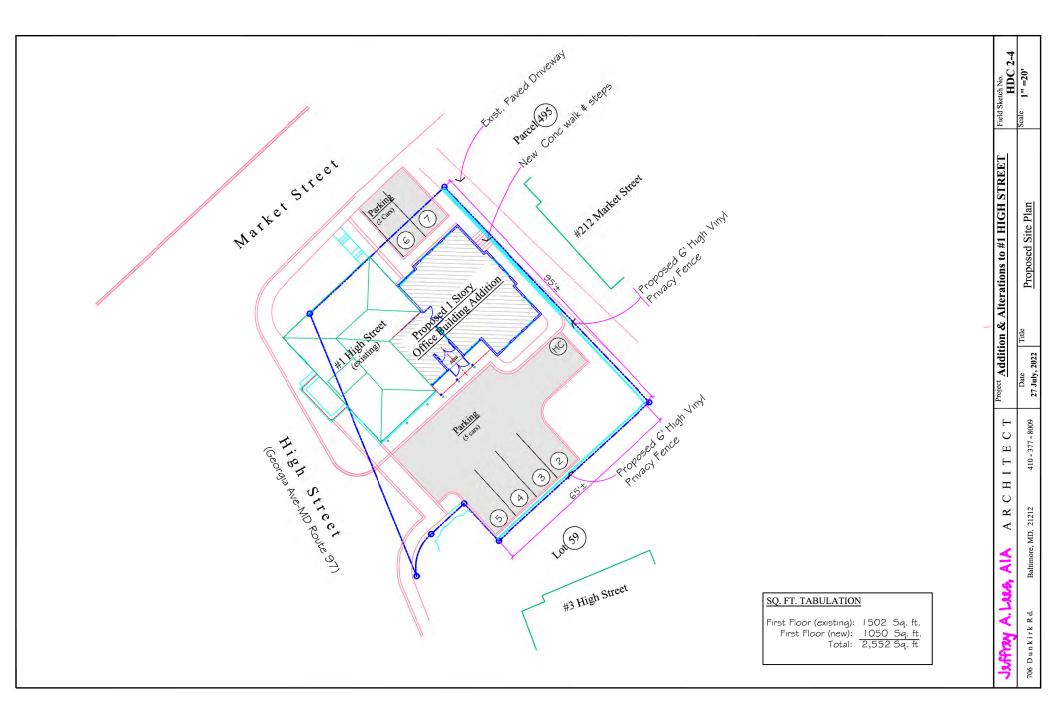
Honoring the property's significance while enhancing the community, will provide a fantastic opportunity to the residents and community as a whole. As the Brookeville Bypass reaches completion, traffic will be reduced tremendously, which we believe further supports the proposed plans.

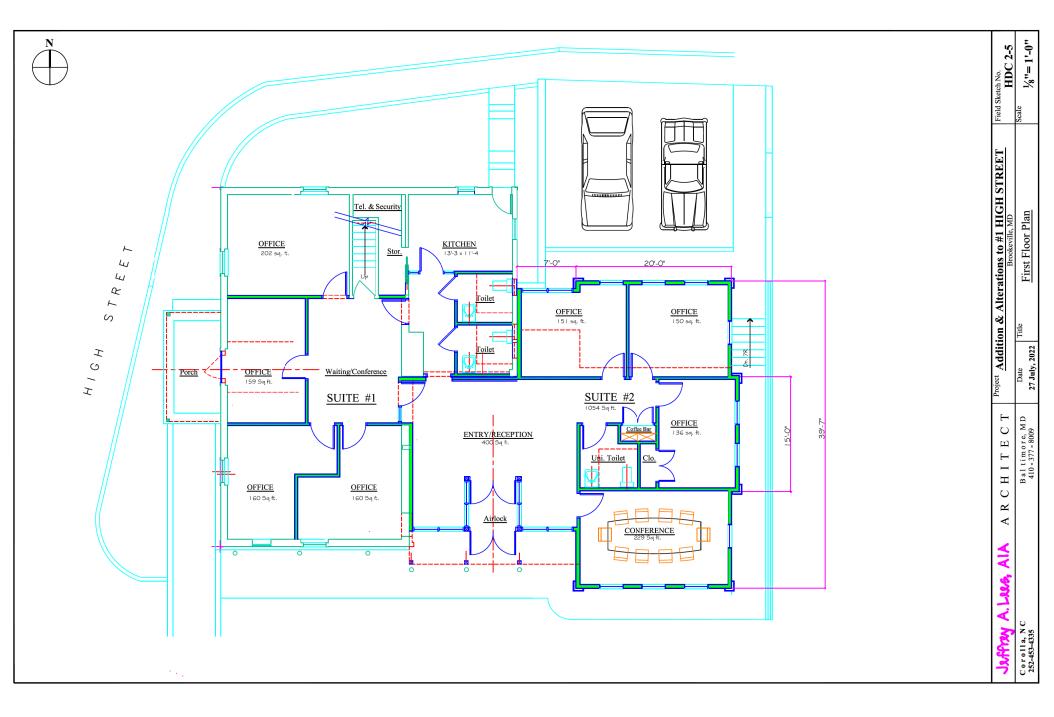
**706 Dunkirk Road** 549 Trolling Lane Baltimore, Maryland 21212 Corolla, NC 27927 **(410) 377-8009** (252) 453-4335

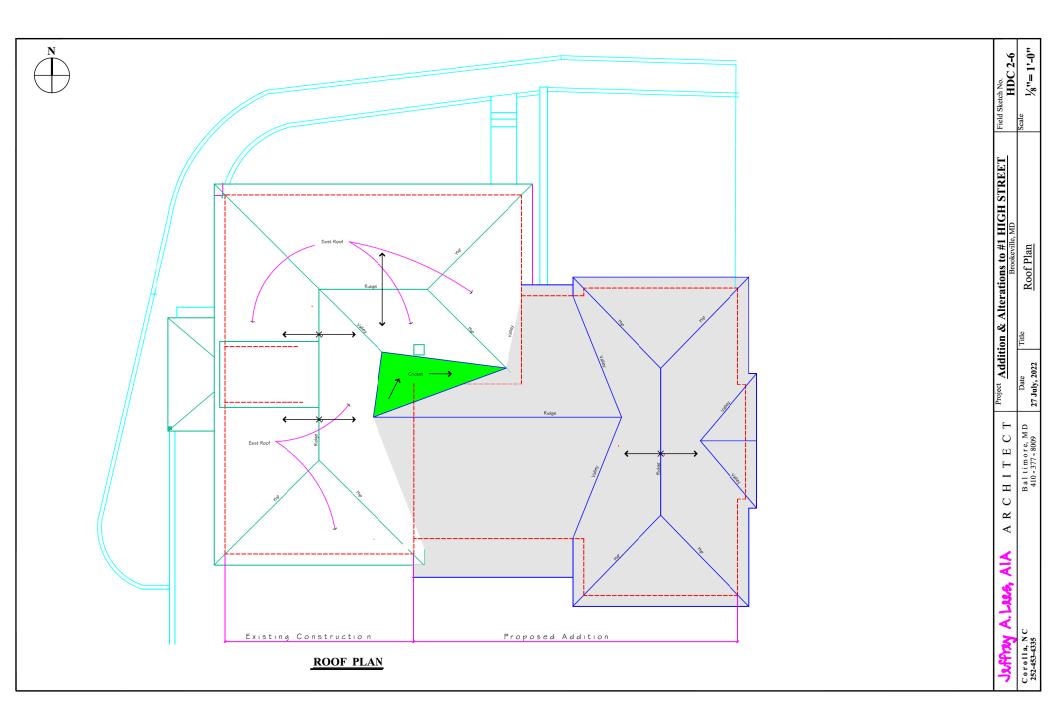


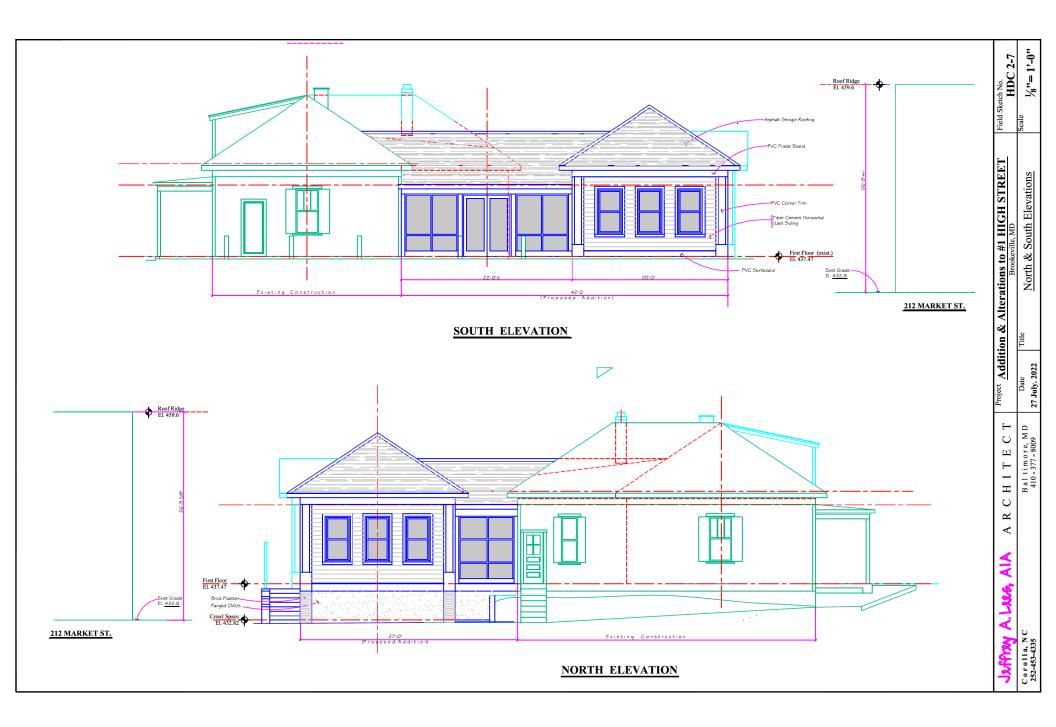


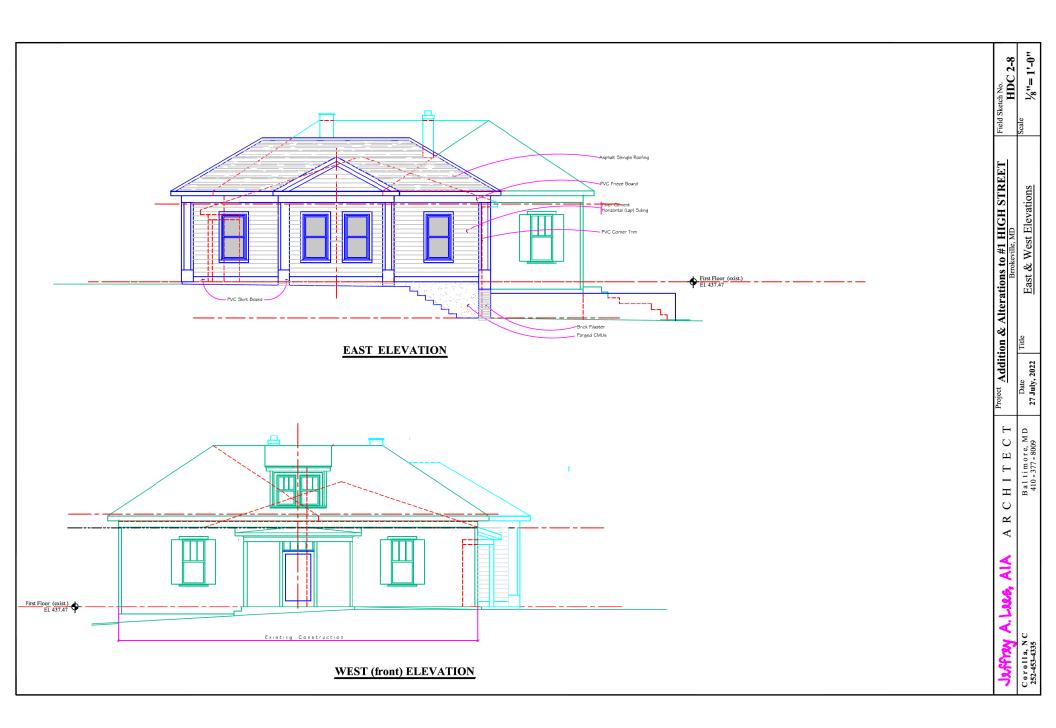




















# hardie® plank

You can't go wrong with this simple, clean look.

## Statement Collection™ Products

Size Options

Weight	0.312" 2.34 lbs. per square foot 144"
Widths	8.25"
Exposures Pcs./Pallet	7" 210

#### Siding Color Options



Need more color/size options for a customer?

+ See Dream Collection™ Colors/Sizes

#### Primed for Paint

#### Size Options

Thickness	0.312"			
Weight	2.40 lbs. pe	er square foot		
Length	144"			
Widths	6.25"	5.25"	7.25"	
Exposures	<u>5</u> *	4"	6"	
Pcs/Pallet	308	360	252	
Widths	8.25"			
Exposures	7-			
Prs /Pallel	230			



# Detailed Product Description - Aluminum-Clad Exterior

#### Frame

- Select softwood, immersion treated with Pella's EnduraGuard<sup>®</sup> wood protection formula in accordance with WDMA I.S.-4. The EnduraGuard formula includes three active ingredients for protection against the effects of moisture, decay, stains from mold and mildew. Plus, an additional ingredient adds protection against termite damage.
- Interior exposed surfaces are clear pine.
- Exterior surfaces are clad with aluminum.
- Components are assembled with screws, staples and concealed corner locks.
- Overall frame depth is 5" (127mm) for a wall depth of 3-11/16" (94mm). .
- Optional factory applied jamb extensions available between 4-9/16" (116mm) and 7-3/16" (183mm) wall depths.
- Vinyl jamb liner [LX includes wood / clad inserts].
- Optional factory installed fold-out installation fins with flexible fin corners.
- Optional factory-applied EnduraClad<sup>®</sup> exterior trim.

#### Sash

- Select softwood, immersion treated with Pella's EnduraGuard® wood protection formula in accordance with WDMA I.S.-4. The EnduraGuard formula includes three active ingredients for protection against the effects of moisture, decay, stains from mold and mildew. Plus, an additional ingredient adds protection against termite damage.
- Interior exposed surfaces are clear pine.
- ۰ Exterior surfaces are clad with aluminum and sealed.
- Corners mortised and tenoned, glued and secured with metal fasteners. •
- ٠ Sash thickness is 1-7/8" (47mm).
- Sash exterior and interior profile is ogee.
- Double-Hung: Upper sash has surface-mounted wash locks] [Single-Hung: Fixed upper sash has surface-mounted wash locks] [Arch Head units have no wash locks].
- Lower sash has concealed wash locks in lower check rail.
- Sashes tilt for easy cleaning.
- Simulated-Hung units have non-operable upper and lower sashes.

#### Weatherstripping

- Water-stop santoprene-wrapped foam at head and sill.
- Thermoplastic elastomer bulb with slip-coating set into lower sash for tight contact at check rail.
- Vinyl-wrapped foam inserted into jamb liner to seal against sides of sash.

#### **Glazing System**

- Quality float glass complying with ASTM C 1036.
- Custom and high altitude glazing available.
- Silicone-glazed 11/16" dual-seal insulating glass [[annealed] [tempered]] [[clear] [[Advanced Low-E] [SunDefense™ Low-E] [AdvancedComfort] [NaturalSun Low-E] with argon]] [[bronze] [gray] [green] Advanced Low-E with argon].

#### Exterior

- Aluminum-clad exteriors shall be finished with EnduraClad<sup>®</sup> protective finish, in a multi-step, baked-on finish.
  - Color is [standard] [feature] [custom]1. - or
- Aluminum-clad exteriors shall be finished with EnduraClad Plus protective finish with 70% fluoropolymer resin in a multi-step, baked-on finish Color is [standard] [feature] [custom]1.

#### Interior

 [Unfinished, ready for site finishing] [factory primed with one coat acrylic latex] [factory prefinished [paint] [stain] 1].

#### Hardware

- Galvanized block-and-tackle balances are connected to self-locking balance shoes which are connected to the sashes using zinc die cast terminals and concealed within the frame.
- Sash lock is [standard (cam-action)] [historic spoon-style] [air-conditioner lock]. Two sash locks on units with frame width 37" and greater.
- Optional sash lift furnished for field installation. Two lifts on units with frame width 37" and greater.
- Hardware finish is [baked enamel [Champagne] [White] [Brown] [Matte Black]] [Satin Nickel] [Oil-rubbed Bronze] [Distressed Bronze] [Distressed Nickel].

#### **Optional Products**

#### Grilles

- Integral Light Technology® grilles
  - Interior grilles are [7/8"] [1-1/4"] [2"] ogee profile that are solid pine. Interior surfaces are [unfinished, ready for site finishing] [factory primed] [factory prefinished [paint] [stain] 1].
  - Exterior grilles are [7/8"] [1-1/4"] [2"] ogee profile that are extruded aluminum.
  - Patterns are [Traditional] [Prairie] [Top Row] [New England] [Victorian] [Diamond].
  - Insulating glass contains non-glare spacer between the panes of glass.
  - Grilles are adhered to both sides of the insulating glass with VHB acrylic adhesive tape and aligned with the non-glare spacer. - or -
- Grilles-Between-the-Glass<sub>2</sub>
  - Insulating glass contains 3/4" contoured aluminum grilles permanently installed between two panes of glass. • Patterns are [Traditional] [9-Lite Prairie] [Cross] [Top Row]

  - Interior color is [White] [Tan 3] [Brown 3] [Putty 3] [Black] [Ivory] [Sand Dune] [Harvest] [Cordovan] [Brickstone].
  - Exterior colors is [standard]1.

#### Screens InView<sup>™</sup> screens

- [Half-Size] [Full-Size 4] black vinyl-coated 18/18 mesh fiberglass screen cloth complying with the performance requirements of SMA 1201, set in aluminum frame fitted to outside of window, supplied complete with all necessary hardware.
- Full screen spreader bar placed on units > 37" width or > 65" height. . Insect screen frame finish is baked enamel, color to match window
- cladding.

#### Hardware

- Optional factory applied limited opening device available for vent units in
- stainless steel; nominal 3-3/4" opening. Limiting device concealed from view. Optional factory applied window opening control device available. Device allows window to open less than 4" with normal operation, with a release mechanism that allows the sash to open completely. Complies with ASTM F2090-10.

(1) Contact your local Pella sales representative for current designs and color options.

(2) Available on units glazed with Low-E insulated glass with argon, clear insulated glass and obscure insulated glass.

(3) Tan, Brown and Putty Interior GBG colors are available in single-tone (Brown/Brown, Tan/Tan or Putty/Putty). Other interior colors are also available with Tan or Brown exterior.

(4) Full screens are available on units ≤ 96" height.

(5) Appearance of exterior grille color will vary depending on Low-E coating on glass.

DH-29

#### TRIFAB® VG (VERSAGLAZE®) TRIFAB® VG 450, 451 & 451T (THERMAL) FRAMING SYSTEMS & TRIFAB® 451UT (ULTRA THERMAL) FRAMING SYSTEM



# Design + Performance Versatility with Unmatched Fabrication Flexibility



Trifab<sup>®</sup> VersaGlaze<sup>®</sup> is built on the proven and successful Trifab<sup>®</sup> platform – with all the versatility its name implies. There are enough framing system choices, fabrication methods, design options and performance levels to please the most discerning building owner, architect and installer. The 4.5" depth Trifab<sup>®</sup> VersaGlaze<sup>®</sup> Framing System family is available with non-thermal, thermal and ultra-thermal performance levels. The ultra-thermal Trifab<sup>®</sup> 451UT Framing System, is designed for the most demanding thermal performance and employs a dual Isolock<sup>®</sup> thermal break.

#### AESTHETICS

Trifab<sup>®</sup> VersaGlaze<sup>®</sup> Framing Systems offer designers a choice of front-, center-, back- or multi-plane glass applications. Structural silicone

glazing (SSG) and weatherseal glazing options further expand designers' choices, allowing for a greater range of possibilities for specific project requirements and architectural styles. All systems have a 4-1/2" frame depth; Trifab<sup>®</sup> VersaGlaze<sup>®</sup> 450 has 1-3/4" sightlines, while Trifab<sup>®</sup> VersaGlaze<sup>®</sup> 451/451T and Trifab<sup>®</sup> 451UT have 2" sightlines.

With seamless incorporation of Kawneer entrances or windows, including GLASSvent<sup>®</sup> visually frameless ventilators, Trifab<sup>®</sup> framing can be used on almost any project. These framing systems can also be packaged with Kawneer curtain walls and overhead glazing, thereby providing a full range of proven, and tested, quality products for the owner, architect and installer from a single-source supplier.

#### ECONOMY

Trifab® VersaGlaze® 450/451/451T/451UT Framing Systems offer a variety of fabrication choices to suit your project:

- Screw Spline for economical continuous runs utilizing two-piece vertical members that provide the option to pre-assemble units with controlled shop labor costs and smaller field crews for handling and installation. (available for all systems)
- Shear Block for punched openings or continuous runs using tubular moldings with shear block clips that provide tight joints for transporting large pre-assembled multi-lite units. (available for 450/451/451T systems)
- Stick for fast, easy field fabrication. Field measurements and material cuts can be done when metal is on the jobsite. (available for 450/451/451T systems)
- Pre-glazed The combination of screw spline construction with pre-glazing in the shop accelerates installation and reduces field labor time while minimizing disruption to the surrounding area or existing tenants. Making it an exceptional choice for new or retrofit applications, particularly in urban areas or where space is limited. (available for 451/451T/451UT framing)



Brighton Landing Cambridge, Massachusetts ARCHITECT ADD Inc., Cambridge, Massachusetts GLAZING CONTRACTOR Ipswich Bay Glass Company,Inc., Rowley, Massachusetts PHOTOGRAPHER © Gordon Schenck, Jr.

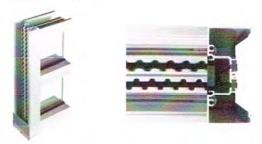
All systems can be flush glazed from either the inside or outside. The weatherseal option provides an alternative to SSG vertical mullions for Trifab® VersaGlaze® 450/451/451T. This ABS/ASA rigid polymer extrusion allows complete inside glazing and creates a flush glass appearance on the building exterior without the added labor of scaffolding or swing stages. Additionally, high-performance flashing options are engineered to eliminate perimeter sill fasteners and associated blind seals.

#### FOR THE FINISHING TOUCH

Architectural Class I anodized aluminum and painted finishes in fluoropolymer (AAMA 2605) and solvent-free powder coatings (AAMA 2604) offer a variety of color choices.

#### PERFORMANCE

Kawneer's Isolock<sup>®</sup> thermal break technology creates a composite section, prevents dry shrinkage and is available on Trifab<sup>®</sup> VersaGlaze<sup>®</sup> 451T. For even greater thermal performance, a dual Isolock<sup>®</sup> thermal break is used on Trifab<sup>®</sup> 451UT.



Trifab® 451UT uses a dual Isolock® thermal break (right) and features a new highperformance sill design, which incorporates a screw-applied end dam (left), ensuring positive engagement and tight joints between the sill flashing and end dam.

U-factor, CRF values and STC ratings for Trifab® framing systems vary depending upon the glass plane application. Project-specific U-factors can be determined for each individual project. (See the Kawneer Architectural Manual or Kawneer.com for additional information.)

Thermal simulations showing temperature variations from exterior/cold side to interior/warm side.







Trifab<sup>®</sup> VersaGlaze<sup>®</sup> 451

17 32 54 42 3P 67 6P 1

# Trifab® 451UT

COLD WARM

#### PERFORMANCE TEST STANDARDS

Air Infiltration	ASTM E283
Water	AAMA 501, ASTM E331
Structural	ASTM E330
Thermal	AAMA 1503
Thermal Break	AAMA 505, AAMA TIR-A8
Acoustical	AAMA 1801, ASTM E1425





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555 Guthridge Court 770.449.5555 Norcross, GA 30092 kawneer.com



ARCHITECTURAL SYSTEMS | ENTRANCES + FRAMING | CURTAIN WALLS | WINDOWS



GRAND MANOR®

Luxury Roofing Shingles





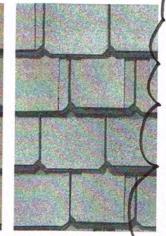
# GRAND MANOR® COLOR PALETTE



Black Pearl

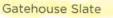


Brownstone



Colonial Slate





Georgian Brick

# Strength with Style

# GRAND MANOR®

- Class A fire resistance
- Algae resistance
- 110 MPH wind warranty, upgradable to 130 MPH
- Lifetime-limited warranty



# SPECIFICATIONS

- Two full-size, fiberglass-based shingles with randomly applied tabs
- Authentic depth and dimension of natural slate
- Virtual five-layer coverage when applied
- · 425 lbs. per square

CertainTeed products are tested to ensure the highest quality and comply with the following industry standards:

## Fire Resistance:

- UL Class A
- UL certified to meet ASTM D3018 Type 1

## Wind Resistance:

- UL certified to meet ASTM D3018 Type 1
- ASTM D3161 Class F

# Tear Resistance:

- UL certified to meet ASTM D3462
- CSA standard A123.5

# Wind Driven Rain Resistance:

 Miami-Dade Product Control Acceptance: Please reference www.certainteed.com to determine approved products by manufacturing location.

### Quality Standards:

• ICC-ES-ESR-1389 & ESR-3537

# WARRANTY

- Lifetime limited transferable warranty against manufacturing defects on residential applications
- 50-year limited transferable warranty against manufacturing defects on group-owned or commercial applications
- STREAK Fighter 15-year algae-resistance warranty
- 10-year SureStart<sup>™</sup> protection
- 15-year 110 MPH wind-resistance warranty
- Wind warranty upgrade to 130 MPH available. CertainTeed starter and CertainTeed hip and ridge required.

See actual warranty for specific details and limitations.



# Staff's Site Visit Photos & Supplemental Fence Information























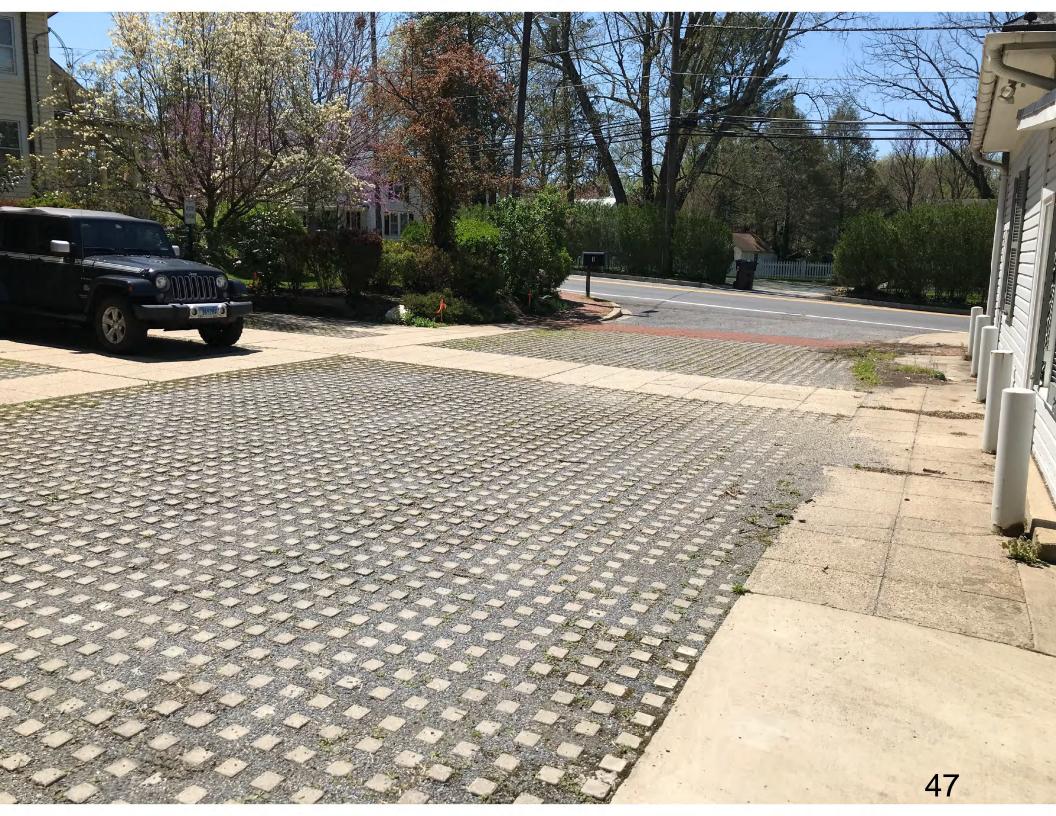
























































Existing and proposed fencing with approximate lengths, as prepared by staff.