Address:	8 Primrose Street, Chevy Chase	Meeting Date:	4/6/2022
<b>Resource:</b>	Contributing Resource Chevy Chase Village Historic District	Report Date:	3/30/2022
Applicant:	Justin & Kimberly Shur	Public Notice:	3/23/2022
<b>Review:</b>	Historic Area Work Permit	Tax Credit:	Partial
Case Number:	984993	Staff:	Dan Bruechert
Proposal:	Building Addition, demolition, door replacement	, and skylight installa	ation

# MONTGOMERY COUNTY HISTORIC PRESERVATION COMMISSION STAFF REPORT

# **STAFF RECOMMENDATION**

Staff recommends that the HPC approve <u>with one (1) condition</u> the HAWP:

1. Removing and patching the window on the second story on the west elevation (EX211) violates the *Design Guidelines* and 24A-8(b)(2) and the window needs to be retained as part of the house rehabilitation. Final approval authority to ensure the condition has been satisfied is delegated to Staff.

# **ARCHITECTURAL DESCRIPTION:**

SIGNIFICANCE:	Contributing Resource within the Chevy Chase Village Historic District
STYLE:	Mediterranean Revival
DATE:	1927



Figure 1: 8 Primrose Street, Chevy Chase

# **PROPOSAL**

The applicant proposes to demolish a non-historic modification to the rear of the house, construct small additions to the rear of the house, replace the front door and portico, install storm windows in the side porch openings, and make several changes to the fenestration.

# **APPLICABLE GUIDELINES**

When reviewing alterations and new construction within the Chevy Chase Village Historic District several documents are to be utilized as guidelines to assist the Commission in developing their decision. These documents include the historic preservation review guidelines in the approved and adopted amendment for the Chevy Chase Village Historic District (*Guidelines*), *Montgomery County Code Chapter 24A* (*Chapter 24A*), and the *Secretary of the Interior's Standards for Rehabilitation (Standards*). The pertinent information in these documents is outlined below.

## Chevy Chase Village Historic District Guidelines

The *Guidelines* break down specific projects into three levels of review - Lenient, Moderate and Strict Scrutiny.

**"Lenient Scrutiny"** means that the emphasis of the review should be on issues of general massing and scale, and compatibility with the surrounding streetscape, and should allow for a very liberal interpretation of preservation rules. Most changes should be permitted unless there are major problems with massing, scale or compatibility.

"**Moderate Scrutiny**" involves a higher standard of review than "lenient scrutiny." Besides issues of massing, scale and compatibility, preserving the integrity of the resource is taken into account. Alterations should be designed so that the altered structure still contributes to the district. Use of compatible new materials, rather than the original building materials, should be permitted. Planned changes should be compatible with the structure's existing design, but should not be required to replicate its architectural style.

"**Strict Scrutiny**" means that the planned changes should be reviewed to insure that the integrity of the significant exterior architectural or landscaping features and details is not compromised. However, strict scrutiny should not be "strict in theory but fatal in fact" i.e. it does not mean that there can be no changes but simply that the proposed changes should be reviewed with extra care.

- <u>*Decks*</u> should be subject to moderate scrutiny if they are visible from the public right-of-way, lenient scrutiny if they are not
- *Doors* should be subject to moderate scrutiny if they are visible from the public right-of-way, lenient scrutiny if they are not.
- <u>*Dormers*</u> should be subject to moderate scrutiny if they are visible from the public right-ofway, lenient scrutiny if they are not.
- <u>Exterior trim</u> (such as moldings on doors and windows) on contributing resources should be subject to moderate scrutiny if it is visible from the public right-of-way, lenient scrutiny if it is not. Exterior trim on Outstanding resources should be subject to strict scrutiny if it is visible from the public right-of-way.
- *Lot coverage* should be subject to strict scrutiny, in view of the critical importance of preserving the Village's open park-like character.
- <u>Major additions</u> should, where feasible, be placed to the rear of the existing structure so that they are less visible from the public right-of-way. Major additions which substantially alter or obscure the front of the structure should be discouraged, but not automatically prohibited.

For example, where lot size does not permit placement to the rear, and the proposed addition is compatible with the street scape, it should be subject to moderate scrutiny for contributing resources, but strict scrutiny for outstanding resources.

- <u>*Porches*</u> should be subject to moderate scrutiny if they are visible from the public right-ofway, lenient scrutiny if they are not. Enclosures of existing side and rear porches have occurred throughout the Village with little or no adverse impact on its character, and they should be permitted where compatibly designed.
- <u>Roofing materials</u> should be subject to moderate scrutiny if they are visible from the public right-of-way, lenient scrutiny if they are not. In general, materials differing from the original should be approved for contributing resources. These guidelines recognize that for outstanding resources replacement in kind is always advocated
- <u>Shutters</u> should be subject to moderate scrutiny if they are visible from the public right-ofway.
- *Siding* should be subject to moderate scrutiny if it is visible from the public right-of-way, lenient scrutiny if it is not.
- <u>Windows</u> (including window replacement) should be subject to moderate scrutiny if they are visible from the public right-of-way, lenient scrutiny if they are not. Addition of compatible exterior storm windows should be encouraged, whether visible from the public-right-of-way or not. Vinyl and aluminum windows (other than storm windows) should be discouraged.
- The *Guidelines* state five basic policies that should be adhered to, including:
  - Preserving the integrity of the Chevy Chase Village Historic District. Any alterations should, at a minimum, perpetuate the ability to perceive the sense of time and place portrayed by the district.
  - Preserving the integrity of contributing structures. Alterations should be designed in such a way that the altered structure still contributes to the district.
  - Maintaining the variety of architectural styles and the tradition of architectural excellence.
  - Design review emphasis should be restricted to changes that will be visible from the front or side public right-of-way, or that would be visible in the absence of vegetation or landscaping.
  - Alterations to the portion of a property that are not visible from the public-right-of-way should be subject to a very lenient review. Most changes to the rear of the properties should be approved as a matter of course.

# Montgomery County Code; Chapter 24A-8

- (b) The commission shall instruct the director to issue a permit, or issue a permit subject to such conditions as are found to be necessary to insure conformity with the purposes and requirements of this chapter, if it finds that:
  - (1) The proposal will not substantially alter the exterior features of an historic site or historic resource within an historic district; or
  - (2) The proposal is compatible in character and nature with the historical, archeological, architectural or cultural features of the historic site or the historic district in which an historic resource is located and would not be detrimental thereto or to the achievement of the purposes of this chapter; or
- (d) In the case of an application for work on an historic resource located within an historic district, the commission shall be lenient in its judgment of plans for structures of little historical or design significance or for plans involving new construction, unless such plans would seriously impair the historic or architectural value of surrounding historic resources or would impair the character of the historic district.

## Secretary of the Interior's Standards for Rehabilitation:

#2: The historic character of a property will be retained and preserved. The removal of distinctive materials or alteration of features, spaces, and spatial relationships that characterize a property will be avoided.

#9: New additions, exterior alterations, or related new construction will not destroy historic materials, features, and spatial relationships that characterize the property. The new work shall be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the integrity of the property and its environment.

#10: New additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

## STAFF DISCUSSION

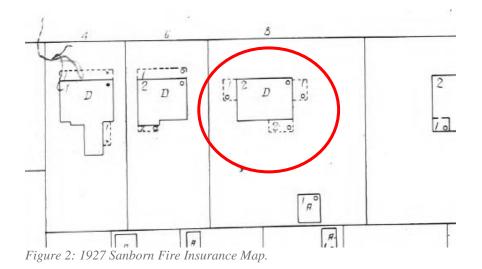
The subject property is a two-story, stucco-sided, Mediterranean Revival house, with a tile roof. The applicant proposes to make several changes to the house as part of a whole-house rehabilitation. The changes include:

- Demolishing non-historic features and constructing two small additions,
- Replacing the front door and portico in-kind,
- Install storm windows in the side porch, and
- Alter the fenestration on three elevations.

Note: changes to the hardscape shown in the plans *are illustrative only and are not part of this HAWP proposal*. The applicant will return with a new HAWP or an amendment to this HAWP in the future with any hardscape alterations.

## **Demolition and Addition Construction**

When the subject property was constructed, there were three open porches. Two one-story porches, one each to the east and west, and a second-story sleeping porch in the rear (see 1927 Sanborn Fire Insurance Map, below). At some point, the sleeping porch was enclosed in stucco to match the house finish and a bay window was constructed at the rear. The applicant proposes to demolish the rear walls and construct a new addition at the rear. The addition will be covered in stucco to match the finish of the existing house and will include a variety of window configurations. On the ground floor, the applicant proposes to install three six-over-one wood sash windows and a pair of six-over-nine wood sash windows. The rear addition's foundation will be stone to match the historic house and the applicant proposes to install a stoop and stairs using stone to match the foundation. The applicant proposes to install skylights to the rear-projecting gable roof.



Staff finds that the alterations to the rear will not be visible from the public right-of-way and, under the *Design Guidelines*, are entitled to a very lenient review. Staff finds that the design and materials for the rear addition are compatible with the house and surrounding district and recommends approval under the *Design Guidelines* and 24A-8(b)(2) and (d).

The applicant also proposes to construct a small addition to the rear of the eastern porch. The proposed addition is 7'  $9'' \times 8' {}^{3}{}^{4}$ " (seven feet, nine inches deep by eight feet and three-quarters inch wide) and is inset approximately 6" (six inches) from the porch wall plane. The addition will be sided with wood panels, with a stone foundation, and will have wood casement windows. The porch roof will be extended to the rear in matching clay tile.

Staff finds this proposed addition, will be partially visible from the public right-of-way and should be reviewed under moderate scrutiny. Staff finds the design and materials are compatible with the house and surrounding district. Extending the roofline to cover the rear addition is a change that will not be visible from the public right-of-way and while the *Design Guidelines* do not require projects in the historic district to adhere to Standards 9 and 10, Staff finds that the siding material and inset will help to differentiate the new addition from the historic construction. Staff recommends the HPC approve the addition to the rear of the east side porch under the *Design Guidelines*, 24A-8(b)(2) and (d), and Standards 2 and 9.



Figure 3: an addition is proposed to the rear of the one-story side porch.

## **Front Door and Portico Replacement**

At the front, the applicant proposes to replace the front door, sidelights and fanlight; and front portico. These materials will be replaced in-kind and therefore do not require a Historic Area Work Permit.

## **Side Porch Enclosure**

The applicant proposes to enclose the east-side one-story porch. The existing screens will be removed and new wood-framed single-pane storm windows will be installed in the existing openings. The new storm windows will fit in the existing openings but will be divided into three vertical sections, instead of the horizontal sections shown above in Fig. 3.

The *Design Guidelines* state enclosed porches have occurred throughout the district and have had little-tono adverse impact on the character of the district. Porch enclosures should be permitted in cases where their design is compatible with the resource. Staff finds, the proposal, utilizing wood frames, filling the existing opening, is a compatible design and should be approved under the *Design Guidelines*.

## **Fenestration Alteration**

The applicant proposes to alter the fenestration on the side and rear elevations. On the right (west) elevation, the applicant proposes to remove the six-over-one wood sash window and shutters in the second floor opening closest to the front wall plane (labeled EX211 in the plans). The opening will be patched with stucco and painted to match the finish of the historic walls. On the second floor of the left (east) elevation, the applicant proposes removing the two rear six-over-one sash windows (EX201 and EX202), patching the walls, and installing a single wood six-over-one sash window. On the rear elevation, the applicant proposes to remove the first-floor casement windows on either side of the

chimney and install a much larger pair of wood casement windows. On the second floor, the applicant proposes to create two new window openings and install two pairs of wood casement windows.

Staff finds the changes at the rear should be approved as a matter of course under a very lenient review, because they are not visible from the public right-of-way. Additionally, Staff finds the proposed new wood casement windows are an appropriate material and design to be compatible with the historic character of the house.

Based on the photographic evidence submitted, Staff finds the existing historic windows on the east elevation (labeled EX201 and EX202) appear to be in acceptable condition. As these windows are visible from the public right-of-way, their removal and replacement need to be reviewed under moderate scrutiny. Staff finds the proposed replacement window is an appropriately detailed wood window to be a compatible replacement for the historic windows at the subject property; the proposed windows match the dimensions and materials of the historic. The larger question is, whether it is appropriate to remove historic window openings and re-locate the openings. The historic house is a symmetrical design, five bays wide on the front and four bays deep on the sides. While removing one window opening on the east elevation and changing the window pattern, Staff finds that the proposal is acceptable on this elevation. Staff finds that while these two windows are visible from the public right-of-way (see Fig. 3), they are the third and fourth bay to the rear and are partially obscured by the porch roof. Because the windows are further removed from the streetscape with reduced visibility, Staff finds they are less significant than the windows closer to the front wall plane. Additionally, Staff finds that installing one window that matches the appearance of the historic and will not leave a blank wall plane and is an acceptable mitigating measure. Staff finds that with these windows removed and the new window installed, the house will still contribute to the district. Staff recommends the HPC approve the east window removal and replacement under the Design Guidelines and 234A-8(d).

While Staff recommends approval for the window replacement on the east elevation, Staff cannot recommend approving the removal of the window on the west elevation, EX211, the window's shutters, and patching of the siding. Window EX211 appears to be in good working order and, it appears to Staff, that the window removal is driven entirely by changes to the interior plan. The proposal places a new sink and vanity in front of this window opening. Staff supports retaining this window for several reasons. Staff notes that proposed changes in the Chevy Chase Village are to be evaluated on how they are viewed from the right-of-way. This requires a stricter application of the approval criteria closer to the front of the house. First, because this window is closer to the front of the house, a large section of stucco at the front corner will disrupt the even window spacing on the right elevation (see the elevation drawings below). Second, Staff finds that removing the window and shutters will disrupt the even window spacing on the right elevation and negatively impact the house's design integrity. Under a moderate scrutiny review, the HPC needs to take preserving the integrity of the resource into consideration. Staff finds that the window and shutters are both original to the house construction and their removal would negatively impact the house's design and material integrity. Staff acknowledges that the house would likely still contribute to the surrounding streetscape; however, the change is not compatible with the original design of the house. Staff finds the removal of window (EX211) does not satisfy the requirements of "Moderate Scrutiny" in the Design Guidelines and does not comply with 24A-8(b)(1) and (2). Staff recommends the HPC add a condition to the approval of this HAWP that retains window EX211 in place with the existing shutters. The HPC should delegate final approval authority to Staff to verify that this condition has been satisfied upon the applicant's submission of the final permit drawings.





*Figure 4: Existing right elevation.* 

Figure 5: Proposed right elevation.

# **STAFF RECOMMENDATION**

Staff recommends that the Commission **approve with one (1) condition** the HAWP application;

1. Removing and patching the window on the second story on the west elevation (EX211) violates the *Design Guidelines* and 24A-8(b)(2) and the window needs to be retained as part of the house rehabilitation. Final approval authority to ensure the condition has been satisfied is delegated to Staff;

Under the Criteria for Issuance in Chapter 24A-8(b)(2) and (d), and the *Chevy Chase Village Historic District Guidelines*, having found that the proposal will not substantially alter the exterior features of the historic resource and is compatible in character with the district and the purposes of Chapter 24A;

and with the Secretary of the Interior's Standards for Rehabilitation #2, 9, and 10;

and with the general condition that the applicant shall present an electronic set of drawings, if applicable, to Historic Preservation Commission (HPC) staff for review and stamping prior to submission for the Montgomery County Department of Permitting Services (DPS) building permits;

and with the general condition that final project design details, not specifically delineated by the Commission, shall be approved by HPC staff or brought back to the Commission as a revised HAWP application at staff's discretion;

and with the general condition that the applicant shall notify the Historic Preservation Staff if they propose to make **any alterations** to the approved plans. Once the work is completed the applicant will <u>contact the staff person</u> assigned to this application at 301-563-3400 or <u>dan.bruechert@montgomeryplanning.org</u> to schedule a follow-up site visit.

Adjacent and Confronting Properties:

Chevy Chase, MD 20815

12 Primrose Street

6 Primrose Street

11 Oxford Street

9 Oxford Street

7 Primrose Street

5 Primrose Street

# Shur Residence

8 Primrose St Chevy Chase, MD 20815

PROJECT	INFO	:

TOTAL FLOOR AREA:

GARAGE AREA: (INCL.)

TOTAL:

HOME WITH ATTIC AND BA	ASEMENT				
ADDRESS:	8 Primrose St Chevy Chase, MD 20815				
LOCATION:	BLOCK 57, LOT 11				
ZONING:	R-60				
SETBACKS:	FRONT: 25', SIDE:	7', REAR: 20'			
LOT AREA:	14,000 SF				
	ALLOWED:	PROPOSED:			
BUILDING HEIGHT:	30' MEAN	NO CHANGE			
LOT COVERAGE:	35% (4,900 SF)	SEE LANDSCAPE DOCS			
Building Area:					
	EXISTING:	PROPOSED:			
BASEMENT AREA:	1,217 SF	NO CHANGE			
FIRST FLOOR AREA:	1,697 SF	1,878 SF			
SECOND FLOOR AREA:	1,475 SF	1,532 SF			
ATTIC AREA:	659 SF	NO CHANGE			

PLANS PREPARED BASED ON THE FOLLOWING CODES:

5,286 SF

6,620 SF

NO CHANGE

5,048 SF

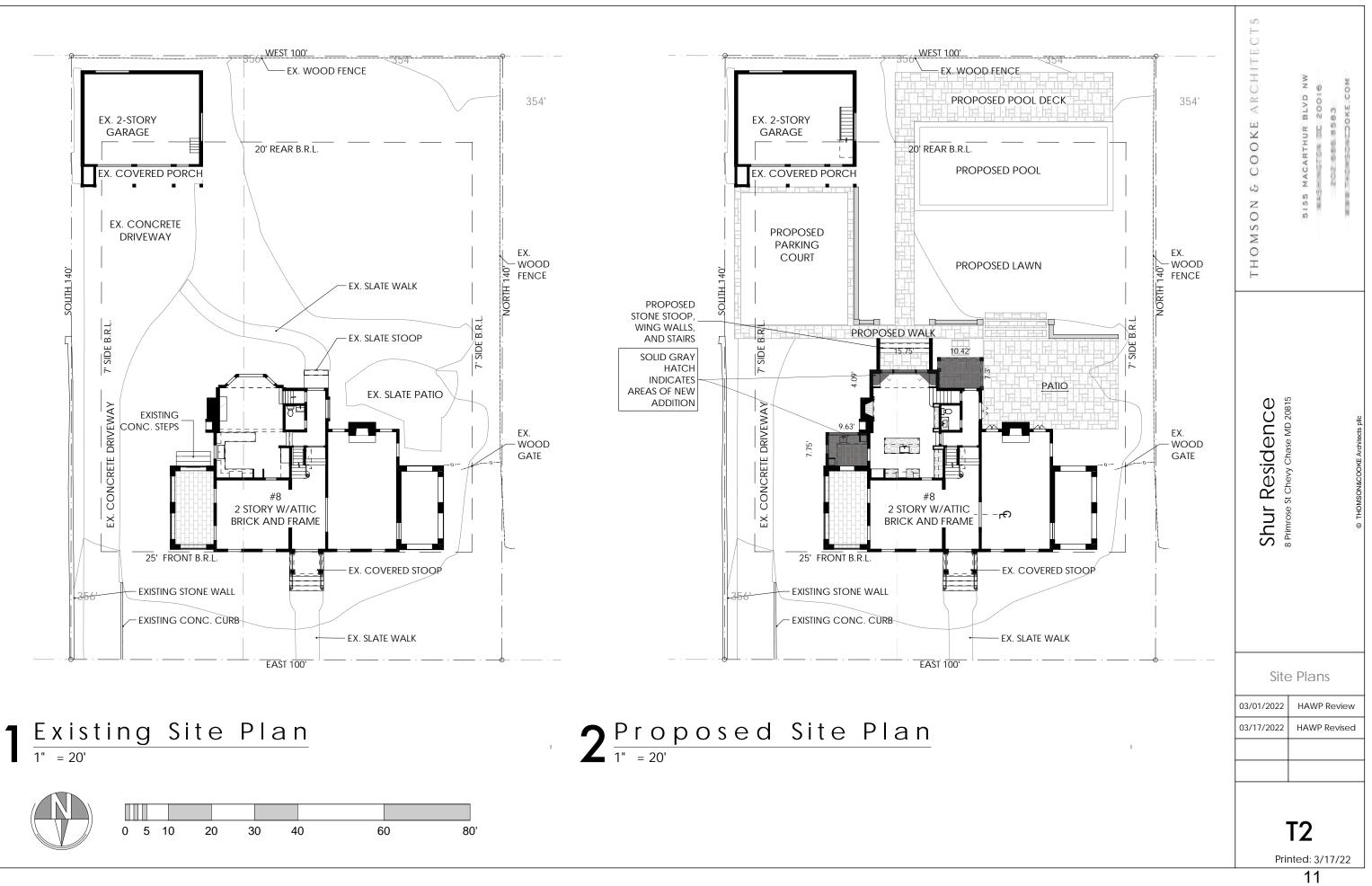
1,334 SF

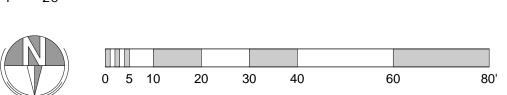
6,382 SF

2018 INTERNATIONAL RESIDENTIAL CODE AND 2018 INTERNATIONAL ENERGY CONSERVATION CODE AS AMENDED BY MONTGOMERY COUNTY EXECUTIVE REGULATION 31-19.



THOMSON & COOKE ARCHITECTS	SISS MACARTHUR BLVD NW	**************************************	T+CMSC#CD0KE.COM
Shur Residence			© THOMSON&COOKE Architects plic
03/01/2022 03/17/2022		PT VP Rev VP Rev	
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## Styles

## Traditional. Push Out and Mission® options.

Standard Features Natural, clear Douglas Fir interior (no visible finger

- 4 9/16" (116 mm) jamb construction
- LowE insulated glazing with 1/2" (13 mm) airspace Roto gear operator and concealed sash locks
- Extruded a
- colors, primed wood or clear fir exterior • Flexible
- Insect screens
- Metal handle, cover and locks

## Hardware

Multiple hardware type and finish choices are available See the Hardware in section A for more information

## Metal Clad Color Spectrum

All Palette colors, including anodized finishes, Available in Cyprium Collection

LowE Double, LowE Triple, Tranguility® and StormForce™

Ogee Profile - 3/4" (19 mm), 1 1/8" (30 mm), 2" (51 mm)

Putty Profile - 5/8" (16 mm), 7/8" (22 mm), 1 1/8" (30 mm),

Square Profile (interior only) - 3/4" (19 mm), 7/8" (22 mm),

Wood: 2" (51 mm) Brickmould, 3 1/2" (89 mm) Flat.

Metal Clad: 2" (51 mm) Brickmould, 3 1/2" (89 mm) Flat,

2" clad frame extension, Nose & Cove, Adams, Williamsburg

5 1/2" (139 mm) Flat, Adams and Williamsburg

StormForce is not available on all products.

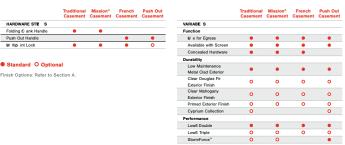
Simulated Divided Lites (SDL)

1 1/8" (30 mm), 2" (51 mm)

Casing

and Kerf.





B2 | Technical Guide Casement Windows

## **Product Features**

Standard O Optiona

Styles Double Hung, Single Hung, Radius Top and Cottage options.

## Standard Features

Natural, clear Do las Fir interior (no visible finge 4 9/16" (116 mm) jamb construction

- LowE insulated glazing with 1/2" (13 mm) airspace Extruded
- colors, primed wood or clear fit exterior
- Flexible

Insect screens

Metal locks

### Hardware Multiple hardware type and finish choices are available.

See the Hardware in section A for more information

## Specifications

## Standards

Most units have been tested by an independent laborator for air and water infiltration, structural performance, and ermal performance requir

## rame & Sash

lanufactured from Coastal Douglas Fir kiln-dried lumbe with frame construction designed for 4 9/16" (116 mm) amb. All wood exterior components are factory primed nless specified as clear exterior. Minor scratches or abrasions in the wood surface or primer are not onsidered defec

## Alternate Species

The entire Loewen p Mahogany

## Preservative Treated

All wood parts are dipped in approved pre Glazing

With countless glazing configurations and LowE coating options, we ensure that you can choose the perfect blend of protection and comfort

## Insulating Glass

Double or triple glass configurations with 1/2" (13 mm) airspace.

## LowE Systems

LowE best describes the benefits of the product that incorporates glazing coatings and Argon gas. LowE systems help reduce heating and cooling costs, providing superior energy efficiency.

**Specifications** 

nermal performance red

Standards

Frame & Sash

Alternate Species

Mahogany.

Glazing

The entire Loewen pro

Preservative Treated

protection and comfort.

Insulating Glass

and exterior Hardware Option

and exterior. Hardware Option

finishes. See section A.



for ease of cleaning. The roto gear operator will hold the sash at any position in its operating radius. The sash is supported by concealed heavy-duty hinges. All steel components are coated for superior corrosion protection

### Double Weatherstrip

The combination of a continuous, flexible foam weatherstrip and a flexible automotive type bulb weatherstrip ensures maximum energy efficiency and protection against air and water infiltration.

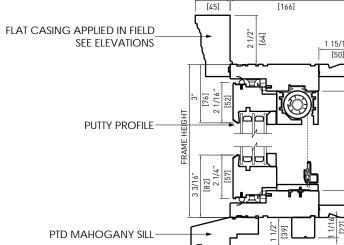
### Screen

Screens available in bronze, linen, Tuscany brown, brushed aluminum or black aluminum frame, screened with antiglare fiberglass cloth. Wood-framed screens and High Transparency mesh available. Optional Retractable Scree and Swinging Screen available. Swinging Screen available on Push Out models only.

### Egress

Consult local building codes for confirmation of size requirements for your area. Special egress hardware is available for Casement windows, which enables some sizes to meet egress codes, eliminating the need to go to the next larger size window. Consult your Authorized Loewen Dealer for more details.

Operator and sash locks are available in a variety of



1 13/16"

6 9/16"

# Casement Wind

NTS

# |<u>1 1/4"</u> |<u>32]</u>| FLAT CASING APPLIED IN FIELD SEE ELEVATIONS [116] - <del>|</del> 3/4 PUTTY PROFILE-PTD MAHOGANY SILL-

# Dbl Hung Sectio NTS

E2 | Technical Guide Double/Single Hung Windows



VARIABE S Function bl e for Egress Available with Screen
Durability
Low Maintenance Metal Clad Exterior Standard O Optional Clear Douglas Fir Exterior ish Ontions: Befer to Section J Clear Mahogany Exterior Finish Performance LowE Double LowE Triple

LOEWEN DBL HUNG WINDOW SPEC

tion opportunities exist – please contact your Authorized Loewen Dealer. S change without notice. Imperial and metric measurements are converted acc cause a 1 mm variance. (Example: 3/4" is shown as 19 mm for all glass meas

# Appearance SDL 0 0 0 0

Glazing LowE Double, LowE Triple and StormForce™. StormForce

## is not available on all products.

Simulated Divided Lites (SDL)

## Ogee Profile - 3/4" (19 mm), 1 1/8" (30 mm), 2" (51 mm)

Putty Profile - 5/8" (16 mm), 7/8" (22 mm), 1 1/8" (30 mm), Square Profile (interior only) - 3/4" (19 mm), 7/8" (22 mm),

1 1/8" (30 mm), 2" (51 mm) Casing

Wood: 2" (51 mm) Brickmould, 3 1/2" (89 mm) Flat, 5 1/2" (139 mm) Flat, Adams and Williamsburg.

Metal Clad: 2" (51 mm) Brickmould, 3 1/2" (89 mm) Elat 2" clad frame extension, Nose & Cove, Adams, Williamsburg and Kerf.

### Metal Clad Color Spectrum

All Palette colors, including anodized finishes.

Double/ Single Hung

# • • 0 .

Simulated Divided Lites (SDL) Standard SDL complete with airspace grilles, where available. Grille bars are permanently applied to the interior

LOEWEN CASEMENT WINDOW SPEC.

Numerous custom window configuration opportunities exist – please contact your Authorized Loewen Dealer. Specifications and technical information are subject to change without notice. Imperial and metric measurements are converte accurately. However, in some cases, industry standards cause a 1 mm variance. [Example: 3/4" is shown as 19 mm for all gias

Casement Windows Technical Guide | B3

Most units have been tested by an independent laboratory for air and water infiltration, structural performance, and can be natural wood Manufactured from Coastal Douglas Fir kiln-dried lumber with frame construction designed for 4 9/16" (116 mm) jam ardware

ardware is standard in bronze, linen, or black. Optional sash lifts are available at an additional charge. Operable eash with single-handle tilt latch enables inward tilting of sash for easy cleaning.

### Weatherstrip

Flexible weatherstrip ensures a positive weather seal. Screen

### Standard screens have a bronze, linen or aluminum frame screened with anti-glare fiberglass cloth. Screen-frame color is matched to exterior finish on metal clad units. High

transparency mesh, full screens and half screens available Egress

Consult local building codes for confirmation of size requirements for your area. Consult your Authorized Loewen Dealer for more details.

## airspace.

Simulated Divided Lites (SDL)

of finishes. See section A.

LowE Systems LowE best describes the benefits of the product that incorporates glazing coatings and Argon gas. LowE systems help reduce heating and cooling costs, providing superior

Standard SDL complete with airspace grilles, where available. Grille bars are permanently applied to the interior

Sash locks and optional sash lifts are available in a variety

Double or triple glass configurations with 1/2" (13 mm)

All wood exterior components are factory primed unless

All wood parts are dipped in approved preservative

With countless glazing configurations and LowE coating

options, we ensure that you can choose the perfect blend of

ne wood surface or primer are not co

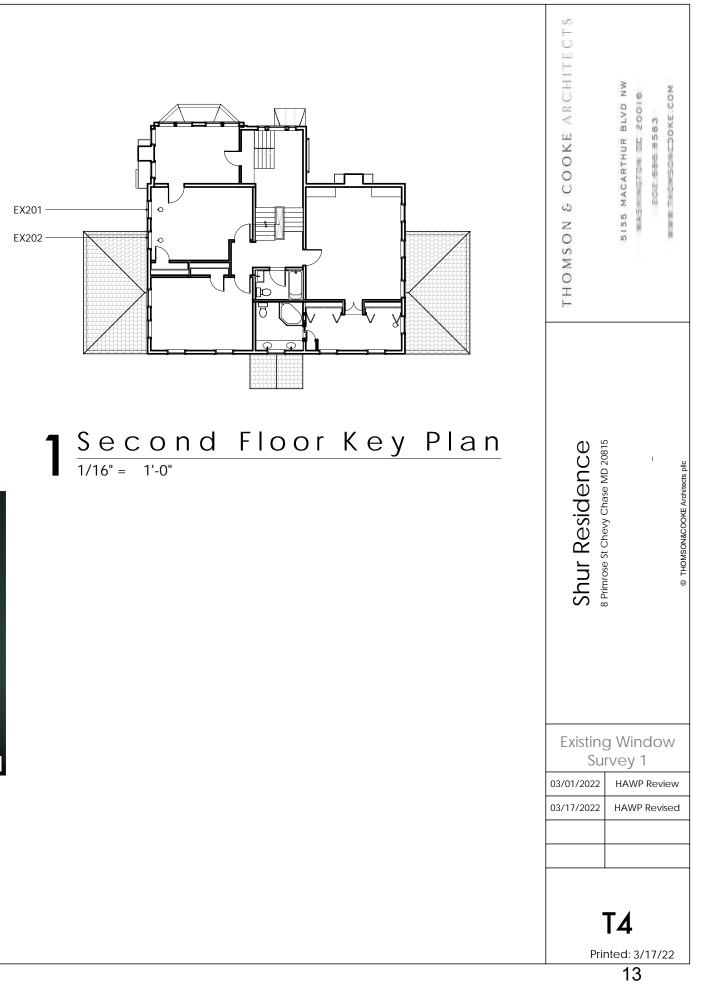
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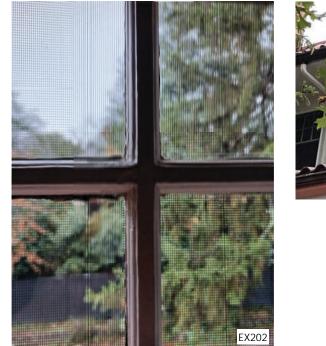
## energy efficiency

	THOMSON & COOKE ARCHITECTS	5155 MACARTHUR BLVD NW 5155 555 555 55016 202 555 5583 7-2 557 200KE.COM
dow Section	Shur Residence <sup>8 Primrose St Chevy Chase MD 20815</sup>	© THOMSON&COOKE Architects plic
<u>0 n</u>	03/01/2022	V Specs HAWP Review HAWP Revised 3 ed: 3/17/22 12





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- EX202

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- EX201

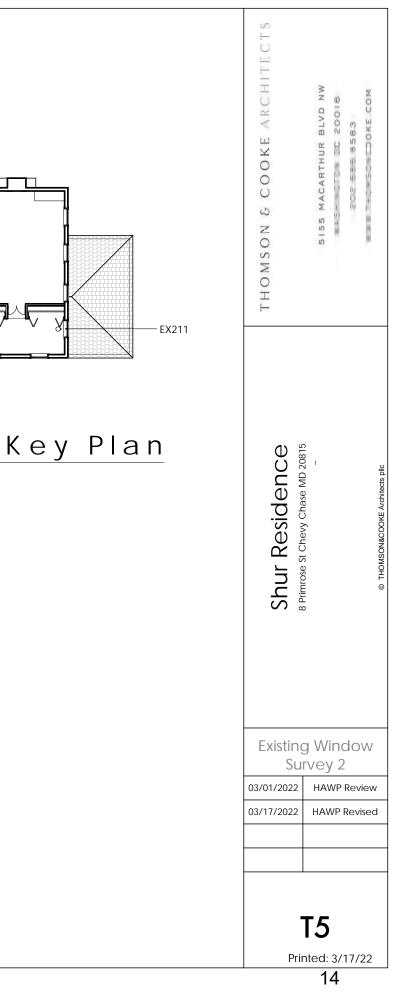


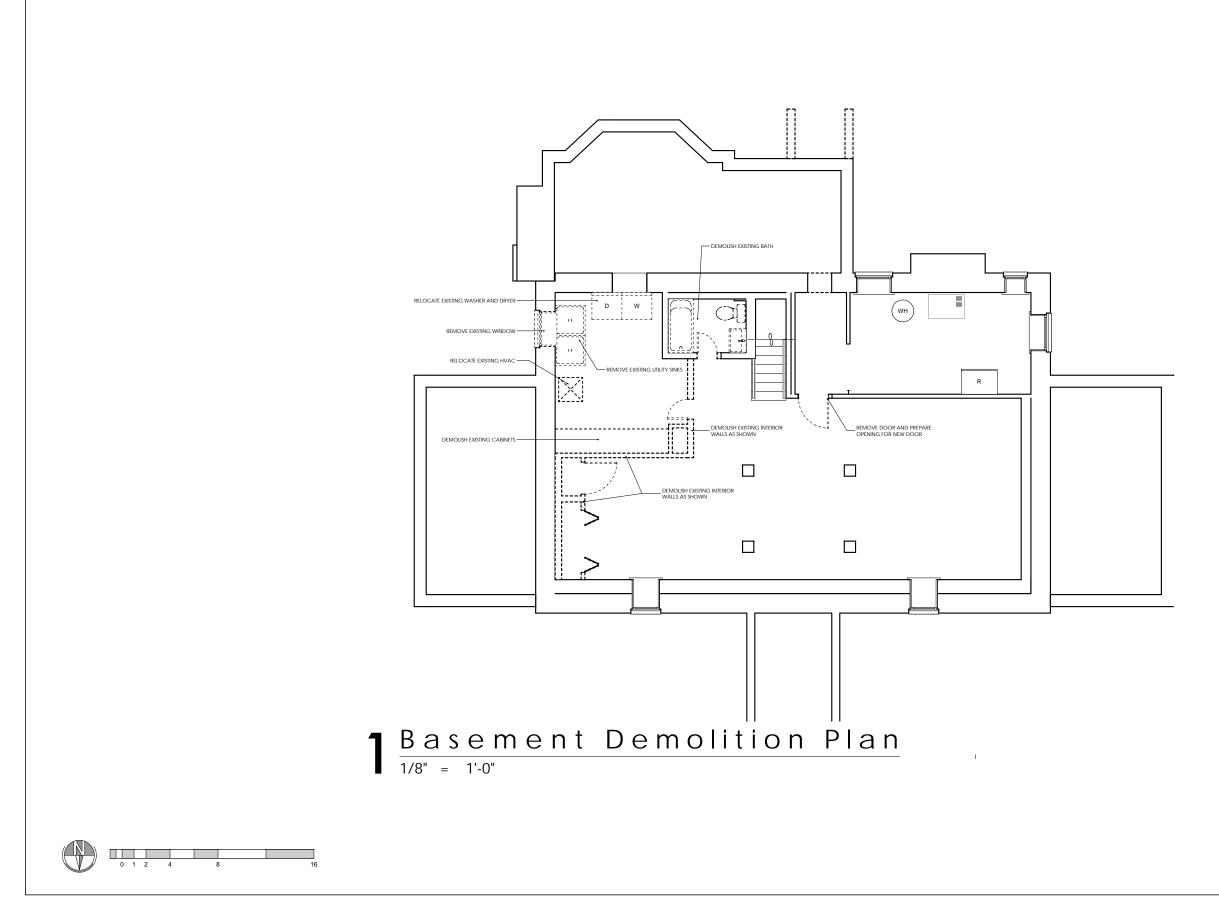


 $2\frac{\text{Second Floor Key Plan}}{\frac{1}{1} - 0"}$ 

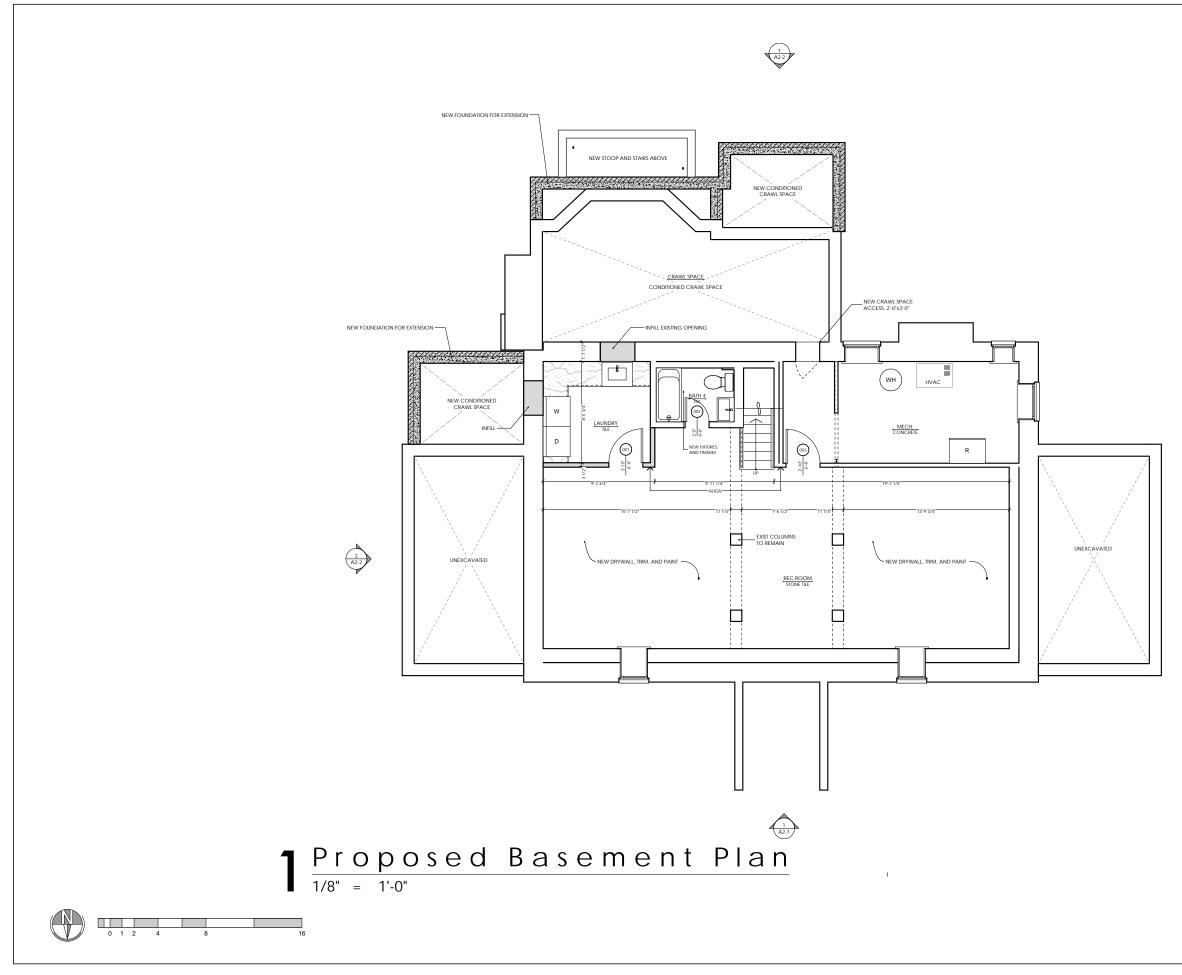






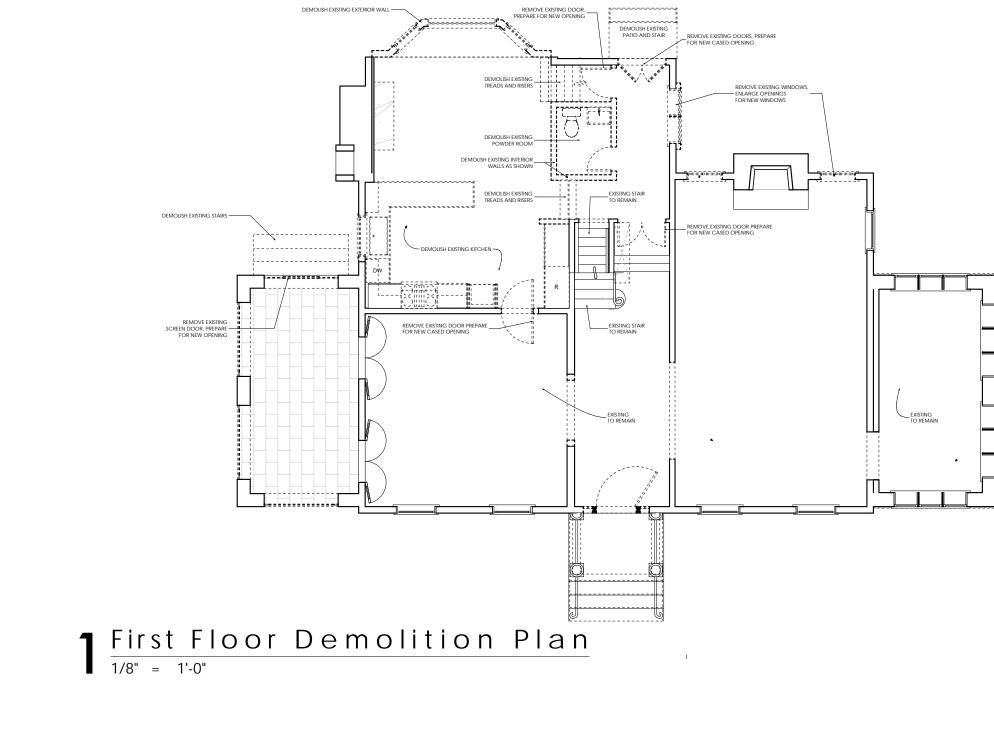






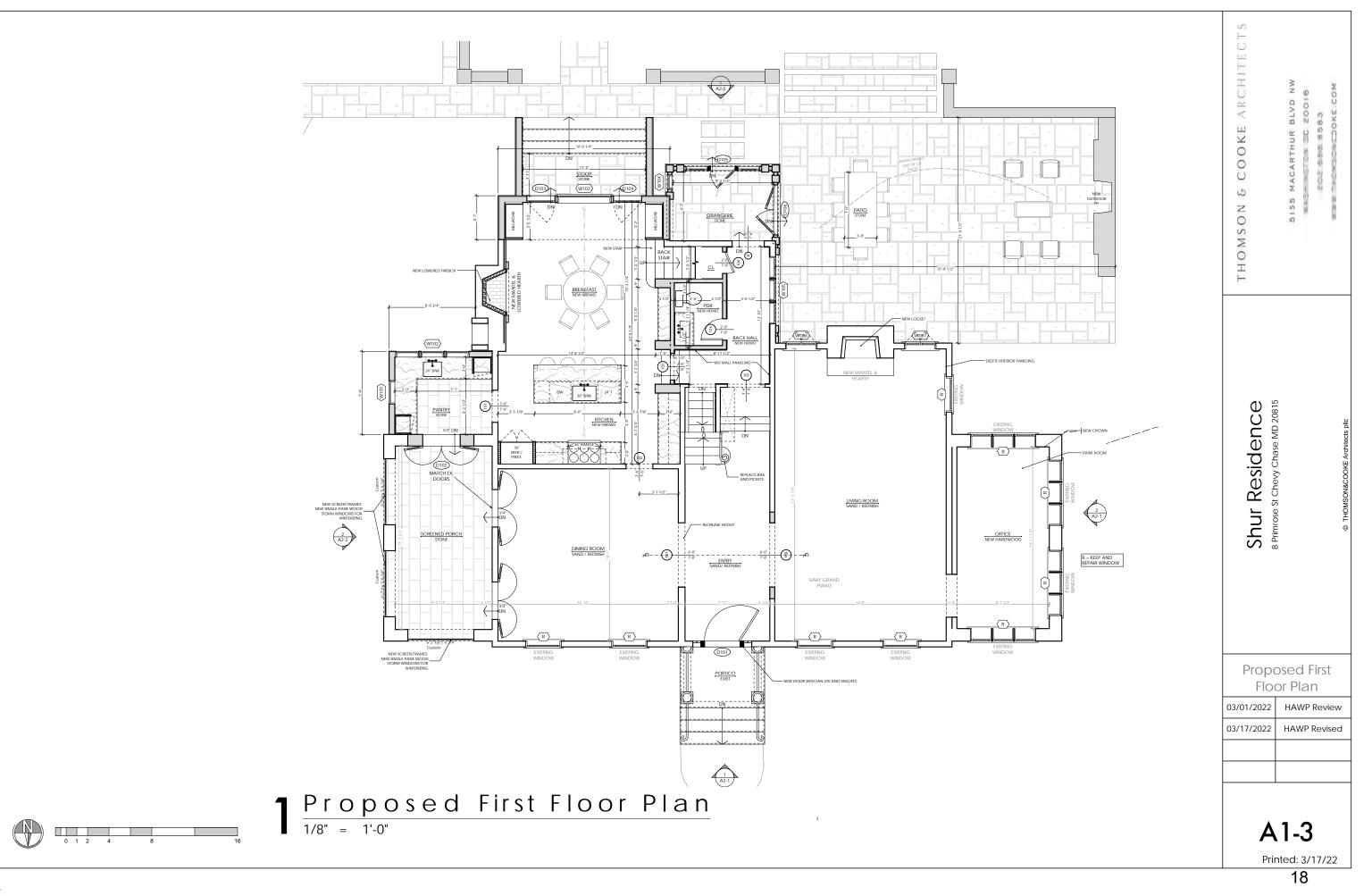
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Shur Residence			© THOMSON&COOKE Architects plic
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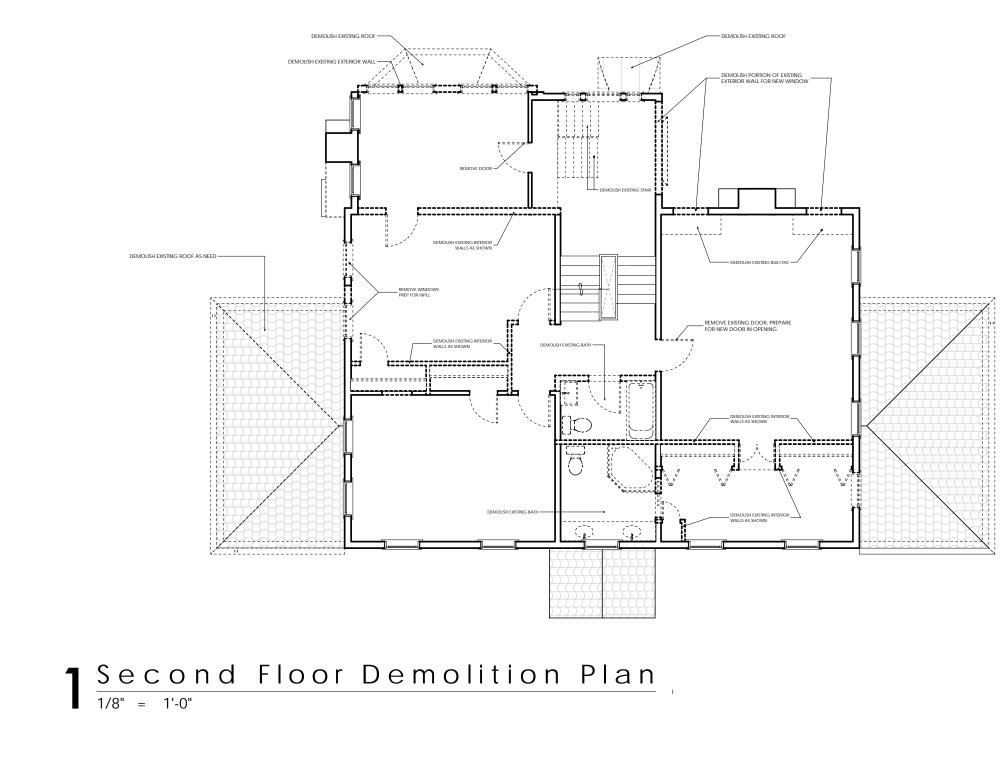






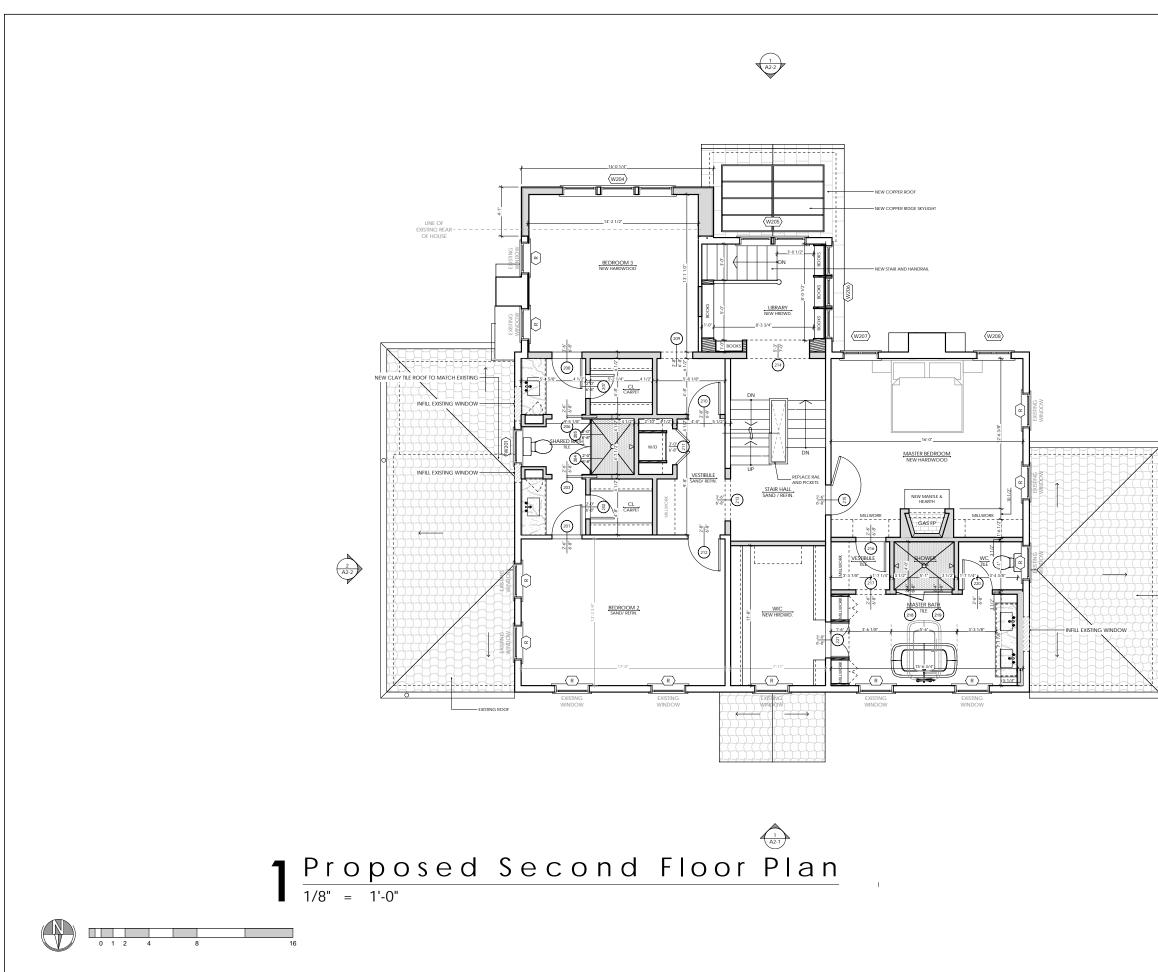
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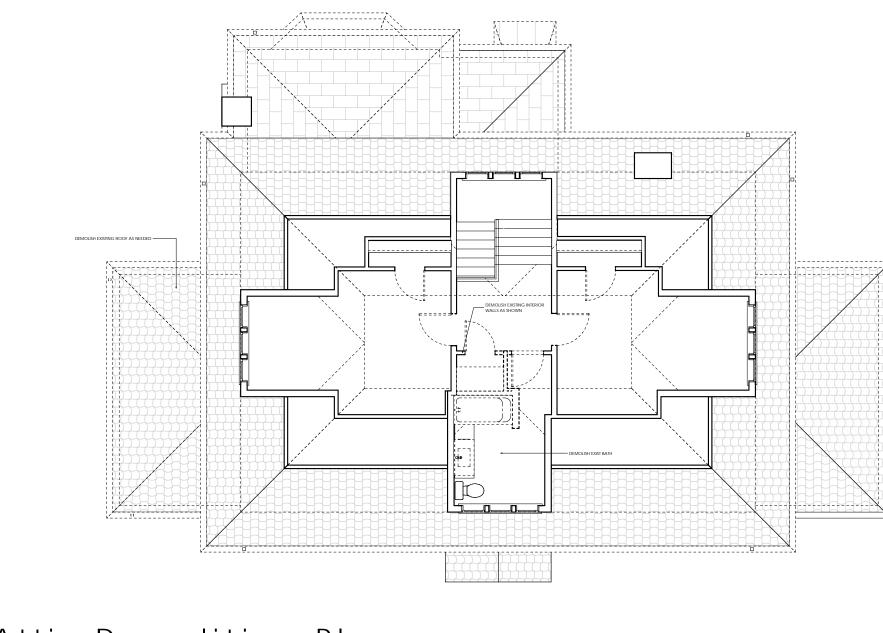




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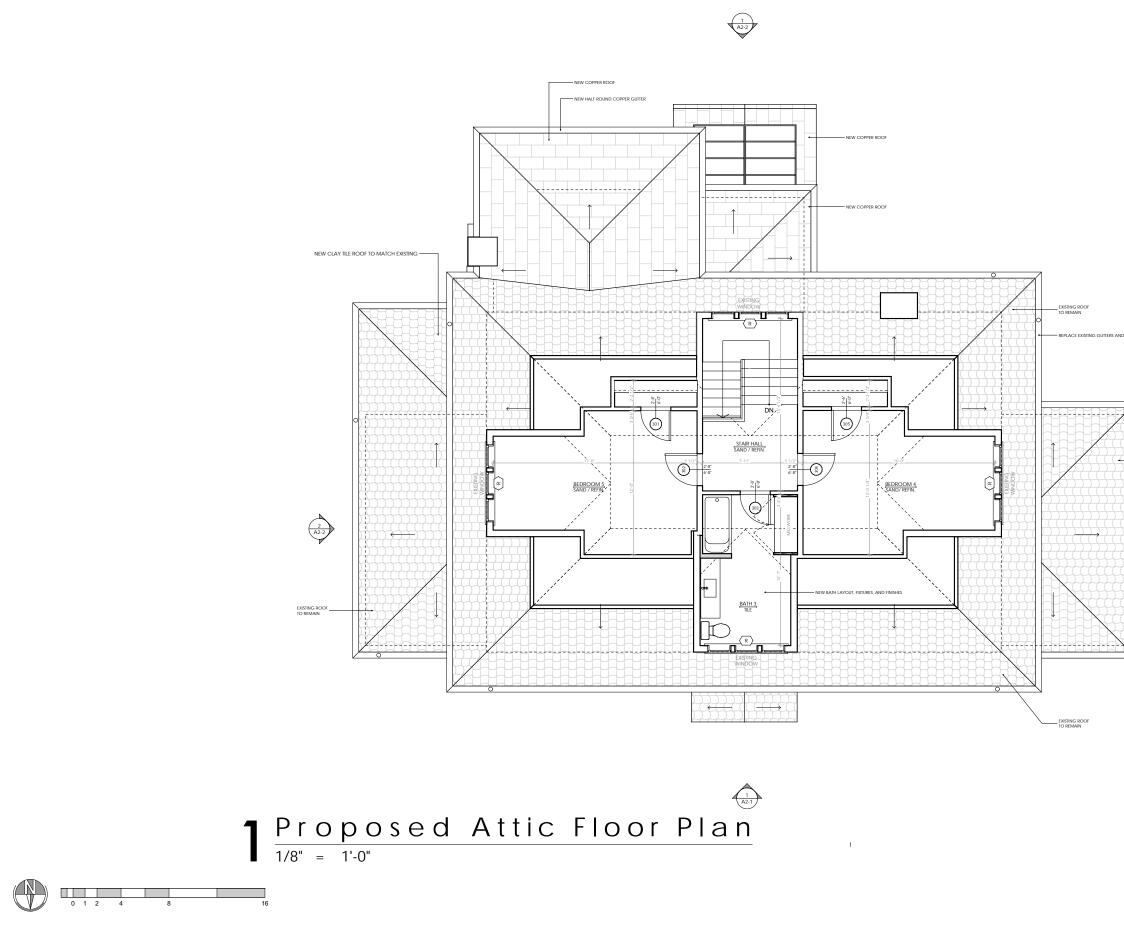


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	Propopsed Second Floor Plan 03/01/2022 HAWP Review 03/17/2022 HAWP Revised
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Shur Residence		© THOMSON&COOKE Architects pltc
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THOMSON & COOKE ARCHITECTS	5155 MACARTHUR BLVD NW	202 555 553 T+C*SC*COOKE.COM
Shur Residence		© THOMSON&COOKE Architects pllc
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# $\frac{Existing Right Elevation}{\frac{1}{8"} = 1'-0"}$

THOMSON & COOKE ARCHITECTS	5155 MACARTHUR BLVD NW	12- 510 E 20016	NOV BEE ESS	
Shur Residence <sup>8 Primrose St Chevy Chase MD 20815</sup>				© THOMSON&COOKE Architects pllc
Existir Elev 3/01/2022 3/17/2022	vati наv	on Np r		

·7'-6 3/4"



# **1** Proposed Right Elevation $\frac{1}{1/8"} = 1'-0"$

THOMSON & COOKE ARCHITECTS	5155 MACARTHUR BLVD NW 255 555 55016 202 555 553 583
Shur Residence 8 Primrose St Chevy Chase MD 20815	<ul> <li>THOMSON&amp;COOKE Architects pllc</li> </ul>
	ed Right vation HAWP Review HAWP Revised
	Shur Residence B Primrose St Chevy Chase MD 20815 03/01/2022



**1** Existing Rear Elevation  $\frac{1}{1/8"} = 1'-0"$ 

	THOMSON & COOKE ARCHITECTS	5155 MACARTHUR BLVD NW	#H5-F #510# IC 20016 202 686 8583	T+CMSC#COKE.COM
_	Shur Residence			© THOMSON&COOKE Architects plic
	Existi Ele 03/01/2022 03/17/2022	vati наv		view



# **1** Proposed Rear Elevation $\frac{1}{1/8"} = 1'-0"$

THOMSON & COOKE ARCHITECTS	5155 MACARTHUR BLVD NW	THO SEE BS83
Shur Residence <sup>8 Primrose St Chevy Chase MD 20815</sup>		© THOMSON&COOKE Architects plic
03/01/2022	ed Re ation HAWP R	eview



# $\frac{Existing Left Elevation}{\frac{1}{8''} = 1'-0''}$

 THOMSON & COOKE ARCHITECT	5155 MACARTHUR BLVD NW	202 225 353 T+C SC COME.COM
 Shur Residence <sup>8 Primrose St Chevy Chase MD 20815</sup>		© THOMSON&COOKE Architects plic
/01/2022	atior HAWP	ר Review Revised



# $\frac{Proposed Left Elevation}{\frac{1}{8''} = 1'-0''}$

⊕ <u>19-7 1/2*</u> Attic Plan	THOMSON & COOKE ARCHITECTS 5155 MACARTHUR BLVD NW 5155 MACARTHUR BLVD NW 5155 MACARTHUR BLVD NW 5155 MACARTHUR BLVD NW
	Shur Residence 8 Primrose St Chevy Chase MD 20815 © THOMSON&COOKE Architects plic
	Proposed Left Elevation 03/01/2022 HAWP Review 03/17/2022 HAWP Revised
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From Garag	D     THOMSON & COOKE ARCHITECTS       Shur Residence     B Primode St Cheve Chase MD 20815	© THOMSON&COOKE Architeds plic
	Persp 03/01/2022 03/17/2022	Dectives HAWP Review HAWP Revised



FRONT ELEVATION (NORTH - PRIMROSE ST)



SIDE (EAST)



FRONT (NORTH EAST)



REAR ELEVATION (SOUTH)

