

**GERMANTOWN
TOWN
CENTER** *D e s i g n S t u d y*

APPROVED BY THE MONTGOMERY COUNTY PLANNING BOARD, DECEMBER 1990



The Maryland-National Capital Park & Planning Commission
MONTGOMERY COUNTY PLANNING DEPARTMENT
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PREFACE

The Germantown Town Center Design Study constitutes the third and final component of the Germantown Town Center Design and Development Study as directed by the Montgomery County Council Resolution of Approval of the "Comprehensive Amendment to the Germantown Master Plan". The first component dealt with increasing transportation capacity and the second (the Halcyon report) addressed promotional, funding, and operational issues. This, the "Design Study", presents the design framework that is proposed to guide the future development of the Town Center. The Design Study expands the Town Center framework provided in the Germantown Master Plan for the Town Center.

The Study identifies the Design Concept envisioned for the Town Center, describes the recommended Town Center-wide Design Framework, and details those components of the framework applicable to each of the Town Center Analysis Areas. Special emphasis is given to the integration of vehicular and pedestrian circulation and open space systems, to the street environments (street-scapes), and to the identification and location of community facilities.

The Germantown Town Center Design Study will serve as the guideline for future design and development within this area. It is to be used by public officials and private individuals in their decision-making process. Several recommendations of the Study recommend amendments to the Master Plan, and this Study, in association with the Master Plan, will form the basis for the review of development proposals within the Town Center.

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INTRODUCTION

COUNTY COUNCIL DIRECTIVE

The Montgomery County Council Resolution of Approval of the 1989 Comprehensive Amendment to the Germantown Master Plan (Resolution No. 11-1498), adopted June 13, 1989, directed the Montgomery County Planning Board to **evaluate alternative actions which the County could take in order to produce a vital Town Center**. This evaluation was to examine all Town Center Analysis Areas and to consider ways to integrate the different Analysis Areas and existing development into a cohesive Town Center. As part of the evaluation, the Planning Board was to seek the input of the County Executive, the landowners, and the public.

In response to the Council's directive several separate but interrelated studies have been conducted to address different aspects of the Town Center.

The first study addressed ways in which the Council could accelerate the development of the Town Center. This also addressed the ways in which the Council could attract business to the Town Center and promote and ensure its ongoing successful operation. This evaluation was presented in two reports.

The first report contained a set of alternative measures for increasing and allocating transportation capacity. This report was reviewed by the Planning Board and transmitted to the County Council on December 13, 1989, so that the alternatives could be taken into consideration during Council's review of the FY '91 Annual Growth Policy and the FY '91-96 Capital Improvements Program (CIP).

The second report, prepared by the consulting firm of Halcyon, Ltd., addressed promotional, funding, and operational issues and presented Town Center development strategies, a promotional events analysis, and alternatives and recommendations for the cultural arts center and the overall implementation of the Town Center. Incorporating comments received from a community workshop, this report was reviewed by the Planning Board and transmitted to the County Council in February 1990.

These reports constitute an important precursor to the Germantown Town Center Design Study.

STUDY PROCESS

The Town Center Design Study represents a continuation of the planning process that resulted in the adoption of the 1989 Comprehensive Amendment to the Germantown Master Plan. The ongoing involvement of the Germantown community was reflected in the February 10, 1990 community workshop at which the Halcyon Report was presented. This workshop also provided the opportunity for citizens to express their views as to what was felt important to be included the Design Study.

A Work Group was appointed by the Planning Board to provide ongoing advice during the study period. The Germantown Town Center Design Study Work Group was composed of fifteen members representing both citizens and landowners. Members included the major landowner/developer interests in the TC-1, TC-2, TC-3, and TC-5 Analysis Areas and representatives of the Germantown Citizens Association, Germantown Alliance, Inc., Germantown Chamber of Commerce, Upcounty Citizens Advisory Board, and each of the surrounding Villages. Representatives of various county departments and the State Highway Administration, State Railroad Administration, and Washington Suburban Sanitary Commission were also identified and participated in the process.

The Design Study was officially initiated March 17, 1990 with a community-wide workshop. This workshop was instrumental in the initial identification of the vision of Town Center. The Work Group met each month thereafter to discuss the overall concept, the design framework, and the design of the Analysis Areas. In that the landowners/developers of TC-1 and TC-5 were actively involved in the planning of these areas (most of the areas of the Town Center are in one or another phase of planning or design), the process was timely and generated considerable interest and enthusiasm. Considerable time was also devoted to definition of the proposed Cultural Arts Center, and the appropriateness of locating the Germantown Recreation Center in the Town Center.

Throughout the Study, staff held meetings at M-NCPPC with Work Group members, including landowners and their representatives. These meetings were highly productive in the resolution of issues that were raised.

A second community-wide meeting was held in June to present the initial concepts developed in the Study.

GERMANTOWN TOWN CENTER DESIGN STUDY

The Design Study presents the design concept that is desired for the Town Center and, in particular, that of the Town Center Core.

The Study provides a detailed design framework with special emphasis on the elements that are proposed to serve to integrate both existing and planned development into a cohesive, imageable, and identifiable Town Center.

This report describes the existing planning and design context that is applicable to the Town Center. The Town Center Design Context section includes a description of the existing and proposed transportation systems (vehicular, transit, and pedestrian circulation), environmental systems (natural open space systems, wetlands, vegetation, and topography), parkland, cultural and historic resources, and existing land use and development (land use, zoning, and existing and proposed development).

Transportation, parkland/open space, and community facilities components of the Town Center-wide Design Framework are described in the context of the overall Town Center and as they are applied to each of the major Analysis Areas. The Town Center-wide Design Framework section includes: the vehicular circulation system, transit, the pedestrian circulation system (including hiker/biker trails, and bikeways), open space systems, parkland, historic resources, and other public facilities.

The Analysis Area Design Framework includes: vehicular circulation (the hierarchy and layout of the street system), pedestrian circulation (the type and location of sidewalks, street crossings, and special corners), streetscapes (Town Center **gateways**, entrances and special features), parkland/open space, building envelopes (the identification of building zones, setbacks, the location of important walls/edges, **significant** facades, and **focal points**), and recommended building height zones. Also described are the locations for parking (including proposed structures) and, where applicable, recommendations for phasing.

The Design Study places special emphasis on the identification and the description of the design elements that constitute the street environment (streetscape). Design prototypes have been developed for all of the Town Center-wide streetscapes, as well as those recommended for application within each of the primary Analysis Areas. These prototypes are provided for use in both the design and review of these areas.

The Design Study is a guideline for the future development of the Germantown Town Center. Although many specific recommendations are contained in the study, the graphics employed throughout are illustrative in nature.

EXECUTIVE SUMMARY

The Germantown Town Center Design Study proposes a detailed design framework with special emphasis on the elements that will serve to integrate both existing and planned development into a cohesive, imageable, and identifiable Town Center.

TOWN CENTER VISION

The **vision** perceived for the Germantown Town Center is that of a **traditional** downtown. The design of the Town Center proposes to reflect the traditional organization and architecture of historic communities in the mid-Atlantic region. These communities provide a model for the Town Center that most closely resembles the attributes sought by the community and the Master Plan, one that appears to address the common theme and architectural character desired.

Attributes identified for incorporation into the Town Center include:

- village center
- main street
- unifying central space
- a sense of community
- overall sense of place
- intimate feel
- pedestrian orientation
- visual focus
- consistent streetscape
- architectural character

ATTRIBUTES

Collectively, the vehicular and pedestrian circulation systems, the parkland and open space systems, and the provision of community facilities constitute the major components defining the Town Center-wide Design Framework. This framework is significant in the integration of the different Analysis Areas and existing Town Center development into a cohesive Town Center. The specific application of the components of this framework to proposed future development, as represented by the Analysis Areas, together with the related design of the streetscapes, will help establish the overall design character of the Town Center.

A summary of the proposed elements that constitute the recommended overall design character for the Germantown Town Center include:

- an urban character with buildings facing on streets
- parking lots behind buildings (in courtyards; not between the building and street); parallel parking on secondary streets
- building heights to a maximum of seven stories (excepting the hotel and high-rise residential towers)
- a pedestrian-oriented Main Street to be a mixed use street of specialty retail and entertainment, offices, and residential
- commercial office and/or multi-family residential uses on MD-118
- multi-family residential uses adjoining the existing residential development to the north (of both TC-1 and TC-5)

DESIGN CHARACTERISTICS

- the development program proposed in the Master Plan
- distinctive and identifiable **gateways** at the major entrances to the Town Center and the entrances to the Town Center Core (TC-1) and the TC-5 retail center
- the establishment of a hierarchy of streetscape treatments
- MD-118 developed as a **boulevard**; Father Hurley, Middlebrook and Crystal Rock also to receive special streetscape treatments
- Lacbury Drive-Waters Road and Old MD-118/Walter Johnson Drive designed as **special streets**, to be pedestrian-oriented and include a hiker/biker trail
- sidewalks and trees on both sides of all streets
- a grid street system in TC-1; an interconnected street system in TC-5
- Waters Road to be upgraded in its current location through TC-5
- an intersection at Waters Road and MD-118, through to Old MD-118
- reinforcement and expansion of the Town Center-wide vehicular and pedestrian circulation linkage as a continuation of Main Street through the existing shopping centers (Middlebrook to Wisteria) to TC-5
- an internal transit system that utilizes Main Street extended
- hiker/biker trails within the Town Center linking the three major adjacent committed open space systems
- a major public park in TC-1 that is recommended to accommodate an outdoor assembly area (amphitheater) and related recreational facilities
- **Town Center Commons** centrally located in the Town Center Core (TC-1) at the intersection of the north-south entrance boulevard and the east-west Main Street; location of the Cultural Arts Center on the Commons
- development of an indoor pool and the possible inclusion of the indoor building component of the Germantown Recreation Center on TC-5.

IMPLEMENTATION

This document proposes that the following actions be taken to implement the spirit and intent of the Master Plan and the detailed recommendations of the Design Study:

- Amendments to the Germantown Master Plan to designate the major streets as Master Plan roads and to provide for the dedication of public parks and recreational and cultural facility sites.
- Establishment of a special tax area or Town Center Maintenance District.
- Development of a funding strategy to ensure funding for the capital cost of the various features recommended.

TOWN CENTER PLANNING CONTEXT

THE GERMANTOWN MASTER PLAN

The 1989 Comprehensive Amendment to the Germantown Master Plan provides a set of comprehensive recommendations and guidelines for the Germantown Town Center.

The Master Plan presents the overall planning context (land use and zoning, transportation, environment, community facilities, etc.) that is applicable to the future development of the Town Center.

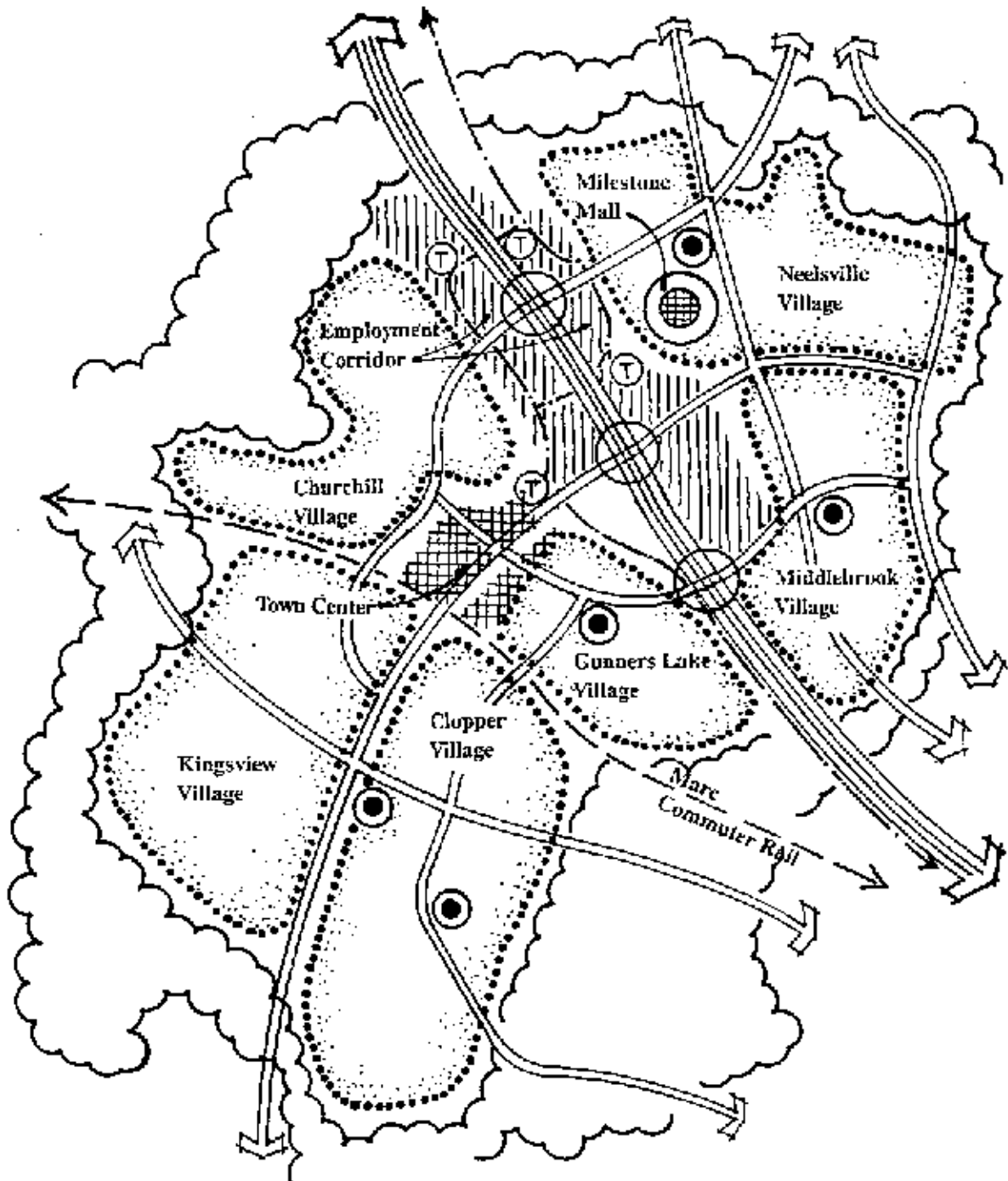
Germantown is an 11,000-acre planning area located in Montgomery County, Maryland, approximately 25 miles northwest of Washington, D.C. The Master Plan describes Germantown in terms of a **new community** and calls on the County government to coordinate the efforts of individual landowners to create a cohesive community with a distinct identity. The Plan describes the development of six residential villages, the I-270 employment corridor, and the Town Center (see *COMMUNITY CONCEPT PLAN*). The development of the Town Center is identified as one of the most critical components of Germantown. This area represents Germantown's **downtown** and is proposed to become the visual and functional center of the community. The design of the Town Center will reflect the image of Germantown.

The Master Plan also incorporates **Townscape Design**, the objective of which is to assist in the development of a sense of community identity. Town Center objectives include:

- the location of a broad mix of land uses, including a cultural arts center, so as to create a focus for community activity;
- the development of a major commercial area that offers a variety of shops, theaters, restaurants, multi-family housing, the Upcounty Government Center and other public facilities, and public open space; and
- the creation of Town Center as the central design element of the Germantown townscape; that which identifies Germantown and reinforces its community identity.

The Town Center Core (TC-1) is described as an essential element of the Town Center, proposed to have the broadest mix of uses in Germantown. The densities and built form in this area are recommended to be sufficiently compact and massed in order to create a **sense of urbanity**.

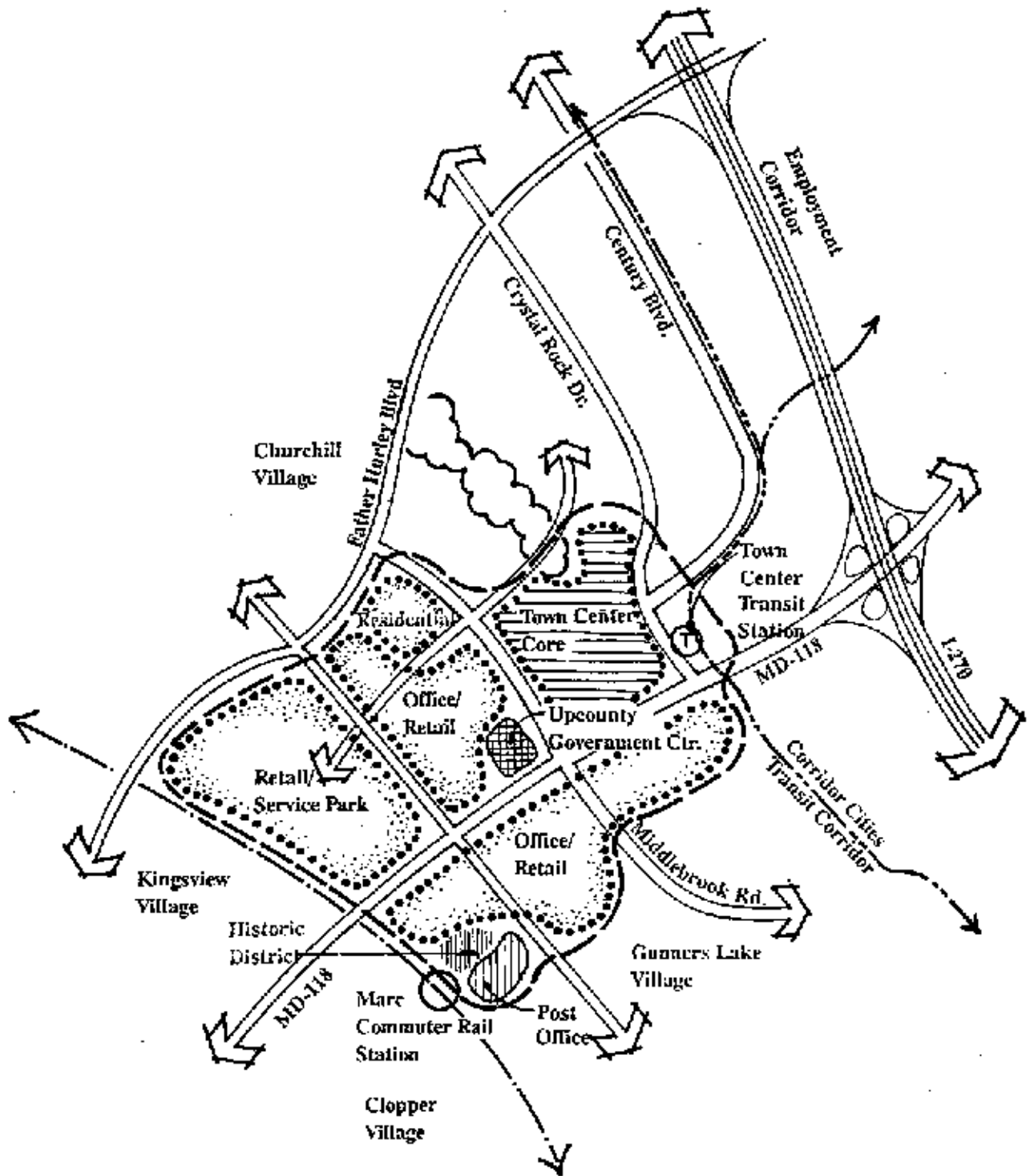
TOWNSCAPE DESIGN



GUIDELINES

The following guidelines were also provided in the Master Plan. They are intended to provide the basis of a design framework that will result in an identifiable and cohesive Town Center:

- Create **gateway** landscaping and signage at the major entrances to the Town Center.
- Establish the visual quality of a landscaped, tree-lined boulevard along MD-118.
- Require a building and parking setback of 30 feet along MD-118 through the Town Center.
- Establish visual continuity along the street through the establishment of low walls or rows of trees.
- Establish a place-making element at each corner of the intersection of MD-118 and Middlebrook Road.
- Create a pedestrian and bike path system that connects the Town Center to all forms of transit and land uses; separate vehicular and pedestrian traffic where possible.
- Establish a specific visual theme including signage, street furnishings, and lighting.
- Provide place-making elements such as sculpture, water features, etc., throughout the Town Center.
- Minimize the visual impact of parking areas from adjacent roadways through the use of berms, decks, fences, landscaping, and trellises.
- Soften all **edges** through the provision of extensive landscaping.



TOWN CENTER CONCEPT PLAN

GERMANTOWN MASTER PLAN

THE HALCYON STUDY

The Town Center development strategy developed by Halcyon, Ltd., states that in order to create a viable Town Center, the center must be recognized as the place to be, the place to go for activities, events, and social gatherings. Concentration of uses is identified as important in that concentration will promote pedestrian activity and vitality. Density is important in establishing the Town Center as a focus of activity in the community.

The conclusions reached in the Halcyon Study were as follows:

- **IDENTITY** - In communities the focal point for activity is frequently centered around shopping, entertainment, and recreation. The major shopping destination in Germantown is likely to be the Milestone Mall. The Town Center, if focused around recreation and entertainment, can be a unique place, a unique destination.
- **CONCENTRATION/COMPACTNESS** - The Town Center Core should conceptually and practically be limited to the TC-1 site. If the uses and densities that need to be in the Core are spread out onto sites beyond TC-1, the concentration of activities required to generate vitality will not be served.
- **DENSITY** - The higher the density of office workers and residents in TC-1 and surrounding Town Center sites, the higher the likelihood that enough activity can be generated to make the Town Center a commercial as well as community success. When the variety and mix of activities are maximized, the Town Center identity becomes more apparent to everyone and hence becomes the place to be, the place to go, the focus for the community.

The Halcyon Study also recommended the following:

- The Town Center should incorporate a cultural facility to be located in the Core (TC-1), and should expand the concept to include a community recreation center, as well as commercial recreational facilities, including multiplex cinemas, nightclubs, a concentration of restaurants, and a health club. The Town Center should also be the place in the community for festivals, promotions, and public events.
- Commercial retail on TC-1 should focus on restaurants, commercial entertainment, convenience retail, galleries and other specialty shops that would not traditionally locate in shopping malls.
- Town Center uses should be clustered around/near a ceremonial public open space, designed to accommodate special events like Oktoberfest, while also providing a focal point for the Town Center at other times.
- Design of the Core area should be distinctive. The design of the structures and the landscape plan must be unique in order to achieve a successful sense of place for the Town Center. The architectural treatment of all components of the TC-1 site are crucial to the success of the program.

TOWN CENTER ANALYSIS AREAS

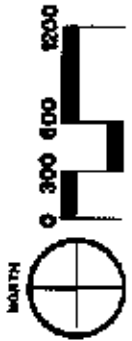
Located immediately to the west of I-270 and its intersection with MD-118, the Germantown Town Center is approximately 1.5 miles in width and nearly two miles in length. The Town Center Design Study Area is bounded by the CSX Railroad tracks on the southwest, Father Hurley Boulevard and Locbury Drive on the north, Aircraft Drive on the east (just east of Crystal Rock Drive), and the extension of Crystal Rock Drive to the Seneca Valley High School on the south. The Town Center consists of approximately 353 acres of land, of which nearly two-thirds are undeveloped.

For planning purposes most of the undeveloped portion of the Town Center has been divided into site specific Analysis Areas. The Germantown Master Plan established seven such areas (TC-1 through TC-7) described as follows:

- **TC-1** is the major 59-acre tract of undeveloped land that is proposed as the Town Center **Core**. Located on the north side of MD-118, between Crystal Rock Drive and Middlebrook Road; this area extends north to the Town Center boundary at Locbury Drive.

This highly visible site is what many refer to as the **Town Center**. As the major available development site in close proximity to the MD-118/I-270 interchange, adjacent to the currently developed Town Center Middlebrook and Sugarloaf shopping centers, and bounded by new access highways, TC-1 is prime for development.

- **TC-2** is also in a highly visible and accessible location. The initial Town Center site encountered on MD-118 at the principal Town Center entrance from I-270, this 10 acre parcel is proposed to be developed as the Corridor Cities Transit Corridor Town Center Station. Long-term Development will constitute the **gateway** to Town Center.
- **TC-3** is located south of MD-118 between Crystal Rock Drive on the east and the existing development fronting on Middlebrook Road to the west. Only eight acres, the TC-3 Analysis Area is also highly visible as well as strategically located across MD-118 from the Town Center Core.
- **TC-4** consists of a one-acre parcel on the south side of MD-118 in the approximate center of the block between Middlebrook Road and Wisteria Drive. The parcel is adjacent to the recently developed Germantown Square Park. Small in size and constrained as to access, this parcel may be best incorporated as an extension to the existing adjacent parkland.
- **TC-5** is a 76-acre area generally bounded by relocated or new MD-118 (extended), the CSX Railroad tracks, Father Hurley Boulevard (extended) and Wisteria Drive. Primarily undeveloped to the west of Waters Road (which crosses this Analysis Area), TC-5 is recommended for development as a **retail and service park**. Although larger in area than TC-1, its location and the fact that both MD-118 and Father Hurley Boulevard have yet to be extended, have resulted in less visibility than the Town Center Core.



GERMANTOWN TOWN CENTER DESIGN STUDY AREA

- **TC-6** is a 23-acre tract generally bounded by the to-be-relocated MD-11B, from the CSX Railroad tracks to Wisteria Drive and includes the properties fronting on the existing MD-11B (Old MD-11B). Included in this 23-acre area is the Germantown Historic District and several historic sites. In multiple ownership and including existing development to remain, the Analysis Area affords limited opportunities for infill development and should incorporate the future expansion of the parking facilities of the MARC Commuter Rail Station, as required.
- **TC-7 Analysis Area** is a 10-acre area immediately to the southeast of TC-6 that has been designated as the site of the proposed Germantown Post Office.



TOWN CENTER ANALYSIS AREAS

TOWN CENTER DESIGN CONTEXT

TRANSPORTATION VEHICULAR CIRCULATION

Town Center is directly accessed from I-270 via MD-118, Middlebrook Road, and Father Hurley Boulevard. The Town Center Core is approximately one mile southwest of the MD-118 and I-270 interchange. A six-lane divided highway, MD-118 is currently constructed to Wisteria Drive. Its planned extension will connect to Clopper Road (MD-117). Engineering has been completed and the construction of the extension of MD-118 is scheduled to start in May 1993. Access from the southwest is currently via Old MD-118 to Wisteria Drive or via Great Seneca Highway.

The other major highways that currently provide access to Town Center are Father Hurley Boulevard and Middlebrook Road. Father Hurley Boulevard is currently constructed as a four-lane divided highway between Wisteria Drive and Crystal Rock Drive; however, the median is designed to provide two additional lanes. When extended southward, Father Hurley Boulevard will connect to MD-118 north of its intersection with Clopper Road. North of the Town Center Father Hurley Boulevard will provide access to and from the north and eastern areas of Germantown and will provide a new interchange with I-270.

Middlebrook Road runs southeast from Father Hurley Boulevard through the Town Center, across MD-118 to a partial interchange with I-270 which is now under construction. Middlebrook Road is currently four lanes divided and is designed as a six-lane divided highway. Significant right-of-way exists for widening both north and south of MD-118.

Crystal Rock Drive also provides access to Town Center from the north. Running northwest, it connects MD-118 with Father Hurley Boulevard. Crystal Rock Drive is a four-lane divided highway also designed with sufficient right-of-way to accommodate six lanes.

Wisteria Drive is a four-lane arterial roadway that parallels Middlebrook Road to the south. This road also connects Father Hurley Boulevard with MD-118 and Great Seneca Highway. Until the completion of both Father Hurley Boulevard and new MD-118 (relocated), Wisteria Drive and Middlebrook Road provide the major connections to Old MD-118 and Great Seneca Highway and the area to the south of Town Center.

There are currently few other roads within the Town Center. Ladbury Drive forms the northern boundary of the Town Center Core (TC-1). Although it currently dead-ends after crossing Middle-

brook Road, it will connect with Waters Road. Waters Road runs west and north from its intersection with Old MD-118, just north of the CSX Railroad bridge. Both Lockbury and Waters are currently two-lane roads, master planned for four lanes.

Aircraft Drive is a two-lane road that currently connects MD-118 to Century Boulevard and Crystal Rock Drive to the north. Century Boulevard is a four-lane arterial that curves to parallel Crystal Rock Drive as it heads toward Father Hurley Drive. The southern end of this road is proposed to be extended across Aircraft Road to Crystal Rock Drive (and the Town Center Core).

In summary, a network of major highways provides access to the Town Center. The intersection of MD-118 and Middlebrook Road lies at the center of the Town Center Study Area. Crystal Rock, Middlebrook and Wisteria all cross MD-118 dividing the Town Center into three major areas. Only Waters Road and, to a lesser extent, Old MD-118 cross these areas.

Because MD-118, Father Hurley Boulevard, Crystal Rock Drive, Middlebrook Road and Wisteria Drive are all either four- or six-lane roads, considerable vehicular access is provided to the Town Center. Access to individual sites is constrained by the limited number of median breaks and curb cuts that are allowed on these divided highways. Fortunately the opportunity exists to structure a new road system within the Town Center that provides internal access, a sense of order and place, and a pedestrian scale.

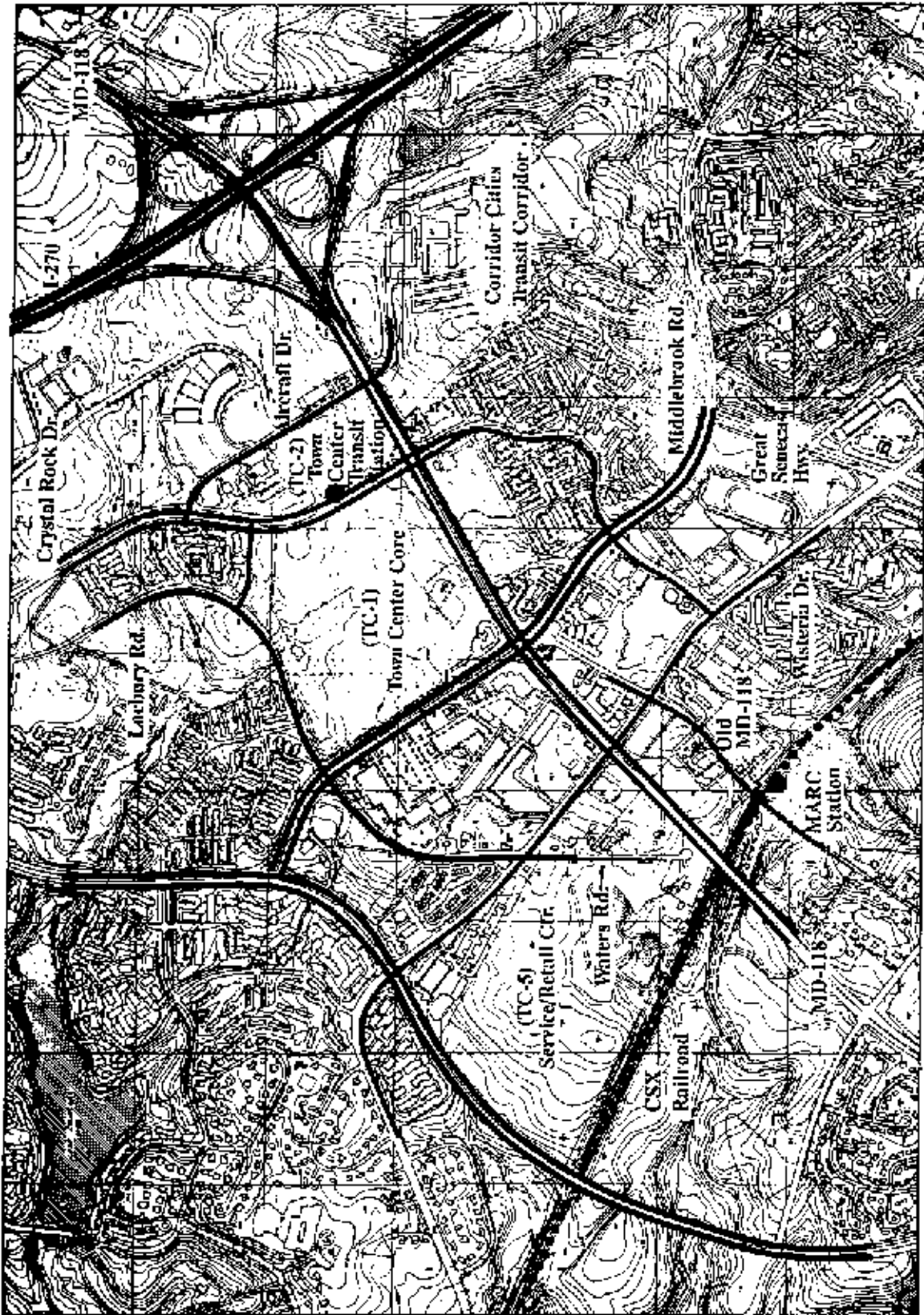
TRANSIT ACCESS

Currently the Town Center is served by bus service that utilizes the vehicular circulation system and by commuter rail. Rail service runs on the CSX Railroad tracks located on the southwestern boundary of the Town Center Study Area. The station has recently upgraded by the construction of a functioning replica of the old station building and the addition of a park-and-ride lot with access from Old MD-118. The program of additional parking capacity is currently proposed with the construction of another lot immediately across old MD-118.

The Town Center Station of the Corridor Cities Transit Corridor Easement is proposed in the TC-2 area bordered by MD-118, Crystal Rock Drive, Century Boulevard and Aircraft Drive. Although still under study, the alignment that is shown in the Master Plan is incorporated into this study.

PEDESTRIAN CIRCULATION

The existing pedestrian circulation system within the Town Center consists primarily of the sidewalks associated with the major streets and those associated with the existing shopping centers. Although the Master Plan proposes sidewalks for both sides of all streets, the current sidewalk system is discontinuous and fragmented. Current development is highly automobile oriented.



EXISTING VEHICULAR ACCESS

BICYCLE ACCESS

An existing bikeway parallels Middlebrook Road from south of the Seneca Valley High School to its intersection with MD-118. This path provides an important link from the High School to the existing shopping centers. The Master Plan proposes bikeways through Town Center along MD-118 to and along Crystal Rock Drive.

A hiker/biker trail currently exists within Churchill Village, following the south shore of Lake Churchill and the stream valley and open space system eastward to and across Father Hurley Boulevard. This system is proposed to be extended to Locbury Drive and into the Town Center Core (TC-1). Opportunities also exist to develop similar trail systems within the two major open space systems that penetrate Town Center from the south.

ENVIRONMENTAL

OPEN SPACE SYSTEMS

Existing and proposed natural open space systems penetrate the Town Center in several areas. These systems are primarily associated with stream valleys, wetlands, woodlands and/or steep slopes.

The major existing open space system follows a tributary of Lake Churchill and lies in a Class IV watershed. One finger of this stream originates within the Core of the the Town Center (TC-1). Two spring-heads, originating near the middle of TC-1, and a small wetland area feed on old farm pond to the north, near Locbury Drive. Although the extent of this wetland area remains to be determined, an initial approximation includes the pond and an area that follows the spring-flow channel.

The area within and surrounding this wetland will contribute to the open space framework of the Town Center and more specifically the parkland within the Town Center Core.

In that the wetland system on TC-1 has already been disturbed through farming activities on the site, minor grading with the intent to create an aesthetic and naturalistic open space may be allowed. Conversion of the farm pond into a stormwater management facility is also proposed. Any disturbance within wetlands (including the farm pond) requires permits from the U.S. Corps of Engineers and the Maryland Department of the Environment.

Another open space system is the stream valley that abuts the Town Center study area at the northwestern boundary (TC-5). Although the head of the stream valley will be altered through the construction of the extension of Father Hurley Boulevard, this system is proposed to be retained as valuable natural open space. It also provides the opportunity to enhance the southwestern entrance to the Town Center.

Another pond currently exists on the southwestern boundary of the Town Center. This facility, which is piped under the CSX Railroad tracks to the stream system to the west, is proposed to be upgraded to serve future development in this area (TC-5). As it is identified as wetlands on the U.S. Fish and Wildlife Service National Wet-



EXISTING PEDESTRIAN CIRCULATION/BICYCLE ACCESS

VEGETATION

lands inventory maps, any disturbance or modification to this pond will require appropriate permits.

The existing regional stormwater management pond adjacent to the MARC Commuter Rail Station park-and-ride lot satisfies a functional role and also provides needed open space. This pond is part of the larger system to the south of the Town Center study area (across the CSX Railroad tracks).

The open space systems that penetrate the Town Center offer opportunities not only to maintain natural open space but also to develop a linked parkland system throughout.

The undeveloped areas of Town Center consist primarily of open field areas (either in active cultivation or fallow and overgrown). These fields are representative of the current open character of the Town Center. They also are the primary areas planned for future development.

As previously described, current natural open space areas are associated with wetlands, stream valleys, and their related and natural open space systems. Other open space occurs along highways and within existing private development. The most significant are the large grass areas at the I-270/MD-118 interchange and the adjacent front lawn of the Department of Energy complex. These open areas are likely to remain and offer the opportunity to maintain the green character of this important gateway to the Town Center. The 530-foot elevation of the MD-118 overpass at I-270 affords a substantial view of the future Town Center Station site (TC-2) and the Town Center Core (TC-1).

The only existing public park in Town Center, recently completed, is located on the triangle of land between the realigned MD-118 and Old MD-118 (Walter Johnson Drive) at the intersection with Middlebrook Road. This park is just over one acre in size.

There is a lack of mature and valued trees in Town Center. A small stand of trees exists in the southwest corner of the TC-2 area and several tree groupings are recommended to be preserved, where possible, and incorporated into the future development of the core. This is proposed at the MD-118 entrance.

The only major stands of trees are on the remainder of the Northlake tract (between Middlebrook Road and Wisteria Drive to the southeast of Walter Johnson Drive) and to the west of Old MD-118 near the CSX Railroad tracks. A major wooded area exists southwest of the tracks for the entire length of the TC-5 zone (MD-118 to Father Hurley Boulevard). Opportunity exists to incorporate these latter wooded areas into the MD-118 and Father Hurley Boulevard gateways of the Town Center.

The absence of existing significant vegetation affords the opportunity (and need) to create new streetscape and park environments unique to the Town Center.

TOPOGRAPHY

Although gently rolling, Town Center's topography is relatively flat overall. There is less than 100 feet of change in elevation, from the highest point of 533 feet at the existing Middlebrook shopping center, to the lowest point (approximately 450 feet) at the edges of the study area where the existing open space systems enter the site.

The Town Center Core (TC-1) is at approximately 500 feet in elevation at its boundaries on Crystal Rock Drive, MD-118 and Middlebrook Road. It slopes gently to the wetlands at the center, and beyond to the farm pond and the open space system across Locbury Drive. (The farm pond is at elevation 456 feet).

The only significant slope within the Town Center is at the Father Hurley Boulevard edge of TC-5 where the topography drops from the 500-foot ridge line to lower than 450 feet at the stream.

The topography of Town Center presents few constraints to development. It offers considerable opportunity for visually linking the various areas of the Town Center and strategically locating public facilities and other major buildings, while maintaining and enhancing long distance views.

The volume of traffic projected for the major highways of the Town Center imposes a substantial level of noise on the adjacent development parcels. This is especially significant on MD-118 and to a lesser extent on Middlebrook Road and Crystal Rock Drive and on Father Hurley Boulevard. The projected 60 dBA, Ldn noise contour (that which is acceptable for residential development) extends from the center line of MD-118 up to 370 feet into the Town Center Core (TC-1). This condition does not prohibit residential development within this area but does require mitigation of the potentially adverse impact of noise on development. Specific noise criteria is required to be established at the time of site plan review.

NOISE

COMMUNITY FACILITIES

PUBLIC PARKS AND RECREATION

As previously described, only one public park currently exists in the Town Center Study Area. Referred to in the Master Plan as Germantown Square, this park is centrally located and has high visibility. It is immediately across MD-118 from the Upcounty Government Center. Although small in size and seemingly isolated, the park provides needed open space and affords the opportunity to create identity within the Town Center.

Limited pedestrian circulation and bikeway systems currently exist in the Town Center; other public recreational facilities are primarily restricted to Seneca Valley High School. Most of the adjacent larger residential subdivisions also have their own privately-maintained recreational facilities.

A County Recreational Center is proposed for Germantown. While a large outdoor pool complex and ballfields is not considered appropriate for location within the Town Center, the indoor facilities may be. This facility, together with the Cultural Arts Center would help increase the overall amount of community activity in the Town Center and would support the other Town Center-wide public facilities and proposed community-related activities, as well as programmed commercial and residential development.

HISTORIC RESOURCES

The Town Center Study Area includes a portion of the Germantown Historic District (Master Plan Site 19-13). Although located primarily to the south and west of the CSX Railroad tracks, this District extends on both sides of Old MD-118 from the bridge over the tracks to Wisteria Drive. Within this District are two historic sites.

The Madeline V. Waters House site is located on the north west corner of Wisteria Drive and Old MD-118. Added to the Montgomery County Master Plan for Historic Preservation (19/13-1) in 1985, the Waters House burned, but several mature trees remain, including those that were adjacent to the house and those that lined the driveway. The Master Plan proposes that these trees be preserved and the site of the Waters House incorporated into the green space of the Town Center. Historic designation does not preclude the development of the Waters House site but it does provide guidelines to ensure that this site be maintained as a reminder of Germantown's history. Development of this historic resource requires approval of the Historic Preservation Commission (HPC).

The Pumphrey/Matney House (19/13-5) is also located in the Town Center Study Area (on Old MD-118 adjacent to the MARC Station park-and-ride lot).

Old MD-118 is viewed as a **special** street that should establish a strong visual and functional link from the new Town Center Core to the majority of the Historic District (the Old Town Center) across the railroad tracks.



ENVIRONMENTAL/OPEN SPACE SYSTEMS

OTHER FACILITIES

The Upcounty Government Center is currently nearing construction completion. Centrally located within the Town Center, at the intersection of MD-118 and Middlebrook Road, this facility is sited so as to be highly visible from the Town Center entrances of both MD-118 (at the I-270 interchange) and Middlebrook Road and most of the Town Center. The Upcounty Government Center will contain a library, a child care center, and the offices of Montgomery County Public Schools, in addition to several County government departments.

Police and fire stations are located between Crystal Rock and Aircraft Drives, north of Century Boulevard. A future regional post office will be located on Wisteria Drive in the southeastern corner of the Town Center study area (TC-6).

Private and institutional facilities also currently exist within the Town Center. These include Mother Seton Parish Church and several day-care facilities.

Although currently scattered, these facilities have the potential to be linked by transportation and open space systems and through the future development of the Town Center. Although land values tend to prohibit the future development of institutional facilities in the Town Center, the identification of locations (including shared space) appropriate for such uses as houses of worship is important to the overall development of the Town Center.

LAND USE AND DEVELOPMENT

LAND USE

Existing private development in the Town Center is primarily of service retail and office commercial uses. Scattered institutional uses and public facilities also exist. The majority of the Town Center study area is undeveloped.

The majority of the existing development is clustered by use. Retail occurs primarily in the two major shopping centers, between Middlebrook Road and Wisteria Drive, north of MD-118. Retail establishments also exist across MD-118, at its intersection with Middlebrook Road, and in a new, small service center between MD-118 and Walter Johnson Drive (Old MD-118). Most of the office development is concentrated in the Northlake One and Two buildings, to the south of MD-118, off Middlebrook Road. A new townhouse office project is located on Wisteria Drive to the west of Waters Road, and medical offices have been developed on Wisteria Drive at Old MD-118. As previously described, public and institutional uses are scattered on the perimeter of the Town Center Study Area. The Upcounty Government Center is, however, located in the very center of the Town Center.

While some older development and new infill development is found along Old MD-118, the majority of Town Center's development is located between Middlebrook Road and Wisteria Drive. With the completion of the Northlake development, this middle area will be, for the most part, built out. On the other hand the areas to the north (TC-1, TC-2, and TC-3) and south (TC-5 and TC-6) are virtually unde-

veloped, and together represent nearly 200 acres of potential development. Enough uncommitted land remains to create a vital town center in terms of both form and function.

Recommendations regarding the future development of the Town Center focus on the mixed use Town Center Core (TC-1) and the adjacent joint-use of the Town Center Transit Station site (TC-2); and on the proposed retail and service park (TC-5).

The Town Center Core offers the opportunity to create an identifiable and distinctive mixed-use activity center in the Town Center. The Master Plan encourages a mix of cultural, retail, office, residential and community uses. Retail uses are recommended to include specialty shops, restaurants, indoor recreation and entertainment. Outdoor areas should also provide opportunities for both entertainment and active and passive recreation. A substantial component of residential use is proposed for the Core. The Cultural Arts Center is also proposed as an integral component of this mixed-use center.

The site of the Town Center Transit Station (TC-2) is prominently located at the I-270 entry **gateway** to the Town Center (on MD-118). Across Crystal Rock Drive from the Core, TC-2 is proposed for joint use development as station, office and high density residential use. Station related retail and commercial along with the incorporation of public amenity in the form of open space are also encouraged.

The Master Plan recommends the development of TC-5 in the southwestern portion of the Town Center, as a comparison shopping center to include general merchandise stores, department stores providing discount and moderately priced merchandise, hardware and building supplies and automotive and business services. There is a strong market potential for these stores, many of which are too large or of an inappropriate nature for location in the Village Centers or along roadways. Their clustering in one area provides yet another opportunity to define a sense of place.

Existing development in the Town Center does not present a coherent image. It is highly fragmented. Future development presents the opportunity to develop a cohesive Town Center with a strong sense of place - a coordinated development linking retail and office activity centers to residential areas and incorporating cultural, recreational and related community facilities and parkland through integration of the vehicular and pedestrian circulation and open space systems.

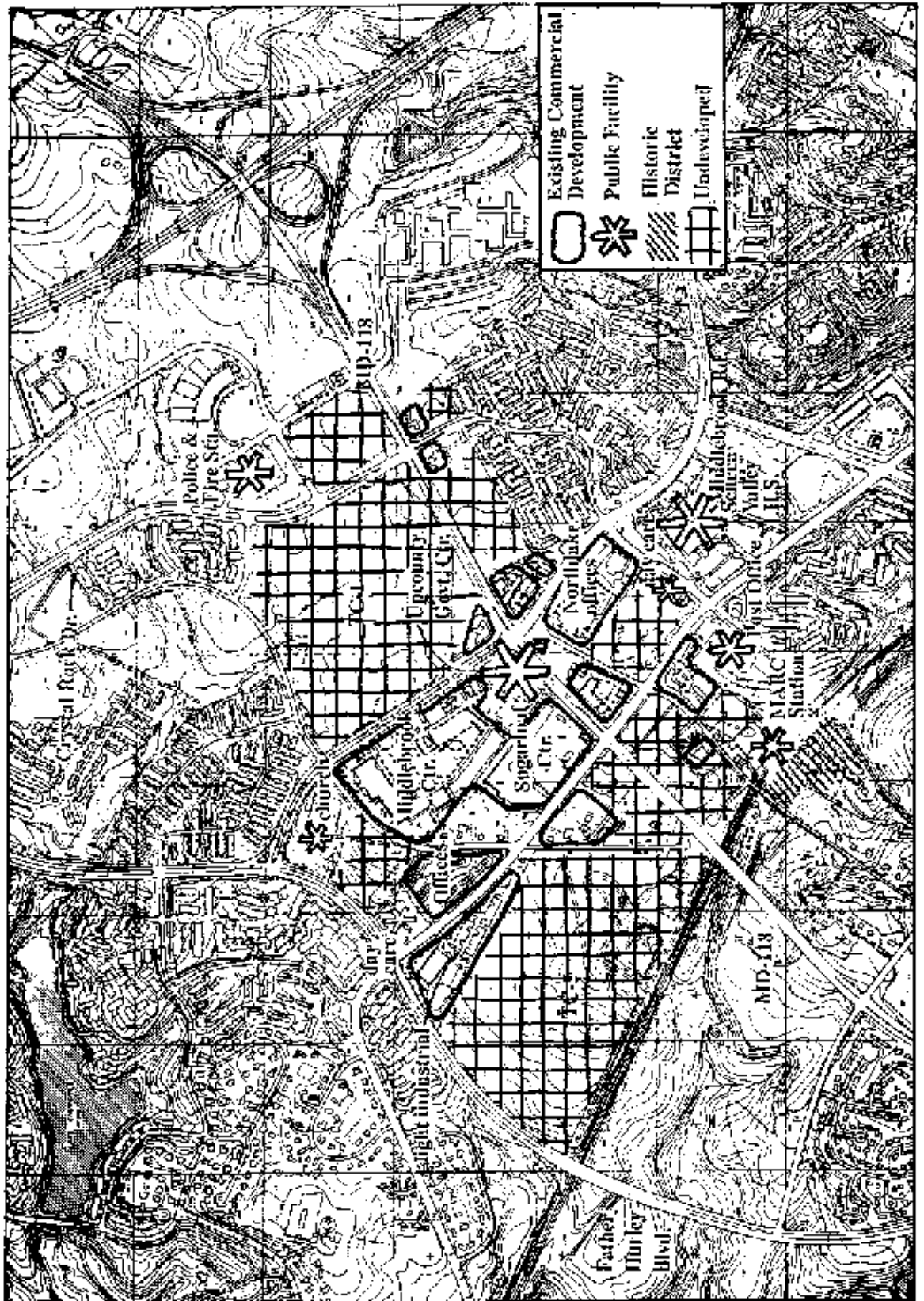
Most of Town Center's sites are in zones that require site plan review. These zones require submittal to the Planning Board of a detailed site plan showing how the property will develop. The uncommitted portions of the Town Center are zoned one of the following floating or project plan zones:

- TS (Town Sector)
- RMX (Residential Mixed Use)
- O-M (Office Building - Moderate Intensity)
- C-T (Commercial Transition)
- C-3 (Highway Commercial)
- P-D (Planned Development)

ZONING



GENERALIZED LAND USE



- **TC-1** is in the Town Sector Zone and requires submission of a supplemental plan (prior to site plan submittal), the intent of which is to achieve an integrated plan for the entire 59 acre Town Center Core.
- **TC-2** is a PD-44 zone which requires site plan approval to insure that the plan can adequately accommodate phased development to include the transit station.
- **TC-3** is zoned C-5. The site is also appropriate for development under the C-T Zone.
- **TC-4** is zoned C-5; and is also appropriate for development under the C-T Zone.
- **TC-5** is zoned RMX-2 (Residential-Mixed Use) and requires submittal of a project plan prior to site plan submittal. Plans for development of this area must establish internal compatibility among the broad mix of uses allowed in the area.
- **TC-6** is zoned R-200 and O-M. The portions zoned R-200 are recommended for office use under the O-M zone. The O-M (Office Building-Moderate Intensity) zone will allow development that may not be compatible with the preservation of the historic character of the area - including the retention of existing trees. To address this compatibility issue a rezoning application using the optional method of application and the use of a schematic development plan may be employed.
- **TC-7 Analysis Zone**, zoned C-T, is the site of the proposed Germantown Post Office facility.

The Town Center consists of approximately 300 acres of land (excluding existing and planned highways), of which 110 acres are committed to development. The Town Center Analysis Areas (TC-1 through TC-7) comprise 187 acres of largely undeveloped land.

Master Plan recommendations for these areas include 575,000 square feet of retail and approximately 800,000 square feet of office development. Also included are up to 1,800 residential dwelling units. Recommended public facilities include the Cultural Arts Center and the provision of an indoor pool facility (possibly including the indoor component of the Germantown Recreation Center). A substantial commitment of open space/parkland, entertainment and recreational facilities is also desired. TC-2 will include the Town Center Transit Station and is recommended to accommodate up to 1,000 parking spaces to serve this facility. TC-7 has been identified as the site of the Germantown Post Office.

Based on the Master Plan's recommendations, proposed Town Center development is presented in the table on the following page (see Page 30):

Development of this program will require parking for a minimum of 7,500 cars plus those parking spaces desired to serve the Transit Station and additional parking that may be required to serve the MARC Commuter Rail Station. Parking to serve public facilities, if not shared, would also be additional. Surface parking for this number of cars would require 70 plus acres. Internal roadways and related infra

DEVELOPMENT PROGRAM

TOWN CENTER RECOMMENDED DEVELOPMENT

Analysis Area No.	Acres	Retail Sq. Ft.	Office Sq. Ft.	Residential DUP's	Community Facilities	Parking Requirements ¹
TC-1	59	150,000	400,000 (including Hotel) ²	600	Cultural Arts Center Indoor/Outdoor Recreation, ³ Parkland	3,000
TC-2	10	10,000	206,000 25,000 ⁴	500 ⁵	Transit Station Parkland (on parking)	1,250 (plus 1,000 for transit)
TC-3	8		75,000		Parkland	250
TC-4	1		10,000			25
TC-5	76	400,000 ⁶	500		Public Amenity Indoor Recreation, ³ Parkland	2,750
TC-6	23		125,000 ⁷		Open Space	325
TC-7	10				Post Office	
Na ⁸		33,500	827,500	130	Public & Fire Daycare Houses of Worship	

NOTES:

- ¹ Parking requirement is estimated and all parking for community facilities is assumed shared with commercial uses.
- ² Hotel development is in addition to office square footage.
- ³ Indoor recreation to possibly include the indoor component of the Germantown Recreation Center.
- ⁴ Retail development to occur with development of the station.
- ⁵ Residential development to utilize station air-rights.
- ⁶ ALL one story service retail.
- ⁷ Scattered site infill development.
- ⁸ Development within Town Center but not in Analysis Area.

structure requirements could require another 35 to 40 acres. Of the 187 developable acres this leaves roughly about 75 acres for buildings and/or open space. At an average of three stories in height the buildings would require approximately 25 acres of land area; thus leaving approximately 50 acres of open space. A number of these Analysis Areas will have considerably more than 10%-20% open space, therefore, others would have less than 10%. The provision of additional open space would require either structured parking or a reduction in development. The overall Town Center Development program recommended above is at an FAR (floor area ratio) of approximately .40.

As previously described, existing and committed development in Town Center occupies approximately 110 of the 300 acres of developable land. This development includes the two major shopping centers that together contain approximately 300,000 square feet of grocery store, convenience and specialty shops, and restaurants. Other significant existing development includes the two Northlake office buildings, totalling approximately 120,000 square feet, and the Upcounty Government Center, currently under construction. Other existing development in the Town Center includes storage and warehouse space, professional office space and several small professional office buildings. A few other service retail uses such as banks, fast food restaurants and gas stations are located on individual sites along MD- 118.

Existing development is at a low FAR and designed to accommodate vehicular circulation and access. Most buildings are one story in height, the Upcounty Government Center is a three story building and the Northlake office buildings are four stories. Surface parking occupies the majority of each site; no parking structures currently exist in the Town Center.

Virtually all of the undeveloped land contained within the Town Center is currently being planned for development. Development is proposed for all of the Town in accordance with the land Analysis Areas. Collectively this development would approach the 3,000,000 square feet of development presented in the Master Plan program. Planned and/or pending developments range from one story to seven stories in height. The Master Plan program for TC-2 would necessitate high-rise development of up to 12 stories, to accommodate the recommended residential development over the transit station. Mid-rise or high-rise towers may also be required to accommodate the residential development recommended for TC-5. The only structured parking proposed in the initial phases of the pending development of the Town Center is that associated with the Town Center Transit Station and the residential development above it.

The greatest opportunity for the future development of Germantown Town Center results from the quantity of available currently undeveloped land. The major constraints are likely to be the unknowns of the market and the costs associated with this development.

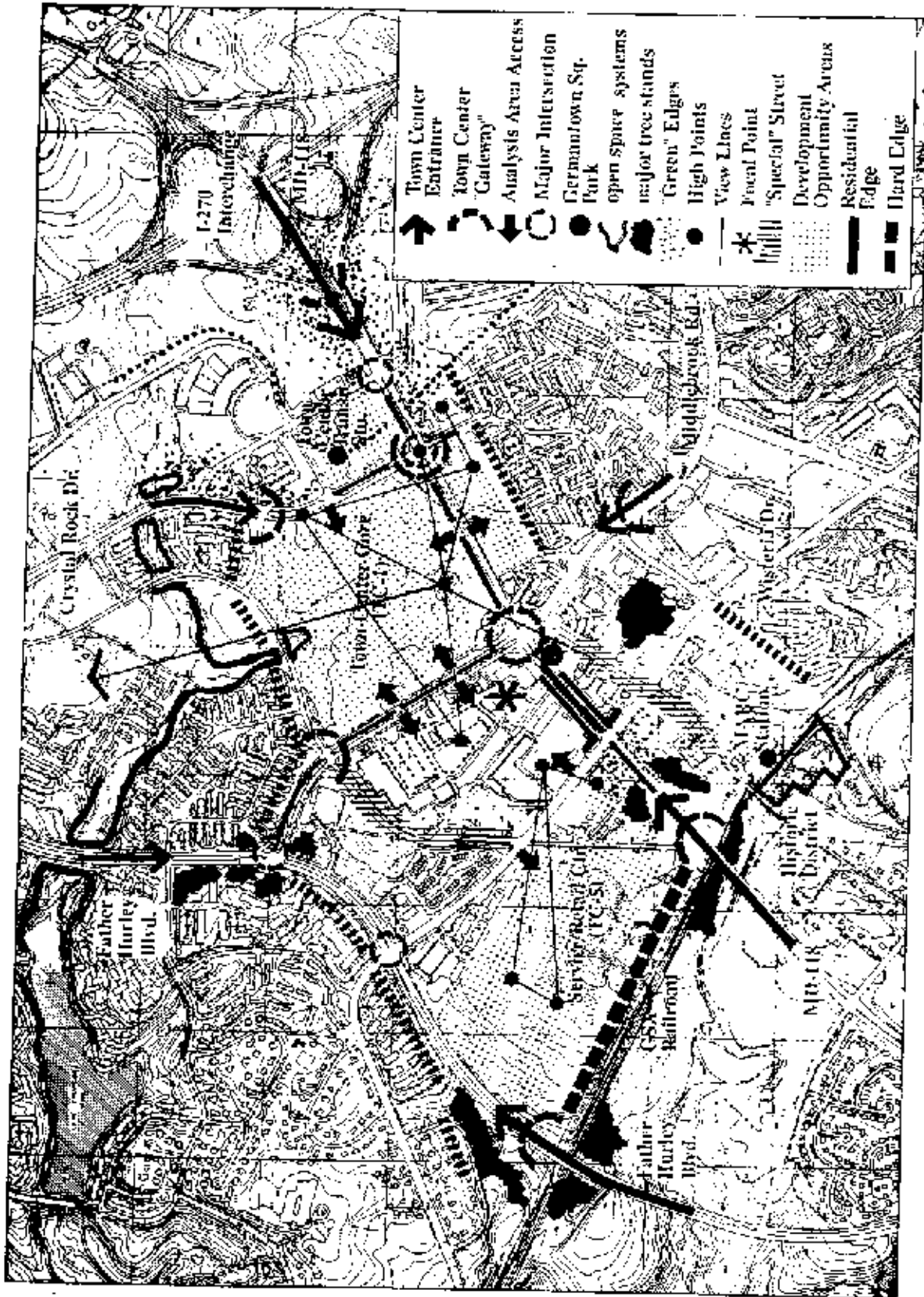
DEVELOPMENT TRENDS

OPPORTUNITIES AND CONSTRAINTS

In addition to these overall considerations, a number of site-specific opportunities and constraints are identified. Opportunities are summarized as follows:

- The major entrances to the Town Center are well defined. They include the MD-118 entrance from the I-270 interchange and the MD-118 entrance over the CSX Railroad from the south, and the nearly identical Father Hurley Boulevard entrance also from the south. Also included are the Father Hurley Boulevard and Crystal Rock Drive entrances from the north, and the Middlebrook Road entrance from the southeast.
- These entrances form **gateways**. Major gateways occur at the Town Center entrances and secondary gateways occur at the entrances to the Town Center Core (TC-1).
- Significant opportunities exist to develop the character of Town Center entrances/gateways. All have some element of green space (woodlands, grass areas, etc.). The MD-118 gateway from the I-270 interchange is characterized by the grass area associated with this interchange and reinforced by the **front lawn** of the adjacent Department of Energy Headquarters. The southern gateways of MD-118 and Father Hurley Boulevard, that will bridge the CSX Railroad tracks at their entrances to the Town Center, will enter through adjacent woodlands. The Father Hurley Boulevard entrance parallels the open space system to the west. Likewise both the Father Hurley Boulevard and Crystal Rock Drive northern entrances both cross over the Lake Churchill open space system. The Middlebrook Road **gateway** occurs at the entrance to, and open space associated with, Seneca Valley High School.
- The proposed Corridor Cities Transit Corridor Town Center Station (TC-2) represents a major opportunity to develop a future **gateway** to the Town Center. Joint development of this site, that is adjacent to the Town Center Core, affords the opportunity to create identity and a sense of place as well as both an origin and a destination within the Town Center. The existing MARC Commuter Rail Station, at the opposite end of the Town Center, affords the same opportunity.
- Significant opportunities are created by the topography and open areas currently found in the Town Center. The MD-118 entrance from I-270 offers major views through the Town Center Core to the existing development across Middlebrook Road. High points and view corridors offer opportunities for siting development.
- A major focal point and symbolic place is created by the new Upcounty Government Center. This building is easily seen from the MD-118 entrance (from I-270) and from the Middlebrook Road **gateway**.
- Other major buildings also represent **givens** to be considered, including the existing shopping centers and the two Northlake office buildings.

Germantown Square Park, centrally located at the intersection of MD-118 and Middlebrook Road and across from the Up-



OPPORTUNITIES AND CONSTRAINTS

county Government Center, also represents a major given to be incorporated into the overall development of the Town Center.

The Germantown Historic District, both within and adjacent to the Town Center Study Area, offers both an opportunity and a constraint. Several historic sites also exist within the Town Center.

The existing hierarchy of streets presents a major opportunity to develop a Town Center identity, organization and structure. MD-118 is not only the primary entrance to the Town Center, it is also the major through street. Father Hurley Boulevard provides the opportunity for a distinctly different experience. Providing additional access, both Middlebrook Road and Crystal Rock Drive also distribute traffic within the Town Center. Wisteria Drive is also a major distributor of traffic. Finally, several of the roads internal to the Town Center, Lockbury Drive, Waters Road and Old MD-118/Walter Johnson Drive have the potential to be developed as special pedestrian streets.

Streetscape treatments are important in defining differences in the roads internal to the Town Center Analysis Areas. The Town Center Core is recommended to include a hierarchy of streets varying from entrance boulevard, and main street, to secondary commercial and residential frontage streets, parking lot access roads, and service alleys.

Walter Johnson Drive currently dead-ends at Germantown Square Park, north of Wisteria Drive and Old MD-118 may be converted to a pedestrian bridge at its crossing of the CSX Railroad tracks in the Historic District. Relieved of traffic, this road presents a major opportunity for redevelopment as a pedestrian-oriented street and bikeway, incorporating the character of historic Germantown.

The major existing and natural open space systems that are adjacent to and enter the Town Center study area offer significant opportunities for the development of natural open space and parkland. In providing stormwater management, all also offer opportunities for incorporating water amenities into the Town Center, such as that currently adjacent to the MARC Commuter Rail Station park-and-ride lot. Two ponds already exist on TC-5 and a farm pond exists in the TC-1 Core.

The few areas of woodlands and major trees offer opportunities for preservation and incorporation into the future development of the Town Center. This is the case at the MD-118 entrance to the Town Center Core, in the build-out of the Northlake project and in the Historic District (TC-6).

Many of these opportunities may also represent constraints. The following constraints are associated with the future development of the Town Center:

- Intersections of the four- and six-lane divided highways that cross the Town Center Study Area represent major potential constraints not only to vehicular movement but also to pedestrian circulation. Specifically, the intersection of MD-118 and Middlebrook Road, in the center of the Town Center, presents a major constraint to the pedestrian crossing of either street.

- Site (parcel) entry from these major highways is limited to those points where median breaks and curb cuts are permitted. Only one entry is afforded to the Town Center Core (TC-1) from MD-118. Although recommended, at present there is no entry to TC-3 from MD-118. Likewise, entrance to TC-5 is restricted to one entry from MD-118 and one from Father Hurley Boulevard.
- The volume of traffic carried on these highways imposes substantial noise impacts on the adjacent development parcels. The projected 60 dBA, Ldn noise contour associated with MD-118 extends from the centerline to 370 feet into the Town Center Core (TC-1). This imposes setback and design requirements on adjacent residential development in this area.
- The wetlands associated with the open space systems of TC-1 and TC-5 limit building development and the development of parkland. Permits are required by both the Corps of Engineers and the Maryland Department of the Environment, prior to development in these areas.
- The proposed development of the Town Center Transit Station (on TC-2) with the associated requirements of easements, bus bays and parking, imposes a constraint on the development of this site. In addition, the recommendation to provide a substantial number of dwelling units over the station presents an additional constraint, in terms of the design, cost of construction and marketability of this Analysis Area.
- The significant length of residential development along the edge of the Town Center must also be taken into consideration. This edge will require adjacent uses to be compatible and may require buffering.

TOWN CENTER DESIGN CONCEPT

The initial community workshop of the Germantown Town Center Design Study was structured to gain an understanding of the community's perception of the future Town Center. During group discussions the following questions were asked:

Those places that were identified as having the **feel** desired of Town Center ranged from a small town in Ohio to Georgetown, Washington, D.C. Most often mentioned was the New England village.

A number of places were also suggested that relate to **main street** concepts. These included the French Quarter in New Orleans, and to main streets of Annapolis, Georgetown, and the recently developed Forrestal Village (outside of Princeton, New Jersey).

Also suggested were Central and Battery Parks in New York.

Attributes that were identified as the most important in describing these places included:

- village center
- main street
- unifying central space
- a sense of community
- overall sense of place
- intimate feel
- pedestrian orientation
- visual focus
- consistent streetscape
- architectural character

Specific characteristics that were identified for incorporation into the Germantown Town Center included the following:

- The Town Center is to include a balanced mix of uses and to include facilities for cultural, recreational, and leisure amenities.

It should include a community building to be centrally located and a focal point.

- The visual focus should be on the open spaces.
- In addition to parkland, open space should also include paved areas for pedestrian gathering, etc.
- Town Center is to be pedestrian-oriented.

COMMUNITY WORKSHOP

DESIRABLE PLACES

ATTRIBUTES

- Parking should be screened and behind the buildings (not between the buildings and the street).
- Buildings are to range from low- to mid-rise in height, with higher buildings in the center.
- The **Main Street** should be pedestrian in scale.
- Overall, the Town Center should have a common theme.
- Architectural quality and character are essential to the success of the Town Center.

TOWN CENTER VISION

The vision perceived for the Germantown Town Center is that of a **traditional** downtown. The design of the Town Center proposes to reflect the traditional urban architecture of the mid-Atlantic region and, as Roger K. Lewis stated in his *Washington Post* article on Shaping the City, **emulate Townscape characteristics of historic communities whose physical organization, streets and public spaces, building form, usage, and landscaping seem so compelling.** These communities provide a model for the Town Center that most closely resembles the attributes sought by the community and the Master Plan; one that appears to address the common theme and architectural character desired.

The following **design conventions** that were identified by Roger Lewis* are descriptive of the design character and **feel** desired for the Town Center. They include:

DOMINANT CENTER . . . defined by the crossing of two main streets lined by shops, stores, offices, and residences. Linked to this commercial cross-roads is the requisite New England **green or common**, surrounded by public streets and sidewalks. . . .

■ **EDGES.** The . . . transition from rural (or suburban) to urban land use, is . . . perceivable . . . there is a strong sense of arrival in the town. . . .

■ **STREETS.** Streets are shared equally by automobiles and pedestrians. Curbside parking . . . is usually available adjacent to sidewalks on both sides of the street. Generally there are no large parking lots between sidewalks and buildings.

Town plan is a typical grid pattern of streets and blocks, but the grid . . . shifts to accommodate topographic and other natural conditions or to reflect (existing development), . . . ownership and use.

Streets are not all the same size. The principal streets are several lanes wide, but . . . narrower residential streets are still wide enough to accommodate on-street parking.

■ **TREES.** Every street is lined along its sidewalks by deciduous trees, sometimes irregularly spaced. The town green is likewise replete with trees. Trees often dominate architecture as they form colonnades and canopies enveloping the streetscape and partially screen the volumes and facades of the town's two- to four-story buildings.

* Roger K. Lewis, *Timeless Design in New Standard*, *The Washington Post*, August 25, 1990.

- **IN-BETWEEN.** The space between sidewalks and buildings is critical in forming the image of a community . . . it is the development, along with the dimension, of the strip of landscape between sidewalks and buildings that matters.

A symbolic relationship exists between street, sidewalk, front yard, and buildings . . .

- **MOVEMENT.** (Although an automobile may be needed to access town) . . . to cross town quickly, to transport goods, . . . walking is a feasible option, especially for socializing and shopping.

Ample sidewalks, other pedestrians, sheltering trees, flowering vegetation, interesting facade details, and inviting storefronts make walking a pleasant experience . . .

- **OFF-STREET PARKING.** . . . parking lots . . . are normally behind or alongside buildings, tucked away within blocks.

The design framework that is proposed for the Germantown Town Center results from a synthesis of the Master Plan, the Town Center Planning and Design Context, and the attributes and vision that has been identified.

DESIGN FRAMEWORK

A summary of the proposed elements that constitute the recommended overall design character include:

- an urban character
- buildings facing on streets
- parking lots behind buildings (in courtyards; not between the building and street)
- parallel parking on secondary streets
- building heights to a maximum of seven stories (excepting the hotel and the high-rise residential towers)
- a pedestrian-oriented Main Street of a consistent character (from Middlebrook to Crystal Rock)
- Main Street to be a mixed use street of specialty retail and entertainment, offices, and residential
- commercial office and/or multi-family residential uses an MD-118
- multi-family residential uses adjoining the existing residential development to the north (of both TC-1 and TC-5)
- the development program proposed in the Master Plan
- distinctive and identifiable **gateways** at the major entrances to the Town Center and the entrances to the Town Center Core (TC-1) and the TC-5 retail center
- a landmark to identify Germantown, to be centrally located within the Town Center Core (TC-1)
- major, identical or related, and highly visible identifying elements at both the MARC Commuter Rail and Town Center Transit Stations
- consistent signage at the entrances to TC-1, 2, 3, and 5 developments

- the establishment of a hierarchy of streetscape treatments
- MD-118 developed as a **boulevard**
- Father Hurley, Middlebrook and Crystal Rock also to receive special streetscape treatments
- Lockbury Drive, Water Road and Old MD-118/Walter Johnson Drive designed as **special streets**, to be pedestrian-oriented and include a hiker/biker trail
- enhancement of the pedestrian circulation elements on Middlebrook (from the High School to the Town Center Core)
- sidewalks and trees on both sides of all streets
- a grid street system internal to TC-1 (connecting the major entrances from MD-118, Crystal Rock and Middlebrook)
- Waters Road to be upgraded in its current location through TC-5
- an intersection at Waters Road and MD-118, through to Old MD-118
- an interconnected street system internal to TC-5 (linking the MD-118, Wisteria Drive and Father Hurley Boulevard entrances)
- reinforcement and expansion of the Town Center-wide vehicular and pedestrian circulation linkages - as a continuation of Main Street through the existing shopping centers (Middlebrook and Wisteria) to TC-5
- an internal transit system that utilizes Main Street and links the proposed Town Center Transit Station (TC-2) through the Town Center Core, the existing shopping centers and the proposed service and retail park (TC-5) to the existing MARC Commuter Rail Station (TC-6) and the Historic District
- hiker/biker trails within the Town Center linking the three major adjacent open space systems to parkland and community facilities
- a major public park in TC-1 (incorporating the existing wetlands on the north of this site); this park is recommended to accommodate an outdoor assembly area (amphitheater) and related recreational facilities
- Town Center Commons centrally located in the Town Center Core (TC-1) on the north-south entrance boulevard and the east-west Main Street
- location of the Cultural Arts Center on the Town Center Commons within TC-1 (this facility is to occupy a focal point and is recommended to be freestanding)
- development of a indoor pool and the possible inclusion of the indoor building component of the Germantown Recreation Center on TC-5.

TOWN CENTER-WIDE DESIGN FRAMEWORK

INTRODUCTION

Collectively, the design frameworks of the vehicular and pedestrian circulation systems, the park and open space systems, and the location of community facilities constitute the major components of the overall design framework for the Town Center. These systems are significant in the proposed integration of existing development and that proposed for the Analysis Areas into a cohesive Town Center. Their specific application to these Analysis Areas, together with the land use and building related components of the framework, and the design of the street-scapes, will result in the establishment of the the overall functional and visual character of the Town Center.

The following describes each of these component systems of the proposed Town Center-wide design framework.

TRANSPORTATION

VEHICULAR ACCESS

The vehicular circulation system for the Town Center utilizes and expands the major highway/roadway and arterial street system that currently exists and is planned to serve the area. Major highways provide access to existing development and the various Analysis Areas that comprise the Town Center. The framework of internal streets is designed to provide clarity and ease of movement within and between these areas.

Each Analysis Area is served by entrances from the surrounding major highways and existing roadways. Internal streets are proposed to link these entrances and serve to distribute traffic. The internal patterns generally are those of grid street systems adapted as necessary to respond to adjoining streets, existing property lines, and natural features. This pattern distributes traffic efficiently by allowing alternative routes.

A hierarchy of access is provided within each Analysis Area. The MD-118 entrances to the Town Center Core (TC-1), TC-3, and TC-5 are four-lane divided boulevards, as are the entrances to these zones from the other major roadways (Crystal Rock Drive, Middlebrook Road, Father Hurley Boulevard, and Wisteria Drive). These boulevards provide definition at the primary entrances to each area. To discourage through traffic, the boulevards are discontinuous. In selected areas, they may also accommodate parallel parking. Most of the internal streets intersect these boulevards distributing traffic to and collecting traffic from parking.

An urban scale environment is provided through limiting the length of the blocks, which range from approximately 200 to 400 feet in length, similar to city blocks. This spacing helps to reduce the size of surface parking lots, also sized to accommodate their later retrofit as parking structures. Development of structured parking (preferably underground) is encouraged in the Town Center to the maximum extent feasible.

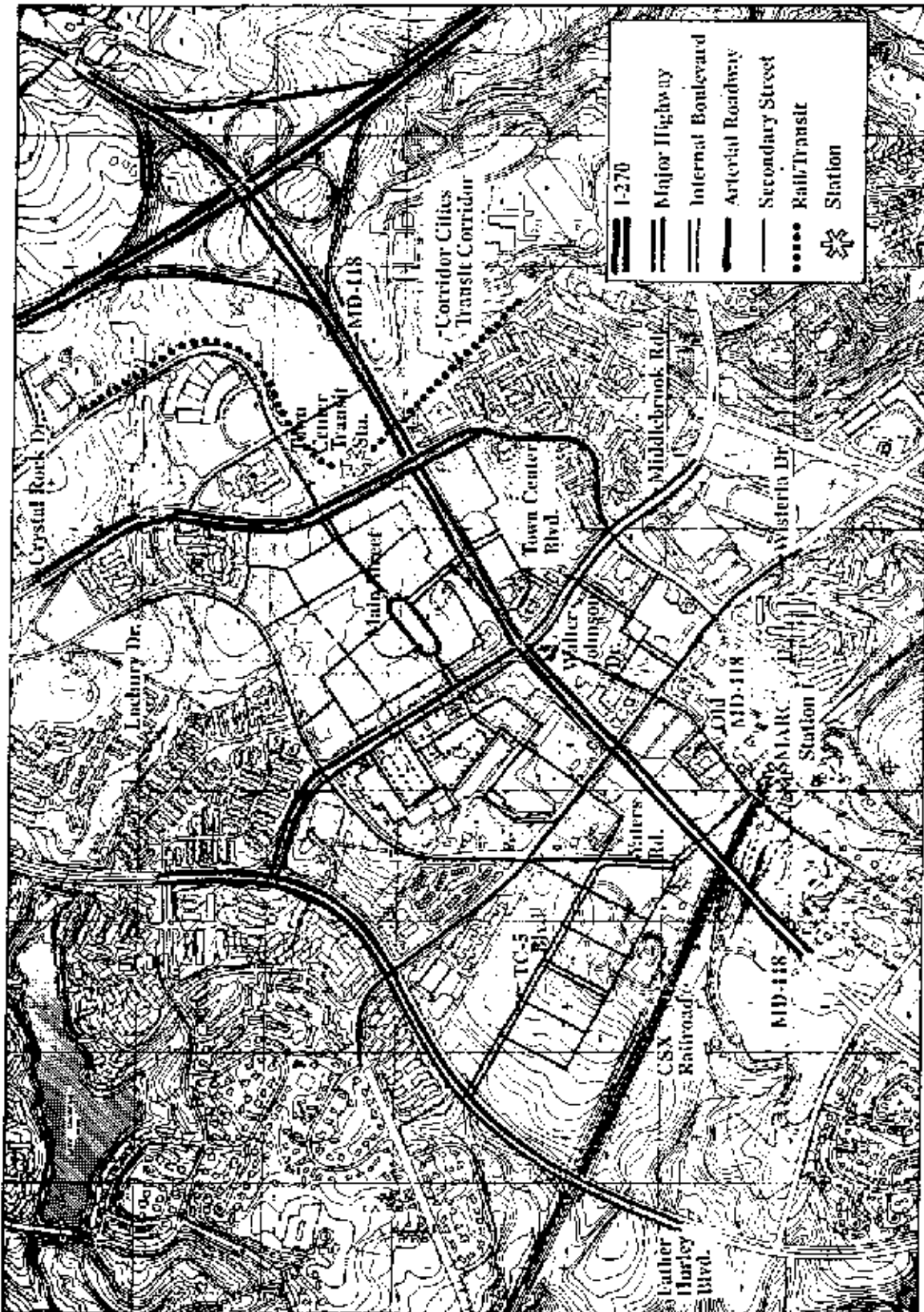
Town Center streetscapes reinforce the hierarchy of the circulation system throughout the Town Center as well as that internal to each Analysis Area. Almost all streets are proposed to have sidewalks and to incorporate street trees.

Several major vehicular connections have also been provided between Analysis Areas. The most important of these is the proposed Main Street link through Town Center. Originating at Crystal Rock Drive (opposite Century Boulevard) Main Street runs through the Town Center Core (TC-1) to Middlebrook Road. Main Street is then extended across Middlebrook Road and, utilizing the existing through tree system within the existing shopping centers, continues across Wisteria Drive to the central internal boulevards within TC-5. These boulevards connect the TC-5 entrances of Father Hurley Boulevard, Wisteria Drive, and MD-118. From TC-5, Main Street Extended (Waters Road) continues across new MD-118 to and across old MD-118 to the MARC Commuter Rail Station. The current Master Plan does not indicate the intersection of Waters Road with extended MD-118. This is recommended as essential in providing access from MD-118 to the proposed TC-5 retail and service center and in accessing the MARC station to the southeast. Although a future parking lot for MARC is proposed to incorporate this road between new and old MD-118, this facility is recommended to be located to the north of Waters Road to provide access from MD-118.

Main Street thus links the future Corridor Cities Transit Corridor Town Center Station and related gateway development (TC-2), the Town Center Core (TC-1), the existing shopping centers, the new retail and service park (TC-5), and the MARC Commuter Rail Station and Historic District beyond.

The connection of Locbury Drive and Waters Road provides another means of linking the Town Center Core (TC-1) with TC-5. Although less circuitous than Main Street Extended, this route is more peripheral and is recommended to be more like a country road. Although Main Street is urban in character, both are proposed to be highly pedestrian oriented with the Locbury Drive and Waters Road connection recommended to incorporate more open space and to accommodate a hiker/biker trail.

Old MD-118 and Walter Johnson Drive are also proposed as special streets. With the extension of MD-118 this road will be relieved of all but local traffic. It dead-ends both at Germantown Square Park, and the bridge over the CSX Railroad tracks proposed to be closed to vehicular traffic. Old MD-118, the historic main street of Germantown, has the potential to provide a pedestrian-oriented street of



VEHICULAR CIRCULATION

unique character linking the new Core of the Town Center to the Historic District.

INTERNAL TRANSIT

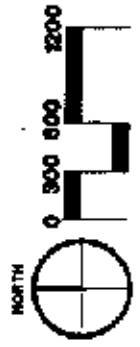
The Circulation Framework provides multiple opportunities to link activities throughout the Town Center. An internal on-street transit system is proposed to not only provide access to, but also within each of the Town Center Analysis Areas and to feed the main Town Center transit stations. Initially a jitney or local bus, this system could evolve into a more advanced mode. Internal transit is also designed to reinforce the imageability and hierarchy of the overall circulation system.

An internal transit route is proposed along Main Street and Main Street Extended to link the primary areas of activity in the Town Center. This route connects the proposed Town Center Corridor Cities Transit Station (TC-2) to the existing MARC Commuter Rail Station (TC-6). The route could vary to stop at specific public facilities/amenities and to serve special events, such as Octoberfest.

When the Town Center Transit Station is constructed it will be accompanied by a substantial parking garage with roof top recreational/open space and/or other amenities as well as the air rights development of a substantial number of high-rise dwelling units. Together with office development, this center of activity will act both as an origin and a destination. The Town Center Core (TC-1), across Crystal Rock Drive, will also be a high density, mixed use, activity center. Main Street in TC-1 will consist of multiple shopping and entertainment activities. Its central focal areas will be the Town Center Commons and the adjacent Cultural Arts Center, related community facilities and parkland. Internal transit is proposed to link these activities across Middlebrook Road to the Upcounty Government Center and library, through the existing shopping centers and across Wisteria Drive to TC-5.

Although primarily a service and retail center, TC-5 is also proposed to contain a substantial number of dwelling units (including a residential high-rise similar to that proposed for TC-2), and a public indoor pool and health club, in addition to the possible indoor component of the Germantown Recreation Center building. Transit will connect these uses and activities to the rest of the Town Center. The internal transit system is proposed to continue to the MARC Commuter Rail park-and-ride lot, its adjacent parkland and associated open space system and (across the railroad tracks) to the Historic District.

A second internal transit route might tie the proposed indoor recreational facility and related multi-family housing in TC-5 to the major parkland, recreational facilities, open space system, and community facilities of the Town Center Core (TC-1). This transit route is recommended to utilize the central boulevard within TC-5 and the Lockury Drive and Waters Road connection to access TC-1 at the Town Center Park. Transit service could proceed on to the Town Center Commons at Main Street. Utilizing this peripheral route, a bus could shuttle people from remote parking in TC-5 to special events in the Town Center Core, such as performances in the park amphitheater, the Cultural Arts Center and **Octoberfest**.



INTERNAL TRANSIT

PEDESTRIAN CIRCULATION

The Town Center design framework incorporates a substantial pedestrian circulation system. This framework is designed to respond to the various functional requirements of access, to provide a safe and efficient environment, to accommodate both active and passive outdoor activities, and to establish an overall pedestrian character in the Town Center.

Sidewalks are the primary pedestrian circulation system elements. Sidewalks occur on almost every street in the Town Center. They link all of the various Analysis Areas as well as all development internal to these areas. Sidewalks vary in character and design in response to their function and edge conditions. In combination with other streetscape elements, Town Center sidewalks are designed to provide structure and order, orientation, and a sense of place.

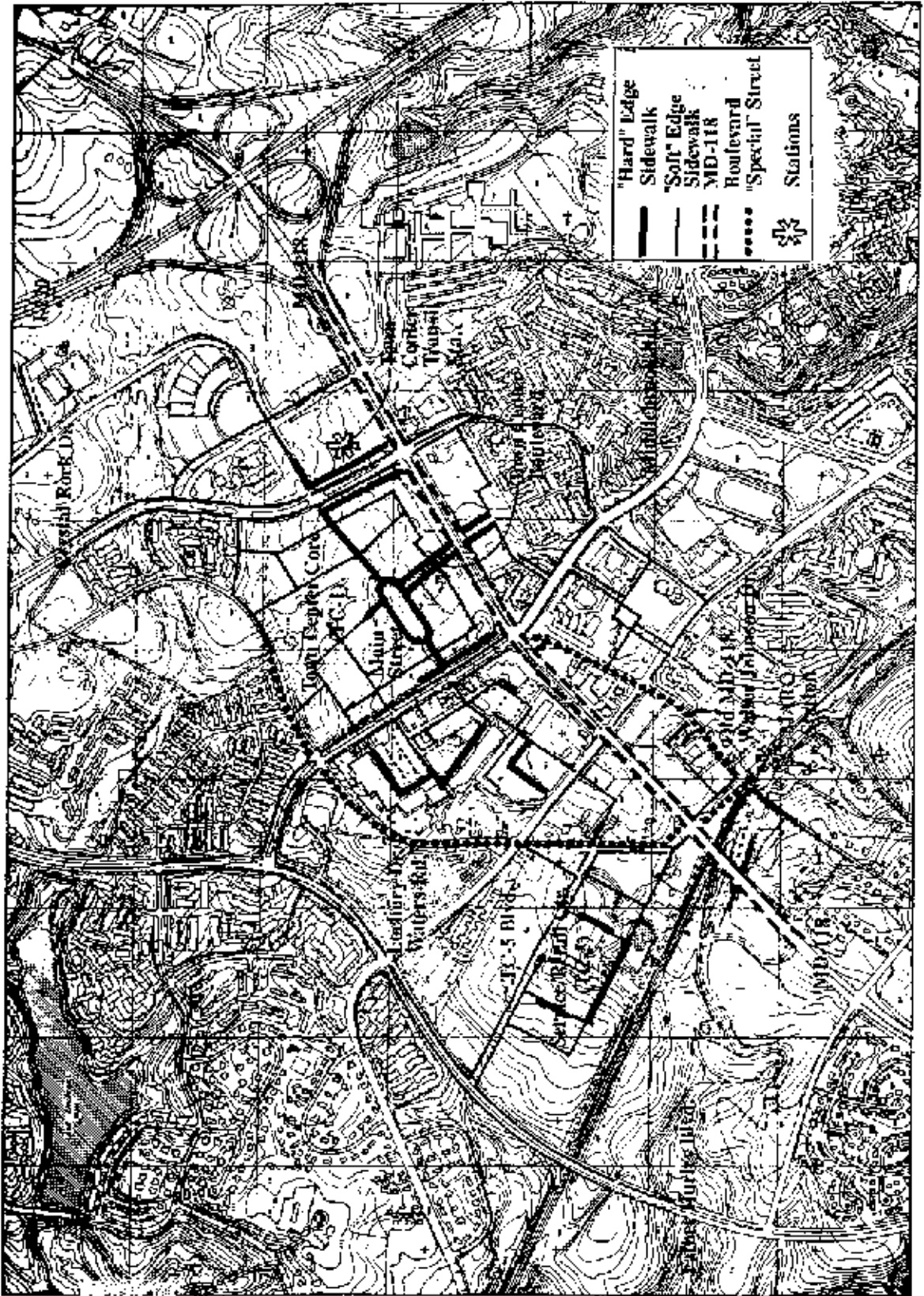
Sidewalks are generally grouped into those having pavement at the curb of the street or building (**hard edge**) and those with planting strips between the walk and the street (**soft edge**). Within these general classifications variety is also provided by such factors as the width of the sidewalk, the width and landscape treatment of the planting strip, treatment of the building edge of the sidewalk, the use of paving materials, etc.

In the Town Center **hard edges** generally are recommended to occur adjacent to retail uses such as are proposed in the Town Center Core (TC-1), and within the service retail center proposed for TC-5. The existing storefront sidewalks of the Middlebrook and Sugarloaf Centers will be upgraded to be consistent with Town Center-wide streetscape treatments.

The majority of the sidewalks within the Town Center are **soft edge**. Sidewalks vary from 4 to 16 feet in width and the width of the planting strips average six feet. The major exception is the unique streetscape treatment of MD-118 Boulevard (see the Streetscape Design Framework). Here the sidewalk is six feet in width and set back from the curb of the road by a 10-foot-wide planting strip. Buildings are recommended to be set back 20 to 25 feet from the street right-of-way. This area is, for the most part, recommended to be landscaped and could contain a second walkway (in front of the buildings).

Locbury Drive, Waters Road, and Old MD-118/Walter Johnson Drive are identified as **special streets**. Although different in character, all are proposed as highly pedestrian in nature. The Old MD-118 and Walter Johnson Drive sidewalks are recommended to be reconstructed in a historic streetscape, possibly incorporating brick paving. This street provides a link from the Germantown Historic District to the Town Center Core.

Locbury Drive and Waters Road connect the Town Center Core with the Germantown Historic District (the old and new town centers). These pedestrian oriented streets afford the opportunity for separate sidewalks and hiker/biker pathways. It also provides an alternative route linking the major open space systems to the south of the MARC Commuter Rail Station (TC-6) and to the north of Locbury Drive and the Town Center Park.



PEDESTRIAN CIRCULATION

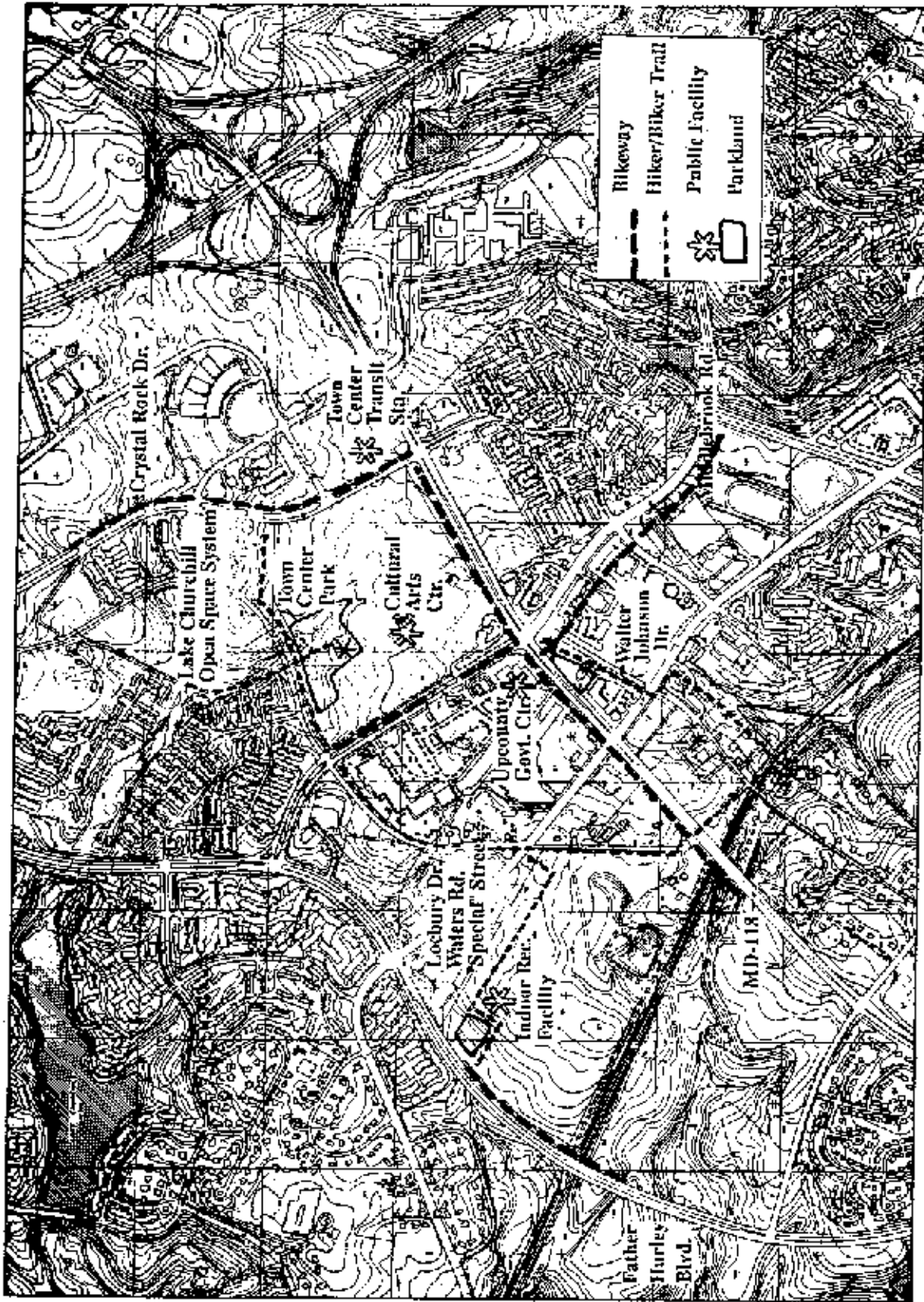
BIKEWAYS

The Town Center circulation framework incorporates and extends the existing bike path on the south side of Middlebrook Road to its intersection with MD-118 and on to Lockbury Drive. This system also connects to a bike path proposed on the south side of Lockbury Drive, where no sidewalk currently exists.

A bike path is also to be provided on the south side of Father Hurley Boulevard, from its Town Center entrance over the CSX tracks to the TC-5 Boulevard and Father Hurley Park, tying to the hiker-biker trail to Lockbury Drive and on to the Town Center. A bike path is also to be provided on the north side of MD-118 from the CSX Railroad tracks to Crystal Rock Drive. Whereas the MD-118 bike path serves as the sidewalk, that on Crystal Rock Drive is separate from the sidewalk. This bike path is recommended to be located on the TC-2 (station) side of Crystal Rock Drive, and continue north of Century Boulevard within the public open space adjacent to the police and fire stations. (This location, serving the future transit station and incorporating an off-sidewalk, biker-only path, is preferred to that of the Master Plan which shows this path on the west or TC-1 side of Crystal Rock Drive, however; other factors may either necessitate this location on the west side of Crystal Rock Drive or preclude its relocation.)

In addition to these bikeways, hiker/biker paths are also incorporated into the development of the Town Center. The major proposed link is that previously discussed along the Lockbury Drive and Waters Road connection. A connection from this trail is also proposed along the northern edge of TC-5 to Father Hurley Park and the associated open space system.

A hiker/biker trail is also proposed to be provided from the open space system in the area of the MARC Commuter Rail Station (TC- 6), along the stormwater management pond to and across Wisteria Drive, to join the pedestrian pathway on Walter Johnson Drive (Old MD-118) to Germantown Square Park. There also exists the opportunity to incorporate the natural system that remains on the North-lake side of this street.



BIKEWAYS/HIKER/BIKER TRAILS

COMMUNITY FACILITIES

PARKLAND/OPEN SPACE

The framework for parkland and open space is designed to provide needed parkland in the Town Center while tying both parkland and open space into a continuous system. This system also incorporates elements of the vehicular circulation, pedestrian circulation, and hiker/biker systems.

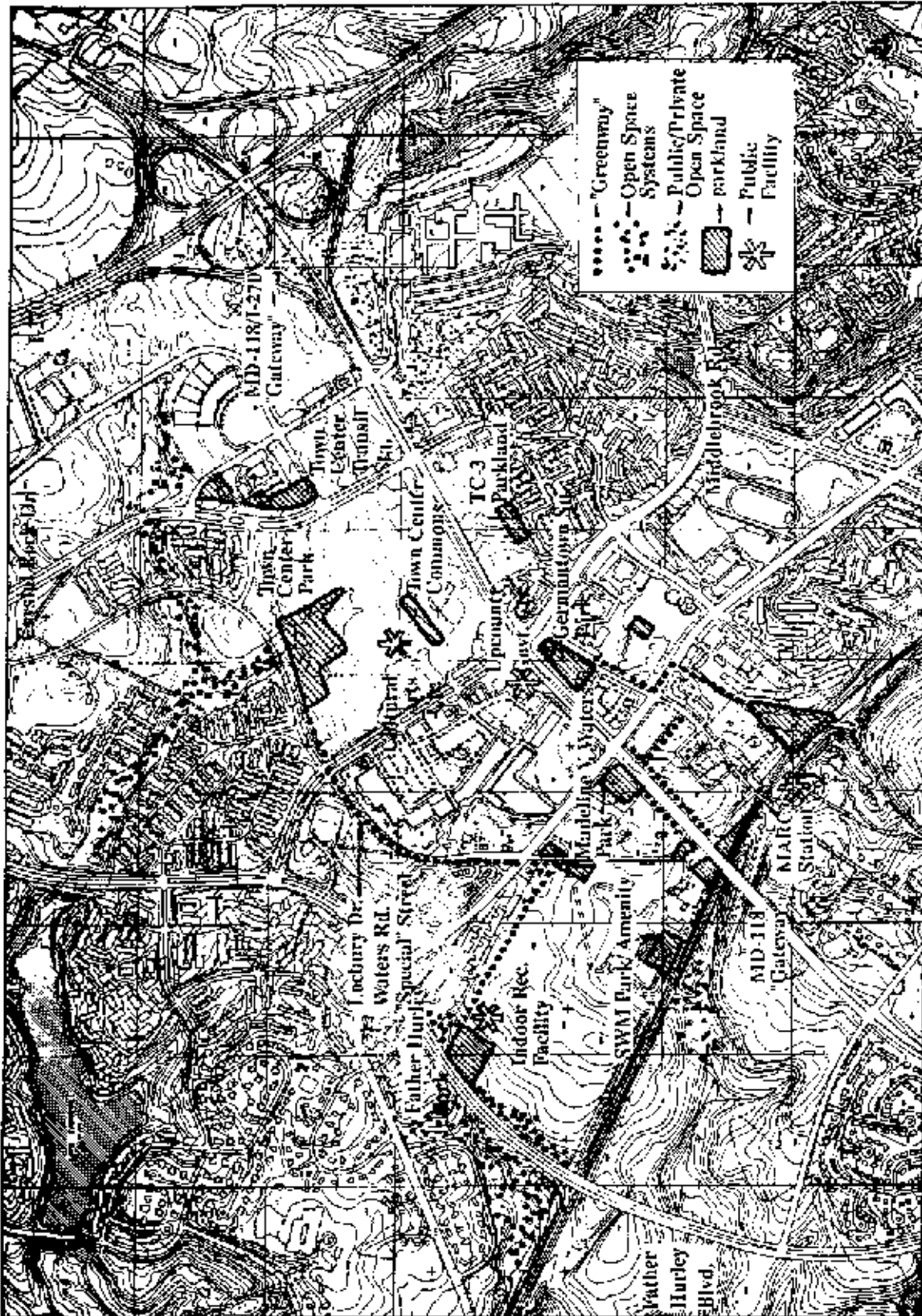
The basis of the Town Center open space system is the linking of the three primary natural open space systems that penetrate the Town Center study area. As previously described, these systems include the Lake Churchill open space system that enters the Town Center Core (TC-1) from the north side of Lacbury Drive; the system that enters the Town Center adjacent to the MARC Commuter Rail Station park-and-ride lot (TC-6), where the stormwater management pond was recently constructed; and the system that enters at Father Hurley Boulevard extended (TC-5).

Parkland is proposed within the Town Center in association with each of these open space systems. Proposed parks include Town Center Commons and Town Center Park within TC-1; parkland at the end of the entry boulevard in TC-3; the existing open space associated with the stormwater management pond adjacent to the MARC Commuter Rail Station (TC-6); a park at the site of the historic Madeline V. Waters House (TC-5); parkland to be developed in association with the improvement of the existing pond in TC-5; a park proposed to be located in the open space system at Father Hurley Boulevard (referred to as Father Hurley Park) on TC-5; and existing Germantown Square Park. Germantown Square Park is currently owned by the Montgomery County Parks Department. Although possibly initially developed and maintained as private open space, Town Center Park, Father Hurley Park, and the Madeline V. Waters Park are recommended to become public in the long term.

The largest parks, all located within or adjacent to the natural open space systems, are proposed as natural in character. Primarily designed for passive activities, all incorporate a component of wetlands. The largest of these is the Town Center Park. Located within the Town Center Core this facility is also proposed to contain a gazebo and amphitheater, and is recommended to include both active and passive activities (see **OCTOBERFEST**).

Town Center Commons, located in the center of the Town Center Core at the intersection of the Town Center Boulevard and Main Street, is intended to serve many purposes. Symbolizing the Town Center, this park is viewed as an outdoor setting for buildings, such as the Cultural Arts Center, that will front onto it. Town Center Commons is proposed as an urban park and a people place.

An open space amenity is also proposed to be developed on the roof of the parking structure associated with the development of the Corridor Cities Transit Corridor Town Center Station (TC-2). This park is recommended to provide green space and possibly a water feature at the base of proposed residential high-rise air rights development over the station.



All Town Center parks and related open space systems, including committed private open space, are proposed to be linked by **greenways**. MD-118, with its streetscape of double rows of trees, wide planting strip, and substantial building setbacks, establishes the most prominent greenway through the Town Center.

As previously described, the **special street** development of Locbury Drive provides a connection from Town Center Park and the associated natural open space system to, within, and through TC-5, to both the proposed Father Hurley Park and the parkland associated with MARC Commuter Rail Station (TC-6) and the natural open space systems related to both of these parks. This latter park and the associated stormwater management pond are also proposed to be linked through the woodland to be retained on the Northlake tract and the Walter Johnson Drive (Old MD-118) right-of-way to Germantown Square Park and the Town Center Core beyond.

Within the Core, Town Center Boulevard serves as a greenway tying the MD-118 entrance to Town Center Commons and Main Street and back to the Town Center Park. Crossing MD-118 to the south, this greenway also ties this system to the proposed parkland terminus within TC-3.

CULTURAL FACILITIES/HISTORIC RESOURCES

The Cultural Arts Center that is proposed to be located within the Town Center Core (TC-1) is viewed as one of the primary components of the new **downtown** of Germantown. The success of the Town Center depends, in part, on its strong identity as the cultural and recreational center of Germantown.

The Cultural Arts Center is recommended to include a 300- to 500-seat auditorium with stage, classrooms for the performing visual arts, and display space for visual arts. It will also contain community meeting rooms and related activity areas. This facility is recommended to be a freestanding building and is to be highly imageable and highly visible. The proposed location for this facility is on the Town Center Commons, at the intersection of the Town Center Boulevard entrance road and Main Street. The Center is shown located in the northwest side of the commons facing the open space. This location is highly visible from the entrance boulevard and from Main Street. An alternative location that may be considered in the siting of this facility is on the axis of the Town Center Boulevard.

Other cultural facilities that will exist within the Town Center include the library located in the Upcounty Government Center. Town Center Park is also recommended to incorporate the development of an outdoor amphitheater, a gazebo, and related facilities and amenities.

As previously described, the Town Center includes part of the Germantown Historic District and several historic sites. The former site of the Madeline V. Waters House (TC-5) is proposed to be developed as a park. The trees that once lined the driveway of this house should be incorporated into the development of the adjoining parcel (TC-6).



COMMUNITY FACILITIES

As with the circulation and open space design frameworks, the linking of community facilities is an important component of the design framework.

OTHER FACILITIES

In addition to the existing facilities of police and fire stations, the proposed Post Office and the Upcounty Government Center, and the Cultural Arts Center; several other public facilities are recommended to be included within the Town Center.

As previously mentioned, an amphitheater is proposed for inclusion on Town Center Park. Although the size of this facility is not specified, it should be large enough to accommodate a reasonably sized crowd. Lawn seating is recommended to be incorporated. Other facilities to be provided in this and other Town Center parks include open areas for casual recreation, frisbee and ball throwing, kite flying, facilities for picnicking, and an ample number of park benches.

It is also suggested that each of the Town Center parks incorporate landscape features (gazebo, trellis, sculpture, water features, etc.) to create interest, variety and identity. The Town Center Commons provides the opportunity to incorporate a landmark focal point.

Development of the TC-5 Analysis Area under RMX-2 zoning requires the provision of a major site amenity. Although this requirement could be satisfied with an off-site contribution such as funding of the operational budget of the Cultural Arts Center, the TC-5 amenity preferably will be provided within the TC-5 area. The community facilities framework for the Town Center identifies the possible inclusion of an indoor recreation facility, to be located adjacent to the proposed Father Hurley Park in TC-5. This facility could include an indoor pool and health club. If felt appropriate, it could also be combined with the indoor component of the Germantown Recreation Center. Considerable additional community discussion is needed before the final determination of the TC-5 amenity. If an appropriate facility is not determined by the time the developer of TC-5 is ready to proceed, in addition to the proposed dedication of Father Hurley Park, a donation or commitment to fund a to-be-determined amenity is required.

In addition, there has been discussion about setting aside land for non-profit organizations, including houses of worship. Although not the the appropriate TC-5 amenity, the identification of a location for additional places of worship in the Town Center is encouraged.

OCTOBERFEST

The accommodation of Oktoberfest in the Germantown Town Center is a basic requirement of the planning and design of the Town Center. Currently located on the 59-acre open field of TC-1, Oktoberfest will continue to be held in this location; however, the setting will be one of the **downtown** of Germantown.

Oktoberfest is envisioned to occupy a major portion of the Town Center Park and the Town Center Commons. In addition, Main Street and portions of the Town Center entrance boulevard will be closed to vehicular traffic and incorporated into Oktoberfest, not only to provide additional area but a linking of all areas and events. This area

is estimated to accommodate some 40,000 to 50,000 persons. Additional capacity could also be obtained by the incorporation of one or more of the TC-1 major parking areas with additional parking provided on the adjacent TC-2, TC-3 and Middlebrook Shopping Center lots. After the completion of the Town Center Transit Station (TC-2) an additional 1,000 car parking spaces, in addition to transit service, can be available on this site.

ANALYSIS AREA DESIGN FRAMEWORK

INTRODUCTION

For purposes of discussion the Town Center Analysis Areas are grouped by their locations and proximity to one another. TC-1, TC-2, and TC-3 Analysis Areas are discussed together as are TC-5 and TC-6. TC-1, TC-2, and TC-3 are located between the I-270 entrance to the Town Center and Middlebrook Road. TC-5 and TC-6 are located between Wisteria Drive and the CSX Railroad tracks. The shopping centers and other existing Town Center development are located in the middle (between Middlebrook Road and Wisteria Drive). The only Analysis Area located in this area is that of TC-4. TC-4 is not included in this discussion of the framework because of its isolated location, its small size, and minimum development potential. It is recommended that TC-4 be considered for incorporation into the adjacent Germantown Square Park. TC-7, which is located to the south, is also not included as it is committed to the development of the Germantown Post Office.

The Analysis Area Design Framework is described in terms of the following components:

- vehicular circulation
- streetscapes
- pedestrian circulation
- building envelope
- building heights
- development phasing, and
- parking.

Each Analysis Area is first described in terms of the Town Center-wide circulation system. The Vehicular Circulation Framework that is proposed for the area is discussed, including the major points of access, entrances, and the primary and secondary streets. Special streets such as Main Street and the Town Center Core Entrance Boulevard are described. Rationale is provided for the hierarchy and layout of the internal street system.

The Streetscapes that are recommended for incorporation into the Analysis Areas are described in relationship to the overall Town Center Streetscape Framework. The proposed internal hierarchy is discussed in terms of the objectives of providing identity, orientation, and the creation of a sense of place. The streetscape prototypes that are proposed are identified. (They are described in the Streetscape Design Framework section.) (The Design Study is not a detailed streetscape study and the prototypes are illustrative in nature. The location and design of the streetscape [street trees, paving, lighting, signage, street furniture, etc.] is controlled through plan review.)

Next the Town Center-wide Pedestrian Circulation Framework is applied to each Analysis Area. Elements of the framework, including the location of **hard** and **soft** edge sidewalks, special corner treatments, special paving, and street crossings are described.

The discussion of the Building Envelope component of the Analysis Area Design Framework includes the identification and clarification of the elements of building zones, building setbacks, and the location and significance of important building edges (walls and corners). Significant building facades, where recommended, are noted. View lines and major focal points are also identified, including the proposed location of open space, parkland special features and major public facilities. The Building Envelopes plan presents a framework that is intended to guide the siting of buildings within these areas without getting to the level of detail of a site plan. With the application of building heights, the resultant design framework, although illustrative, implies scale and massing.

Phasing plans, where applicable, recommend the location of both initial and later phases of development, within the Analysis Areas. The long term build-out of these areas may take the form of infill and/or redevelopment. Parking is also identified in this context; to include the retrofit of surface parking with structured parking, as may be not only desired but required to allow a greater density of development.

ANALYSIS AREAS TC-1, TC-2 AND TC-3

VEHICULAR CIRCULATION

TC-1 Surrounded by the major highways of MD-118, Middlebrook Road, and Crystal Rock Drive, access to the Town Center Core (TC-1) is limited to the four points where median breaks are allowed. The primary entrance is from MD-118, mid-block between Crystal Rock Drive and Middlebrook Road. The Crystal Rock Drive entrance is proposed opposite the Century Boulevard intersection, and the Middlebrook Road entrances are at the existing median breaks that provide access to the existing shopping center. Another optional entrance is possible from Middlebrook Road, opposite another existing entrance to the shopping center. As there is no median break in this location this entrance to TC-1 would be restricted to right-turn movements. Other entrances to this Analysis Area are proposed from Lockbury Drive and from Rexmore Drive. Access from Lockbury Drive will relieve congestion at the primary entrances to the site, and will allow for through movement. The entrances off Lockbury Drive and Rexmore Drive would primarily serve the residential areas of TC-1. Two entrances are proposed from Lockbury Drive, one on either side of Town Center Park. The exact location of these entrances is somewhat flexible; however, wherever possible they should be opposite other street intersections. As the TC-1 Analysis Area is currently in multiple ownership, both of the Lockbury Road entrances are recommended as not only desirable but required for the possible phased development of the Town Center Core. The

Town Center Boulevard and its extension beyond the commons follows the property line between the western and center properties of TC-1; and, although not shown to follow the property line to the east (due to the potential for conflict with the wetlands, the Lockbury Road access to the east of the Town Center Park may also ultimately follow this boundary. Although the property line is shown to divide these roadways, they may ultimately be located on one property, as may be required for implementation.

The MD-118 entrance to TC-1 is proposed as a four-lane divided boulevard (Town Center Boulevard). This boulevard leads to the Town Center Commons and its intersection with Main Street, at the center of the Core. The entrance road continues around the Commons, past the site of the Cultural Arts Center to Town Center Park. This street also intersects the second Middlebrook Road entrance road and then continues on to Lockbury Drive on the north. Town Center Boulevard allows for two moving lanes of traffic in each direction and widens to incorporate parallel parking lanes at selected locations adjacent to development. A 28-30-foot-wide median is proposed to link the MD-118 open space system to the Town Center Commons. From the Commons to Town Center Park, the boulevard reverts to a four-lane undivided street (with the two outside lanes available for curbside parking).

An alternative Vehicular Circulation Framework is illustrated in order to accommodate an alternative location for the Cultural Arts Center. In this alternative this facility is located on the axis of the Town Center Boulevard and the extension of the Boulevard to the north of the commons is divided with two lanes running on each side of the Cultural Arts Center (a one way couplet). These lanes rejoin at Town Center Park and continue on to Lockbury Drive. All other street locations remain as described.

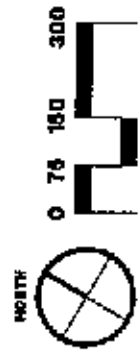
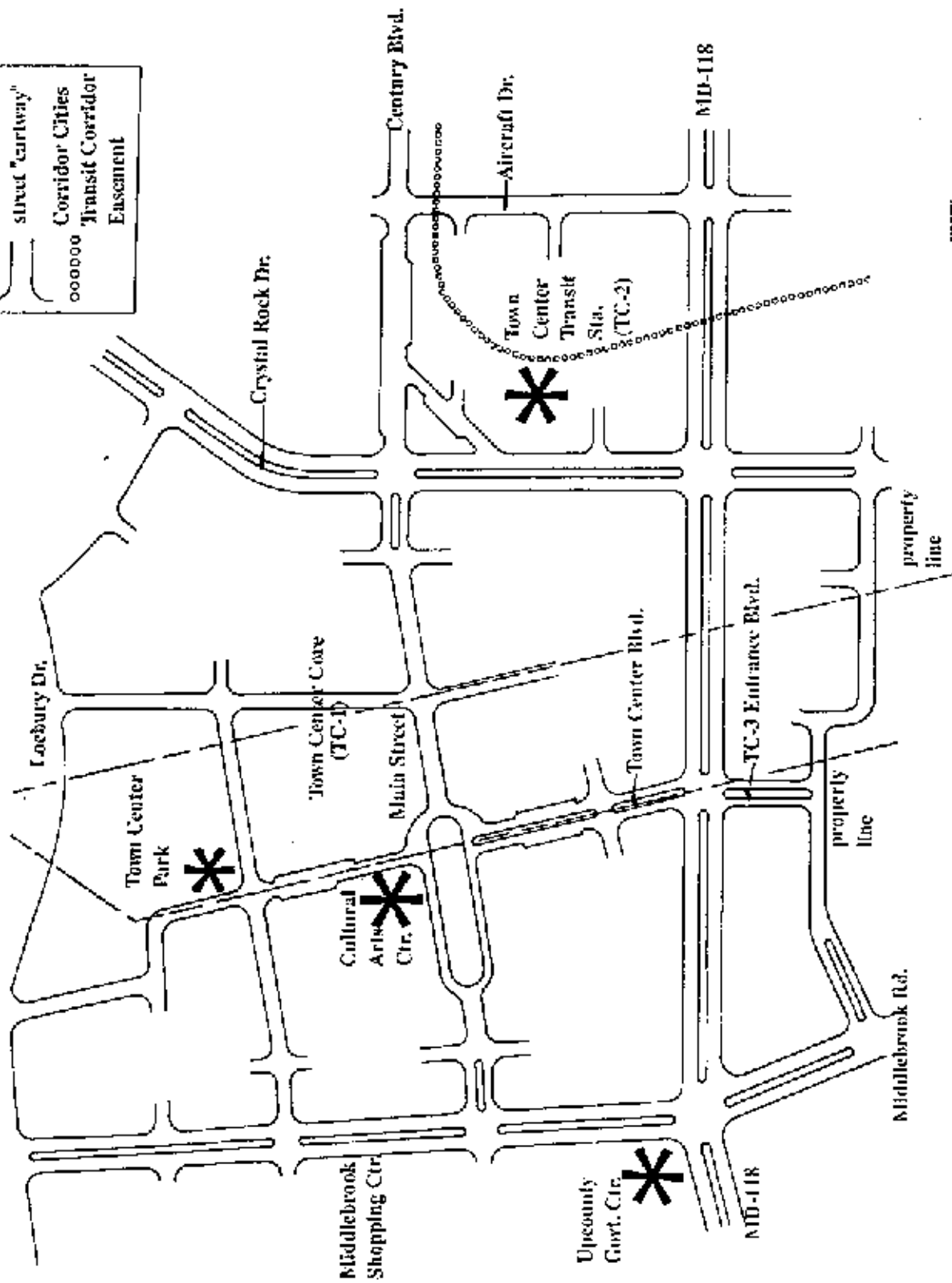
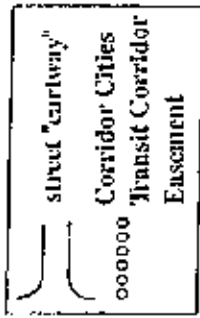
Main Street is the other major street internal to the Town Center Core. Linking the Crystal Rock Drive and Middlebrook Road entrances, this street also leads to and goes around the Town Center Commons (traffic flow is recommended to be one way counter-clockwise around the Commons). Main Street is designed to function as a pedestrian-oriented shopping street. The two outside lanes will be used primarily as parking lanes.

The secondary streets in TC-1 act as collector-distributor roads. An inner loop provide access to all perimeter development and related parking. This two-lane loop roadway system is recommended to intersect all of the major entrance roads including the Town Center Boulevard and Main Street (at both ends), thereby relieving them of traffic with destinations in the Core. This road also allows both the Boulevard and Main Street, in the interior of the Core, to be closed to vehicular traffic during special events.

As previously described, this internal grid street system provides many choices and options for access. All streets are interconnected. Internal intersections are proposed to occur at approximately 200- to 400-foot intervals resulting in a manageable and understandable **downtown** scale of development.

TC-2 No access is provided to the TC-2 Analysis Area directly from MD- 118. The site of the proposed Corridor Cities Transit Corridor Town Center Station, TC-2 must accommodate the drop-off, bus circulation, and park-and-ride requirements of the station, as well as the parking requirements of the planned on-site development. Access to garage parking is recommended to include right-turn only movements on Crystal Rock Drive, Century Boulevard, and Aircraft Drive. From MD-118 to Century Boulevard, the elevation drops in excess of 20 feet and direct access to multiple garage levels is possible. Although a diagonal auto drop-off is shown at the Crystal Rock Drive and Century Boulevard corner of the site, this is illustrative only and subject to future site development and transportation analysis. Bus traffic might also enter the Station here. Curb-side drop-off is also recommended on both sides of Century Boulevard. (If needed, this area might also be restricted to buses.)

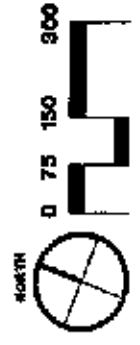
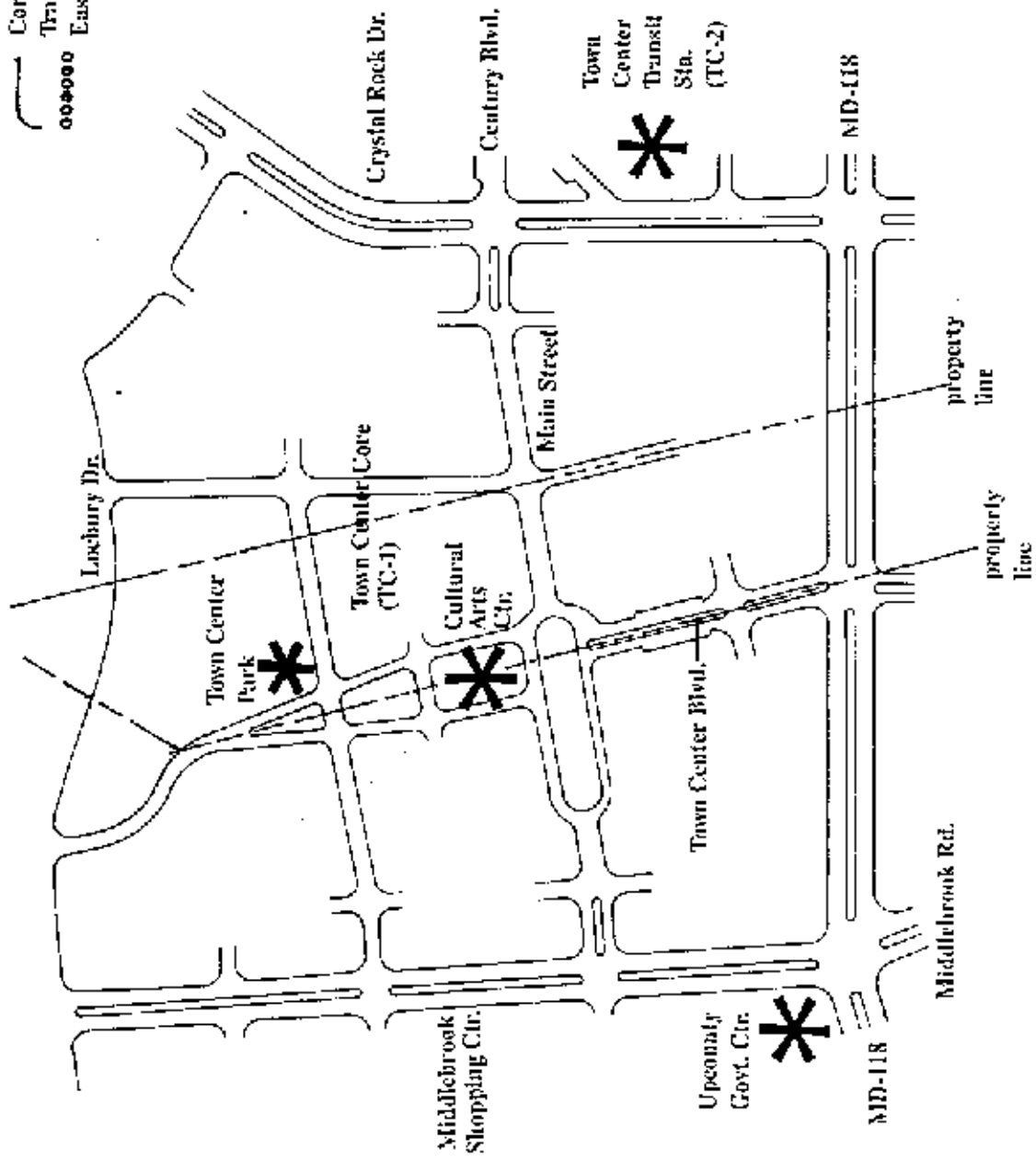
The eventual alignment (both horizontal and vertical) of the Corridor Cities Transitway through the TC-2 site impacts the feasibility and design of access to the site. Assumed to be entering TC-2 below grade (under MD-118), this line is recommended to be sufficiently depressed so as to exit under Century Boulevard and/or Aircraft Drive. This will allow unrestricted movement through the site above the station. If the line were to exit at grade especially if the turn to parallel Century Boulevard is maintained, access both to and through the site would be substantially disrupted. Should the Transitway not be grade-separated, its preferred location in the center of the site would prove infeasible in that this would prohibit the programmed development of TC-2. Resolution of the alignment of the transit alignment within this area is essential to the future planning and design of this station site.



VEHICULAR CIRCULATION FRAMEWORK

TC-3 Although no median break currently exists and approval is required, the desired primary entrance to TC-3 is from MD-118, directly across from the main entrance to TC-1. An entrance boulevard is recommended to feed an internal secondary collector/distributor road. This interior street is proposed to link the existing access road from Middlebrook Road to the access road off Crystal Rock Drive. Entrance from MD-118 is required to be via a public street. Currently both Middlebrook Road and Crystal Rock Drive access is private and these roads are recommended to be dedicated; or the MD-118 entrance boulevard be designed with a turnaround (within the parkland identified to be located at its terminus within TC-3).

- street "railway"
- Corridor Closures
- Transit Corridor
- Easement



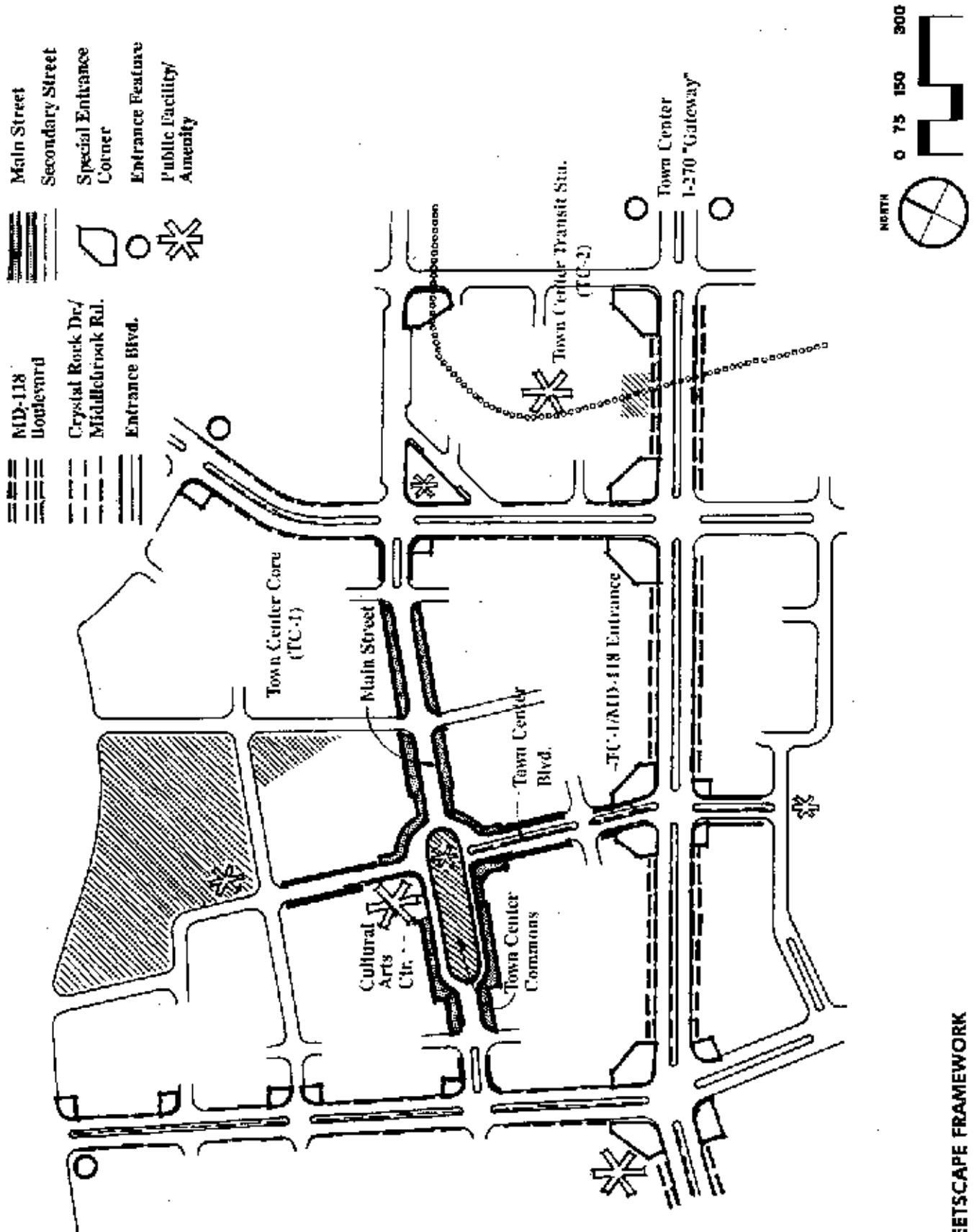
VEHICULAR CIRCULATION (ALTERNATIVE)

STREETSCAPE

The Town Center Core (TC-1) is proposed to incorporate a variety of streetscapes that respond to the various functions and characteristics of the vehicular and pedestrian circulation systems, as well as to the development proposed within this Analysis Area. Town Center Core streetscapes incorporate and expand the hierarchy established by the overall Town Center streetscapes (MD-118, Middlebrook Road, Crystal Rock Drive, etc.). Town Center Boulevard presents the major transition into the Core. Main Street is designed as a highly pedestrian-oriented street. All streets have sidewalks, and prototypes have been developed for both curbside and development side conditions. All streets are proposed to have trees on both sides, varying in location, spacing, and species according to the streetscape. Many streets, including the Town Center Boulevard and Main Street incorporate varying amounts of parallel parking. Although secondary street streetscapes are recommended to have common characteristics, many variations occur (as presented in the STREETSCAPE DESIGN FRAMEWORK that follows).

The streetscapes of TC-1, TC-2, and TC-3 identify a number of special features that serve the pedestrian circulation and open space systems. Included are corner treatments, entrance landscaping, and special features. Areas identified for special treatment include all of the major corners of the Analysis Areas along MD-118. Particularly significant corners occur at Aircraft Drive (TC-2) and at the Crystal Rock Drive and Middlebrook Road intersections (on TC-1). The Town Center Boulevard entrance to the Town Center Core is of primary importance, as are both of the Main Street entrances. Special entrance features are proposed at both ends of Main Street. The TC-2 corner of Crystal Rock Drive and Century Boulevard is also recommended to incorporate open space and a special feature (sculpture, water element, etc.). (Although shown as a triangular area, this special corner could take any shape and the triangle is only illustrative.)

The major open space areas of the Town Center Core are the Town Center Commons and Town Center Park. As previously described, these are also recommended to incorporate special recreational amenities and landscape features. Although the location and size of the commons is defined by the proposed Main Street prototype (see STREETSCAPE DESIGN FRAMEWORK) the Town Center Park is defined only roughly by the surrounding roadways. It is recommended that this parkland not be divided by streets, if at all possible and it is desired to incorporate storm water management into only one or two ponds; however, phased development may necessitate the development of separate facilities for the east, central and west sides of the site. The location of the adjoining streets is dependent to a large extent upon the final delineations of the wetlands in this area.



STREETSCAPE FRAMEWORK

PEDESTRIAN CIRCULATION

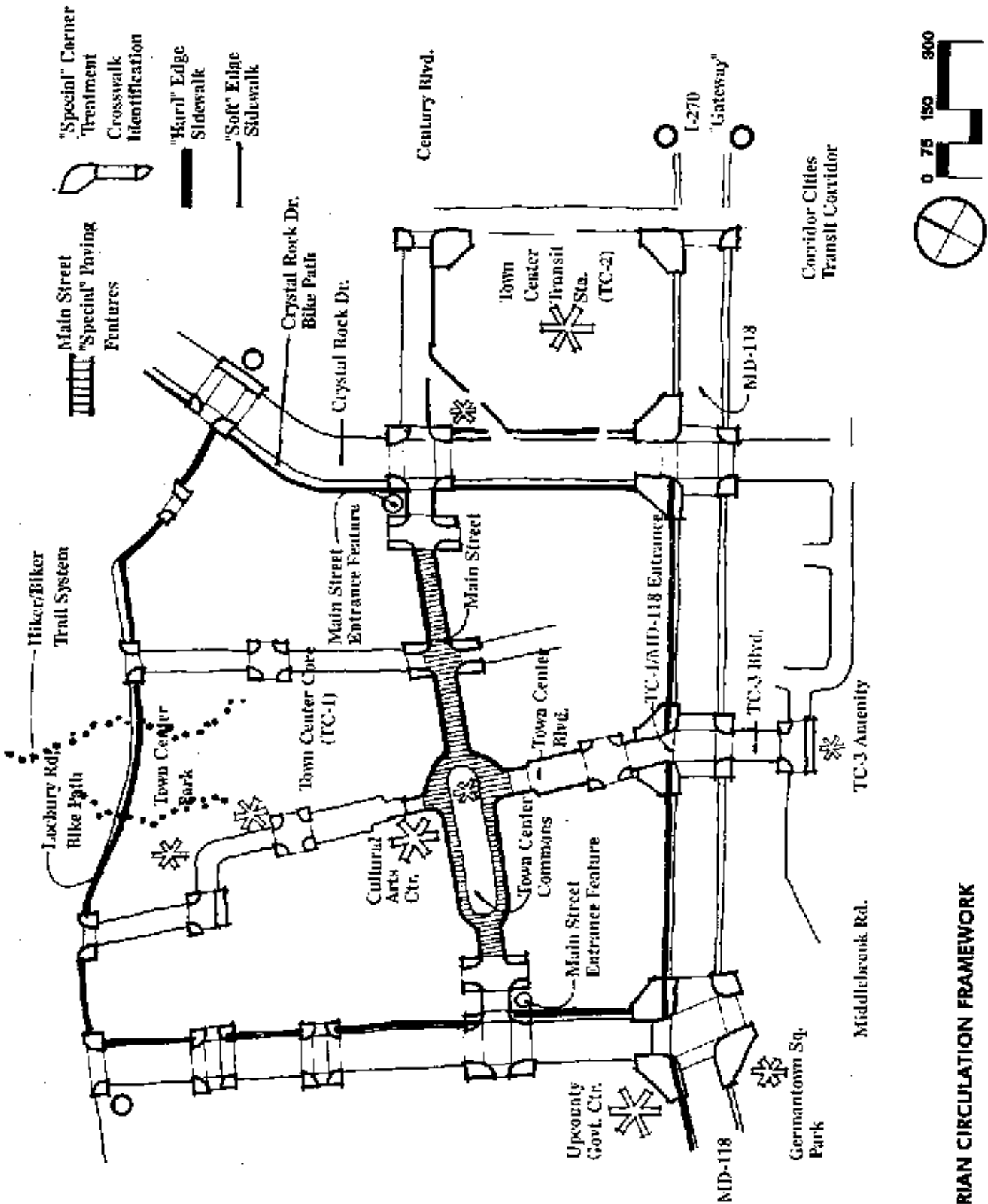
As with streetscapes, the Town Center-wide Pedestrian Circulation Framework is incorporated and expanded within the design framework of these Analysis Areas. This is particularly the case in the Town Center Core (TC-1). The Pedestrian Circulation Framework responds to differences in roadway design and function and to the variety of use and development conditions within the Analysis Area.

As previously described, sidewalks are primarily **hard edge** (to the curb) or **soft edge** (incorporating planting strips). Trees are planted in **hard edge** sidewalks either in openings in the sidewalk or in planters. Although not always the case, **hard edge** sidewalks usually also extend to the edge of the adjacent buildings. Paving at the curb accommodates access from cars parked on the street, and paving to the building facilitates multiple entrances. **Hard edge** sidewalks are usually wider and can accommodate greater numbers of people.

Town Center Boulevard and Main Street are recommended to incorporate **hard edges** (with the sidewalk to the face of the buildings). Many of the sidewalks in TC-1 are proposed as **soft edge**. These allow for planting (grass, ground cover, flowers, shrubs, trees, etc.) on one or both sides of the sidewalk. (Although **soft edge** sidewalks are less appropriate for street parking, this condition is preferred on the predominantly residential streets within TC-1.) Foundation planting is recommended where commercial office development is proposed and similarly where residential development is to occur. Where additional setback is provided this treatment is recommended to be expanded. Where parking lots and/or structures occur, the planting strip should also be used for the purpose of screening.

A special condition occurs on both the Middlebrook Road and Crystal Rock Drive blocks of TC-1, between MD-118 and the Main Street entrances. These blocks are recommended to incorporate both **hard** and **soft edge** sidewalks. The outer or street-side sidewalk has a **soft edge** and the inner or building edge sidewalk is recommended to extend to the face of the buildings. This is also recommended across Crystal Rock Drive on the TC-2 Station Gateway site. This condition incorporates the Town Center-wide streetscapes of these highways and also accommodates the proposed development in these areas.

The Pedestrian Circulation Framework also incorporates special entrance and corner treatments, as well as the special features discussed in the Streetscapes Design Framework. Most of the intersections within the TC-1 Analysis Area are recommended to receive special treatment to identify and accommodate pedestrian street crossings. Crosswalks, possibly of special paving, are indicated. Although most of the streets within TC-1 are private, and Main Street is a developer street, the intersections of the major highways of MD-118, Middlebrook Road, and Crystal Rock Drive are also recommended to incorporate specially designated crosswalks. These may not be allowed to use special pavers, etc. but should allow more than the minimal standard striping. Main Street is also recommended to contain areas of special paving and accent features.



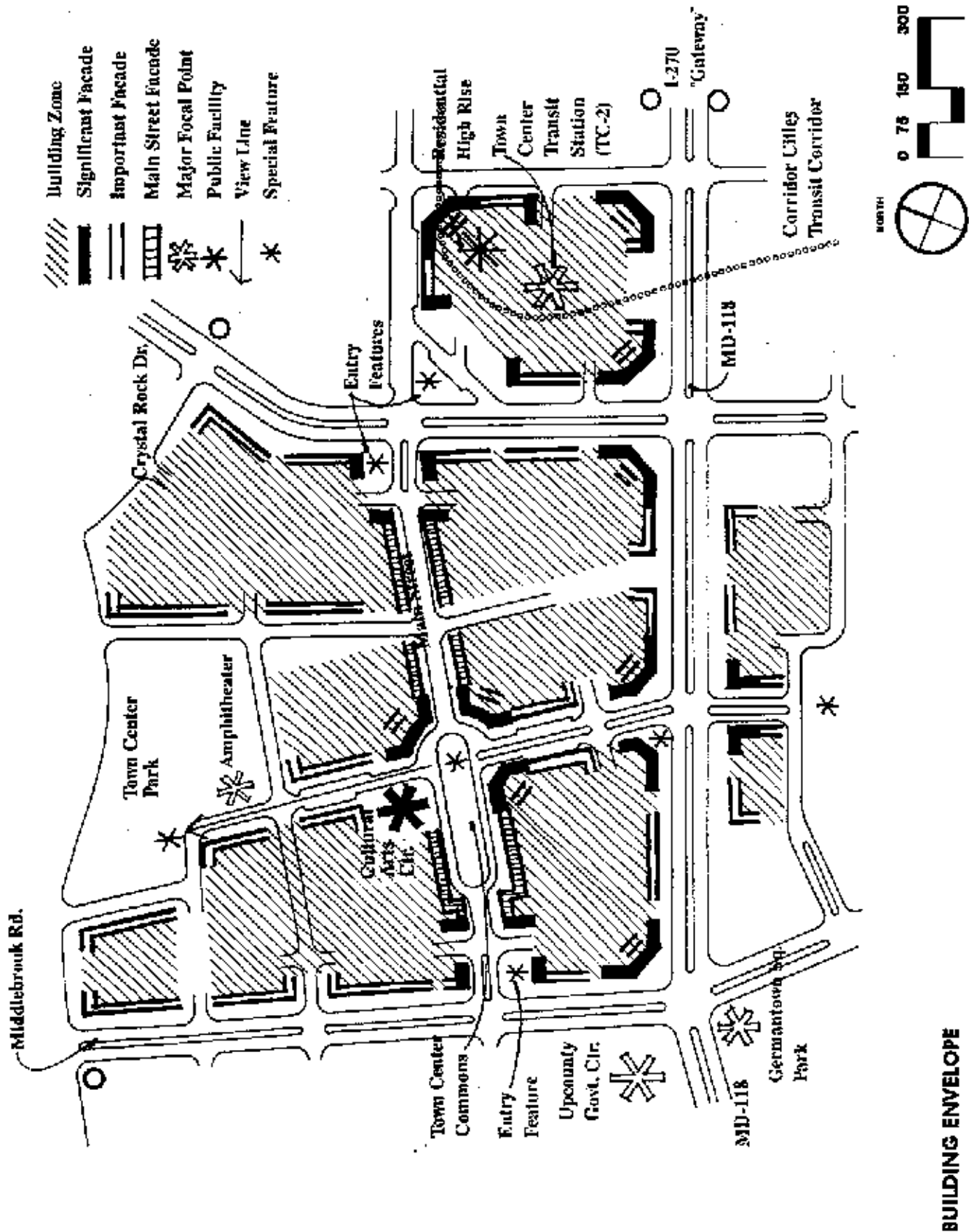
PEDESTRIAN CIRCULATION FRAMEWORK

BUILDING ENVELOPE

The Building Envelope Framework is intended to guide the siting of buildings within the Analysis Areas. This is especially important in TC-1 where a variety of use and building types are programmed for development. The location and massing of buildings, their relationship to open space, to the street, and to one another will, to a great extent, determine the character of the Town Center Core.

The Building Envelope Framework is also of major significance to the development of TC-2. The planned development of this site as a transit station, incorporating office and air rights residential development, will result in a substantial building mass and high-rise structures. Guidelines are essential for the integration of this gateway development into the overall Town Center.

The Town Center-wide framework orients buildings to the street with parking behind. The location of buildings (building zones) and their primary walls/edges is important. In addition, the designation of important facades is also significant. A **significant facade** is defined as a building wall/edge that is felt to be in an important location and one that deserves special design attention due to its high visibility. Buildings in these locations help establish the overall scale and definition of space. The design of these facades is critical to the definition of the character of the Town Center.



BUILDING ENVELOPE

TC-1 The most important buildings in the TC-1 Core are those that face onto the Town Center Commons. As discussed elsewhere (Town Center-wide Design Framework, Community Facilities), this is also the location of the Cultural Arts Center.

The Cultural Arts Center is proposed to be a stand-alone building of significant architectural merit and incorporating a clock tower or spire that will provide a viewing point from not only within the Town Center Core but also possibly throughout and beyond the Town Center. Shown on the corner of the extension of the Town Center Boulevard to the north, an alternate location for this facility is on the axis of this entrance boulevard. The building should be set back from both the boulevard and Main Street beyond the adjacent structures - so as to further break the row of building fronts and also to provide a plaza for pedestrian activity and the provision of landscaping and amenities appropriate to this facility.

The buildings that define the MD-118 entrance are also significant, as are the buildings located on the MD-118 corners of Crystal Rock Drive and Middlebrook Road. The facades that face these roads are all considered to be of high visibility.

The facades of the buildings lining both sides of the Main Street are significant in the establishment of the character of this street.

Generally, the blocks defined by the major streets of the circulation framework determine the majority of the building zones that occur within TC-1. The exceptions are the major open spaces of the Commons and Town Center Park. All of the major streets in the Core are recommended to be lined with buildings. In addition, the major streets of MD-118, Crystal Rock Drive, and Middlebrook Road are also proposed to be developed on both sides within these Analysis Areas. Likewise, buildings are recommended to line the streets facing Town Center Park.

Depending on use, a building's width will vary. Buildings proposed for development within the Town Center Core range from approximately 35 to 120 feet in width. This allows for residential units as well as commercial, retail, and office development. The area behind the building is typically identified as parking. Most of the development incorporates surface parking with the long-term build-out of the Master Plan program for TC-1 requiring structured parking. When constructed, garages are proposed to be treated as buildings that face onto these internal streets.

TC-2 The entire TC-2 Master Plan site constitutes a building zone (the station parking garage will fill the majority of the site). As in TC-1, buildings are recommended to be located on all of the perimeter streets. The most significant building facades are also those at the corners. The building located at the corner of MD- 118 and Aircraft Drive is of special significance because it is the first building to be seen when entering the Town Center from the I-270 interchange.

Of equal and possibly greater significance, because of its height, is the proposed air rights residential tower in the rear portion of

TC-2 (on the corner of Century Boulevard and Aircraft Drive). This building will constitute a major focal point not only for the TC-2 gateway development, but for all of Town Center.

The alignment of the Corridor Cities Transitway through TC-2 will impact the development of this site. The alignment as currently shown in the Master Plan, if sufficiently depressed, will allow development above the station. This alignment does, however, have significant cost implications for future **air rights** development. The alignment is currently being studied in relation to mode of operation, station design, and engineering feasibility. Although the current Master Plan alignment is incorporated in this study, a straightening of the line and its location in the middle of the site are preferred so as to enhance the feasibility of the future development of TC-2.

TC-3 Development within this Analysis Area is recommended to be located on MD-118. The two buildings that frame the MD-118 entrance (opposite the entrance to TC-1), are identified as the most significant buildings.

BUILDING HEIGHTS

Building Heights, in combination with the Building Envelope Framework helps to define the desired overall scale and massing of the Town Center.

TC-1 Buildings in TC-1 are recommended to range from two to seven stories in height. The highest and most significant buildings are located on the east side of TownCenter Commons at the intersection of Town Center Boulevard and the Main Street and on the corners of MD-118 and Crystal Rock Drive, and MD-118 and Middlebrook Road, at the MD-118 Town Center Core entrance. The hotel proposed to be located on the east side of the MD-118/Town Center Boulevard entrance is recommended to be up to seven stories in height. And the building on the MD-118 and Middlebrook Road, across from the Upcounty Government Center, is recommended to be from four to seven stories high.

The minimum height is considered as important as the maximum height of these buildings. Height and mass help establish the definition of space. In terms of building bulk, the establishment of **critical mass** in key locations is essential to the success of the Town Center Core.

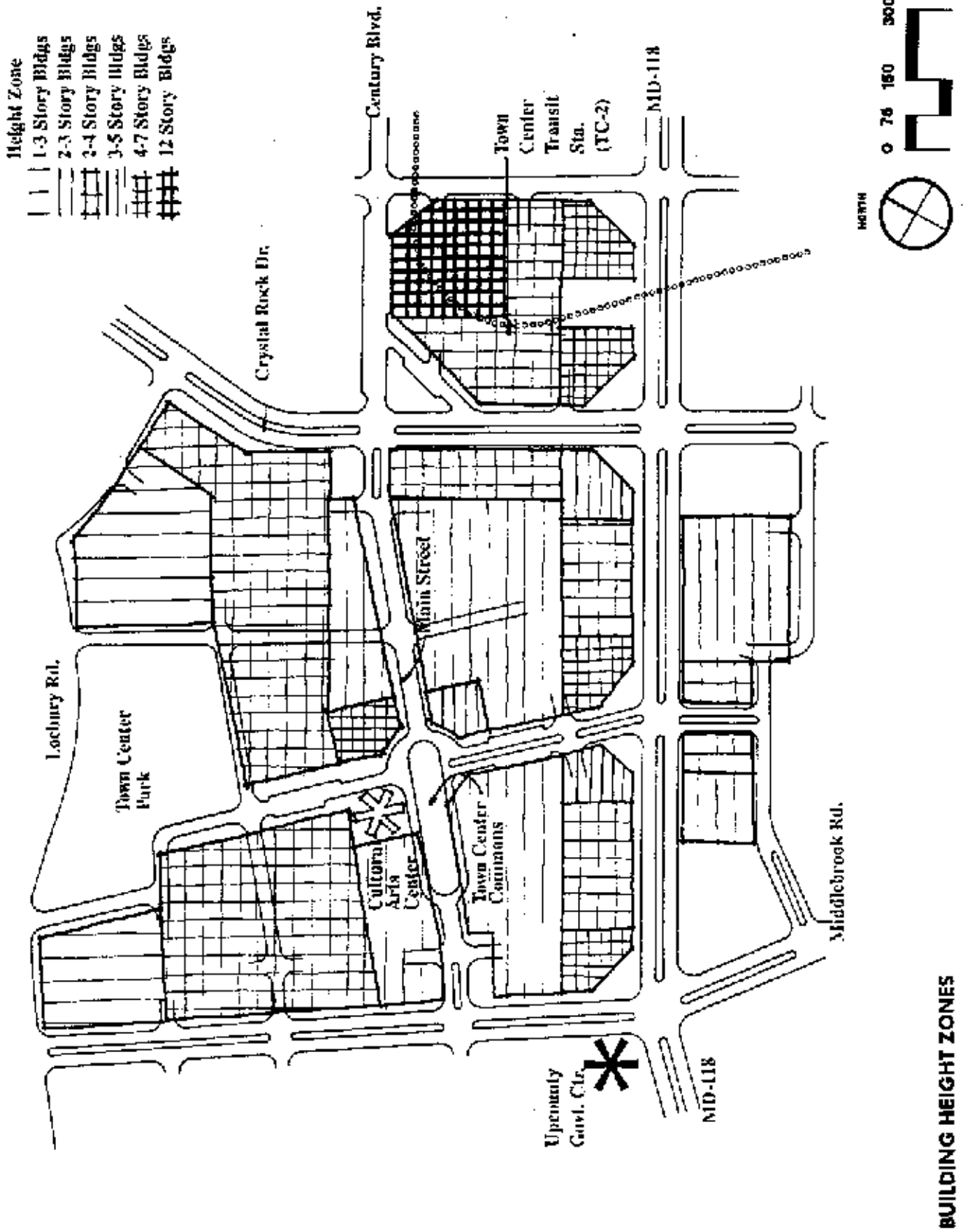
With the exception of the buildings on the east end of Town Center Commons, the height of the buildings on Main Street are recommended to be two to three stories. The intent is to maintain a pedestrian-oriented scale appropriate to this **shopping** street.

The proposed Cultural Arts Center is recommended to be two and one half to three stories in height and incorporate a tower or spire appropriate to its design and designation as a landmark building. This facility should be greater in height than the immediately adjacent Main Street structures, but its tower need not be higher than the buildings at the end of the Commons. The freestanding nature of the Cultural Arts Center, together with the design of this facility, will provide its identity.

In general, buildings of from two to four stories in height are recommended on the perimeter streets of TC-1 (MD-118, Crystal Rock Drive, and Middlebrook Road), and on internal streets (including the possible long-range construction of parking structures). Development in the rear portion of TC-1 adjacent to the housing across Locbury Drive, is indicated to be from one to three stories in height.

Although recommended height zones allow for an overall development of equal height, the intent is to promote a variety in height in the overall context of the relative relationship of one building to another and to the maximum building heights proposed. Buildings in important locations and/or of greater significance are recommended to be higher.

TC-2 The buildings on the TC-2 **gateway** site, at the MD-118 corners are recommended to be from four to seven stories. As discussed, the accommodation of the planned **air rights** development on TC-2 is proposed to incorporate high-rise development. A height of up to 12 stories is required to satisfy the residential program. Residential



development of up to 12 stories in height is identified on TC-5 as well. If constructed, these two towers would be highly visible and their locations would create a dynamic relationship within the Town Center.

PHASING

TC-1 Ideally, all of the Town Center Core would be developed and constructed at one time. Although this is a possibility, the timing of the development of TC-1 is dependent upon a number of factors ranging from plan approvals to the real estate market.

In addition, TC-1 has more than one owner. Although coordinated planning of this area is possible, its simultaneous development may not occur. The design framework for this Analysis Area acknowledges the multiple ownership and the possibility of phased development.

The phasing of the development of the Town Center Core is based on the component of the Master Plan program that can be accommodated without having to build structured parking. Although it is possible to add stories to existing buildings, it is more likely that the initial buildings will be constructed to remain, and later development will occur in new infill construction.

The buildings most critical to establishing the overall definition of the Core are those facing the Town Center Commons and those lining Main Street. Equally important are the buildings that define the corners of this area (MD-118 and Crystal Rock Drive, and MD-118 and Middlebrook Road). Other recommended areas of initial development include the TC-1 perimeter (on both Crystal Rock Drive and Middlebrook Road) and the buildings that face onto Town Center Park.

Ideally, all of the buildings on MD-118 would be constructed in the initial development of TC-1. It is also recommended that the two buildings that flank the Town Center Boulevard entrance be constructed early-on. The building on the eastern side of this entrance has been identified as the possible site of a major hotel. Proposed as a hotel, although desirable, may not be feasible in the initial phase of the Town Center Core. If not initially developed, this site is recommended to be substantially landscaped and incorporate an appropriate entrance feature. In order to establish the identity of this entrance at least one of the buildings is to be constructed in the initial phase of the development of the Town Center.

TC-2 Identified as the future location of the Corridor Cities Transit Corridor Town Center Station Easement, TC-2 will not be fully developed for several years. Because of its **gateway** location, construction of the office component of the TC-2 program is encouraged concurrent with the initial development of TC-1. The development of the buildings on MD-118 at both the Crystal Rock Drive and Aircraft Drive intersections are important to the establishment of the overall identity and definition of the Town Center. (The proposed location of these buildings allows for the later development of the transit line and station development).

TC-3 If phased, the initial development of TC-3 is proposed to include the two buildings that define the parcel's MD-118 entrance.

PARKING

The design framework that has been established for the TC-1, TC- 2, and TC-3 Analysis Areas, includes the recommended location of parking zones, on-street parking, and the locations of proposed parking structures, as they may be desired or required in the long-term build-out of these Analysis Areas.

On-street parking is felt to be desirable not only to assist in the provision of required parking spaces, but also to bring activity to the street. Windshield shopping is desirable.

With the initial development of the TC-1, TC-2, and TC-3 areas, the provision of additional parking for the shared use by nearby community facilities (Cultural Arts, Town Center Park, etc.) and for Park-and-Ride and bus patrons may be needed. The development of excess surface parking on the TC-2 site, prior to the development of the transit station, could assist in the provision of these needs. On the other hand, the provision of landscaped open space as interim use on future development sites is encouraged.

Although limited by financial feasibility, the maximum structuring of parking is recommended as early as possible in the development of the Town Center, in particular in the Town Center Core (TC-1). Underground parking is preferred over parking garages and the innovative incorporation of lease space at the street and roof top amenity is encouraged.

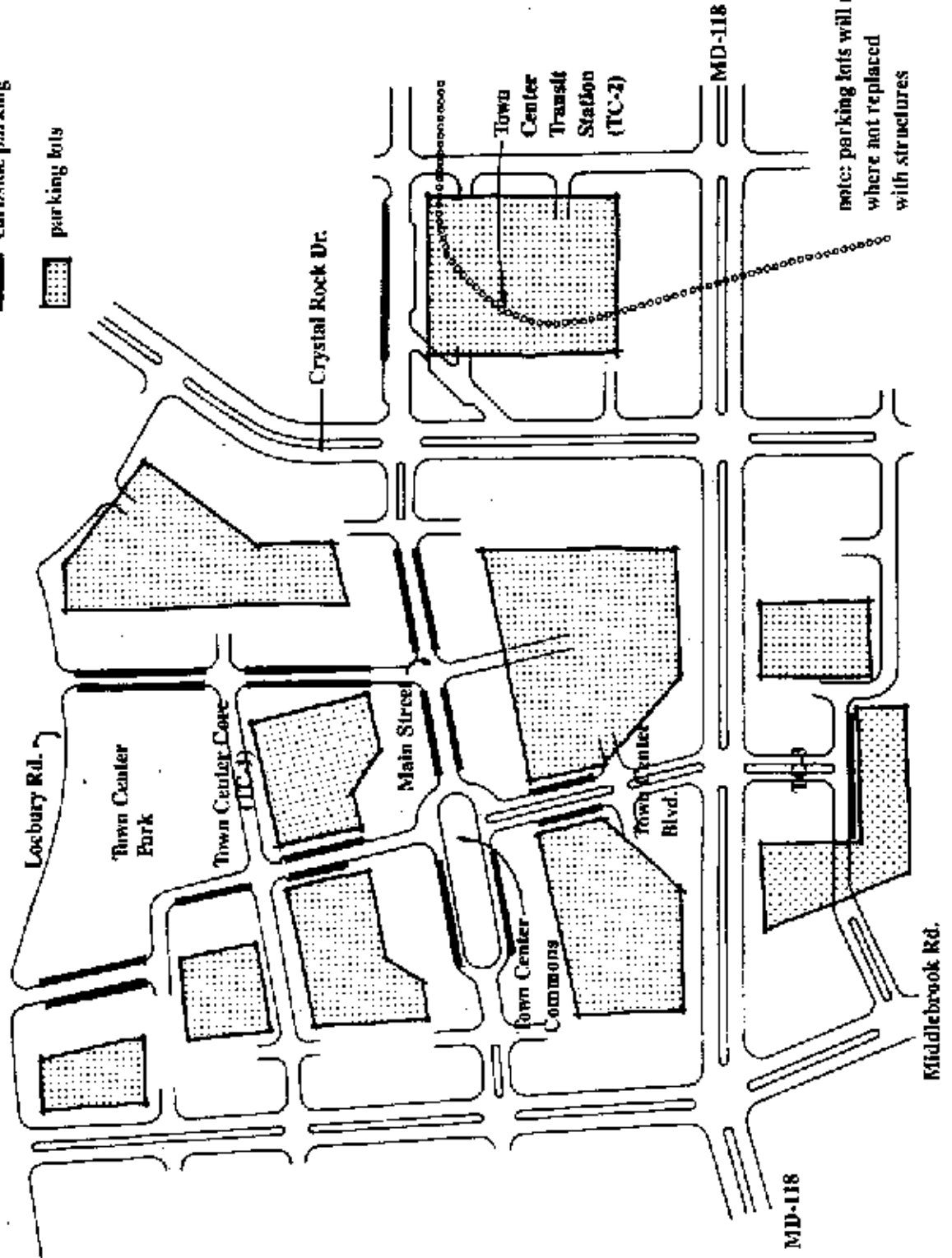
As previously described, buildings are recommended to be oriented to the major streets with parking located in courtyards behind. In the initial development phase (that assumed not to incorporate structured parking), a limited amount of parking may also be located adjacent to some of these streets. Later displaced for infill development, this, and the additionally required, parking is proposed to be accommodated in garages. These structures are to have a relationship to the internal streets that is consistent with that of other development.

PARKING FRAMEWORK

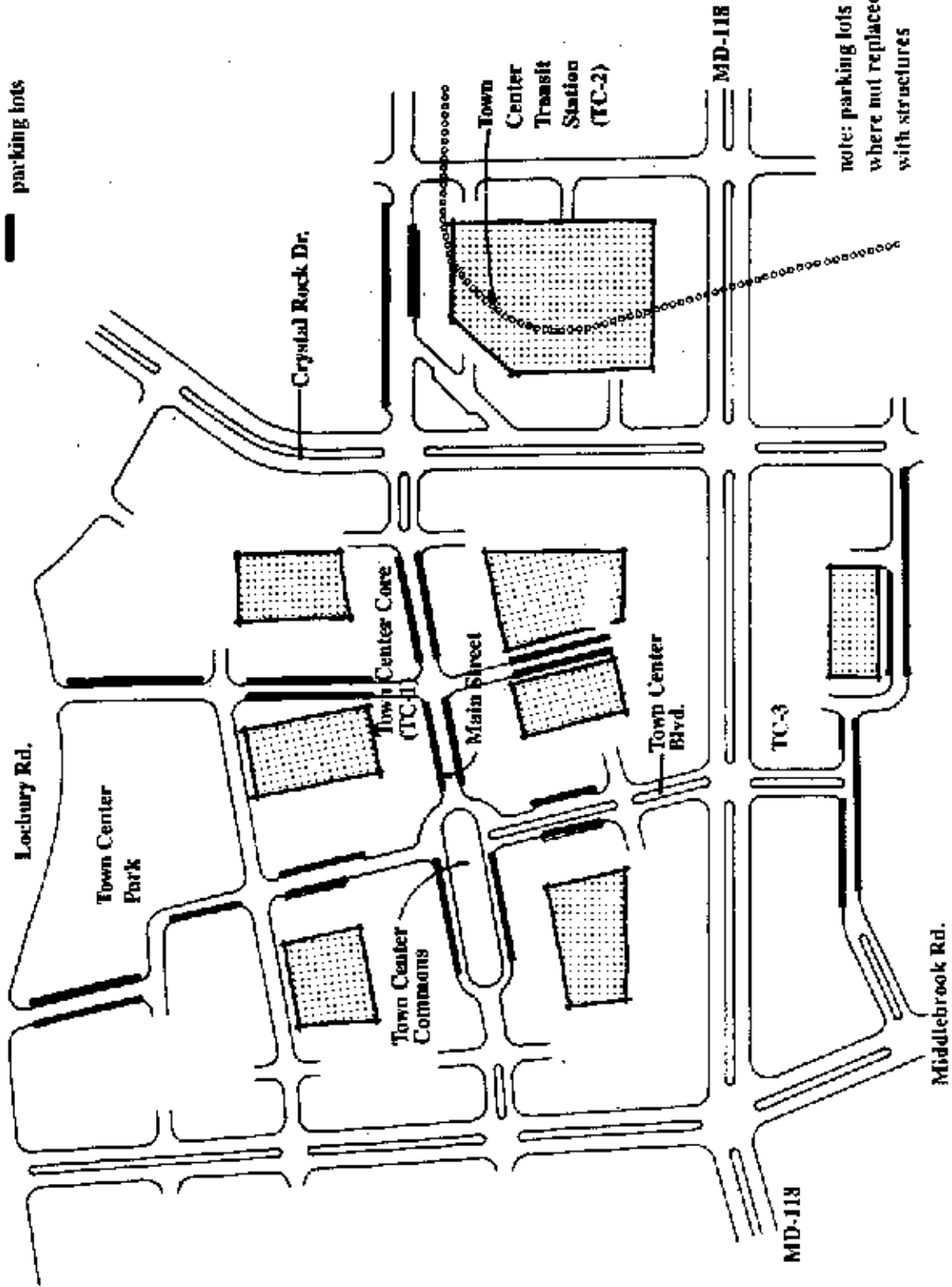


carbside parking

parking lots



curbside parking
parking lots



PARKING FRAMEWORK (LONG-TERM BUILD-OUT)

ANALYSIS AREAS TC-5 AND TC-6

VEHICULAR CIRCULATION

Access to the TC-5 Analysis Area is limited to entrances at the Waters Road intersection with Wisteria Drive and at the major highway entrances from MD-118 and Father Hurley Boulevard.

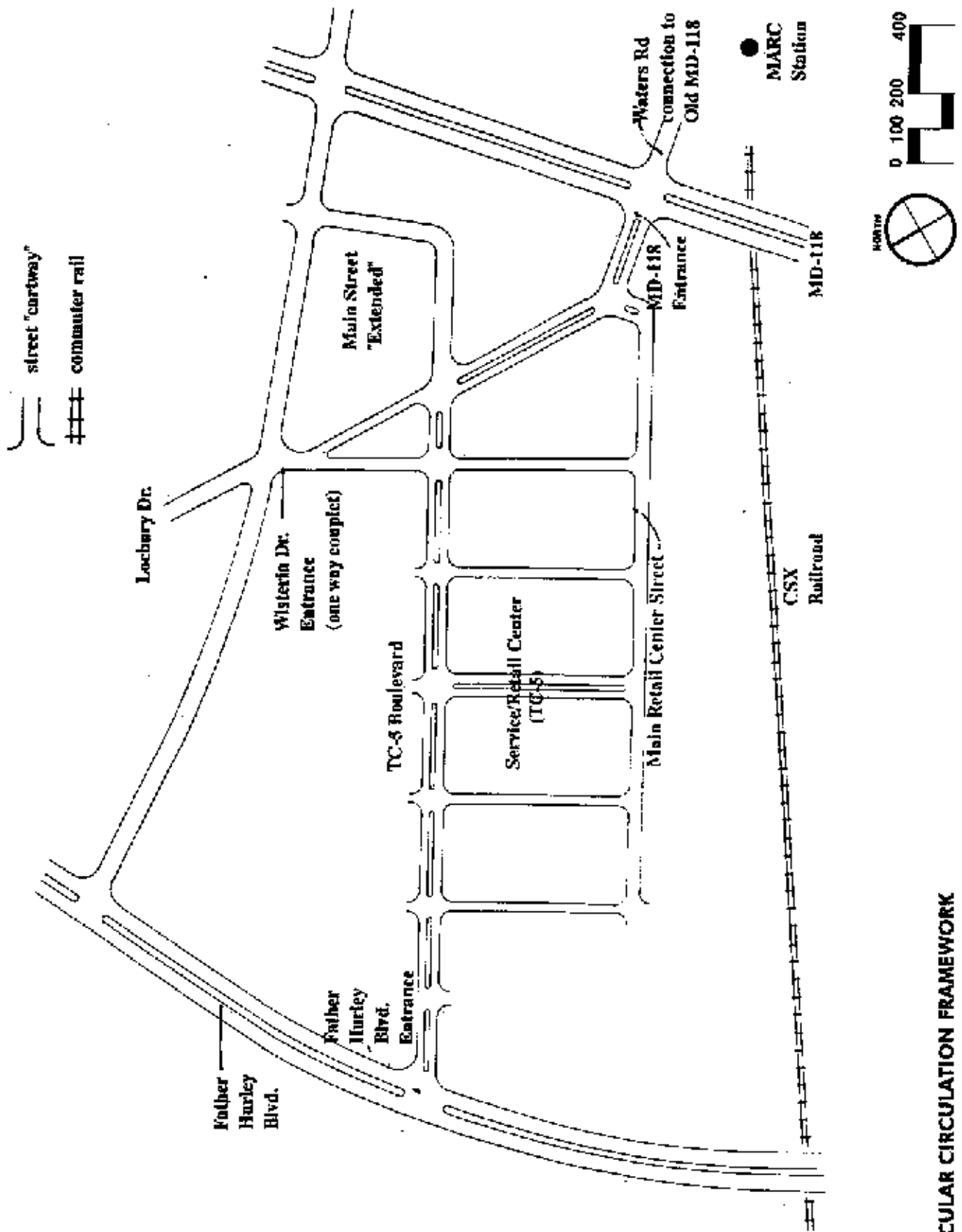
The proposed MD-118 entrance to TC-5 is located approximately 200 feet north of the CSX Railroad tracks. Although not shown in the Master Plan, this intersection has been determined to be feasible by MCDOT. Waters Road is also recommended to connect MD-118 with Old MD-118 at a point that is approximately opposite the existing MARC Commuter Rail Station park-and-ride lot and to serve as an entrance to both the existing and the proposed MARC station parking facilities. The location of the Father Hurley entrance is somewhat flexible.

The Wisteria Drive entrance to TC-5 is recommended as a one-way couplet. The exit lane would utilize the existing right-of-way of Waters Road and a new entrance road will be constructed. Through-movement of traffic on Waters Road to its intersection with Wisteria Drive is also accommodated. The island created between these lanes is recommended to accommodate an entrance feature.

The four-lane divided boulevard (TC-5 Boulevard) that connects all three of these major entrances constitutes the major internal roadway and is the backbone of the vehicular circulation framework within this Analysis Area. An undivided extension of the boulevard provides access to the parcels to the southeast of Waters Road and across Wisteria Drive to the existing shopping centers. The TC-5 Boulevard is proposed to be intersected at approximately 300-foot intervals by streets that distribute traffic to the service and retail center.

The TC-5 street configuration is primarily a grid. Streets intersect the Boulevard at right angles and the main retail street runs parallel to this boulevard. As in TC-1, this grid is adapted to respect existing streets, ownership patterns, and natural features. The major exception to the grid is Waters Road.

Access to TC-6 is provided off MD-118 by both the proposed Waters Road connection and by Wisteria Drive. Old MD-118 serves the center of this Analysis Area. When the existing bridge over the CSX Railroad tracks is closed to vehicular traffic, the Waters Road connection will provide the only outlet to the southwest. This also provides a direct and essential link to the MARC Commuter Rail Station from MD-118. (Although currently incorporated into the development of an additional MARC parking lot, this road is recommended to be maintained to provide access to both existing and proposed parking lots and the station.)



VEHICULAR CIRCULATION FRAMEWORK

STREETSCAPE

The Town Center-wide streetscapes of the major highways of MD-118 and Father Hurley Boulevard, the Wisteria Drive streetscape, and the special streetscapes of both Lockbury Drive and Waters Road and Old MD-118/Walter Johnson Drive establish much of the character of these Analysis Areas.

The major streetscape internal to TC-5 is that of the TC-5 Boulevard. The central entrance street within the retail center is also recommended to incorporate a median, although this is more symbolic than functional. Characteristics of the streetscapes of the secondary streets vary according their edge conditions and development (these are described in the Streetscape Design Framework section).

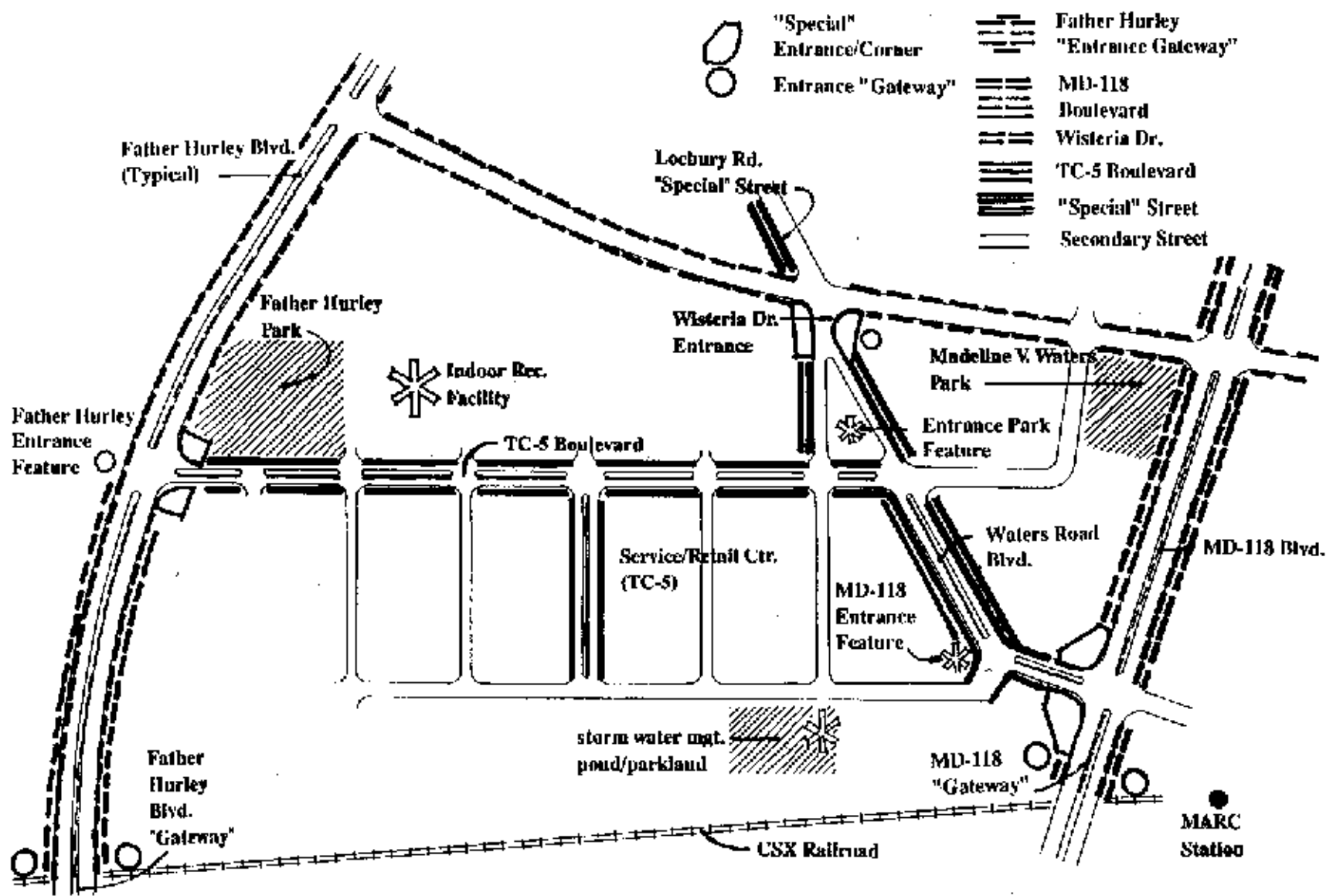
The streetscape framework proposed for TC-5 provides opportunities for the incorporation of open space, special landscape treatment; and special features at key entrances, intersections, and focal points.

The MD-118 entrance to TC-5, in close proximity to this southern gateway to the Town Center, affords the opportunity to incorporate the natural woodlands and other characteristics of this area. Inside TC-5, the turning of Waters Road at its intersection with the main retail center access road, presents a highly visible site for the recommended location of signage and other entry features specific to the shopping center.

The Father Hurley Boulevard entrance is recommended to incorporate the natural open space system that exists along both sides of this highway as well as the proposed development of Father Hurley Park. This is the only major Analysis Area entrance in the Town Center with a T intersection. Within the right-of-way, the area across the road from the entrance boulevard is proposed to be incorporated into the design of this entry. This area presents a major potential focal point from Father Hurley Boulevard in both directions as well as from the the TC-5 Boulevard.

As previously described, the Wisteria Drive entrance to TC-5 is proposed to incorporate a one-way couplet and the entrance and exit roads create an island that is recommended to be developed as a special entry feature, similar to that of the MD-118 entrance. In addition, the Wisteria Drive entrance is also recommended to incorporate additional landscaped open spaces on all four sides of this intersection. Lockbury Drive and Waters Road are proposed to be developed as pedestrian-oriented streetscapes to include a hiker/biker trail and open space areas.

The focal point at the end of the Wisteria Drive entrance road is recommended to be developed as a special feature in conjunction with the proposed enhancement of the existing pond for storm water management and parkland. Several other focal opportunities are identified for development within TC-5. The proposed indoor recreation facility might occupy one of these locations. There is also a focal point at the extension of the boulevard, across Waters Road, at its turn toward Wisteria Drive. Conversely, this corner also presents a focal point opportunity when crossing Wisteria Drive from the existing shopping centers. (This is the extension of the TC-1 Main Street into TC-5.)



STREETScape FRAMEWORK

The site of the Madeline V. Waters House is adjacent to this entrance. Located between this proposed entrance road and MD-118 (relocated), this historic site is recommended to be developed as parkland. If not, the future development of this site is recommended to retain the existing trees and to incorporate an open space that includes the trees and the site of the House. Development of this historic resource will require approval of the Historic Preservation Commission (HPC).










PEDESTRIAN CIRCULATION

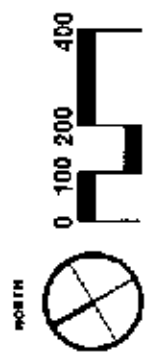
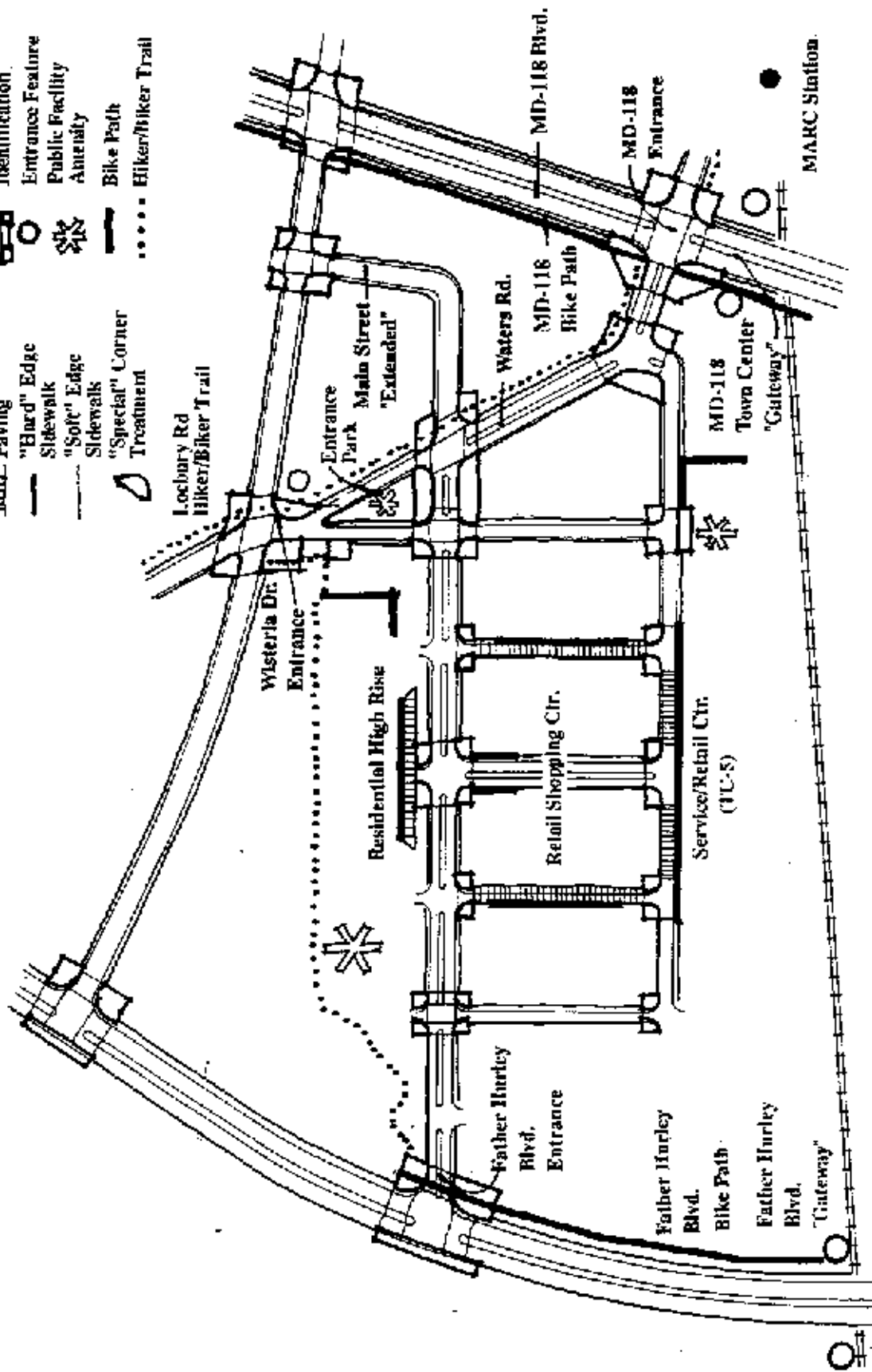
The majority of the sidewalks within the TC-5 and TC-6 Analysis Area are recommended to have soft edges. The width of the planting strip, ground cover, and tree locations and species vary according to the type and function of the street, edge conditions, etc. (see the Streetscape Design Framework prototypes). Hard edge sidewalks are recommended to occur in front of the buildings within the retail center. These sidewalks will facilitate drop-off and loading and allow pedestrian entry to the adjoining storefronts.

TC-5 and TC-6 contain several hiker/biker trails and major components of the Town Center bikeway system. The MD-118 bike path is located on the northwest side of the highway (from the CSX Railroad tracks to Wisteria Drive). Father Hurley Boulevard also includes a bike path on the south and east side of the highway. This facility is proposed to be located in the adjacent open space system (TC-5).

The Locbury Drive and Waters Road special streetscape is proposed to include a hiker/biker trail. This trail replaces the sidewalk on the south and east sides of both Locbury Drive and the Waters Road boulevard. Across MD-118 it is proposed to be located on the southwest side of the Waters Road connector street, within the existing woodlands. At Old MD-118 the trail continues over the proposed pedestrian-only bridge to the Historic District. This trail also connects to the MARC Commuter Rail Station and the trails associated with the pond and the adjacent natural open space system. A hiker/biker trail is also proposed to connect to the indoor recreational facility, Father Hurley Park, and the related natural open space system in this Analysis Area. This trail might be located within the open space buffer between TC-5 and the existing development on Wisteria Drive, or it could utilize the north sidewalk of the TC-5 boulevard.

Special corner treatment is proposed to be developed at all of the intersections within the TC-5 and TC-6 Analysis Areas. The use of special paving is recommended at the pedestrian crossings of all intersections; including all of the crossings of Wisteria Drive, the TC-5 boulevards, and the Waters Road intersection with Old MD-118. Special paving will be incorporated in the streets that are adjacent to the fronts of the buildings in the TC-5 retail center and the crossings that occur at entrances to these buildings will be emphasized. Special features are recommended for incorporation into the design framework of the pedestrian circulation system at entrance gateways, within parkland, and at focal point locations.

-  Special Street: Paving
-  "Hard" Edge Sidewalk
-  "Soft" Edge Sidewalk
-  "Spectral" Corner Treatment
-  Crosswalk Identification
-  Entrance Feature
-  Public Facility Amenity
-  Bike Path
-  Hiker/Biker Trail



PEDESTRIAN CIRCULATION FRAMEWORK

BUILDING ENVELOPE

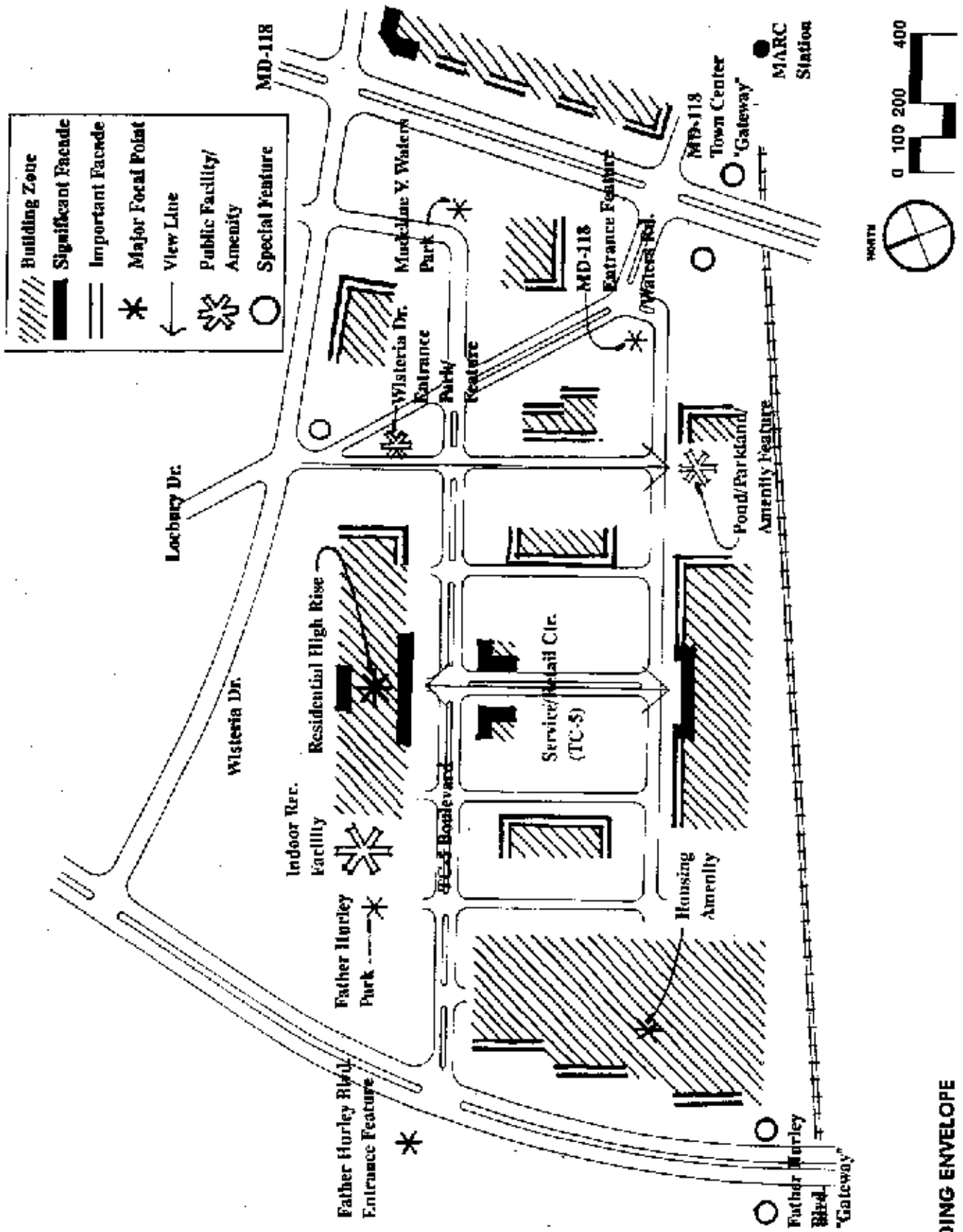
TC-5 The Building Envelope Design Framework for the TC-5 Analysis Area reflects the development program and the use zones that have been planned for this area. The northern part of the site that is adjacent to Father Hurley Boulevard is recommended to incorporate medium-density residential development. A major retail center is proposed to be located in the middle of the site. A high-rise residential building is recommended within this zone. The area on both sides of Waters Road is identified for the multiple-site development of automobile sales and related service uses.

The most highly structured of these areas is that of the retail shopping center. The framework for this center incorporates a traditional parking-to-storefront relationship. As previously described, the streets have a grid pattern. Buildings fronting on these streets either face each other or are at right angles to one another. The major anchor stores are recommended to be located at the end of this quadrangle. Buildings on the sides help provide spatial definition to the center. Two smaller buildings are located on either side of the central entrance boulevard and help to define the space. All of the building walls that face this central area are considered important. The major building at the end of the entrance boulevard is identified as potentially having the most important facade. This building will act as a focal point and strongly influences the architectural character of the center.

The high-rise building is located on axis at the other end of this boulevard. This residential tower is proposed as up to 12 stories in height and to incorporate structured parking. It will be the major focal point, not only within this Analysis Area but within this area of the Town Center. As previously described, this tower, in combination with the similar tower proposed to be developed on TC-2, has the potential to establish a broader identity for Town Center than that of the Core alone. This structure is at a considerable distance from Father Hurley Boulevard and both existing and proposed residential development. All of the commercial buildings within TC-5 are recommended to be one or two stories in height. No structured parking is anticipated.

The medium-density residential development is recommended to be located in its own complex. Due to the slope in this area, only the initial row of these buildings will be visible from inside TC-5. The buildings on the opposite side will, however, be in full view from Father Hurley Boulevard. These facades are important not only in the identification of TC-5 but also in that they constitute the initial Town Center development encountered from the Father Hurley Boulevard Town Center entrance. The other buildings within TC-5 are independent of one another; nevertheless, their siting, location, orientation, and architecture are all important in the establishment of the overall character of this area. The building edges that face MD- 118, Wisteria Drive, and Waters Road are identified as the most important.

Beyond the focal points of the residential high-rise and the anchor building of the retail center, the TC-5 design framework incorporates several other potential view lines and focal point opportunities. With the exception of the indoor recreation facility, these are primarily



BUILDING ENVELOPE

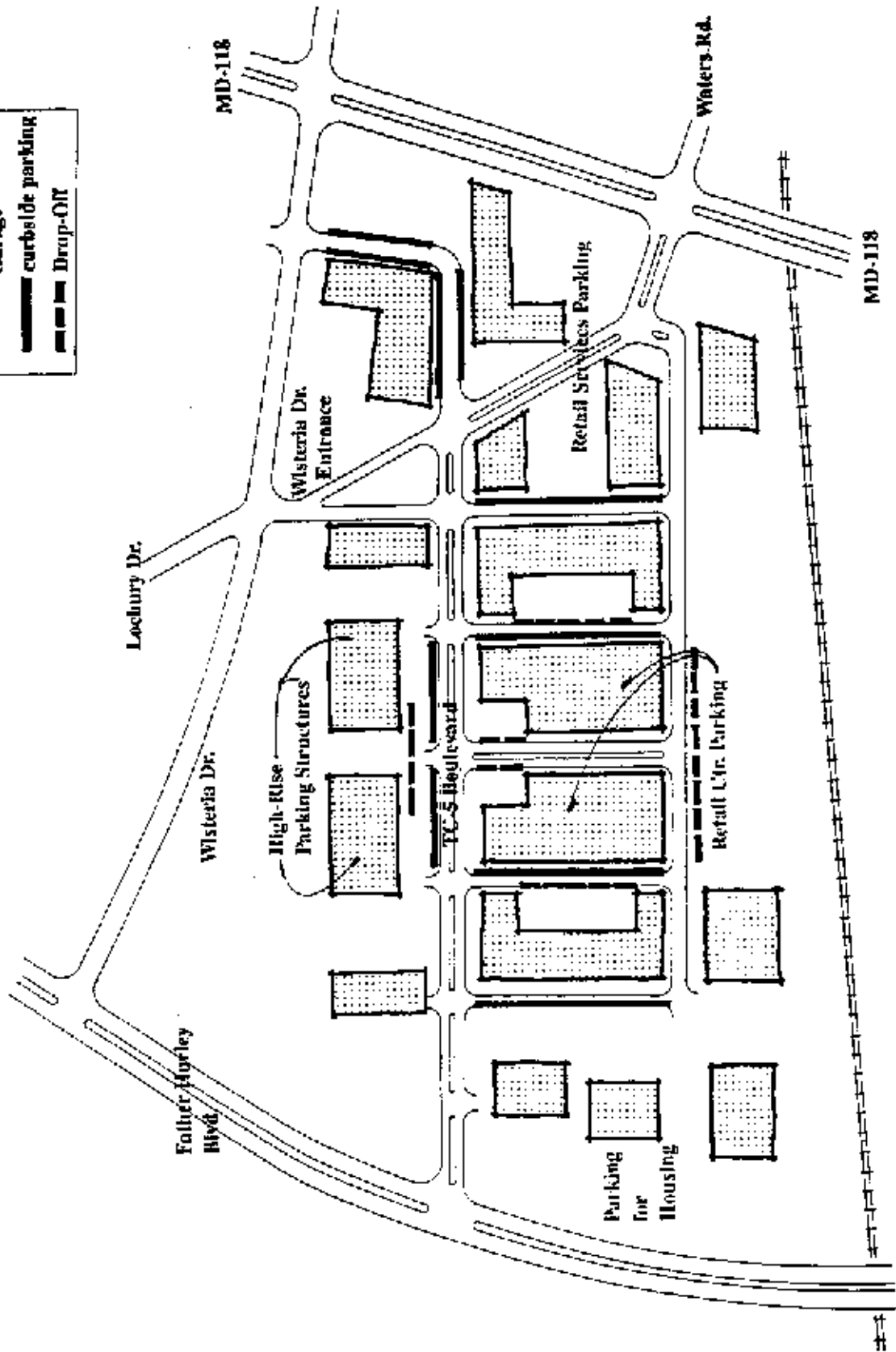
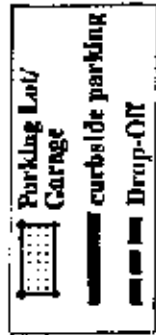
site and special feature opportunities. Collectively, the implementation of these proposed framework elements will lead to the establishment of the desired overall character of TC-5, one that will contribute to that of the Town Center as a whole.

TC-6 The character of the TC-6 Analysis Area is considerably different than that of TC-5 or any of the other Analysis Areas. Whereas exclusively new development is proposed for TC-5, that of TC-6 is infill in an historic context.

All of the buildings to be developed within the TC-6 area are proposed to be small in area and limited to three stories in height. Because they constitute infill development, and are within and in close proximity to the Germantown Historic District, the design of their facades is important. The location of the buildings along MD-118 is of particular importance in that this is the development that is initially encountered from within the MD-118 Town Center gateway from the southwest.

PHASING

Although the development program proposed for Analysis Areas TC-5 and TC-6 can be fully developed without structured parking (with the exception of the residential high-rise) there is no assurance as to the timing or phasing of this development. Although the retail center can be assumed to be developed at one time, residential development may occur in a later phase and that of the high-rise may occur even later. Development of those properties to the southeast side of Waters Road that are in multiple ownership, as well as the infill development of TC-6, is likely to occur over time. The design framework developed for these Analysis Areas, as that for the overall Town Center, is structured to accommodate phased implementation.



PARKING FRAMEWORK

STREETSCAPE DESIGN FRAMEWORK

INTRODUCTION

The hierarchy of the current vehicular circulation system within the Town Center provides the opportunity to create a logical and identifiable series of streetscapes. As has been discussed, these streetscapes were identified in the internal development of the Town Center Analysis Areas. The Streetscape Framework has been designed not only to enhance the experience of moving to and through the Town Center by automobile, bicycle, and on foot, but to reinforce orientation, identity and a sense of place.

The major highways of MD-118 and Father Hurley Boulevard constitute the primary roads within the Town Center. MD-118 serves as the primary entrance to the Town Center. The streetscape of MD-118 emphasizes its significance with a formal arrangement of street trees, flanking the sidewalks on both sides of the roadway, and with a single row in the median. The Town Center **gateways** at I-270 and the CSX Railroad are emphasized with double rows of ornamental trees.

Father Hurley Boulevard traverses and links residential areas and, as such, has a different character than MD-118. The Father Hurley Boulevard streetscape is recommended as less formal and more natural in appearance. Setbacks to buildings are also greater, with the intent of enhancing the open space between the highway and adjacent development.

Second in the overall Town Center streetscape hierarchy are the major highways of Middlebrook Road, Crystal Rock Drive, and the major arterial of Wisteria Drive. These roadways provide access to the Town Center, connect the major through roads of MD-118 and Father Hurley Boulevard and provide access to development within the Town Center Analysis Areas of TC-1, TC-2, TC-5, and the existing shopping centers and related development.

The character of these streetscapes, although formal, is recommended to be less monumental than those of the major through roadways. The existing sidewalks will remain five feet from the curb, with single rows of street trees planted beyond. Single rows of trees will also be planted in the medians of Middlebrook Road and Crystal Rock Drive.

Third in the hierarchy are the secondary roads of Lockbury Drive and Waters Road, Old MD-118, and Walter Johnson Drive. These streets carry local vehicular and pedestrian traffic between zones of the Town Center. They also serve to connect the major open space systems. Their streetscapes are designed to enhance pedestrian and bicycle movement. Lockbury Drive is proposed to incorporate a park-like character with lawn areas and informal plantings extending beyond the sidewalk and the hiker/biker trails.

Old MD-118 will incorporate street trees, lighting, and other street furniture that extend the historic character of the recently reconstructed MARC Commuter Rail Station and the Historic District.

Fourth in the streetscape hierarchy are those streets internal to the Analysis Areas of the Town Center. Within TC-1 the major street of Town Center Boulevard is the MD-118 entrance to the Town Center Core. This street is anticipated to carry the highest volume of traffic to TC-1 and is recommended to have a wide section. As the main entrance it has a more monumental streetscape than the other streets. The boulevard will have a 28- to 30-foot median, planted with street trees, four travel lanes, and two parking lanes. The sidewalk will be primarily **hard edge**, to accommodate pedestrian use and to reinforce the commercial character. Street trees will be planted in cut-outs, tree grates, or planters within the pavement.

Main Street is another major street in the TC-1 hierarchy. Connecting Middlebrook Road and Crystal Rock Drive through the Core, Main Street is the downtown shopping street of the Town Center. To emphasize pedestrian usage, this street will have fewer travel lanes and a **hard edge**. To provide a more intimate cross-section buildings are to be closer to the curb than on the Boulevard. Where Main Street follows the perimeter of the Town Center Commons, the streetscape will be the same on both sides in order to provide the desired continuity. The exception is proposed in relation to the Cultural Arts Center where additional setbacks, sidewalk, and landscaping is recommended. Street trees will be planted in cut-outs, tree grates, or planters within the sidewalk.

Next in the hierarchy of TC-1 streetscapes are those associated with the secondary streets that provide access to the Core. These streets incorporate a **soft edge**, consisting of grassed planting strips between the curb and the sidewalk and street trees.

Last in the TC-1 streetscape hierarchy are those streets which provide access to and through parking areas. Street edges that abut commercial or office buildings will have a **hard edge**, with cut-outs and planting beds incorporated. Those adjacent to residences will have a **soft edge** consisting of grass strip or ground cover between the curb and the sidewalk planted with street trees. A planting area is to be provided between the sidewalk and the building as well.

The major streetscapes in the hierarchy internal to TC-5 are associated with those streets that provide access from MD-118, Father Hurley Boulevard, and Wisteria Drive. These streets are four lanes in width and contain a median. Trees will be planted both in the median and on both sides (between the curb and the sidewalk).

Next in the hierarchy is the axial boulevard leading from the high-rise residential tower to the middle of the retail shopping center. This boulevard has fewer lanes than the major boulevards and will have smaller ornamental trees in the median to signal arrival.



STREETSCAPES

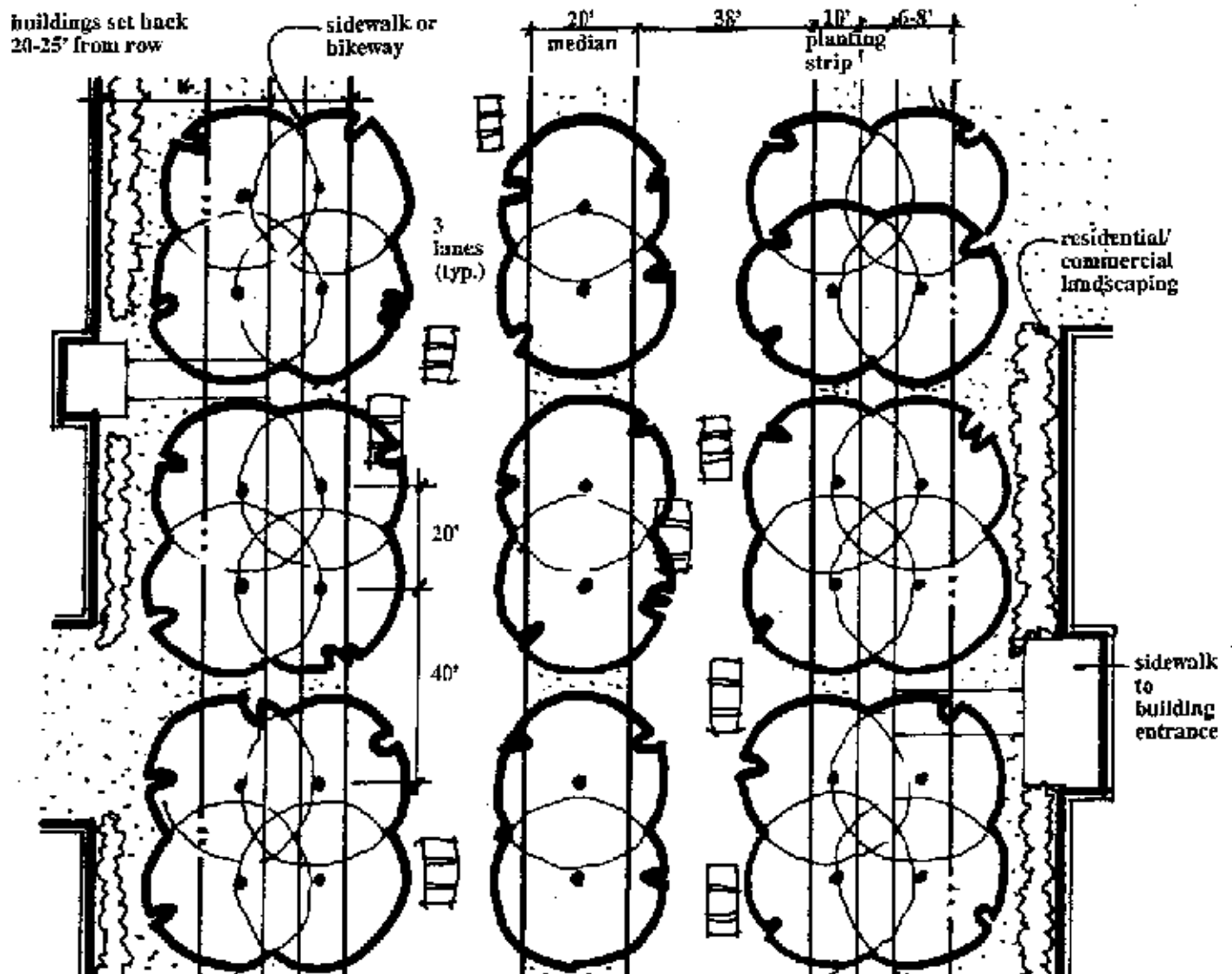
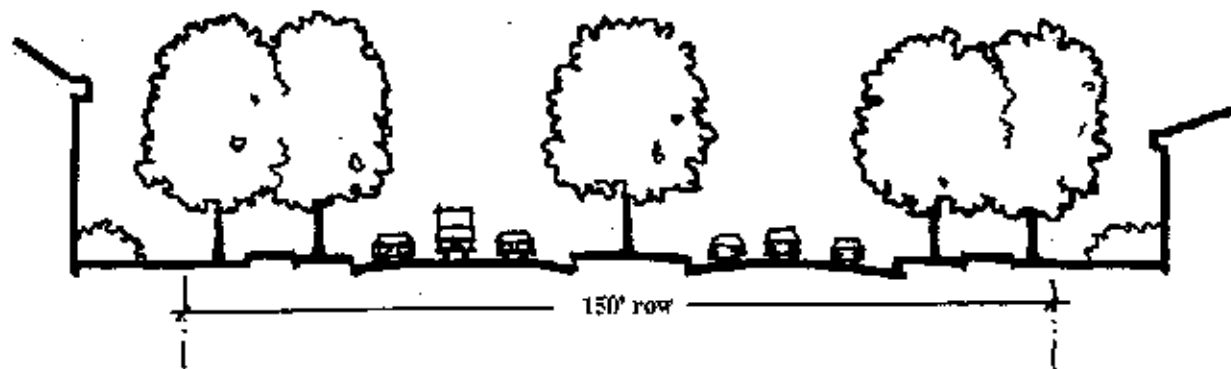
DESIGN PROTOTYPES

MARYLAND ROUTE 118

MD-118 is the primary access highway to the Germantown Town Center and also serves through traffic. MD-118 consists of a 150-foot right-of-way with six lanes and a 20-foot median. (Although master planned at 24 feet, these medians were constructed at 20 feet in width.) The proposed streetscape for MD-118 has a formal pattern of street trees, sidewalks, and bike paths.

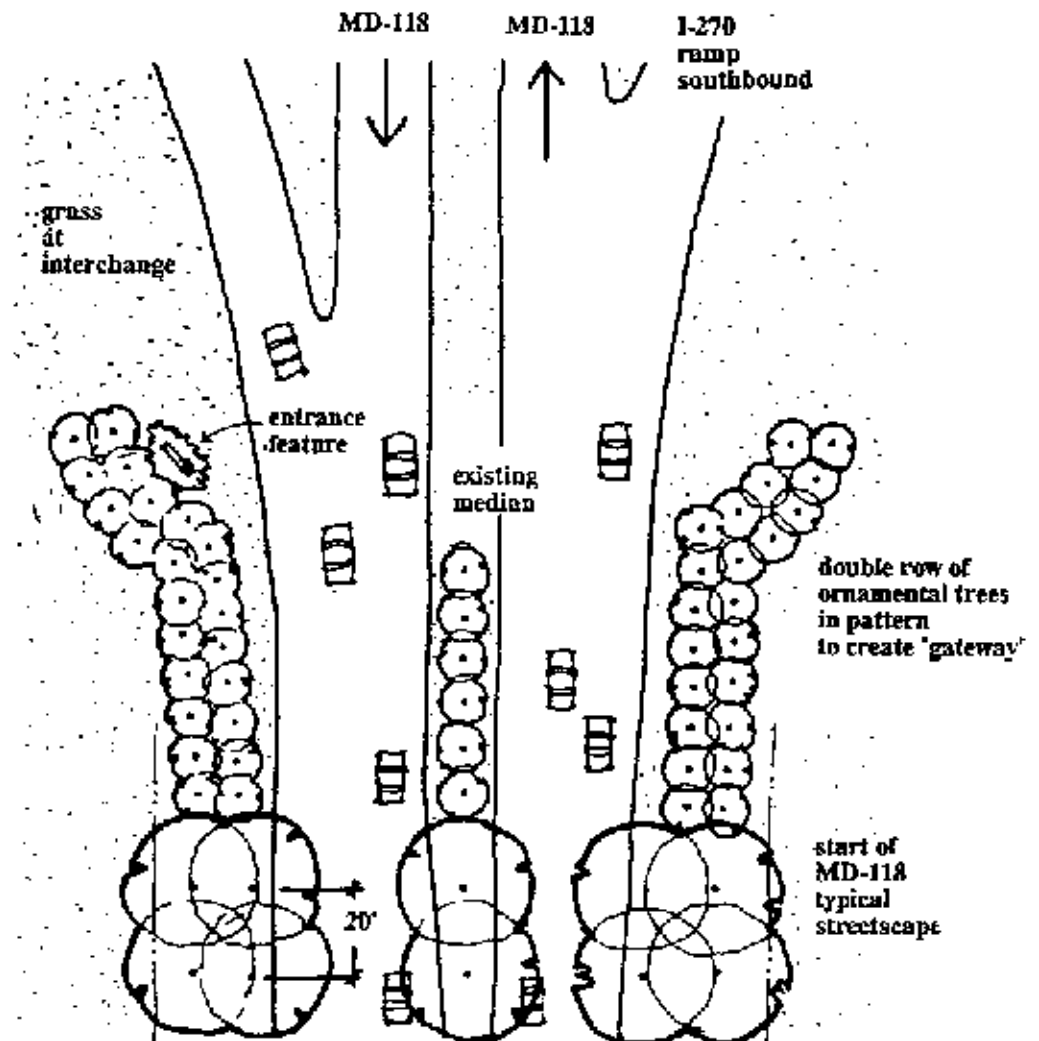
TYPICAL The typical streetscape along MD-118 will consist of a six-foot sidewalk or eight-foot bike path set back ten feet from the roadway. A double row of trees will flank the sidewalk and bike path in an alternating 20-foot/40-foot spacing pattern. This will result in regularly-spaced clusters of four trees. The intent of this pattern is to establish a formal green edge to the roadway. The 40-foot openings will allow views beyond the roadway to adjacent buildings and open space. Red Oak, a species that has a stately, formal form, is proposed. Buildings are recommended to be set back 20 to 25 feet from the street right-of-way. This is desired to allow sufficient area for expansion of the open space adjacent to the streetscape proposed within the right-of-way. It is, however, understood that not all buildings can or should be set back this distance and the flexibility is provided to allow some buildings to be located closer to the right-of-way pending site plan review.

The 20-foot median of MD-118 will also incorporate a single row of trees in the same 20-foot/40-foot pattern.



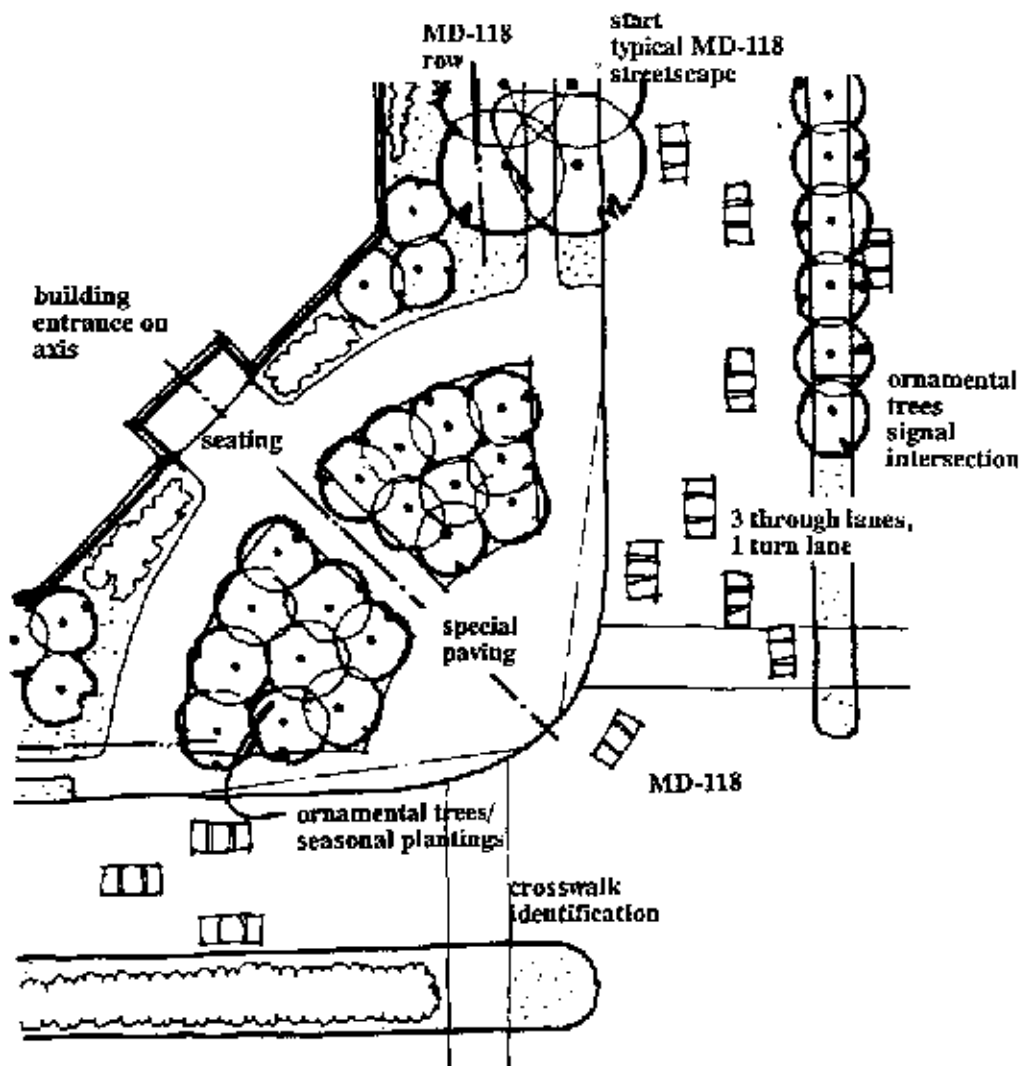
MD-118 BOULEVARD - TYPICAL CONDITION

GATEWAYS At the Town Center's major MD-118 gateway (from I-270), the character of the entry is established by formally arranged masses of ornamental trees. Double rows of ornamental trees are proposed to flank the roadway for a distance of 100 feet. A single row of ornamental trees are recommended to be planted in the median. The first row is to be set back ten feet from the curb. Although no sidewalk is currently planned for this portion of MD-118, because of the I-270 traffic, consideration should be given to the provision of pedestrian circulation over I-270. The gateway at MD-118 and the CSX Railroad tracks is proposed to incorporate a similar streetscape treatment.



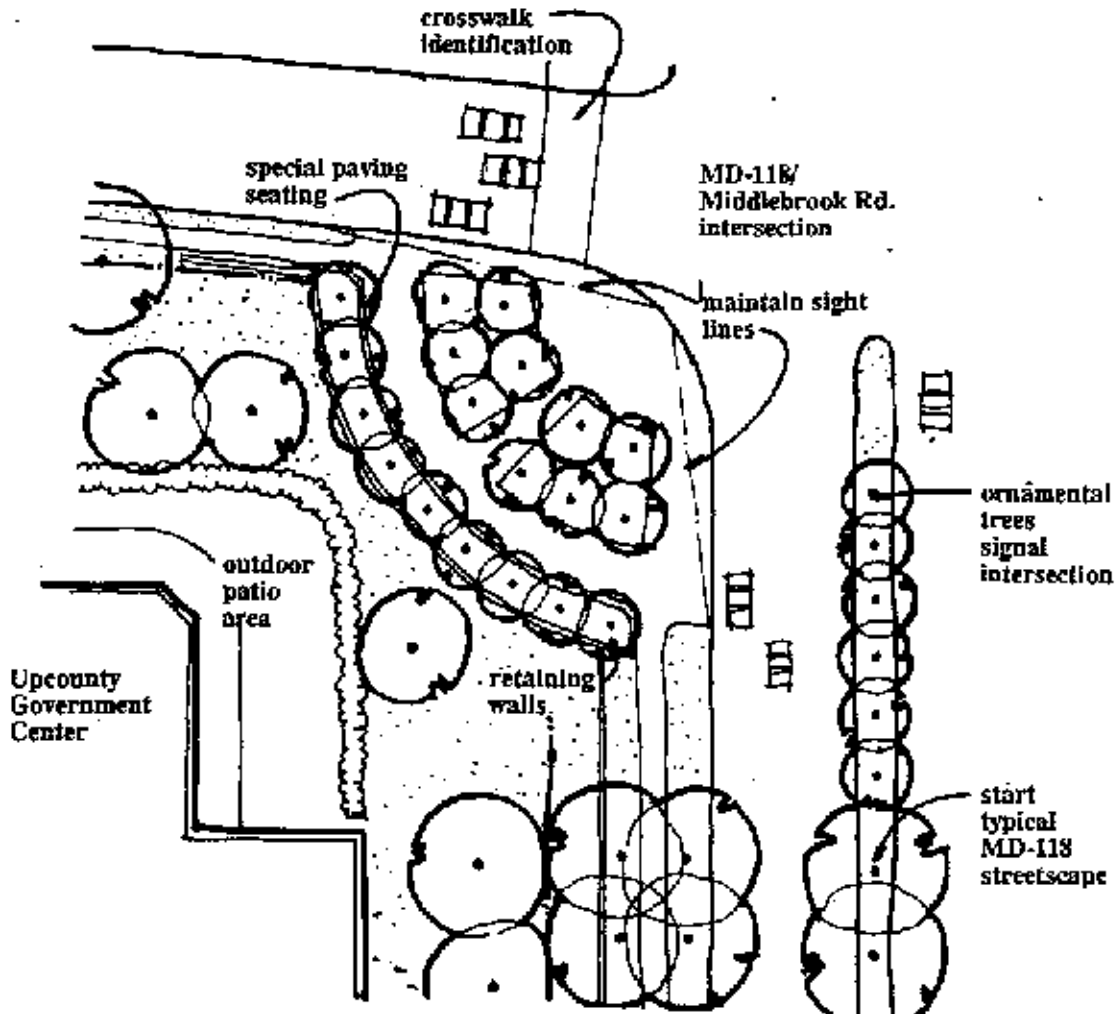
I-270 GATEWAY

INTERSECTIONS/CORNERS At the major intersections of Crystal Rock Drive, Middlebrook Road and Wisteria Drive - trees, sidewalks, and pedestrian areas are recommended to be arranged to create the character of a pedestrian entry. A single row of ornamental trees in the median will signal arrival. Planting beds with double rows of ornamental trees are proposed to provide a colorful backdrop for the pedestrian areas to be incorporated on these corners. Benches, lighting, and other site furniture are recommended to encourage use of these areas.



TYPICAL CORNER (TC-1, TC-2)

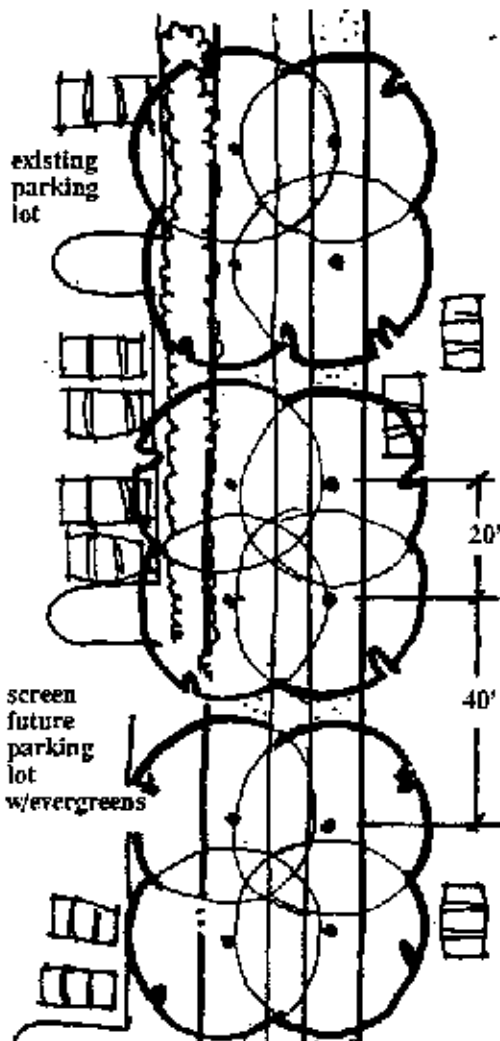
Corners of major intersections are also designed to encourage pedestrian activity. Enlarged areas of pedestrian paving will be provided where sidewalks meet. Ornamental trees and associated plantings and garden areas are recommended to provide visual interest and a backdrop for pedestrian activity. Retaining walls may be incorporated in selected steeply sloped areas to accommodate streetscape plantings (i.e., the Upcounty Government Center).



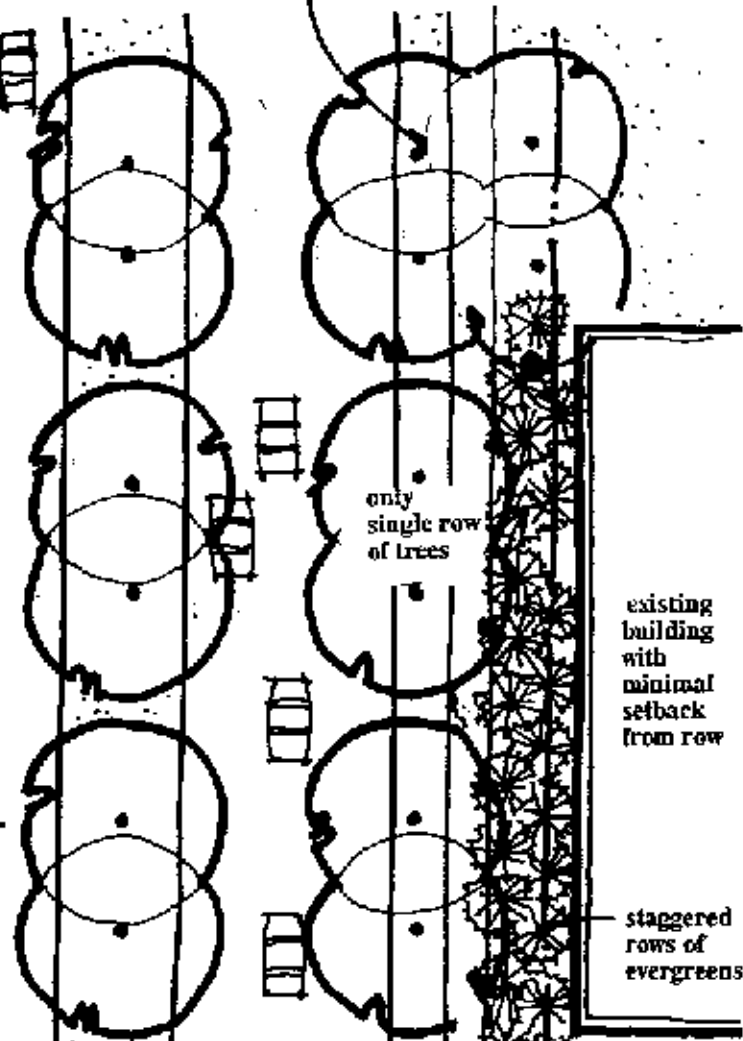
CORNER (UPCOUNTY GOVERNMENT CENTER)

SCREENING In locations where existing buildings or parking are adjacent to the roadway, the typical MD-118 streetscape may be required to be altered. Staggered rows of evergreen trees are proposed to be planted between the sidewalk and the buildings. Existing parking areas are proposed to incorporate low walls and/or be screened by hedges, shrubs and trees. Future parking lots are proposed to also include a double row of evergreen trees.

screen parking with hedge and/or wall



typical MD-118 streetscape (wherever possible)



TYPICAL SCREENING

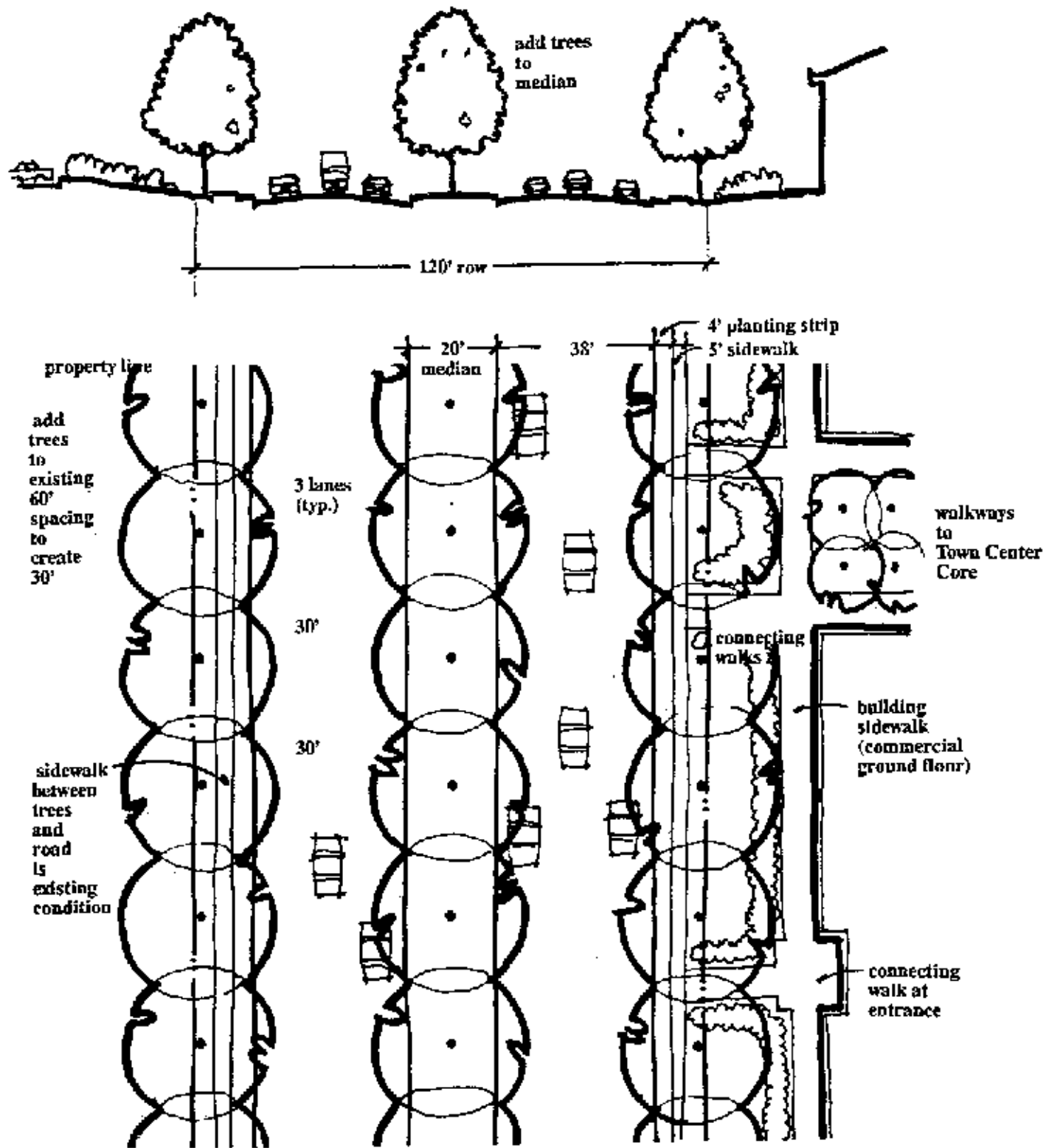
MIDDLEBROOK ROAD AND CRYSTAL ROCK DRIVE

Middlebrook Road and Crystal Rock Drive both serve as major distributors connecting MD-118, Father Hurley Boulevard, etc., to the various sites within the Town Center. Although still formal in nature, their proposed streetscapes are recommended be less monumental than that of MD-118.

Both streets consist of a 120-foot right-of-way with four lanes and a median. The Germantown Master Plan recommends six lanes for both roads.

TYPICAL - MIDDLEBROOK ROAD The existing streetscape on Middlebrook Road between MD-118 and Father Hurley Boulevard typically consists of a five-foot sidewalk set back four feet from the roadway. An existing eight-foot bike path is set back two to twenty feet from the roadway on the west side, south of MD 118. A bike path is recommended on the east side of Middlebrook Road from MD-118 to Locbury Drive. Street trees (Willow Oaks) are planted approximately three feet beyond the sidewalk, 60 feet on center on both sides of the road. They are not planted in the median which is 24 feet in width. New Willow Oaks are proposed to be planted between the existing trees to establish a 30-foot spacing. The median is also recommended to be planted with Willow Oaks, 30 feet on center in areas where the future force main sewer is not proposed to be located. Where this force main sewer is proposed to be constructed in the median no trees can be accommodated. It is also recommended that consideration be given to locating this force main on the TC-1 side of Middlebrook Road between the trees and future development, where feasible, depending on edge conditions.

TYPICAL - CRYSTAL ROCK DRIVE The existing streetscape on Crystal Rock Drive between MD-118 and Father Hurley Boulevard consists of a five-foot sidewalk set back four feet from the roadway. An eight-foot bike path is proposed on the east side of the roadway. Street trees are proposed to be planted 30 feet on center, beyond the sidewalk on both sides of the roadway as well as in the median. A species of Zelkova is recommended. As on Middlebrook Road, buildings are proposed to be set back a minimum of 25 feet from the right-of-way. For retail or office use, a building sidewalk is also recommended.

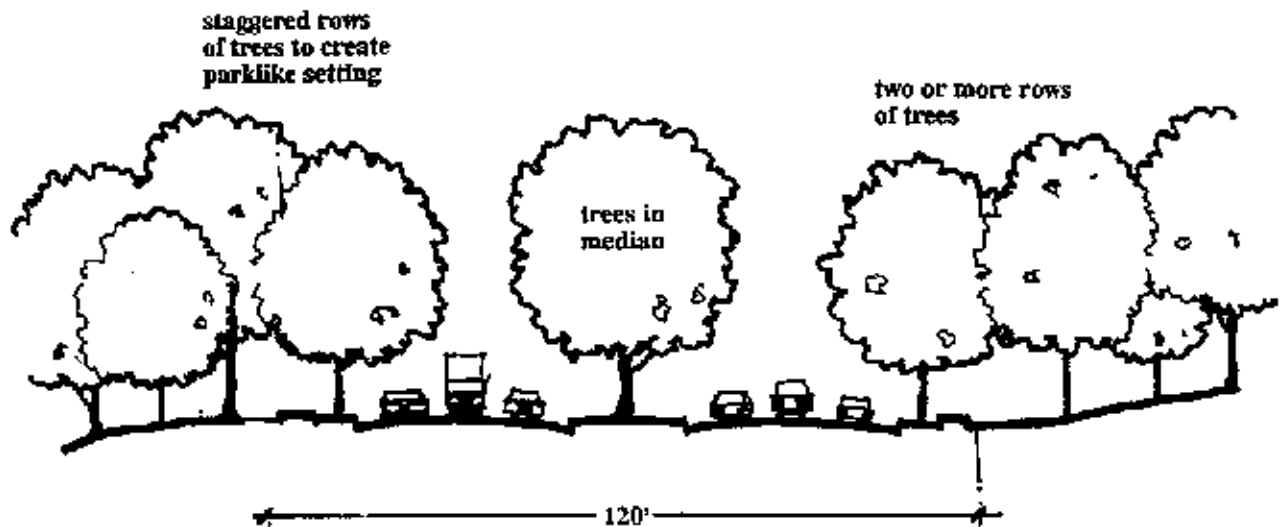


MIDDLEBROOK ROAD AND CRYSTAL ROCK DRIVE

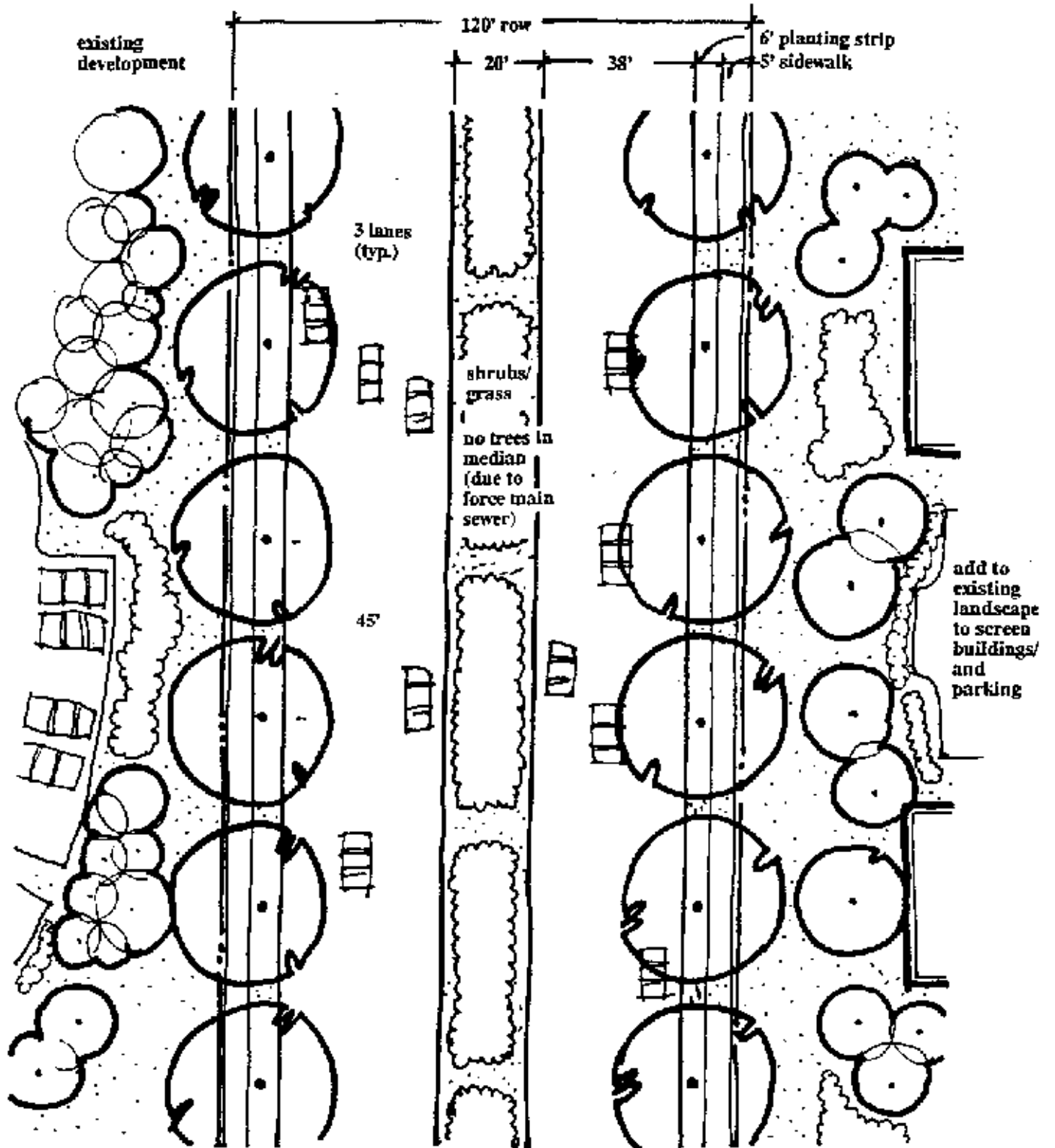
FATHER HURLEY BOULEVARD

Father Hurley Boulevard is a major highway that forms the boundary between the Town Center and residential development to the west. It consists of a 120-foot right-of-way, currently with four lanes and a median. The master plan recommends that it have six lanes and a 24-foot median.

TYPICAL - EXISTING DEVELOPMENT The existing streetscape on Father Hurley Boulevard as constructed to Wisteria Drive, includes a five-foot sidewalk on one side set back six feet from the curb. Street trees (Red Oaks) are planted beyond the sidewalk, 45 feet on center and on both sides of the road. No trees are currently in the median. A sidewalk will be added to the south side of the road. A bike path will be added instead of a sidewalk on the south side of Father Hurley from the Town Center entrance to the TC-5 Boulevard and Father Hurley Park. The proposed force main sewer is proposed to be constructed in the median and no trees can be located here; shrubs are recommended. However, in the to-be-developed right-of-way below Wisteria Drive, this sewer is proposed to be located on the side of the roadway.



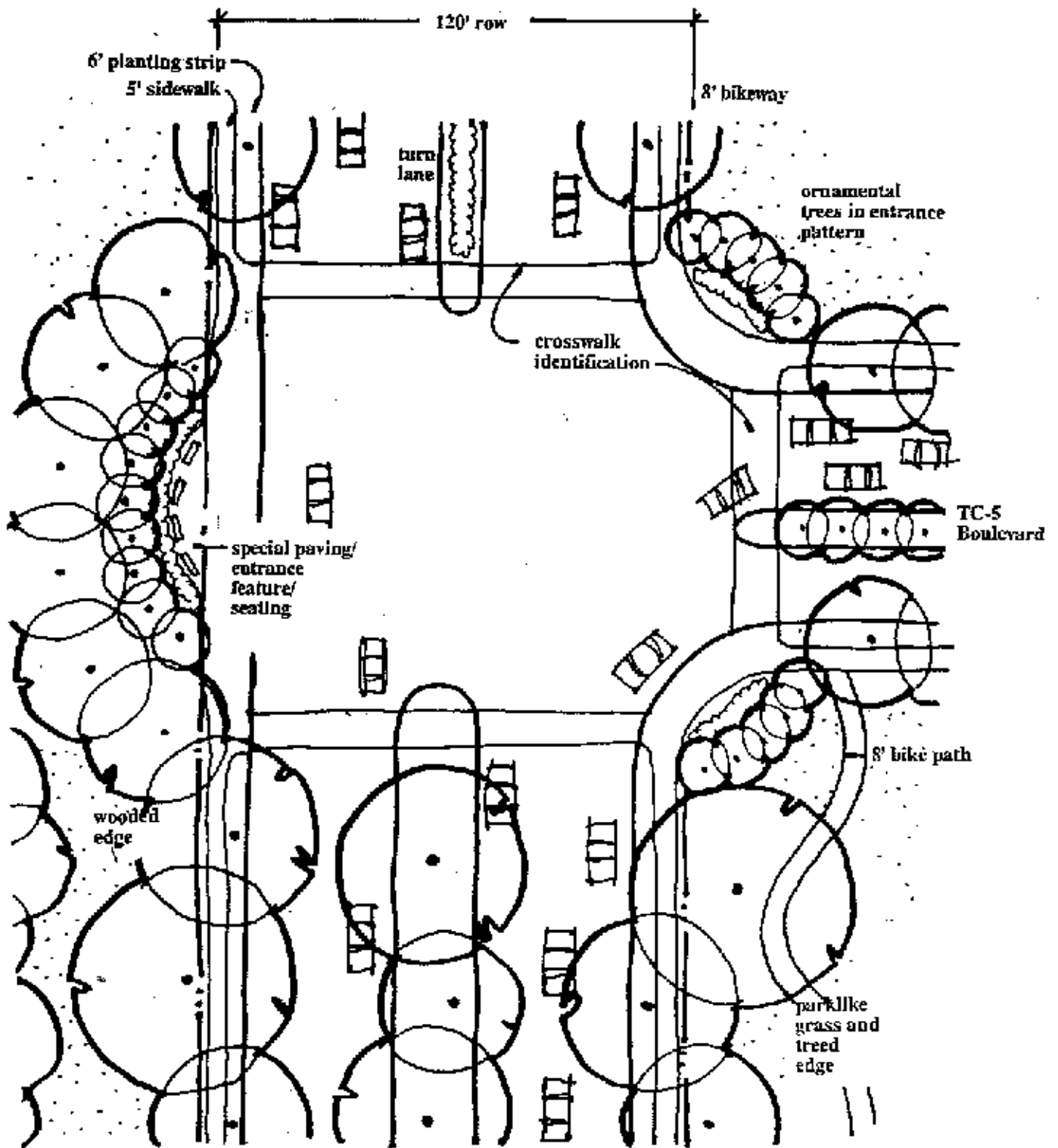
FATHER HURLEY BOULEVARD - TOWN CENTER GATEWAY



FATHER HURLEY BOULEVARD

TOWN CENTER GATEWAY/TC-5 ENTRANCE The less formal, natural character of the area near the CSX Railroad tracks provides the opportunity for the creation of a park-like **gateway** to the Town Center. Larger, less formal tree species are to be incorporated at irregular spacings (both on the sides of the roadway and in the median) to strengthen the natural character of this entrance. This area is also part of a major **to be committed** open space system.

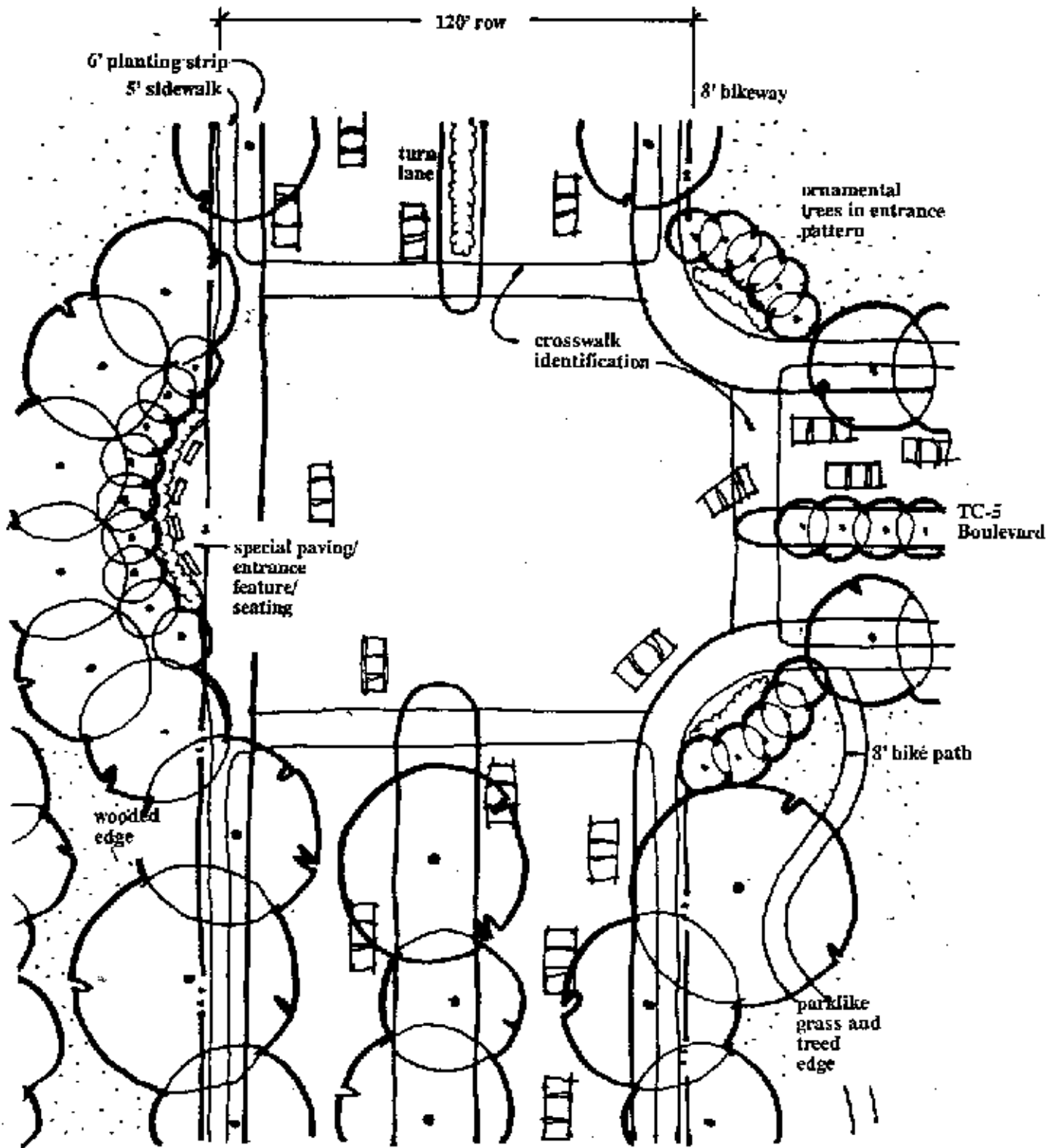
The transition between the typical Father Hurley Boulevard streetscape and the **gateway** streetscape will occur at the intersection of the TC-5 entrance boulevard. A single row of ornamental trees in the median, along with the plantings of ornamental trees at each corner and across Father Hurley Boulevard opposite the entrance, will signal the intersection. Pedestrian crosswalks and widened sidewalks, are proposed to identify and accommodate pedestrian activity at this intersection.



FATHER HURLEY BOULEVARD - TC-5 ENTRANCE BOULEVARD

WISTERIA DRIVE

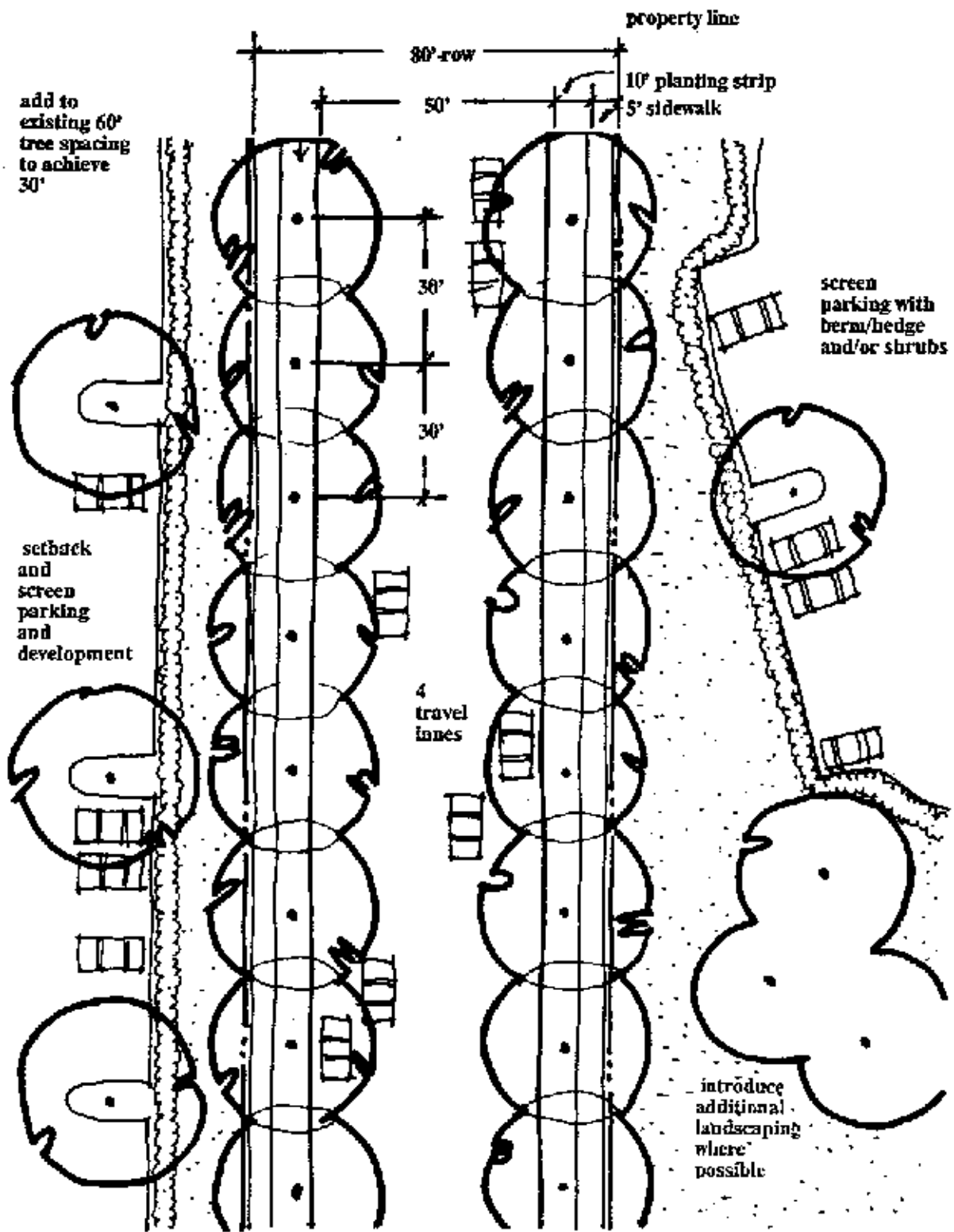
Wisteria Drive, from Father Hurley Boulevard to Great Seneca Highway, is a four-lane roadway with no median within an 80-foot right-of-way. Sidewalks, five feet wide, are proposed to be set back from the curb eight to ten feet on both sides. Currently, Sugar Maples are planted 60 feet on center on both sides of a majority of Wisteria Drive (between Father Hurley Boulevard and MD-118). New Sugar Maples are proposed to be planted to establish a 30-foot spacing. On the east side of MD-118, existing oaks will be supplemented to achieve a similar spacing.



FATHER HURLEY BOULEVARD - TC-5 ENTRANCE BOULEVARD

WISTERIA DRIVE

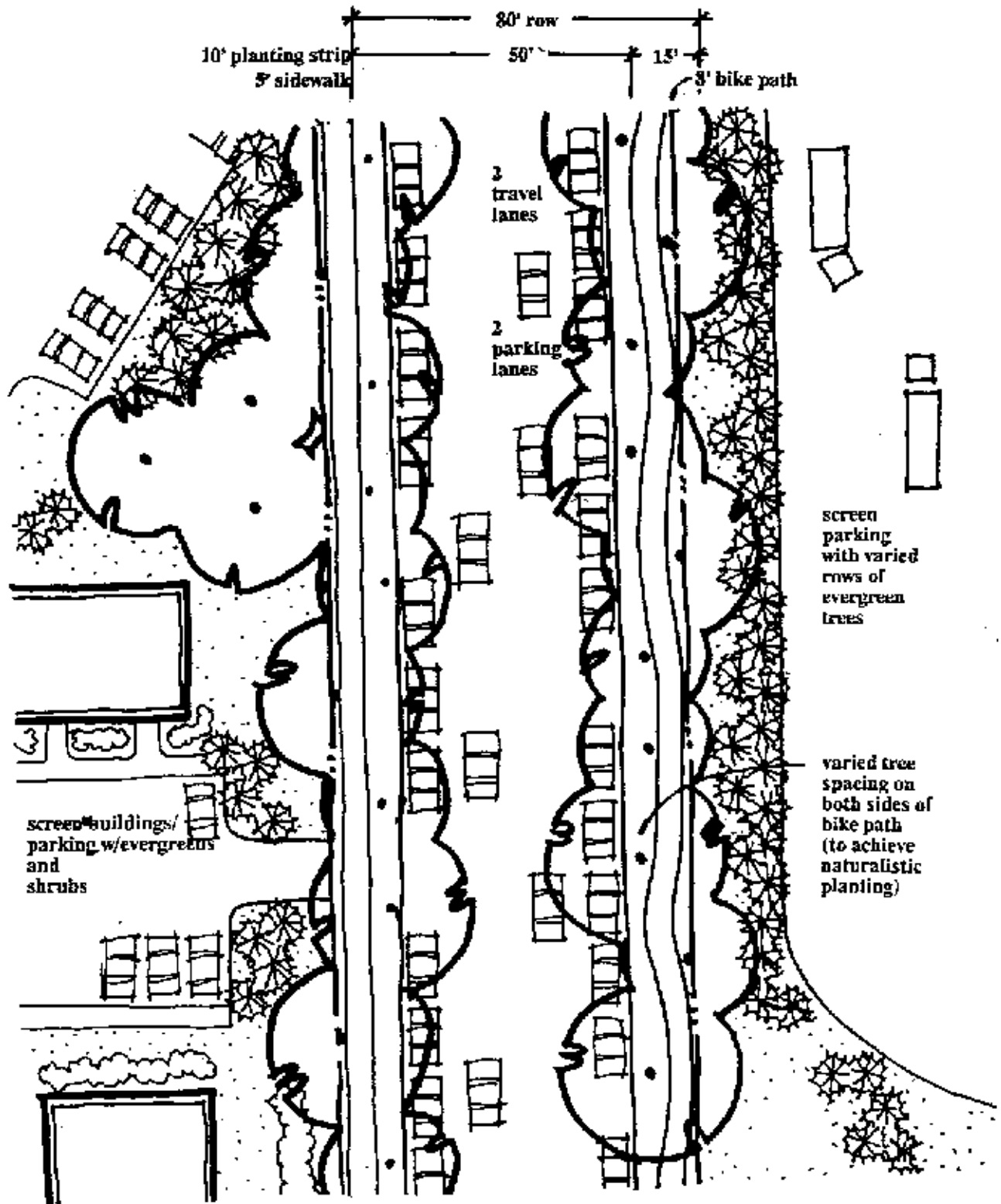
Wisteria Drive, from Father Hurley Boulevard to Great Seneca Highway, is a four-lane roadway with no median within an 80-foot right-of-way. Sidewalks, five feet wide, are proposed to be set back from the curb eight to ten feet on both sides. Currently, Sugar Maples are planted 60 feet on center on both sides of a majority of Wisteria Drive (between Father Hurley Boulevard and MD-118). New Sugar Maples are proposed to be planted to establish a 30-foot spacing. On the east side of MD-118, existing oaks will be supplemented to achieve a similar spacing.



WISTERIA DRIVE

LOCBURY DRIVE

Locbury Drive will provide a connection for local traffic between the Town Center Core (TC-1) and TC-5, and is also proposed to serve as a park-like pedestrian and bike route. It consists of a 70-foot right-of-way and, in accordance with the Germantown Master Plan, will have four lanes with no median. The outside lanes are proposed to be used for parking. A pedestrian path, six feet in width, will be set back ten feet from the curb on one side and a bike path, eight feet in width, will be incorporated on the other side. Street trees will be planted between these pathways and the roadway. Setbacks and spacing may vary. Buildings are recommended to be set back so as to allow space for park-like landscaping beyond the sidewalk.



LOCBURY DRIVE - SPECIAL STREET

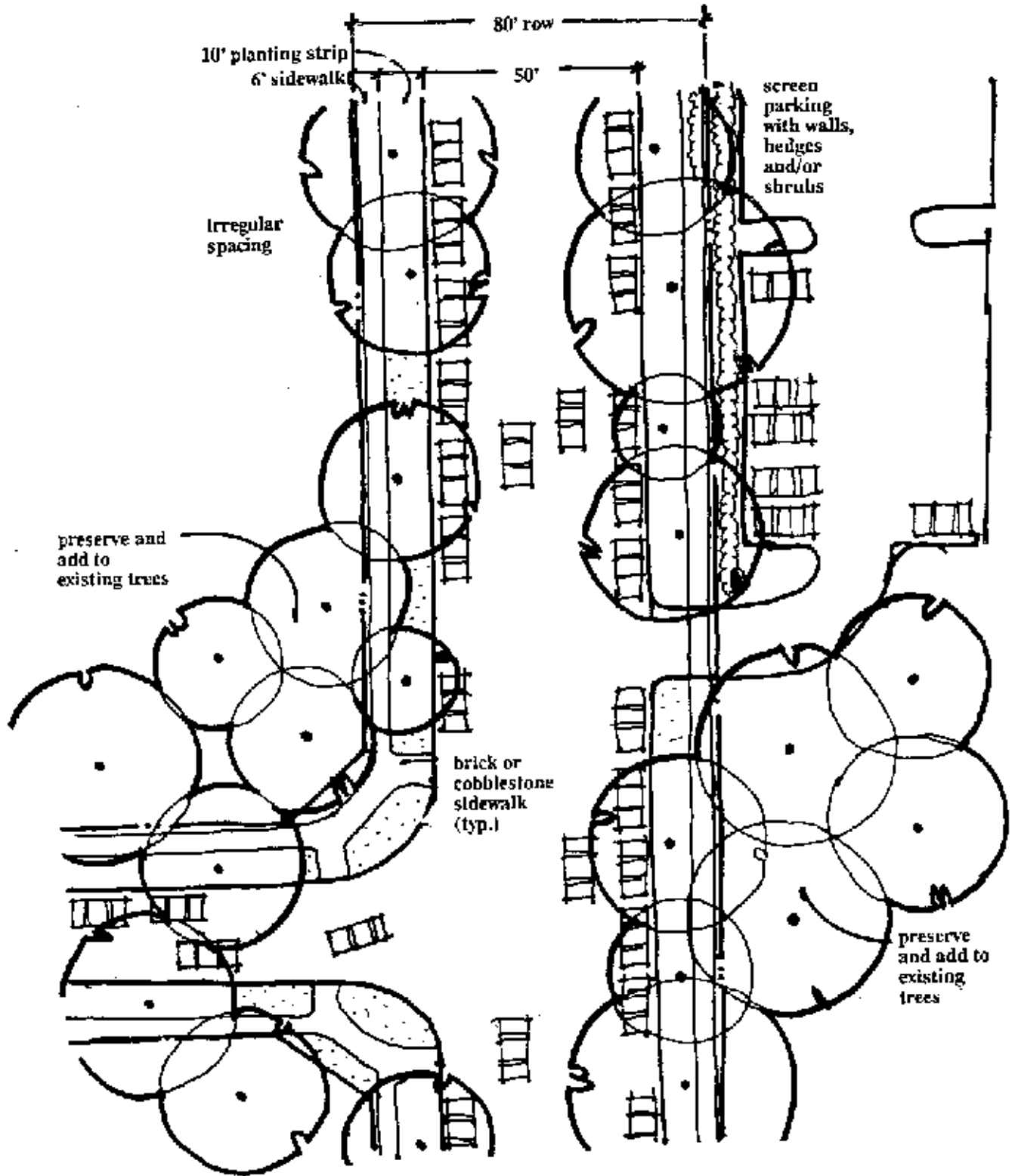
WALTER JOHNSON DRIVE (OLD MD-118)

The Germantown Master Plan recommends that Walter Johnson Drive (Old MD-118) end in a one-way, two-lane loop road approximately 750 feet southwest of Wisteria Drive. The bridge across the CSX Railroad tracks is proposed to be closed to vehicular traffic. This design study recommends connecting Old MD-118 to new MD-118 with a two-way, four-lane road, to intersect at the proposed Waters Road entrance to TC-5. This connection should have a 100-foot right-of-way. Another parking facility (lot or garage) is proposed to include this roadway and land on both sides. This would preclude connection from MD-118 to Old MD-118 and the MARC station. It is recommended that, if feasible, this facility be located to the northeast of Waters Road so as to allow direct access from MD-118 to both the existing and proposed MARC parking facilities and the station as well as Old MD-118.

Old MD-118 is a four-lane roadway (two moving lanes) with a 100-foot right-of-way. Six-foot-wide sidewalks on both sides are proposed to be set back ten feet from the curb. Street trees will be planted at varying spacing on both sides between the sidewalk and the roadway to enhance the character of this informal open space link.

The portion of Walter Johnson Drive (OLD MD-118) to the north of Wisteria Drive is approximately 750 feet long, with a right-of-way of 80 feet. The roadway ends at Germantown Square Park. The Master Plan recommends four lanes with sidewalks and street trees on both sides. The outside lanes are designated as parking lanes. A six-foot-wide sidewalk is recommended to be located ten feet from the curb on the north side of this road and a pedestrian path is proposed for the south or Northlake side. This trail may vary in its distance from the street. Street trees are recommended to be planted at varying spacing on both sides of the street.

The historic character desired for this street is to be created by the use of lighting, sidewalk paving, and street furniture similar in style to that of the recently completed MARC Commuter Rail Station. The street may also incorporate special paving materials.

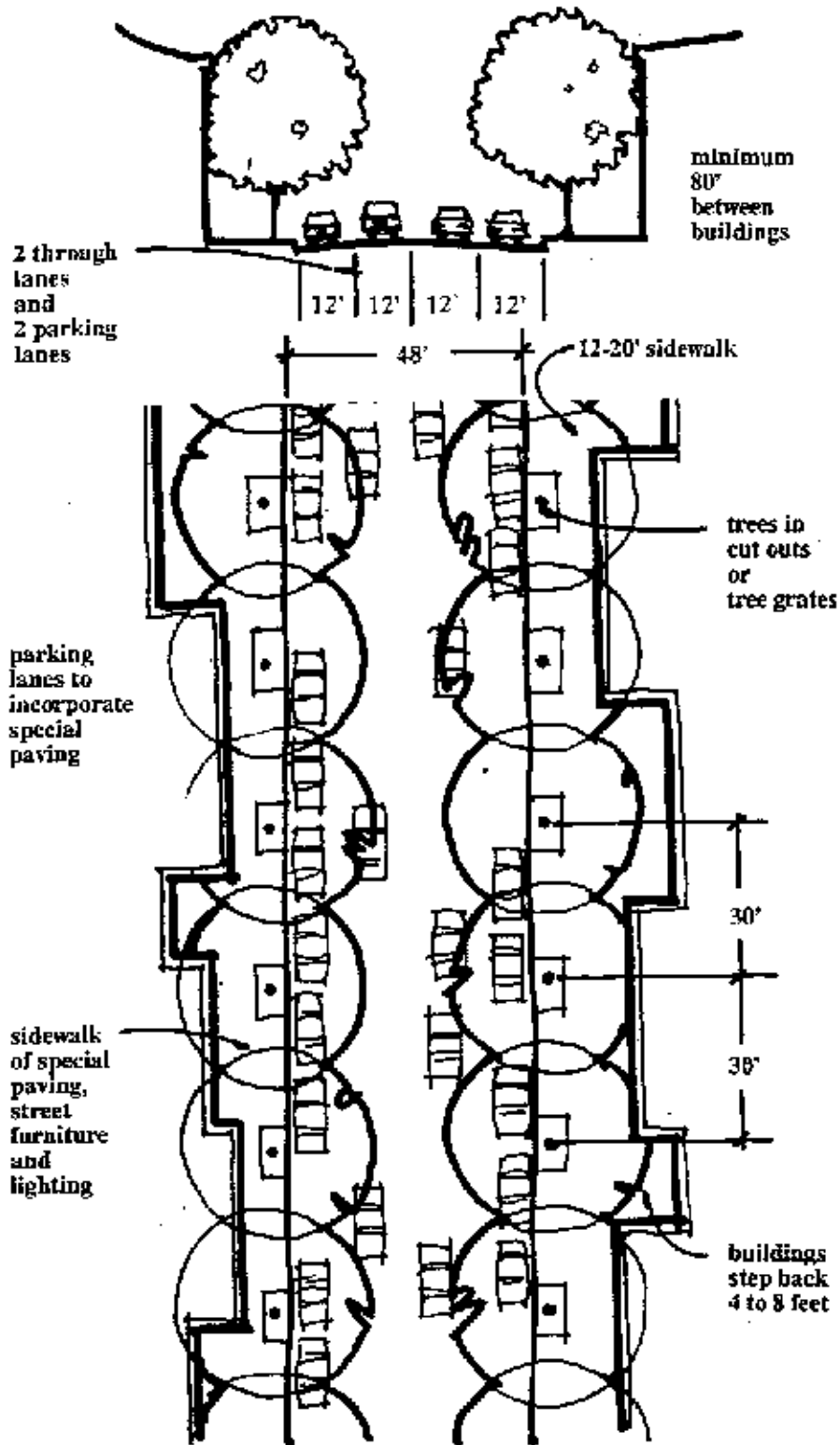


WALTER JOHNSON DRIVE (OLD MD-118)

MAIN STREET (TC-1)

The richest concentration of special paving, lighting, street furniture, and landscaping will be incorporated into the Town Center Core and Main Street. The recommended streetscape is designed to promote and enhance pedestrian use.

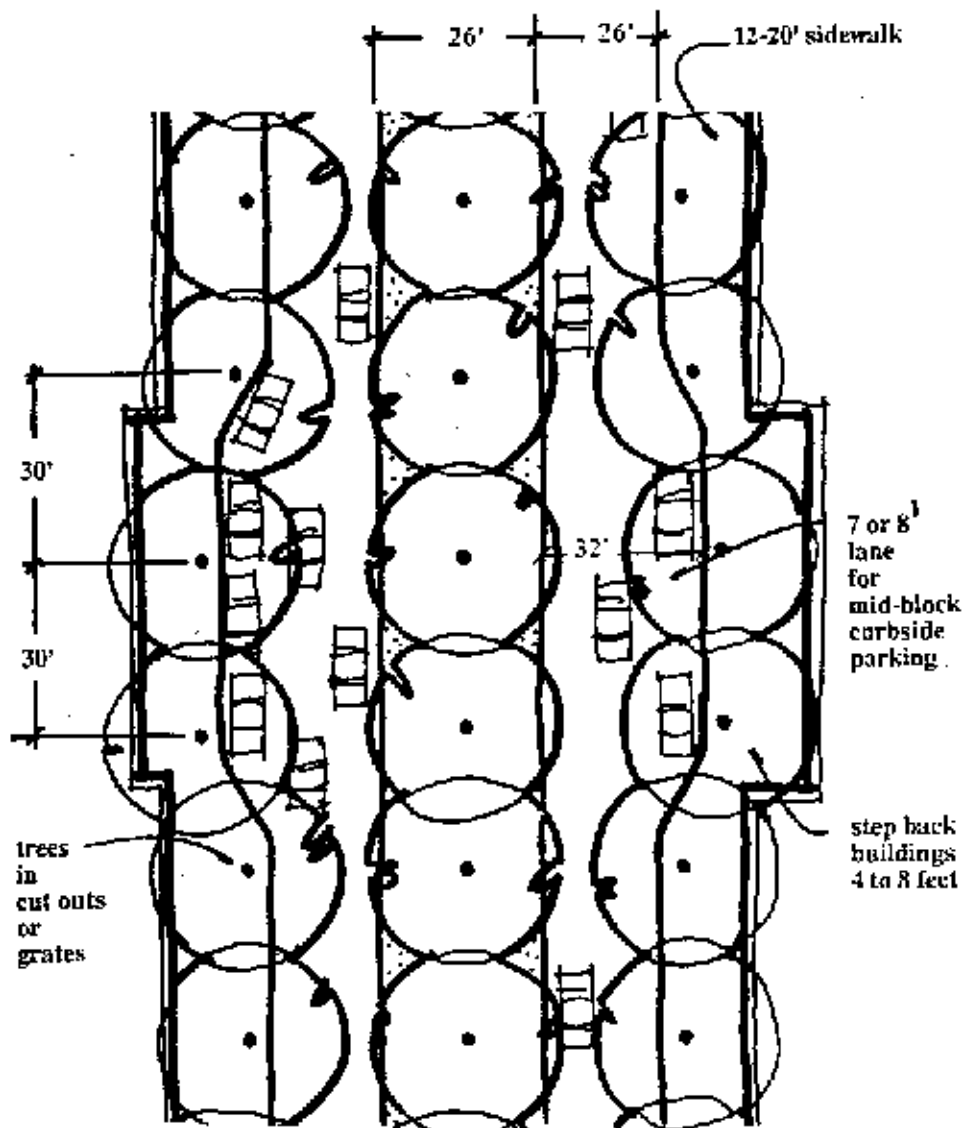
TYPICAL Main Street will typically maintain a distance of between 70 and 86 feet between buildings. The street incorporates four lanes, two of which will primarily serve as curbside parking. These lanes are recommended to incorporate special paving. Sidewalks, extending from the street to the face of the buildings, are proposed to vary from 12 to no more than 20 feet in width. Areas of planting are recommended to be incorporated as appropriate to the adjacent development. Trees of a pedestrian scale are to be planted 30 feet on center on both sides of the street in either planting beds or planters. The Main Street entrances to TC-1 at Middlebrook Road and Crystal Rock Drive each are recommended to incorporate entrance features and special landscaping.



MAIN STREET (TC-1)

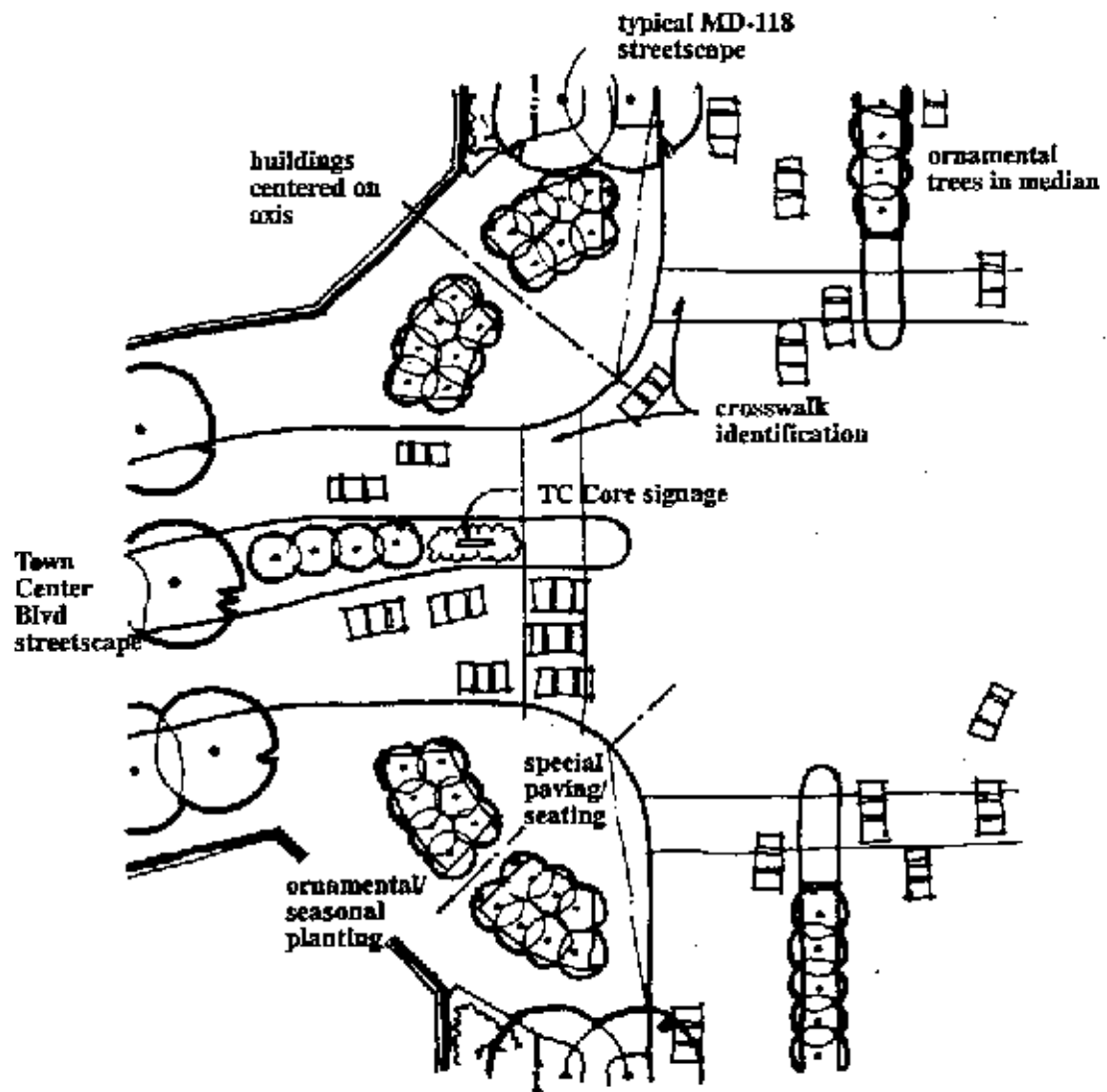
**TOWN CENTER
BOULEVARD (TC-1)**

TYPICAL SECTION The boulevard connecting Main Street and the MD-118 entrance to the Town Center Core will typically have a distance of 106-116 feet between buildings and incorporate four traffic lanes, a 26-foot-wide median, and a 12- to 16-foot-wide sidewalk. Curbside parking will be provided mid-block in cut-out bays. Street trees are to be planted 30 feet on center on both sides of the roadway, as well as in the median.



TOWN CENTER BOULEVARD (TC-1)

ENTRANCES The Town Center Boulevard entrance is recommended to have an enhanced landscape treatment similar to the other major MD-118 corners. Ornamental trees are to be arranged in a pattern to serve as a backdrop for the pedestrian zone at the street corners. Ornamental trees will also be planted in the median.



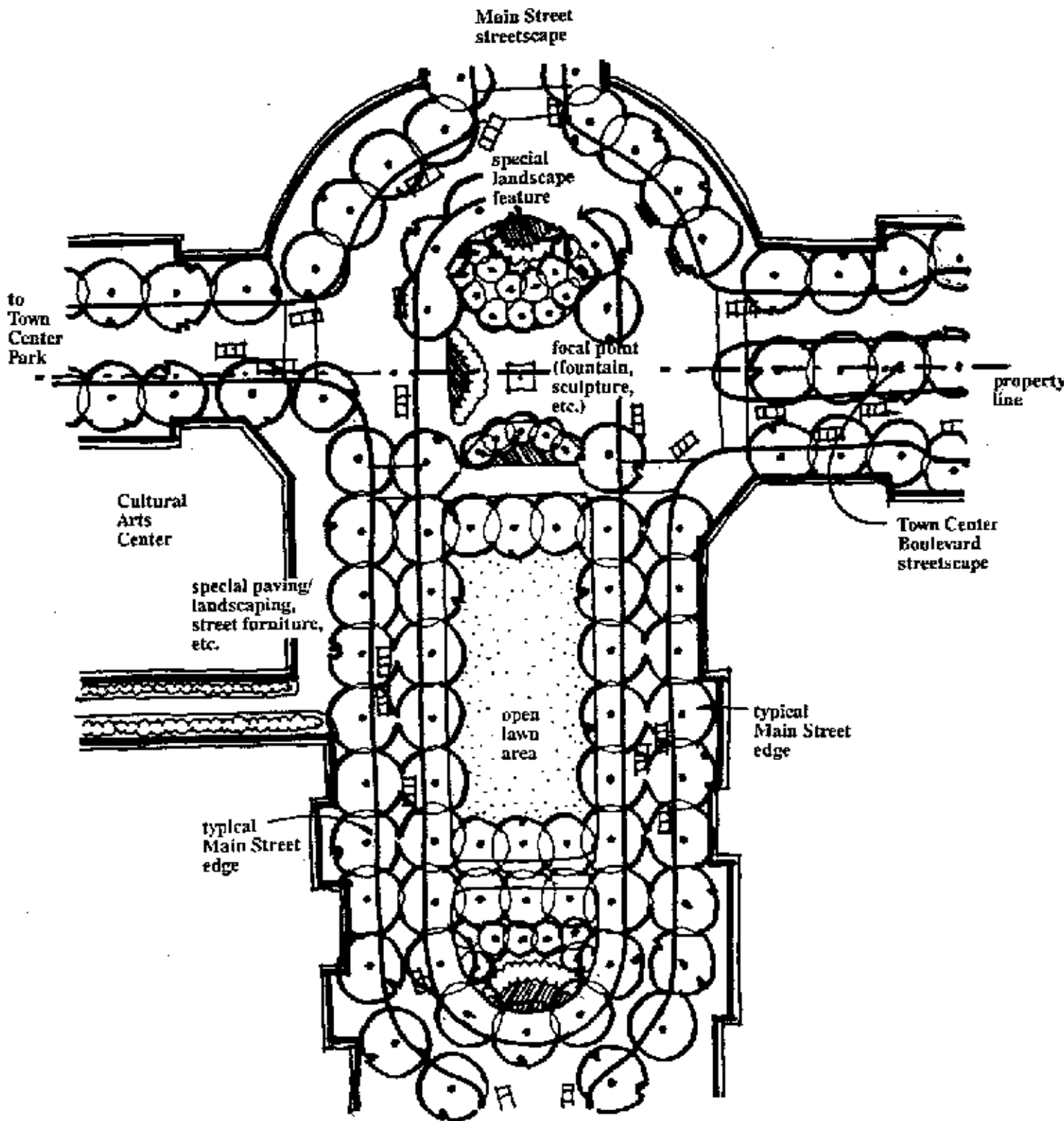
MD-118 ENTRANCE

TOWN CENTER COMMONS (TC-1)

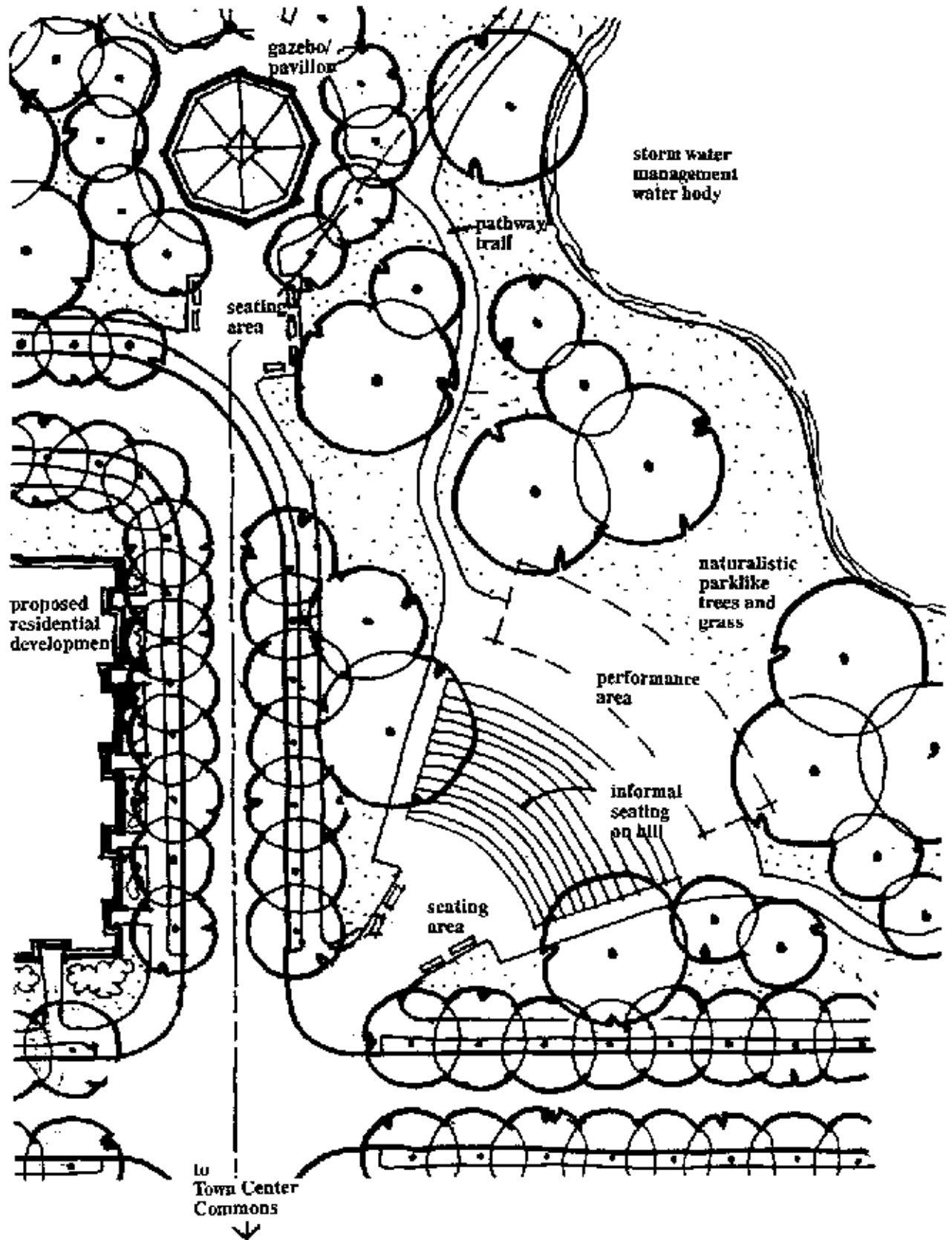
Town Center Commons is the central urban open space in the Town Center Core. Buildings facing onto the commons are proposed to be from 176 to 184 feet apart. The Commons is proposed to be 112 feet wide. Street trees are to be planted 30 feet on center, set back three feet from the curb, within six-foot cut-outs in a 12-foot-wide sidewalk. Landscaping is recommended to include shade trees, ornamental trees, and gardens, as well as a significant lawn area. Walking, sitting, and outdoor events can be accommodated within this Commons.

TOWN CENTER PARK (TC-1)

Town Center Park is proposed to incorporate one or more ponds, a natural wetlands area, and areas for outdoor gatherings and passive recreation. An amphitheater is also proposed. Along the perimeter of the park the streetscape is recommended to consist of a four- or six-foot-wide sidewalk set back seven feet from the curb, with street trees planted 30 feet on center, between the sidewalk and the curb. Pathways will lead into and through the parkland.



TOWN CENTER COMMONS (TC-1)



TOWN CENTER PARK (TC-1)

INTERNAL STREETS (TC-1)

PARKING ACCESS

Streets between commercial buildings and surface parking areas are proposed to have two through lanes and one seven- or eight-foot-wide discontinuous parallel parking lane on the side of the street adjacent to the building(s). Buildings are recommended to be set back 16 to 20 feet from the curb. This setback will allow a sidewalk with street trees planted in cut-outs, and up to a ten-foot-wide planting strip adjacent to the building.

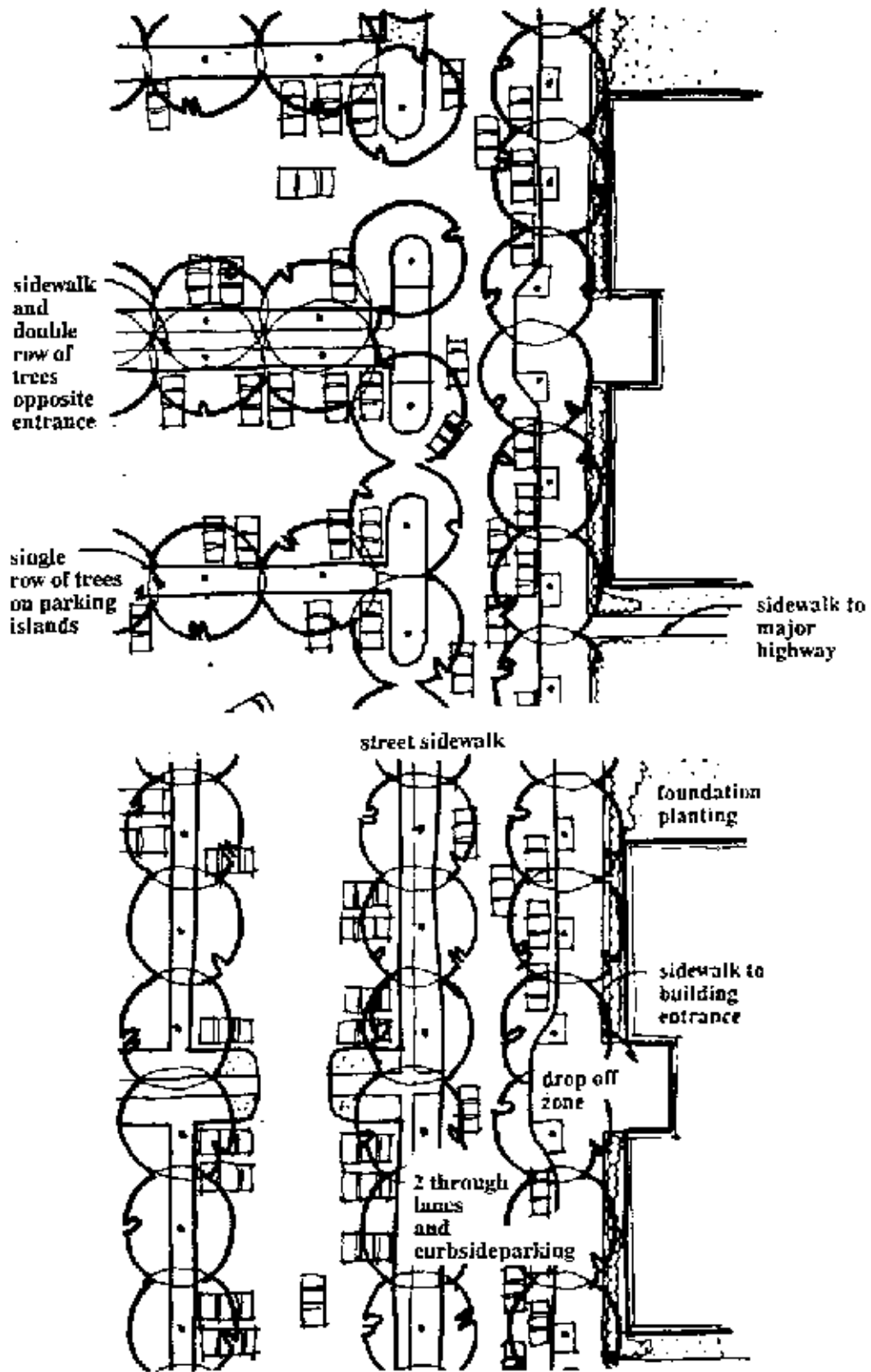
Within the parking area, parking bay medians are proposed as typically six feet in width, also incorporating a 15-foot-wide median across from some building entrances, where possible. The six-foot medians would have a single row of shade trees, 30 feet on center. The 15-foot median would consist of a walkway, four or five feet in width, flanked by planting strips five or six feet in width, with shade trees 30 feet on center.

PARKING EDGE

Streets between commercial buildings and parking lots and/or structures are proposed to have two 10-foot-wide through lanes and either one or two seven- or eight-foot-wide discontinuous parking lanes. Buildings will be set back 16 to 20 feet from the curb. This setback area will allow a sidewalk with street trees planted 30 feet on center in cut-outs, and a planting strip adjacent to the building.

Streets that occur between blocks of parking are proposed to have two moving traffic lanes and two discontinuous parallel parking lanes. Sidewalks are incorporated on both sides of the street. Curbs of the adjacent parking areas will be located five to six feet beyond the edge of the sidewalk. Sidewalks are flanked by street trees planted 30 feet on center.

The intent of this streetscape treatment is to provide the character of a real street in an area where parking flanks the street and to allow for the retrofit of the parking lot with buildings or structured parking.

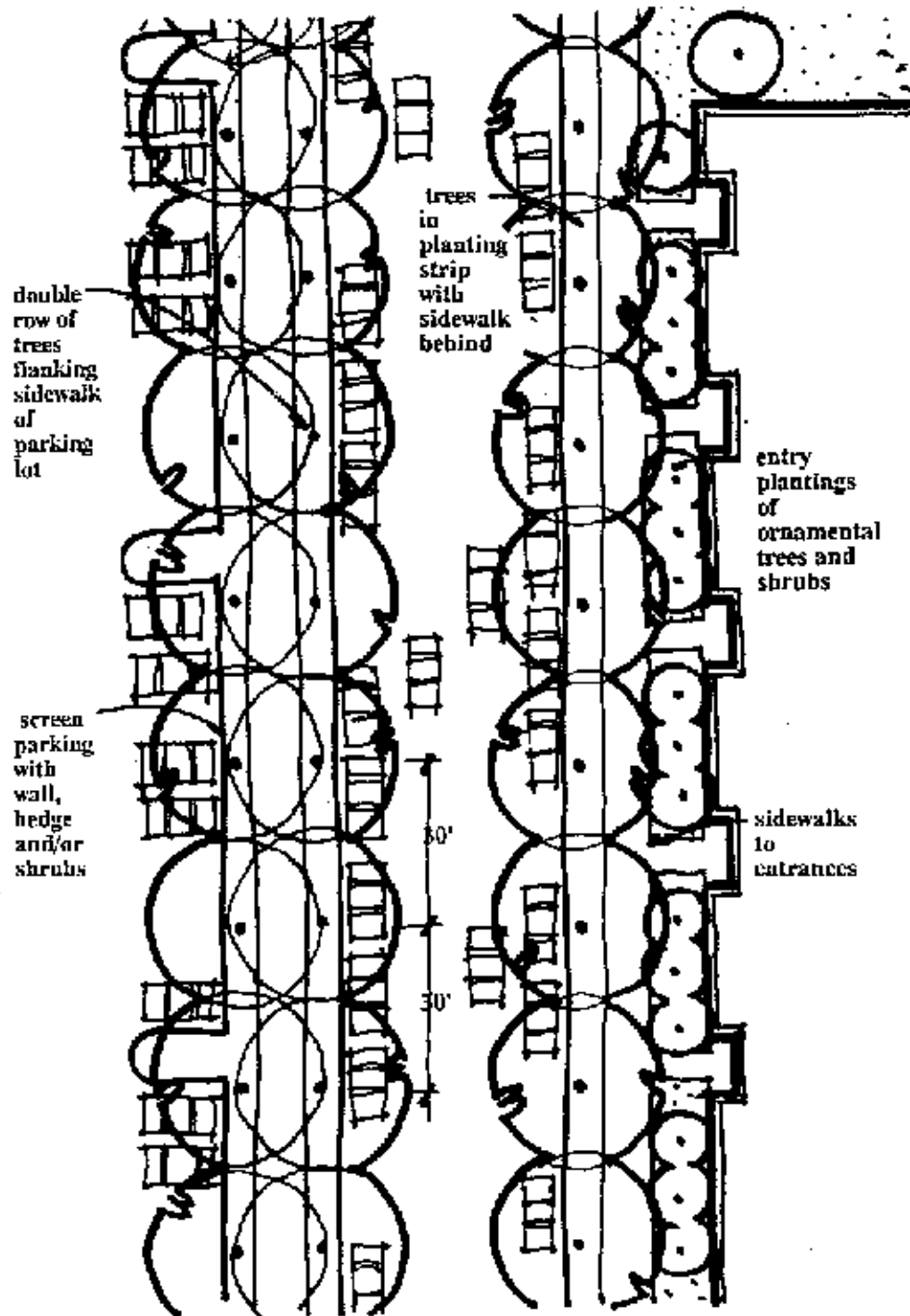


INTERNAL STREET (TC-1)

**RESIDENTIAL STREET
(TC-1)**

Streets between residential buildings and parking lots are proposed to have three or four lanes, one or two of which will be used for curbside parking. Residential buildings are recommended to be set back a minimum of 20 feet from the curb. The setback area will consist of a grass strip planted with street trees 20 to 30 feet on center, a four- to six-foot-wide sidewalk, and a minimum of a ten-foot-wide area for private entryway gardens. The strip between the street and the parking area is also 20 feet wide and will consist of a grass strip planted with street trees, a sidewalk, and a buffer strip incorporating shade trees between the sidewalk and the parking.

3 or 4 lanes
(one or two parking)

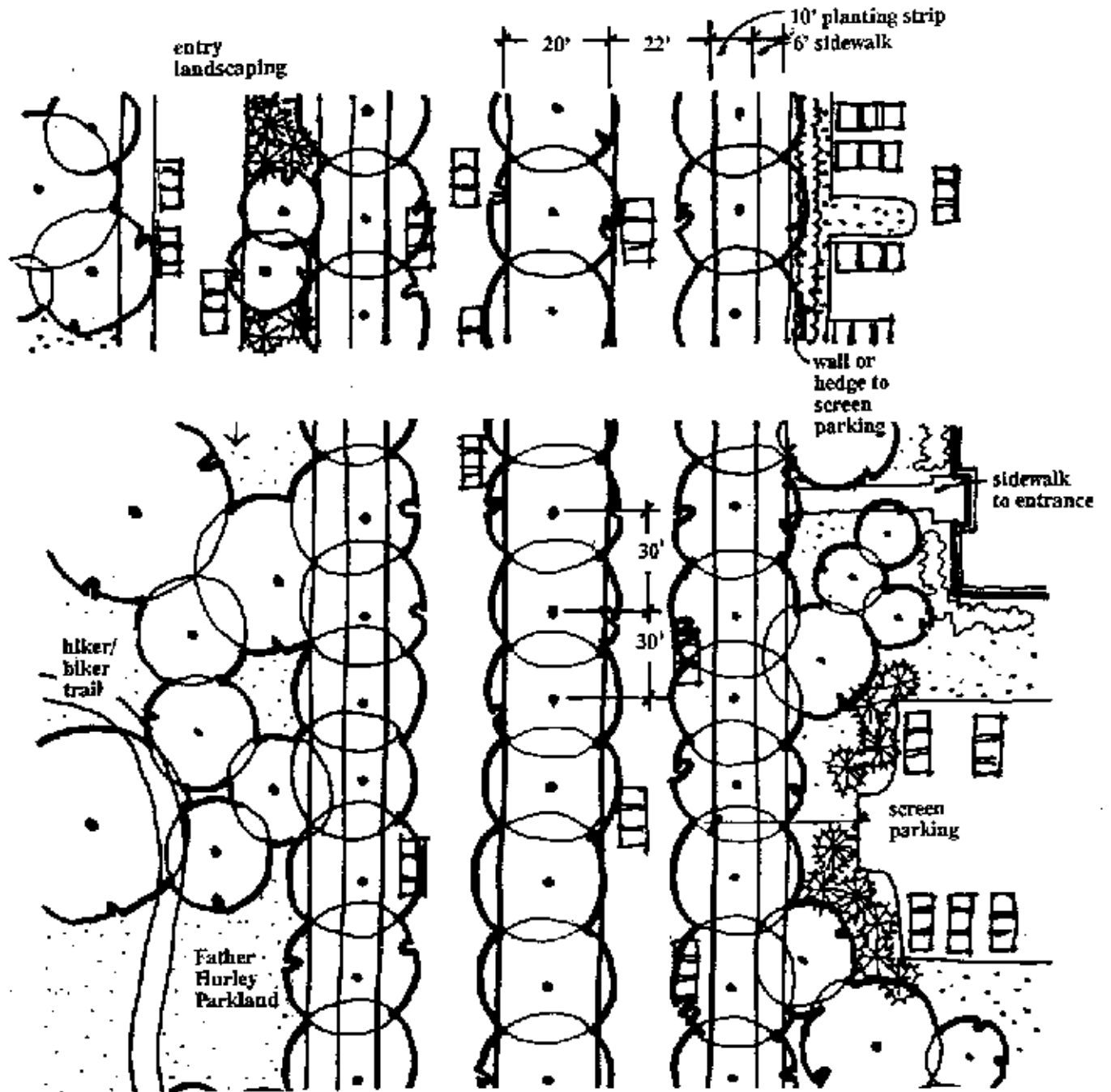


RESIDENTIAL STREET (TC-1)

BOULEVARDS (TC-5)

The boulevards connecting the entrances to TC-5 are proposed to consist of four lanes and 28- to 30-foot-wide medians.

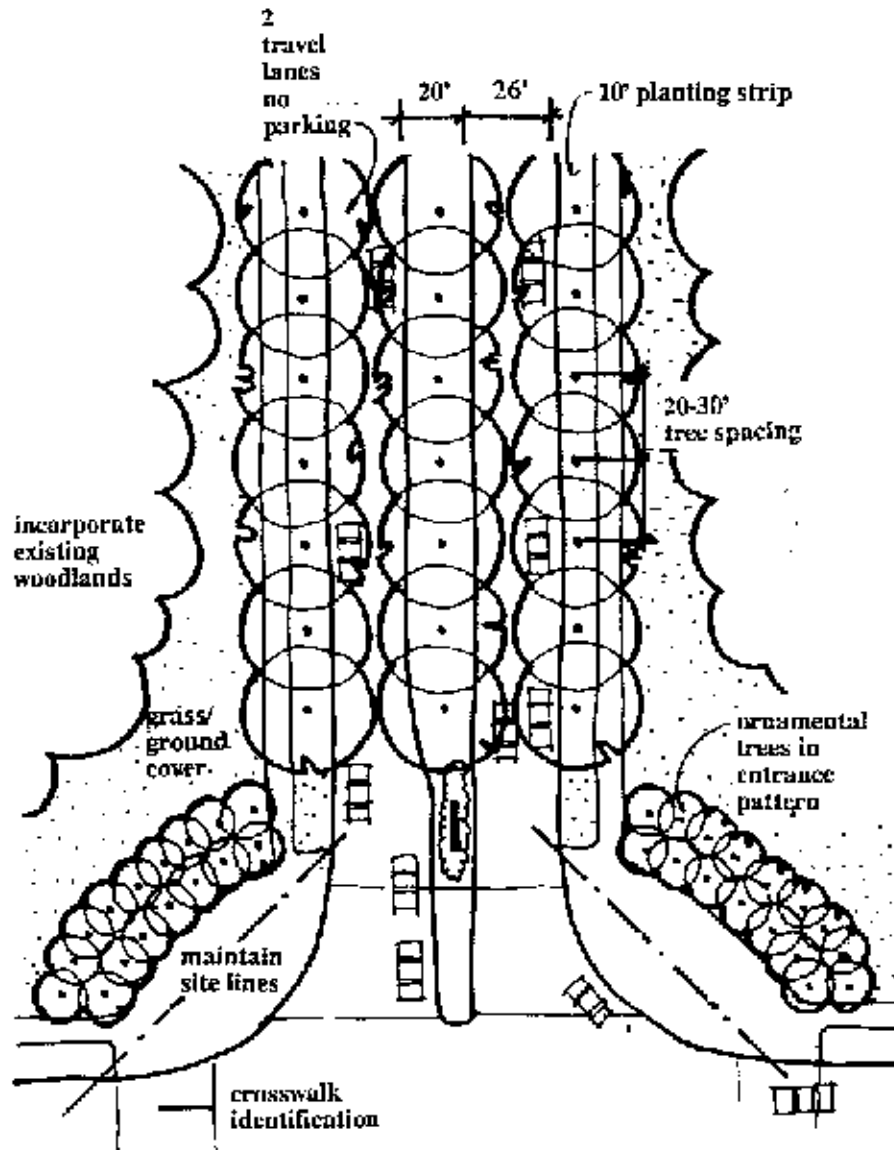
Typical Section (Residential and Parking Edges) Six-foot-wide sidewalks will be separated from the curb by an eight-foot grass planting strip. Street trees will be planted 30 feet on center on both sides of the roadway and in the median. Where surface parking areas abut the roadway an additional seven- or eight-foot-wide strip, incorporating a wall or planted with shrubs or hedge, will separate the sidewalk from the parking. Where the TC-5 boulevard abuts residential, a substantial setback between the curb and the buildings is proposed to be provided. This area will be planted with a combination of evergreen trees, shade trees, and flowering trees, informally arranged to create a residential character. The opposite side of the boulevard will also be informal in response to the development of the proposed Father Hurley Park in this location.



BOULEVARDS (TC-5)

MD-118 ENTRANCE

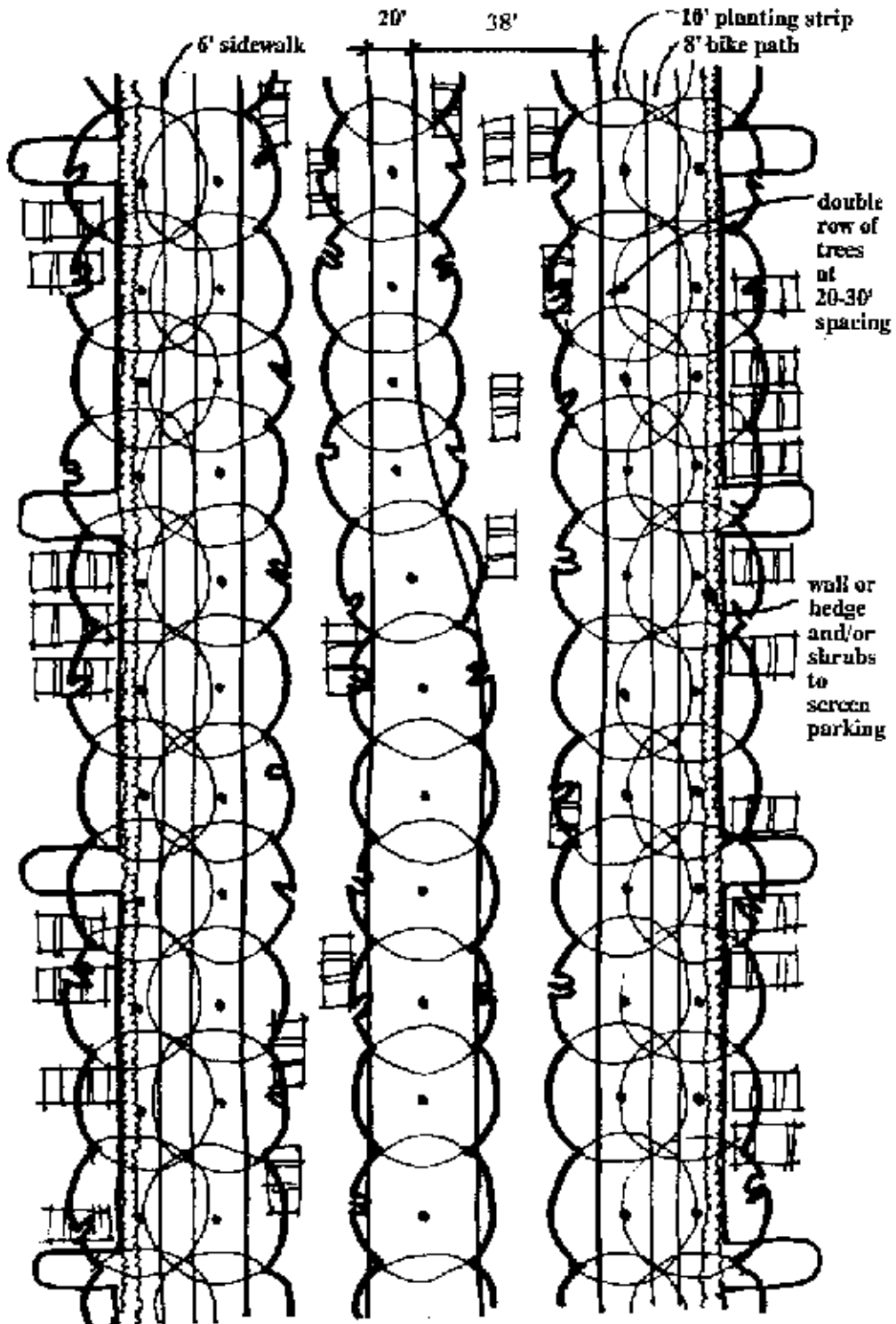
The MD-118 TC-5 entrances will consist of plantings of ornamental trees set back from the corners to allow for an enlarged pedestrian area where sidewalks intersect. Signage, landscaping, and seasonal plantings are recommended to be incorporated into the median of the boulevard. Existing stands of trees will be preserved beyond the streetscape so as to retain the natural character of this area of Town Center (also the MD-118 **gateway** from the south and west).



MD-118 ENTRANCE

WATERS ROAD (TC-5)

The Waters Road boulevard incorporates a second row of trees on the parking lot side of the sidewalk on both sides. This, the major boulevard through TC-5, is differentiated from the other streetscapes in this Analysis Area as well as provided with more separation and screening from the adjacent parking. The southeast side of the roadway also continues the Locbury Drive special pedestrian street, incorporating the hiker-biker trail that links the TC-1 Town Center Core to the TC-6 Historic District.



WATERS ROAD (TC-5)

IMPLEMENTATION FRAMEWORK

Identification of a well-defined implementation strategy to orchestrate the delivery of projects that will shape Germantown's Town Center, as envisioned in the Design Study, is imperative. Design, installation, and maintenance of all elements must all be carefully planned.

This document contains recommended guidelines for use by citizens, applicants, and Executive and Planning Board staff in the review of development proposals and for the County's Capital Improvements Program. This document proposes that the following actions be taken to implement the spirit and intent of the Master Plan and the detailed recommendations of the Design Study:

- Amend the Germantown Master Plan to designate the major streets identified in the Design Study as Master Plan roads and to provide for the dedication of public parks and recreational and cultural facility sites at the locations that have also been identified.
- Establish a special tax area for the Town Center or a Town Center Maintenance District to provide for the maintenance of the streetscape elements within the Town Center boundaries, and the means for a continuing program of promotional events for the Town Center.
- Develop a funding strategy to ensure funding for the capital cost of the various features recommended, and to ensure that the appropriate promotional, operating, and maintenance entities and methods are established. The County Office of Economic Development is recommended to undertake this task.

The following roads and points of access are recommended for inclusion in the Germantown Master Plan and are to be dedicated in public right-of-way. The precise alignment will be determined at the time of subdivision and site plan review. Driveways serving parking, loading, and service access are proposed to be constructed and maintained by the developer:

- Town Center Core (TC-1) Entrance Boulevard (north from MD-118)
- Main Street (from Crystal Rock Drive to Middlebrook Road)
- Secondary commercial and residential streets directly serving buildings, but excluding parking and service driveways

INTRODUCTION

MASTER PLAN AMENDMENTS

ROAD DEDICATIONS

TC-1

- TC-3
 - TC-3 Entrance Boulevard (including the turn-around connections or through street to Crystal Rock Drive or Middlebrook Road as required to meet MCDOT standards)

- TC-5
 - TC-5 Boulevard (from Father Hurley Boulevard) and Main Street Extended (to Wisteria Drive)
 - Waters Road (to new MD-118)

- TC-6
 - Waters Road (from new MD-118 to Old MD-118 - Old Georgetown Road)

**GATEWAY
AND
ENTRANCE
FEATURE
DEDICATIONS**

The following intersection improvements and dedicated area necessary for inclusion into the right-of-way for Gateways, Entrance Features, and Corner Features, are proposed for inclusion in the Germantown Master Plan:

- Town Center Gateways
 - I-270 and MD-118 Gateway
 - East side Crystal Rock Drive at Rexmore Drive
 - MD-118 at CSX Railroad overpass
 - Father Hurley Drive and CSX Railroad overpass Gateway

- Town Center Core
(TC-1) Entrances
 - Crystal Rock Drive and MD-118
 - Middlebrook Road and MD-118
 - Town Center Entrance Boulevard and MD-118

- TC-1 Corners
 - Crystal Rock Drive and MD-118
 - Crystal Rock Drive and Rexmore Drive
 - Middlebrook Road and MD-118
 - Middlebrook Road and Locbury Drive

- All four corners of the TC-2 site TC-2 Corners

- Waters Road entrance at MD-118 TC-5 Entrances
- Waters Road entrance at Wisteria Drive
- TC-5 Boulevard Entrance at Father Hurley Boulevard

- Wisteria Drive and new MD-118 TC-5 Corners
- TC-5 Boulevard and Waters Road

- Middlebrook Road and MD-118 Middlebrook And
Sugarloaf Centers
- Middlebrook Road and Loebury Drive
- Loebury Road and Wisteria Drive
- Main Street Extended and Wisteria Drive

**BIKEWAYS
AND
TRAILS**

The following bikeways and hiker/biker trails are recommended for inclusion in the Master Plan of Bikeways, Greenways Plan, and Germantown Master Plan:

- MD-118 (north side) Bikeways
- Crystal Rock (east side)
- Father Hurley Boulevard from CSX tracks to TC-5 Boulevard (south side)
- Middlebrook Road (east side)

- Loebury Drive/Waters Road (south and east side) Trails
- Old MD-118/Walter Johnson Drive (south side)
- Wisteria Drive (south side)
- Town Center Park (north within committed open space system)

TRANSIT ROUTE

The following Town Center-wide internal Transit route is recommended for inclusion in the Germantown Master Plan:

Town Center Transit Route

- Proposed to connect the Corridor Cities Transit Station at TC-2 with Main Street through TC-1, the Middlebrook and Sugar Leaf centers, to the TC-5 Boulevard, through the TC-5 shopping center, to the crossing of MD-118 at the proposed Waters Road entrance, and to the commuter rail station. The route will be in the public right-of-way on streets proposed for dedication. Easements will be required for the portions of the route through existing and proposed shopping centers.

PARK AND OPEN SPACE DEDICATION

The following Parkland is recommended for dedication to Montgomery County Department of Parks and for inclusion in the Germantown Master Plan. (Recommended improvements must be made prior to dedication.)

TC-1

- Town Center Park (already in Master Plan as floating symbol)

TC-5

- Father Hurley Park
- Madeline V. Waters Park

LAND DEDICATION FOR CULTURAL AND RECREATIONAL FACILITIES

The following land areas associated with cultural and recreational facilities are recommended for dedication to Montgomery County, maintenance by the Recreation Department, and inclusion in the Germantown Master Plan.

TC-1

- Cultural Arts Facility land

TC-5

- Land for Indoor Pool or other amenity

The following public open space areas identified in the study are recommended for dedication to MCDOT, maintenance by the Town Center Maintenance District, and inclusion in the Germantown Master Plan:

**PUBLIC
OPEN SPACE**

- Town Center Commons TC-1
- Waters Road entrance feature at Wisteria Drive on TC-5 site TC-5
- Medians of all dedicated streets and boulevards

The following open space areas are recommended to be held in private ownership, funded and installed by the respective developers, and included in the Germantown Master Plan.

**QUASI-PUBLIC
OPEN SPACE**

- Town Center Station Park TC-2
- Parkland at end of the Entrance Boulevard TC-3
- Stormwater management and amenity feature adjacent to CSX Railroad TC-5

**STREETSCAPE IMPROVEMENTS
IN
EXISTING RIGHTS-OF-WAY
INCLUDED IN CIP**

All Streetscape Improvements in existing County or State rights-of-way and within the Town Center boundaries are recommended for maintenance by the Town Center Maintenance District. Existing major public roadway streetscape improvements are to be included in the County CIP:

- MD-118, both sides, from the I-270 intersection to the CSX Railroad
- Middlebrook Road Improvements
- Walter Johnson Drive (Old MD-118) Special Street Improvements
- Loebury Drive and Father Hurley Drive medians and streetscape not included as part of development (below) to be funded by CIP

**STREETSCAPE IMPROVEMENTS
IN
EXISTING RIGHTS-OF-WAY
BY DEVELOPERS**

The following existing County Roadway frontage streetscape is recommended for inclusion as part of adjacent development proposals and maintained by the County or Maintenance District (where not adjacent to proposed development; to be included in the CIP):

- Crystal Rock Drive
- Father Hurley Drive
- Locbury Drive/Waters Road
- Wisteria Drive
- Waters Road (TC-5)

**STREETSCAPE IN
FUTURE RIGHTS-OF-WAY
BY DEVELOPERS**

The following streetscape is recommended for provision by the respective developments, dedication to MCDOT, and maintenance by the Maintenance District:

- Entrance Boulevard (TC-1 and TC-3)
- Main Street (TC-1)
- TC-1 secondary commercial and residential streets
- TC-5 Boulevard
- Recreation and Cultural Facilities and Amenities

**RECREATION AND CULTURAL
FACILITIES AND AMENITIES**

TC-1

- The Cultural Arts Center building and site improvements on the TC-1 site are to be funded and constructed by the Milestone Mall developer. The Mall developer must also provide either an agreement with an approved company, community arts group, or public agency willing to operate the center, or an endowment or annual subsidy for operating expenses.

TC-5

- The Amenity Facility and site improvements are to be funded and constructed by the developers of the TC-5 development and turned over to the County.

**TOWN
CENTER
MAINTENANCE
DISTRICT**

A Town Center Maintenance District is recommended for establishment through Chapter 68A of the Montgomery County Code, Montgomery County Urban Districts, and administration by the Montgomery County Department of Transportation. A Germantown Town Center Advisory Committee, made up of representatives of local businesses and residents, would provide oversight and community representation.

The District's primary function will be to maintain the streetscape and public areas within the Town Center and to promote and coordinate community-oriented activities and associated budgetary functions. District activities will be funded through the Urban District Tax levied against all assessable property in the District.

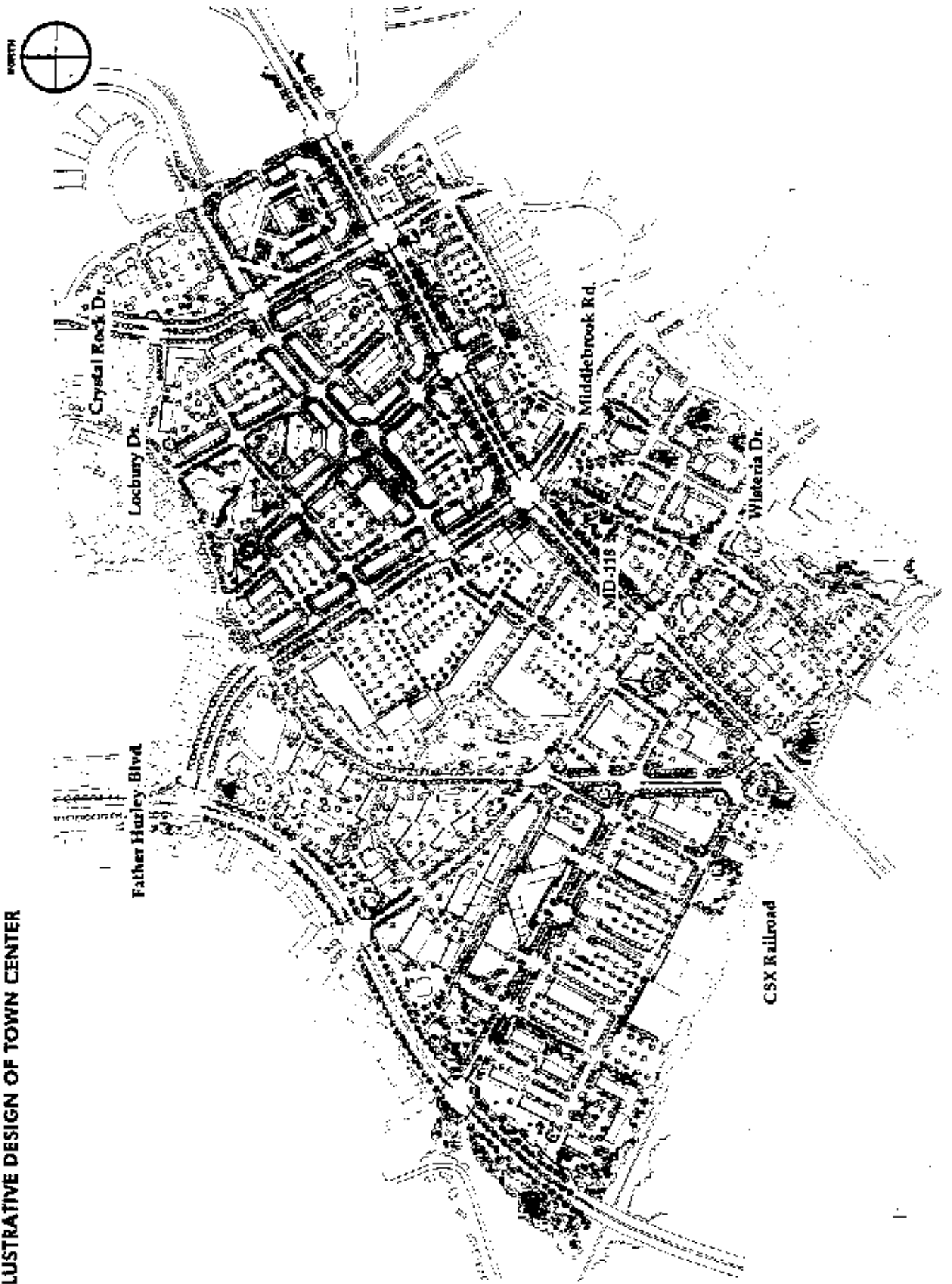
Specifically, the intent of the district under the act is to:

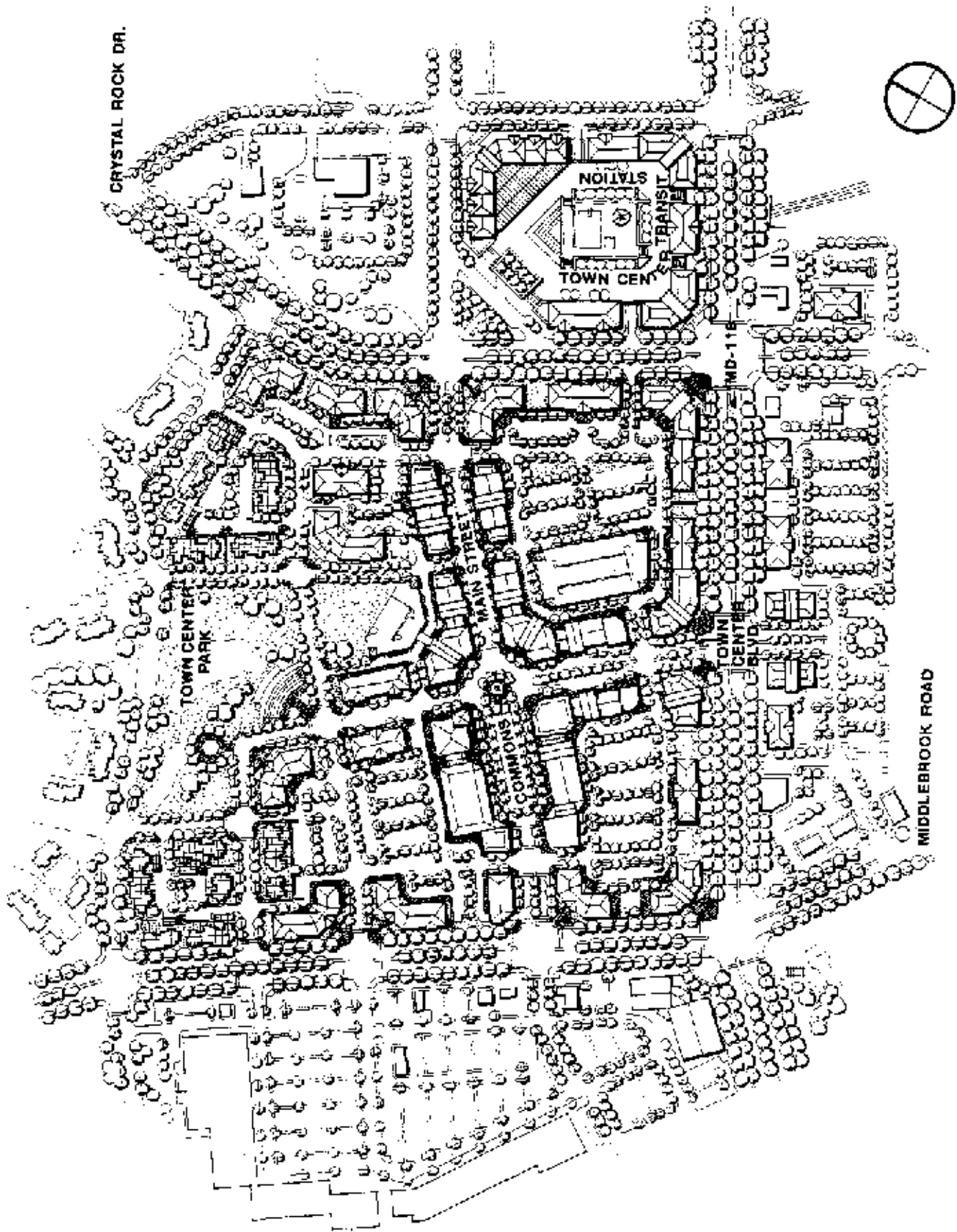
- Increase the maintenance of the streetscape and its amenities;
- Provide additional public amenities such as plantings, seating, shelters, and works of art;
- Promote the commercial and residential interests of the area; and
- Program cultural and community activities.

Urban districts have already been created for the Bethesda, Silver Spring, and Wheaton CBD's and have a history of successful application in these areas.

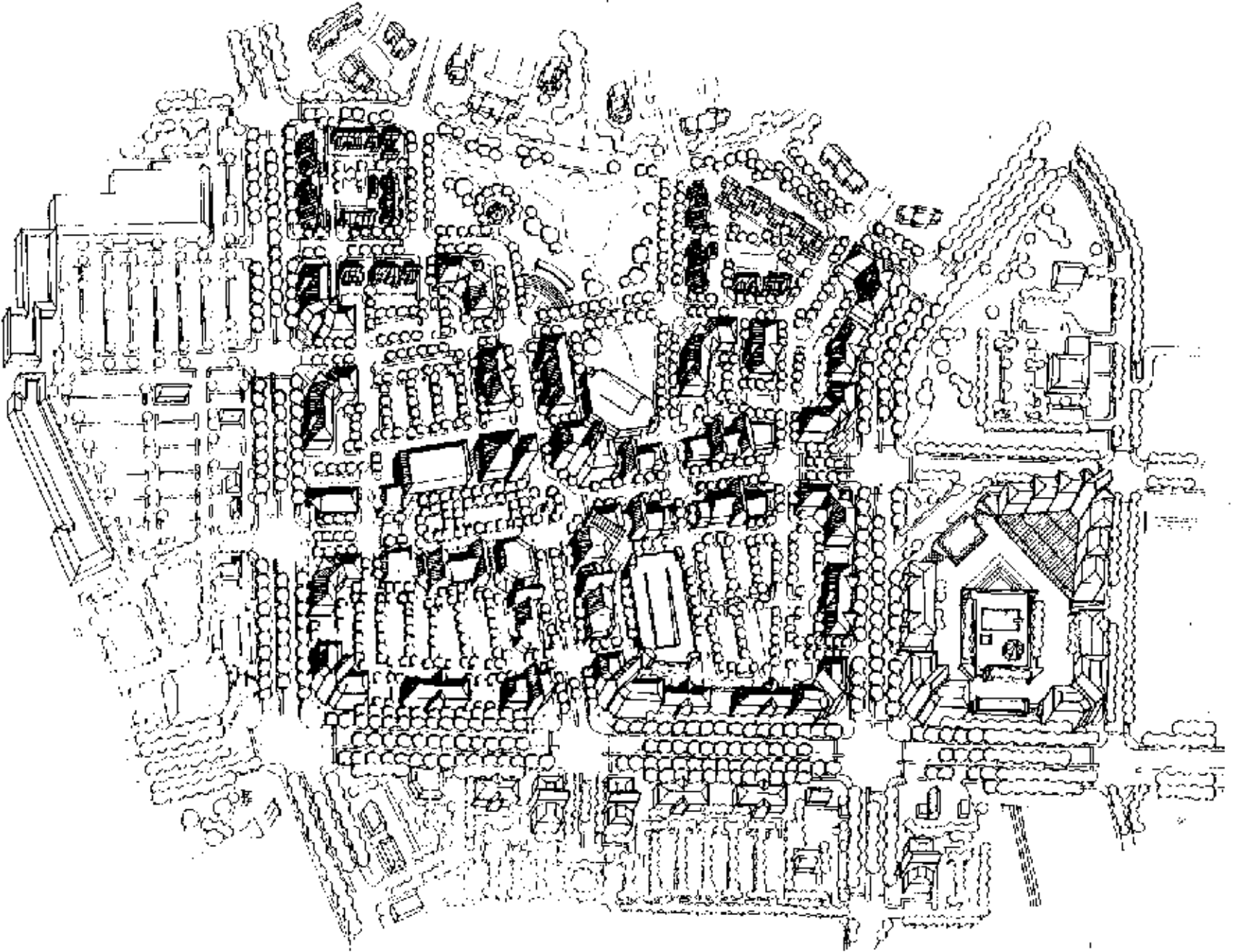
APPENDIX *Illustrative
Design
of
Town
Center*

ILLUSTRATIVE DESIGN OF TOWN CENTER

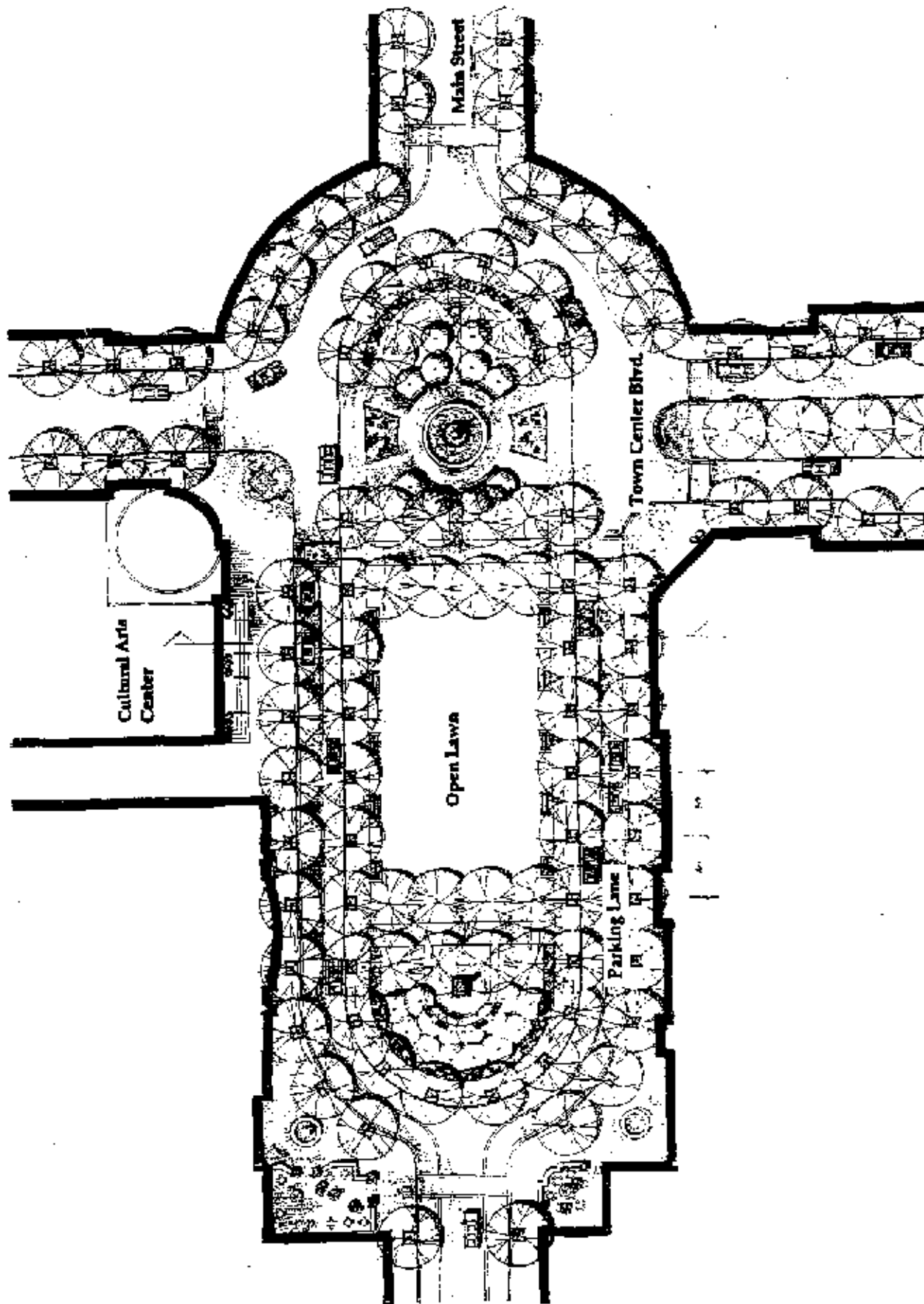




ILLUSTRATIVE DESIGN OF TOWN CENTER - TC-1: PLAN



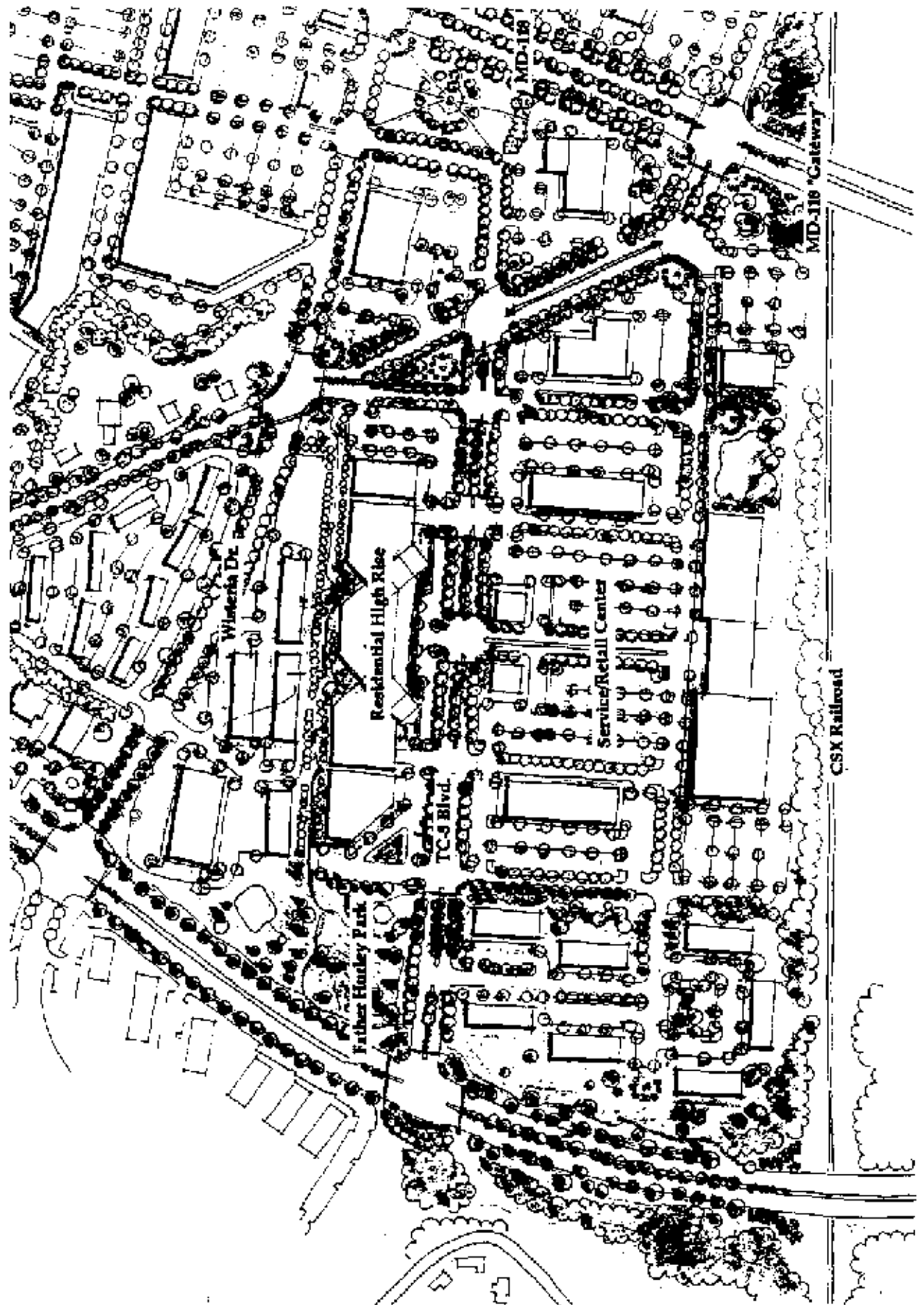
ILLUSTRATIVE DESIGN OF TOWN CENTER - TC-1: ISOMETRIC



ILLUSTRATIVE DESIGN OF TOWN CENTER - TOWN CENTER COMMONS



ILLUSTRATIVE DESIGN OF TOWN CENTER - TC-5



GLOSSARY

Privately provided attributes within a development that enhance public enjoyment. Amenities can include physical enhancements to public and private spaces which make those spaces more inviting to the general public.

AMENITY

One of six areas into which the Town Center Study Area was divided for detailed analysis.

ANALYSIS AREA

A divided roadway with a wide median and a formal streetscape of regularly spaced trees on both sides, as well as in the median.

BOULEVARD

The total volume and configuration of potential building mass within which the actual building must be located.

BUILDING ENVELOPE

A development area for which a recommended building height is established in a number of floors.

BUILDING HEIGHT ZONE

The distance a building is allowed to be located from (usually) a public right-of-way, street, park, or property line, etc.

BUILDING SETBACK

Those parcels within the Town Center that have received plan approval.

COMMITTED DEVELOPMENT

The public use space located at the center of the town core (TC-1).

COMMONS

A street crossing that is demarcated by striping or special paving.

CROSSWALK

A set of design principles or ideas intended to integrate existing and new development and guide future development in the Town Center.

DESIGN FRAMEWORK

A planted (**soft**) or paved (**hard**) area between the buildings and roadway or parking that is designed to provide a combination of access, visual transition, and spatial separation.

EDGE (also *soft edge*, *hard edge*)

The ratio of the gross floor area of a building to the area of the lot on which it is located.

FAR (floor area ratio)

A town landmark or feature, such as a tower, fountain, prominent building, or open space that serves as a reference point in the townscape.

FOCAL POINT

The portion of the streetscape located at the entrances of the Town Center or major development area.

GATEWAY

A planned feature that is assumed to become part of the future Town Center.

GIVEN

An area at the edge of the Town Center consisting of natural open space.

GREEN EDGE

GREENWAY	A continuous open space corridor usually with pedestrian and/or bicycle pathway linking parkland and/or open space area.
INTERNAL TRANSIT	A transit system exclusively serving the Town Center area.
LANDMARK	See focal point - a landmark has special historic or other significance.
MAIN STREET	The main shopping street of the Germantown Town Center or of any town center.
PEDESTRIAN-ORIENTED	Equipped with features to facilitate pedestrian activity and safety and provide a pleasant atmosphere for pedestrians.
PLACE-MAKING ELEMENT	A townscape feature such as a fountain, plaza, or park that is memorable to the citizens or visitors to an area.
PROJECT PLAN REVIEW	Required in the optional method of development in the Central Business District and Residential- Mixed Use Zones, project plan review allows greater density and mix of uses, provided the developer provides certain public amenities and an environment capable of supporting the extra density.
SENSE-OF-PLACE	The sense of identity that a particular place, landmark, or overall character provides to the Town Center.
SITE PLAN REVIEW	The detailed site development plans that carry out the policies and recommendations of the master plan. As there is flexibility in the layout of buildings, open space, circulation, and other features on the site, the Planning Board and its staff carefully review these elements with ample opportunity for public input. Site plan review examines such elements as building mass and location, open space and recreation, on-site vehicular and pedestrian circulation, parking area design, grading, landscaping, lighting, fencing, and signage. Through this review, issues of compatibility with adjacent land uses can be resolved.
SPECIAL STREET	A street with historical or open space character that serves as a pedestrian link.
STAGING	An element of a master plan and the County's growth management system that coordinates the schedule of public facility construction with the pace of private development.
STREETSCAPE	The design elements associated with a street that enhance its visual appeal and functional use for pedestrians as well as motorists and transit, such as trees, lighting, special paving, building setbacks, etc.

Analysis Area TC-1, the central area of Town Center with the broadest mix of uses and sufficiently compact to create a sense of urbanity.

TOWN CENTER CORE

The overall planning and design of the various elements that define the town, i.e., mass, scale, form, order, spacial relationship, and streetscapes.

TOWNSCAPE

Urban design is a profession tht deals with the relationship between buildings and space, i.e., the form, mass, scale, configuration, character and location of buildings, and the design and character of public spaces.

URBAN DESIGN

A special purpose tax area for an urban location.

URBAN DISTRICT

An attribute consisting of perceived cohesiveness, created by the repetitive use of elements (scale, mass, height, and materials).

VISUAL CONTINUITY

The degree of attractiveness or orderliness of an environment visible from a vantage point or series of points.

VISUAL QUALITY

An area that is inundated or saturated by surface water or groundwater at a frequency and duration sufficient to support a prevalence of vegetation typically adapted for life in saturated soil conditions, commonly known as hydrophytic vegetation.

WETLAND